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SOUTHEND-ON-SEA BOROUGH COUNCIL

**Place Scrutiny Committee**

Date: Monday, 10th October, 2016

Time: 6.30 pm

Place: Committee Room 1 - Civic Suite

Contact: Tim Row - Principal Committee Officer

Email: [committeesection@southend.gov.uk](mailto:committeesection@southend.gov.uk)

**AGENDA**

- 1 **Apologies for Absence**
- 2 **Declarations of Interest**
- 3 **Questions from Members of the Public**
- 4 **Minutes of the Meeting held on Monday 11th July 2016**
- 5 **Monthly Performance Report**  
Members are reminded to bring with them the most recent MPR for period end August 2016 which will be circulated on 5<sup>th</sup> October 2016. Comments / questions should be made at the appropriate Scrutiny Committee relevant to the subject matter.

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**ITEMS CALLED-IN/REFERRED DIRECT FROM CABINET,  
Tuesday 20th September, 2016**

- 6 **Annual Report - Comments, Compliments and Complaints - 2015/16**  
(Pages 13 - 60)  
**Minute 265 (Cabinet Book 1 Item 9 refers)**  
Referred direct by Cabinet to all 3 Scrutiny Committees  
Called in by Councillors Gilbert and McDonald
- 7 **Interim Affordable Housing Policy**  
**Minute 274 (Cabinet Book 2 Item 18 refers)**  
Called-in by Councillors Assenheim and Woodley
- 8 **SCAAP**  
**Minute 275 (Item 9 Circulated separately)**  
Called in to all three Scrutiny Committees by Councillors Gilbert and McDonald
- 9 **Air Quality Management Area**  
**Minute 276 (Cabinet Book 2 Item 20 refers)**  
Called in by Councillors Gilbert, McDonald, Mulroney and Wexham
- 10 **Low Carbon Strategy**  
**Minute 277 (Cabinet Book 2 Item 21 refers)**  
Called in by Councillors Gilbert, McDonald, Mulroney and Wexham

- 11 **Joint Development Brief for Land at Fossets Way**  
**Minute 278 (Cabinet Book 2 Item 22 refers)**  
Called-in by Councillors Assenheim and Woodley
- 12 **Carriage and Wagon Shed Options**  
**Minute 279 (Cabinet Book 2 Item 23 refers)**  
Called-in by Councillors Assenheim and Woodley
- 13 **Minutes of the Local Development Framework Working Party held 6th September 2016**  
**Minute 282 (Cabinet Book 2 Item 26 refers)**  
Called-in by Councillors Assenheim and Woodley

\*\*\*\* **ITEMS CALLED-IN/REFERRED DIRECT FROM CABINET COMMITTEE,**  
**Monday 19th September, 2016**

- 14 **Members Requests List (Request Ref No. 16/08 - Propose waiting restrictions in Thorpe Hall Close to protect driveway)**  
**Minute 251**  
Called-in by Councillors Assenheim and Woodley
- 15 **Petition Requesting Permit Parking Controls Southend East**  
**Minute 253**  
Called-in by Councillors Assenheim and Woodley
- 16 **Petition Requesting Amendment to Existing Parking Controls Shaftsbury Avenue**  
**Minute 254**  
Called-in by Councillors Assenheim and Woodley

\*\*\*\* **PRE-CABINET SCRUTINY ITEMS**

- 17 **Skills Development**  
Report of Corporate Director for Place attached

\*\*\*\* **ITEMS CALLED-IN FROM THE FORWARD PLAN**

None

\*\*\*\* **OTHER SCRUTINY MATTERS**

- 18 **in-depth scrutiny report - 20mph speed restrictions in residential streets**  
Report attached
- 19 **In-depth Scrutiny Project - 'To investigate the case for additional enforcement resources for Southend'** (Pages 831 - 832)  
Place / Policy & Resources Scrutiny Committees In-depth Study 2016/17 - Proposed Project Plan

**TO: The Chairman & Members of the Place Scrutiny Committee:**

Councillor K Robinson (Chairman),  
Councillors P Wexham (Vice-Chairman), M Assenheim, A Bright, D Burzotta, T Callaghan,  
M Davidson, F Evans, N Folkard, J Garston, S Habermel, D Jarvis, D Kenyon,  
H McDonald, D McGlone, M Terry and C Willis

# SOUTHEND-ON-SEA BOROUGH COUNCIL

## Meeting of Place Scrutiny Committee

**Date: Monday, 11th July, 2016**  
**Place: Committee Room 1 - Civic Suite**

# 4

- Present:** Councillor K Robinson (Chair)  
Councillors P Wexham (Vice-Chair), S Habermel, F Evans,  
M Assenheim, J Garston, D Jarvis, C Willis, D Burzotta, M Davidson,  
N Folkard, M Terry, H McDonald and G Phillips\*  
\*Substitute in accordance with Council Procedure Rule 31.
- In Attendance:** Councillors S Aylen, T Byford, T Cox, M Flewitt and C Mulroney  
A Lewis, J K Williams, P Geraghty, D Patel, S Wheeler, M Sargood  
and T Row
- Start/End Time:** 6.30 pm - 8.40 pm

### 89 Apologies for Absence

Apologies for absence were received from Councillors Bright (Substitute: Councillor Phillips) and Kenyon (no substitute).

### 90 Declarations of Interest

The following interests were declared at the meeting:

- (a) Councillors Byford, Cox and Flewitt (Executive Councillors) – interests in all the referred items; attended pursuant to the dispensation agreed at Council on 19th July 2012, under S.33 of the Localism Act 2011;
- (b) Councillor Burzotta – Agenda Item No. 12 (Feed & Food Safety Service Plan 2016/17) – Pecuniary interest: Family establishments/restaurants in the Borough (withdrew);
- (c) Councillor Cox – Agenda Item No. 5 (Petition - Alcohol Free Zone Westcliff Library) – Non-pecuniary interest: DPPO in Shoeburyness referred to which includes the road where he lives;
- (d) Councillor J Garston – Agenda Item No. 18 (West Leigh Area – Report on Ward Councillor Consultation for Parking Controls) – Non-pecuniary interest: relatives live in one of the roads consulted;
- (e) Councillor Phillips Garston – Agenda Item No. 18 (West Leigh Area – Report on Ward Councillor Consultation for Parking Controls) – Disqualifying non-pecuniary interest: Lives in one of the roads consulted (withdrew);
- (f) Councillor Robinson – Agenda Item No. 5 (Petition - Alcohol Free Zone Westcliff Library) – Non-pecuniary interest: Ward Councillor and occasional user of the library;

(g) Councillor Wexham – Agenda Item No. 10 (Hackney Carriage Unmet Demand Survey) – Non-pecuniary interest: Son is a cabbie;

(h) Councillor Wexham – Agenda Item No. 18 (West Leigh Area – Report on Ward Councillor Consultation for Parking Controls) – Non-pecuniary interest: Acquainted with residents in the area consulted;

(i) Councillor Willis – Agenda Item No. 18 (West Leigh Area – Report on Ward Councillor Consultation for Parking Controls) – Non-pecuniary interest: Acquainted with someone who lives in one of the roads consulted.

#### **91 Questions from Members of the Public**

The Executive Councillor for Transport, Waste & Cleansing responded to two written questions from Mr David Webb.

#### **92 Minutes of the Meeting held on Monday 11th April 2016**

Resolved:-

That the Minutes of the Meeting held on Monday 11<sup>th</sup> April 2016 be received, confirmed as a correct record and signed.

#### **93 Petition - Alcohol Free Zone Westcliff Library**

The Committee considered Minute 44 of Cabinet held on 28<sup>th</sup> June 2016, which had been referred direct by Cabinet, concerning the petition comprising 775 signatures requesting the Council to introduce an Alcohol Free Zone around Westcliff Library on London Road, Westcliff on Sea.

Resolved:-

1. That the following decision of Cabinet be noted:

“That the matter be reviewed in 6 months to identify if there is sufficient evidence to prove the activities highlighted in the petition are of a persistent and continuing nature to warrant the making of a Designated Public Place Order.”

2. That in accordance with Council Procedure Rule 39, the matter be referred to full Council for consideration.

Note:- This is an Executive Function.

Executive Councillor:- Flewitt

#### **94 Petition - Playfootball Licensed Hours**

The Committee considered Minute 46 of Cabinet held on 28<sup>th</sup> June 2016, which had been referred direct by Cabinet, concerning a petition containing 33 signatures requesting the Council to reconsider the licensed hours of trade of PlayFootball in Prittlewell Chase.

Resolved:-

That the following decision of Cabinet be noted:

“1. That it be noted that the Council is unable to take any formal action in respect of the operating hours of the PlayFootball facility in Prittlewell Chase.

2. That the Council assists in facilitating mediation between PlayFootball and the local residents to improve the understanding of the concerns of each other and develop a voluntary code of conduct for use outside.”

Note:- This is an Executive Function.

Executive Councillor:- Flewitt

## **95 Corporate Plan and Annual Report - 2016**

The Committee considered Minute 49 of Cabinet held on 28th June 2016, which had been referred direct by Cabinet to all three Scrutiny Committees, together a report of the Chief Executive presenting the Council’s draft Corporate Plan and Annual Report for 2016.

Resolved:-

That the following recommendation of Cabinet be noted:

“That the draft Corporate Plan and Annual Report 2016 be approved.”

Note:- This is a Council Function

Executive Councillor:- Lamb

## **96 2015/16 Year End Performance Report**

The Committee considered Minute 50 of Cabinet held on 28<sup>th</sup> June 2016, which had been referred direct by Cabinet to all three Scrutiny Committees, together with a report of the Chief Executive detailing the end of year position of the Council’s corporate performance.

Resolved:-

That the following decision of Cabinet be noted:

“That the 2015/16 end of year position and accompanying analysis, be noted.”

Note:- This is an Executive Function

Executive Councillor:- Lamb

## **97 Information Management Strategy**

The Committee considered Minute 51 of Cabinet held on 28<sup>th</sup> June 2016, which had been referred direct by Cabinet to all three Scrutiny Committees, together with a report the Corporate Director for Corporate Services presenting the Council’s revised Information Management Strategy

Resolved:-

That the following decision of Cabinet be noted:

“That the Council’s Information Management Strategy, as set out at Appendix 1 to the submitted report, be approved.”

Note:- This is an Executive Function

Executive Councillor:- Lamb

**98 Hackney Carriage Unmet Demand Survey**

The Committee considered Minute 58 of Cabinet held on 28<sup>th</sup> June 2016, which had been referred direct by Cabinet, together with a report of the Corporate Director for Place presenting the conclusions of a Hackney Carriage Demand Study 2015 undertaken by CTS Traffic & Transportation on behalf of this Authority.

Resolved:

That the following decision of Cabinet be noted:

“That the authority maintains the current entry control and limit of 276 Hackney Carriages.”

Note:- This is a Council Function

Executive Councillor:- Flewitt

**99 Preparation of New Southend Local Plan for Southend on Sea**

The Committee considered Minute 59 of Cabinet held on 28<sup>th</sup> June 2016, which had been referred direct by Cabinet, together with a report of the Corporate Director for Place on the preparation of a new Local Plan for Southend on Sea.

Resolved:

That the following recommendations of Cabinet be noted:

“1. That the preparation of the new Southend-on-Sea Local Plan as a corporate priority, which will provide a positive planning framework to manage and guide regeneration and development in Southend on Sea over at least the next 15 year period, be approved.

2. That it be noted that preparation of a new Local Plan for Southend-on-Sea will include a financial and human resource commitment on behalf of the Council to facilitate delivery of a robust, long term strategic planning policy document, in a timely manner.

3. That it be noted that the new Southend-on-Sea Local Plan will replace, once adopted, the Core Strategy and associated local development documents, where indicated.

4. That a number of key evidence based documents be produced to support the preparation of the new Southend-on-Sea Local Plan, including an environmental constraints and green belt review at an early stage.

5. That the Corporate Director for Place, in consultation with the Executive Councillor for Housing, Planning and Public Protection, in conjunction with the LDF Working Party, be authorised to make the necessary amendments which may be required, and agree a draft Local Plan and associated documentation for all statutory public consultation under Regulation 18 of the Town and Country Planning Regulations (Local Planning) (England) 2012.”

Note:- This is a Council Function  
Executive Councillor:- Flewitt

#### **100 Feed and Food Safety Service Plan 2016/17**

The Committee considered Minute 61 of Cabinet held on 28th June 2016, which had been referred direct by Cabinet, together with a report of the Corporate Director for Place presenting the Official Feed and Food Controls Service Plan 2016/17 required by the Food Standards Agency (FSA).

Resolved:

That the following recommendation of Cabinet be noted:

“That the Official Feed and Food Controls Service Plan 2016/17 set out in Appendix 1 to the submitted be approved.”

Note:- This is a Council Function  
Executive Councillor:- Flewitt

#### **101 Community Infrastructure Levy Financial Report 2015/16**

The Committee considered Minute 62 of Cabinet held on 28th June 2016, which had been referred direct by Cabinet, together with a report of the Corporate Director for Place presenting the (CIL) Community Infrastructure Levy Annual Financial Report for the financial year 2015/16.

Resolved:

That the following recommendation of Cabinet be noted:

“That the content of the CIL Annual Financial Report 2015/16 be noted, and that the CIL receipts to date (except Leigh Town Council Neighbourhood Allocation and 5% administrative expenses) be carried forward to the next financial year (2017/18), when spending plans will be reviewed.”

Note:- This is a Council Function  
Executive Councillor:- Flewitt

## **102 Former Beecroft Art Gallery Building - Artist Studios Feasibility Study**

The Committee considered Minute 63 of Cabinet held on 28th June 2016, which had been referred direct by Cabinet, together with a report of the Corporate Director for Place providing an update on the outcome of the feasibility study for using the Station Road, Westcliff building for artist studios and sought approval for further work to be undertaken.

Resolved:

That the following recommendations of Cabinet be noted:

- “1. That the potential for a viable business case to support the creation and management of artist studios within the former gallery building be acknowledged (by the Council acting as the Beecroft Art Gallery Trustees).
2. That the preparation of a formal bid to Arts Council England’s Capital Grant funds and other external funding sources to undertake the works, be approved (by the Council acting as Trustees).”

That the following decision of Cabinet be noted:

- “3. That a grant of £20k for the preparation of the bid be made to the Trust to be met from the Council’s contingency.”

Note:- The recommendations in 1 and 2 above constitute a Council Function. The decision in 3 above constitutes an Executive Function  
Executive Councillor:- Flewitt

## **103 Devolution**

The Committee considered Minute 64 of Cabinet held on 28th June 2016, which had been referred direct by Cabinet, together with a report of the Corporate Director for Place outlining the recent activity in relation to the devolution negotiations.

Resolved:

That the following decisions of Cabinet be noted:

- “1. That the position and activity in relation to devolution negotiations, be noted.
2. That the emergence of the Thames Estuary 2050 Growth Commission, be noted.”

Note:- This is an Executive Function  
Executive Councillor:- Lamb



#### **104 Physical Activity Strategy**

The Committee considered Minute 65 of Cabinet held on 28th June 2016, which had been referred direct by Cabinet, together with a report of the Director of Public Health presenting the Southend-on-Sea Physical Activity Strategy 2016-2021.

Resolved:

That the following decision of Cabinet be noted:

“That the Southend-on-Sea Physical Activity Strategy 2016-2021 and associated action plan, be approved.”

Note:- This is an Executive Function  
Executive Councillors:-Salter/Holland

#### **105 Council Procedure Rule 46**

The Committee considered Minute 67 of Cabinet which had been referred to Scrutiny concerning actions taken under Council Procedure Rule 46.

Resolved:-

That the following decision of Cabinet be noted:-

“That the submitted report be noted.”

Note:- This is an Executive Function.  
Executive Councillor: As appropriate to the item.

#### **106 Objections to Traffic Regulation Orders – Various Locations**

The Committee considered Minute 37 of Cabinet Committee held on 16th June 2016, which had been referred direct by Cabinet Committee, together with a report of the Corporate Director for Place that appraised Members of the representations that had been received in response to the statutory consultation for proposed Traffic Regulation Orders in respect of various proposals within the Borough.

Resolved:

That the following decisions of Cabinet Committee be noted:

“1. That no further action be taken and that the Traffic Regulation Orders not be confirmed in respect of the following:

- The introduction of no waiting junction protection 11.00 a.m. to 12.00 noon Mondays to Fridays in Marcus Avenue;
- The introduction of no waiting junction protection at any time for 10m from Johnstone Road southwards; and

- The introduction of no waiting at any time in Burgess Terrace from March to October from 9.00 a.m. to 6.00 p.m. on the west side between Burgess Terrace and Thorpe Esplanade.

2. That the Corporate Director for Place be authorised to confirm the traffic regulation order as advertised for the introduction of no waiting in St James Avenue from 11.00 a.m. to 12.00 noon Monday's to Friday's and to arrange for the proposals to be implemented."

Note:- This is an Executive Function  
Executive Councillor: Councillor Cox

#### **107 West Leigh Area – Report on Ward Councillor Consultation for Parking Controls**

The Committee considered Minute 38 of Cabinet Committee held on 16th June 2016, which had been referred direct by Cabinet, together with a report of the Corporate Director for Place which appraised Members of the results of the consultation on parking controls in the West Leigh area, that had been undertaken by the Ward Councillors and, having considered the views of the Traffic & Parking Working Party, sought Members' approval on the appropriate way forward.

Resolved:

That the following decisions of Cabinet Committee be noted:

"1. That the Ward Councillors be thanked for their efforts in compiling the and distributing the questionnaires in relation to the consultation.

2. That no further action be taken in respect of this request on the basis that the outcome of the consultation does not meet minimum policy thresholds for formal public consultation."

Note:- This is an Executive Function  
Executive Councillor: Councillor Cox

#### **108 Greenways - Residents' Permit Parking Scheme Update**

The Committee considered Minute 39 of Cabinet Committee held on 16th June 2016, which had been referred direct by Cabinet Committee, together with a report of the Corporate Director for Place concerning the Members' request for a resident's parking scheme in The Greenways. The report outlined the further consultation work that had been undertaken by the Ward Councillors and, having regard to the views of the Traffic & Parking Working Party, sought Members' approval to proceed with the implementation of the proposed scheme.

Resolved:

That the following decisions of Cabinet Committee be noted:

“1. That Officers comments as set out in paragraph 4.1 be noted and, recognising the safety of children and parents in the vicinity of the school, the Corporate Director for Place be authorised to advertise the necessary traffic regulation orders and notices for the introduction of a Residents’ Parking Scheme (RPS) in The Greenways, covering 8am to 5pm, Mondays to Fridays.

2. Subject to there being no objections received following statutory advertisement, the Corporate Director for Place be authorised to arrange for the orders to be sealed and the proposals implemented.

3. That all unresolved objections be referred to the Traffic & Parking Working Party and Cabinet Committee for consideration.”

Note:- This is an Executive Function  
Executive Councillor: Councillor Cox

### **109 Temporary Traffic Management Measures - The Fairway, A127 and Bellhouse Lane**

The Committee considered Minute 40 of Cabinet Committee held on 16th June 2016, which had been referred direct by Cabinet Committee. This related to the traffic management measures to reduce, where possible, the impact on traffic flow as a result of the temporary closure of The Fairway to enable essential gas main replacement works due to be undertaken by National Grid commencing on 23<sup>rd</sup> July 2016 and scheduled to last for up to 18 weeks.

The Committee also received an oral report by the Head of Planning & Transport that informed Members of the procedures, constraints and safety impacts of temporary traffic control measures. He also explained that:

(i) the phasing of the traffic lights to increase the capacity of vehicle movements at the junctions affected for the duration of these works would be implemented at the commencement of these works and no traffic regulation orders were required. A separately signalled left turn from Bellhouse Lane into A127 would be incorporated subject to a “U-turn prohibition being implemented;

(ii) a temporary traffic regulation order prohibiting “U-turn” manoeuvres at the junction of the A127 with Bellhouse Lane had been advertised, following the completion of a safety audit of the proposal. This would be implemented as soon as possible, subject to there being no statutory objections; and

(iii) in terms of other measures to minimise the impact of the works, the existing variable message signage (VMS) to give advance warning. However, the situation would be kept under review.

The Executive Councillor for Transport, Waste & Cleansing undertook to hold an urgent meeting of the Traffic & Parking Working Party, to expedite matters where appropriate, should it be necessary.

Resolved:

1. That the following decision of Cabinet Committee be noted:

“That Officers be requested to investigate as a matter of extreme urgency, the phasing of the traffic control signals (traffic lights at the junctions of The Fairway and Bellhouse Lane with the A127, the possibility of introducing a traffic regulation to prohibit U turns at these junctions and any other appropriate measures to minimise the impact of the works on traffic congestion and disruption.”

2. That the update by the Council’s Head of Planning & Transport regarding the temporary traffic management measures be noted.

Note:- This is an Executive Function  
Executive Councillor: Councillor Cox

**110 Suggested in depth Scrutiny projects - 2016 / 17**

The Committee considered a report by the Corporate Director for Corporate Services concerning the possible in depth scrutiny project to be undertaken by the Scrutiny Committee in 2016/17. The report also attached some information about the work carried out by the Scrutiny Committees in the 2015/16 Municipal Year.

The Committee welcomed the Chairman’s suggestion for a joint in-depth study in conjunction with the Policy & Resources Scrutiny Committee to investigate the case for additional enforcement resources for Southend.

Resolved:

1. That the in-depth scrutiny project for 2016/17 be:

(i) To investigate the possibility of the Council increasing resources for enforcement activity including consideration of the Council employing its own PCSOs or financing the provision of additional “Specials” by the Police. In the context of “Specials” specific consideration should be given to whether financial support could be offered to such officers and how they would be dedicated to the Borough of Southend-on-Sea.

(ii) To consider how such PCSO’s or additional “Specials” could contribute to an improved level of service in connection with the enforcement of public protection, waste, graffiti, street scene etc..

2. That the in-depth study be a joint project with the Policy & Resources Scrutiny Committee (subject to that Committee is in agreement).

3. That the Council be requested to appoint a joint Working Party comprising 8 Members (proportionality to apply) which will manage the in depth project.

4. That officers proceed with background work in advance of the scope of the topic being fully developed.

5. That the information attached at Appendix 3 to the Report, the summary of work of the 3 Scrutiny Committees during 2015/2016, be noted.

Note:- This is a Scrutiny Function save for Resolution 3 above which is a Council function.

**111 Minutes of the Meeting of Chairmen's Scrutiny Forum held on Tuesday, 28th June, 2016**

Resolved:

That the Minutes of the meeting of Chairmen's Scrutiny Forum held on Tuesday, 28th June, 2016 be received and noted and the recommendations therein endorsed.

Note: This is a Scrutiny Function.

**112 Exclusion of the Public**

Resolved:-

That, under Section 100(A)(4) of the Local Government Act 1972, the public be excluded from the meeting for the items of business set out below, on the grounds that they would involve the likely disclosure of exempt information as defined in Part 1 of Schedule 12A to the Act and that the public interest in maintaining the exemption outweighs the public interest in disclosing the information.

**113 Waste Disposal - Contract Issues**

The Committee considered Minute 70 of Cabinet held on 28th June 2016, which had been referred direct by Cabinet, together with a report of the Corporate Director for Place on the this matter.

Resolved:

That the following decision of Cabinet be noted:

“That the submitted report be noted.”

Note:- This is an Executive Function  
Executive Councillor:- Cox

**Chairman:** \_\_\_\_\_

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# Southend-on-Sea Borough Council

Report of Corporate Director for Corporate Services  
to  
Cabinet

On  
20th September 2016

Report prepared by: Tim MacGregor – Team Leader, Policy  
and Information Management/  
Charlotte McCulloch – Customer Service & Complaints  
Manager

Agenda  
Item No.

6

## Annual Report – Comments, Compliments and Complaints – 2015/16

### All Scrutiny Committees

Executive Councillors: Councillor Lamb, Councillor Salter, Councillor Courtney

A Part 1 Public Agenda Item.

#### 1. Purpose of Report

1.1 This report is to:

- Fulfil the Council's statutory duty to produce an annual report on compliments and complaints received about its Children and Adult social care functions.
- Provide performance information about comments, compliments and complaints received across the Council for 2015-16
- Contribute towards the Council's values to be open, honest and transparent.

#### 2. Recommendation

2.1. To note the Council's performance in respect of compliments, comments and complaints for 2015-16 and to refer each separate report to the respective Scrutiny Committee.

#### 3. Background

3.1. Legislation requires that statutory processes are in place to deal with complaints relating to children and adults social care, to advertise that process and produce annual reports.

3.2. As the statutory process requires the Children and Adults' Social Care reports to be shared with the Care Quality Commission and the Department of Health this necessitates three separate reports for the Council, including a separate report on the Council's corporate comments, complaints and compliments process.

3.3. Details of performance are contained in the respective reports under

**Appendix A** – Compliments, Concerns and Complaints – Adult Social Care Services.

**Appendix B** – Compliments and Complaints – Children’s Social Care Services.

**Appendix C** - Corporate Comments, Complaints and Compliments.

- 3.4. The table below sets out a comparison of the total number of complaints received for the previous three years by Department. As can be seen, the figures reflect a steady upward trend in the number of complaints being received by the Council (8.5% up on 2014/15).

Department	2012/2013	2013/2014	2014/2015	2015/16
Corporate Services	74	44	43	66
Department for People (including statutory)	218	227	246	304
Department for Place	233	375	376	351
Public Health	0	0	0	1
<b>Grand Total</b>	<b>525</b>	<b>646</b>	<b>665</b>	<b>722</b>

This trend reflects the nationwide picture as outlined in the Local Government Ombudsman’s (LGO) ‘Annual Review of Local Government Complaints’ (2015/16) which highlights a 6% rise in complaints and enquiries received by them. Reasons cited for this upward trend include the impact of declining resources on council services and growing willingness of the public to make complaints.

- 3.5. Comments and compliments are also received, with numbers shown below.

Department	2012/2013	2013/2014	2014/2015	2015/16
Corporate Services	1653	1694	1326	1673
Department for People (including statutory)	477	521	474	416
Place	219	288	222	337
<b>Grand Total</b>	<b>2349</b>	<b>2503</b>	<b>2022</b>	<b>2426</b>

#### 4. Lessons Learnt and Service Improvements

- 4.1 Whilst responding to feedback in a timely manner it is important for Council services to reflect on lessons learnt and improving outcomes. This is recognised by the Local Government Ombudsman’s principles of good complaints handling of being customer focused, putting things right and seeking continuous improvement.



Examples of service improvements undertaken throughout the year as a result of customer feedback include:

- A revised policy on dealing with abandoned vehicles, to make the process easier for those reporting incidents was agreed;
- Information on the rights of appeal for benefit claimants was revised on standard letters and the website;
- School transport appeals - reasoning is set out more in more detail both in appeal reports and letters to appellants;
- Procedures were improved to ensure that care providers have a clearly defined retention and disposal policy - a copy of which is sent to the contracts team for review;
- The hospital discharge pack provided by the Hospital Social Work Team was improved;
- In response to a complaint about lack of transparency, the South Essex Homes Decant and Management Move Procedure was updated and made a publicly available on the SEH website.

Further examples are contained in App A (Appendix 8), Appendix B (paragraph 12) and Appendix C (para 4.7).

## **5. Future developments**

- 5.1 In May 2015 the government announced its intention to introduce a Public Services Ombudsman Bill to set up a Public Services Ombudsman in England which will absorb the functions of the Parliamentary and Health Service Ombudsman, the Local Government Ombudsman and potentially the Housing Ombudsman. This is intended to provide better value for money, reflect increasing cross sector working and provide a more joined up service with simpler access for the public.
- 5.2 To date no draft bill has been published, and such a bill was not included in the May 2016 Queens Speech, however, the LGO, Parliamentary and Health Service Ombudsman are now investigating health and social care services cases through a single team based in the LGO's office.

## **6. Corporate Implications**

### **6.1 Contribution to Council's Vision & Corporate Priorities**

Customer feedback and complaints management is directly relevant to the Council's corporate priorities to deliver strong, relevant and targeted services that meet the needs of our community. This remains important in the coming years as budget constraints continue to impact on service delivery.

### **6.2 Financial Implications**

The commissioning of independent people to deal with children's stage two statutory complaints incurs additional cost. The decrease in stage 2 complaints this year has reduced the costs of investigations. The use of mediation and

early intervention within all the processes is used in an effort to restrict the number of complaints escalating, limiting the amount of officer time spent on complaints as well as improving the outcome for the complainant.

A limited number of compensation payments to customers to acknowledge the time and trouble that they have expended have been made this year.

### 6.3 Legal Implications

To ensure compliance with the statutory complaints processes.

### 6.4 People and Property Implications

People and property implications are considered through the Council's normal business management processes.

### 6.5 Consultation

The Advocacy Services and Representations Procedure (Children) (Amendment) Regulations 2004 confer a duty on local authorities to provide information about advocacy services and offer help to obtain an advocate to a child or young person wishing to make a complaint. The Authority has a contract with the National Youth Advocacy Service. All children and young people wishing to make a complaint in 2013-14 were offered the services of an advocate.

### 6.6 Equalities and Diversity Implications

All three processes are receiving feedback from customers from Southend communities including minority groups. Similarly, alternative approaches to facilitate complaint resolution are offered including advocacy and meetings.

Corporate equalities considerations continue to be part of the process.

### 6.7 Risk Assessment

Processes are reviewed periodically and reduce any risk which could adversely affect the Council's reputation in the community and reduce public trust/satisfaction. Whilst an anticipated increase in complaints did materialise after 2013, notably in respect of services delivered corporately, the number recorded is still significantly less than the 1100 reported for 2009 at the beginning of the revised process.

### 6.8 Value for Money

Early resolution of complaints, together with learning lessons from the process, contribute to service improvements and getting things right first time.

### 6.9 Community Safety and Environmental Impact Implications

The process is implemented to ensure both community safety and effects on the environment are fully considered.

**7. Background Papers**

None

**8. Appendices**

**Appendix A - Compliments Concerns & Complaints received throughout 2015-16 for Adult Social Care Services**

**Appendix B - Compliments and complaints – Children’s Social Care.**

**Appendix C - Corporate comments, complaints and compliments – 2015-16.**



# Southend-on-Sea Borough Council

**Report of Corporate Director for People  
to  
Cabinet  
on  
20<sup>th</sup> September 2016**

Report prepared by: Charlotte McCulloch

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**Compliments Concerns & Complaints received throughout 2015-16  
for Adult Social Care Services  
People Scrutiny Committee  
– Executive Councillor: Lesley Salter  
*A Part 1 Public Agenda Item***

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**1. Purpose of Report**

- 1.1 To discharge the local authority's statutory duty to produce an annual report on compliments concerns and complaints received about its adults' social care function throughout the year.
- 1.2 To provide statistical and performance information about compliments concerns and complaints received throughout 2015/2015.

**2. Recommendation**

- 2.1 That the Department's performance during 2015/2016, and comparison to the previous three years be noted.
- 2.2 That the report be referred to the People Scrutiny Committee for detailed examination.

**3. Background**

- 3.1 This is the seventh Annual Report following the changes to the legislation governing the statutory complaints process for adult social care services. The Local Authority Social Services and National Health Service Complaints (England) Regulations 2009 came into force on 1 April 2009 and created a single process for health and social care services. With the increase in integrated services, the single process makes it easier for patients and service users to make complaints and allows them to make their complaint to any of the organisations involved in their care. One of the organisations will take the lead and co-ordinate a single response.
- 3.2 Strong working relationships have been established with complaints colleagues within the Health organisations in the area. This, together with a joint protocol

agreed by the Essex Complaints Network, has made it easier for people making complaints that span Health and social care services. In 2015/2016 there were 3 joint complaints.

- 3.3 The new process is based on the principles of the Department of Health's *Making Experiences Count* and on the Ombudsman's principles of good complaints handling:
- Getting it right
  - Being customer focused
  - Being open and accountable
  - Acting fairly and proportionately
  - Putting things right
  - Seeking continuous improvement.
- 3.4 There is a single local resolution stage that allows a more flexible, customer focused approach to suit each individual complainant. At the outset, a plan of action is agreed with the complainant to address their complaint. Amendments to the plan can be agreed at any stage of the process.
- 3.5 The regulations do not specify timescales for resolution and a date for response is agreed and included in each plan. Response times are measured against the agreed dates in the plans.
- 3.6 When the local authority believes that it has exhausted all efforts to achieve a local resolution, and the customer remains dissatisfied, the next step is referral to the Local Government Ombudsman.

#### 4 Compliments; concerns and comments received in 2015/2016

- 4.1 Compliments are a very important feedback and motivational tool and members of staff are encouraged to report all compliments they receive to the Customer Services Manager for recording. All compliments are reported to the Group Manager of the Service to pass on their thanks to the staff member and the team. This practice has been well received by staff. Data gathered from compliments are used to inform commissioning decisions of the authority.
- 4.2 Adult and Community Services received 341 compliments about its social care services in 2015/2016.

*Table to show the number of compliments received in 2015/2016 and a comparison with previous three years*

Apr 12 – Mar 13	Apr 13- Mar 14	Apr 14 – Mar 15	Apr 15 – Mar 16
Number	Number	Number	Number
429	470	407	341

There has been a decline in the number of compliments received, however we are unable to ascertain why this is the case. Examples of the types of compliments received can be found in Appendix 1

- 4.3 The current regulations require the local authority to record concerns and comments as well as complaints. Some people wish to provide feedback to help improve services but they do not wish to make a complaint, and this process facilitates that.
- 4.4 Adult and Community Services received 8 concerns about its social care services in 2015/2016. Of these, 7 were regarding commissioned homecare services and 1 was about internal services provided directly by Southend Council.
- 4.5 All concerns and comments are considered to identify areas for improvement and responses are made where appropriate or requested.

### 5 Complaints received in 2015/2016

- 5.1 Adult and Community Services received 176 complaints about its social care services in 2015/2016. 75 of which were about internal services provided directly by Southend Council, and 101 were about services supplied through externally commissioned providers (domiciliary care & residential care)

*Table to show the total number of complaints received during 2015/2016 and comparison with previous three years*

Apr 12 – Mar 13	Apr 13 – Mar 14	Apr 14 – Mar 15	Apr 15 – Mar 16
Number	Number	Number	Number
111	136	166	176

This represents an overall increase of 6% in the number of complaints received during the previous year, and a 58.5% increase in the last 4 years.

Whilst there has been an increase year on year 176 complaints is still only 4.9% of the number of service users receiving support throughout the year.

The main increase in complaints has been seen within Internal Services with an increase from 56 in 2014/15 to 75 in 2015/16, an increase of 34%. The increase has been seen over a number of different service areas, rather than in one significant area.

A reduction has been seen in complaints about overall commissioned services where the number of complaints has decreased from 109 last year to 101 this year, an overall decrease of 13%. Complaints about commissioned homecare service having the greatest reduction from 101 in 2014/15 to 93 in 2015/16 an 8% decrease. Our Contracts Team and Complaints Manager continue to work with the home care providers to address issues and effect improvements around complaints handling.

- 5.2 Appendix 2 shows complaints by internal and commissioned services. Appendix 3 shows the number of complaints received about internal services by team.

Appendix 4 shows the number of complaints by service user group. The majority of complaints (150) were received about services to older people. This is the largest service user group and the 150 complaints represent 6.2% of the number of older people who receive services from the department.

- 5.3 Of the 176 complaints, 101(57%) refer to services commissioned from external providers. 93 of these were about home care services, and this figure accounts for 52% of the total complaints,
- 5.4 Southend Borough Council commissions South Essex Partnership Foundation Trust (SEPT) to provide its mental health and substance misuse services and SEPT received 10 complaints from Southend clients. 6 were not upheld & 4 were partially upheld. These were dealt with by SEPT and are not included in the figures in the table in section 5.1 above.
- 5.5 Under the current regulations, any complaints received verbally and resolved to the complainant's satisfaction within 24 hours do not have to be recorded as complaints. During 2015/2016, 5 such complaints were received.

### 6. Complaints subject to independent investigation

- 6.1 An independent investigation is an option for reaching a local resolution but it is not an automatic progression. Action taken to address a complaint will be discussed with the complainant at the outset and the primary aim is to find a resolution but action must be proportionate.
- 6.2 There were no independent investigations in 2015/2016. An independent investigation can be costly and if staff can resolve complaints satisfactorily without them, this represents a saving.

*Table to show the number of complaints subject to independent investigation, and as a percentage of the total number complaints during 2015/2016, and comparison with the previous three years.*

Apr 12 – Mar 13	Apr 13 – Mar 14	Apr 14 – Mar 15	Apr 15 – Mar 16
Number	Number	Number	Number
0 (<1%)	0 (0%)	0 (0%)	0 (0%)

- 6.2 Other ways used to resolve complaints include:
- Written response/explanation
  - Acknowledgment if there has been a failure
  - Apology
  - Change to service
  - Mediation/conciliation
  - Meeting
  - Internal review
  - Redress

### 7 Complaints referred to the Local Government Ombudsman



## Appendix A

- 7.1 There were 4 social care complaints considered by the Local Government Ombudsman in 2015/2016.
- 7.2 One complaint was referred to the LGO, they found minor fault and we agreed to apologise to the complainant.
- 7.3 One complaint was referred to the LGO, following an investigation no fault found
- 7.4 One complaint was referred to the LGO and following an investigation, did not find fault with SBC regarding the safeguarding investigation however did find fault with the care provider as they had not properly kept their records. We agreed to pay £250 in respect of the uncertainty caused by the unavailability of these records.
- 7.5 One complaint was referred to the LGO, following an investigation we were found at fault for not completing a carers assessment. We were asked to rectify this and paid the complainant for retrospective carers budget.

### **8 Response times**

- 8.1 Adherence to response times is measured by compliance with the agreed dates set out in the individual complaints plans.
- 8.2 113 complaints were responded to within the timescales agreed. This represents 64.2% of responses made and is reduction on last year's 66%. We recognise the importance of trying to achieve a speedy resolution to complaints and generally aim to resolve complaints within 10 working days. However depending on the complexity of the complaint raised, agreement is made with complainants on an acceptable timescale for a response.
- 8.3 Of the 63 not responded to within the agreed timescale, 38 were attributed to our contracted care providers. Our Contracts Team and Complaints Manager continue to work with the home care providers to address this issue and effect improvements around complaints handling. A target has been introduced and their performance will be discussed with each provider at their quarterly contract review meetings.
- 8.4 Compliance with response times is shown at [Appendix 2](#)

### **9 Types of issues raised**

- 9.1 The bar chart at [Appendix 5](#) shows all the issues split between internal and commissioned services.
- 9.2 Overall, the top 5 issues were:
  - I. Communication / Consultation
  - II. Conduct / Behaviour of staff
  - III. Late Calls

- IV. Missed Calls
- V. Quality of Service provided

## **10 Outcome status of complaints (upheld; partially upheld; not upheld)**

- 10.1 The 176 complaints, refers to 237 issues which were reported and responded to, 106 were upheld; 32 were partially upheld; 88 were not upheld, 10 we were unable to reach a finding and 1 is still ongoing due to legal implications.
- 10.2 Overall the number of complaints upheld or partially upheld has slightly decreased from 59% in 2014/15 to 58% in 2015/16.
- 10.3 Tables at Appendix 7 show outcomes of the main issues in internal, homecare and residential complaints. There has been a decrease in complaints upheld or partially upheld regarding missed and late home care calls from 56 in 2014/15 to 40 in 2015/16, whilst there remains the challenge by many providers to recruit and retain good quality care staff, the decrease in complaints regarding this issue demonstrates the ongoing commitment by providers to continually monitor and address these issues.

## **11. Monitoring & Reporting**

- 11.1 Statistical data regarding complaints about our commissioned home care providers are provided quarterly to inform the Contract Monitoring Meetings.
- 11.2 Complaints are monitored by the Complaints Manager for any trends/emerging themes and alerts the relevant service accordingly.
- 11.3 Complaints information is fed into the monthly Safeguarding meetings regarding providers to ensure a full picture is gathered regarding the providers service delivery and indentify any concerns or trends that may be emerging.

## **12 Learning from Complaints**

- 12.1 The Council continues to use complaints as a learning tool to improve services and to plan for the future. Local authorities are being asked to show what has changed as a result of complaints and other feedback that it receives.
- 12.2 Improvements have been categorised under the following headings:
  - Improved process
  - Increased awareness of improved outcomes for Adults
  - Increased staff awareness/training
  - Improved conduct of staff
  - Improved performance of provider

Examples of improvements made as a result of complaints are shown in [Appendix 8](#).

- 12.3 Complaints about communication are a reoccurring theme for internal services and whilst they are not particularly high in proportion to the number of service

users being dealt with on a daily basis, this is the most common issue. The Complaints Manager continues to work with the Service Managers & Team Managers on identifying ways to improve client satisfaction with all channels of communication.

### **13. Corporate Implications**

#### **13.1 Resource Implications (Finance, People, Property).**

If resolutions are not found at an early stage and there are undue delays, compensation may have to be paid to acknowledge the time and trouble that the complainant has expended.

In some cases, the initial input in terms of staff time to find a resolution through a meeting/conciliation may be quite intensive but where the complainant has an ongoing relationship with the service, it can save resources in the long term.

#### **13.2 Contribution to Council's Vision & Critical Priorities**

A robust and responsive complaint handling process adds to the public's confidence and satisfaction with the way they are dealt with by the local authority when they have concerns to raise.

Effective complaints handling and a well advertised procedure contributes to the corporate priorities:

- Work with and listen to our communities and partners to achieve better outcomes for all
- Look after and safeguard our children and vulnerable adults

#### **13.3 Equalities and Diversity Implications**

The gender of all complainants was noted and 117 were female and 59 were male. 47 complaints (26%) were made by the person receiving the service and the remaining 73% were made by another person, usually a relative, on behalf of the service user. Leaflets on how to make a complaint or compliment are left with the service user when they are assessed. It is recognised that some relatives do not live locally and there is information on the Council's website about how to give feedback and the facility to send it electronically.

#### **13.4 Value for Money**

Some complaints may have elements where improvements may be made to ensure value for money.

#### **13.5 Community Safety Implications**

Some complaints may have elements where improvements may be made to ensure community safety.

## 14. Background Papers

Complaints papers are kept by the Customer Services & Complaints Manager. Data about individual compliments concerns comments and complaints are held electronically.

## 15. Appendices

Appendix 1	Examples of complimentary comments received regarding Internal teams
Appendix 2	Number of complaints by internal and commissioned services (residential & homecare) Compliance with response times
Appendix 3	Internal service complaints by team
Appendix 4	Commissioned and internal service complaints by service user group
Appendix 5	Issues raised in complaints
Appendix 6	Issues outcomes split between internal and commissioned services
Appendix 7	Outcome status of the top issues split between internal; homecare and residential care complaints
Appendix 8	Examples of learning/service improvements

## Appendix 1

### Compliments received 2015 -16

Some examples from the 341 compliments received about Southend Council's Internal Services :-

*I sincerely thank you for all the help and support you have provide Kathleen and I over the last year. I am relieved that Kathleen has a new home which will support her needs and that she will receive kindness and understanding. I am also grateful that you have introduced Helen into her life which will provide continuity and a voice for Kathleen. It hasn't been an easy situation for anyone involved however your determination and tenacity to complete this case beyond your remit is to your credit; thank you. This ends your involvement with Kathleen, I know, however your work will have a lasting benefit for her health and well being.*

~

*At all times you have the well-being of the individual at heart. You always listen & understand what I am trying to tell you about my father. I wish I'd met you sooner, as your help & advice have been invaluable.*

~

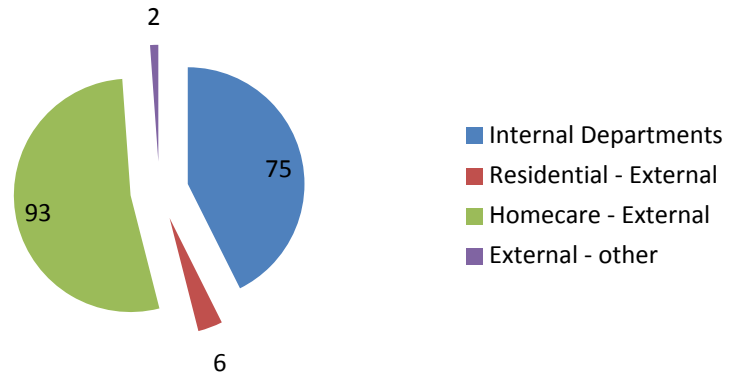
*The thought of the interview depressed me. I didn't want to accept the fact that I now needed help. Glynn's visit changed all that. She was cheerful, kind & kept complementing me on how much independence I was maintaining. She lifted my spirits enormously. 3 days later the equipment arrived. The man who delivered them was equally helpful, cheerful & encouraging. They have both improved my mobility & my attitude to life.*

~

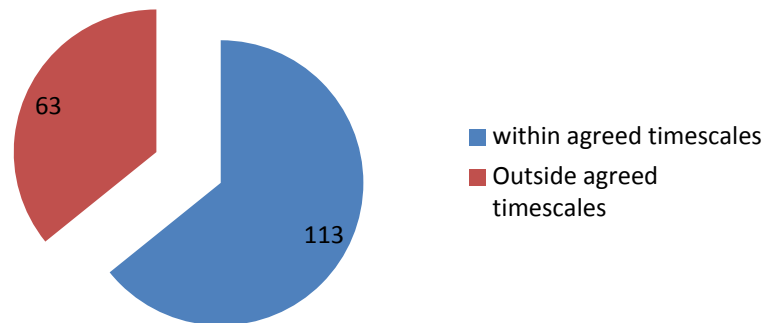
*We would like to thank you all so very much for your dedicated care & love you gave to our Mum, Rosie, while she was in your care. Priory House is second to none, one big happy family, who we were part of. Thank you once again.*

## Appendix 2

**Distribution of 2015/16 Complaints**  
Total = 176

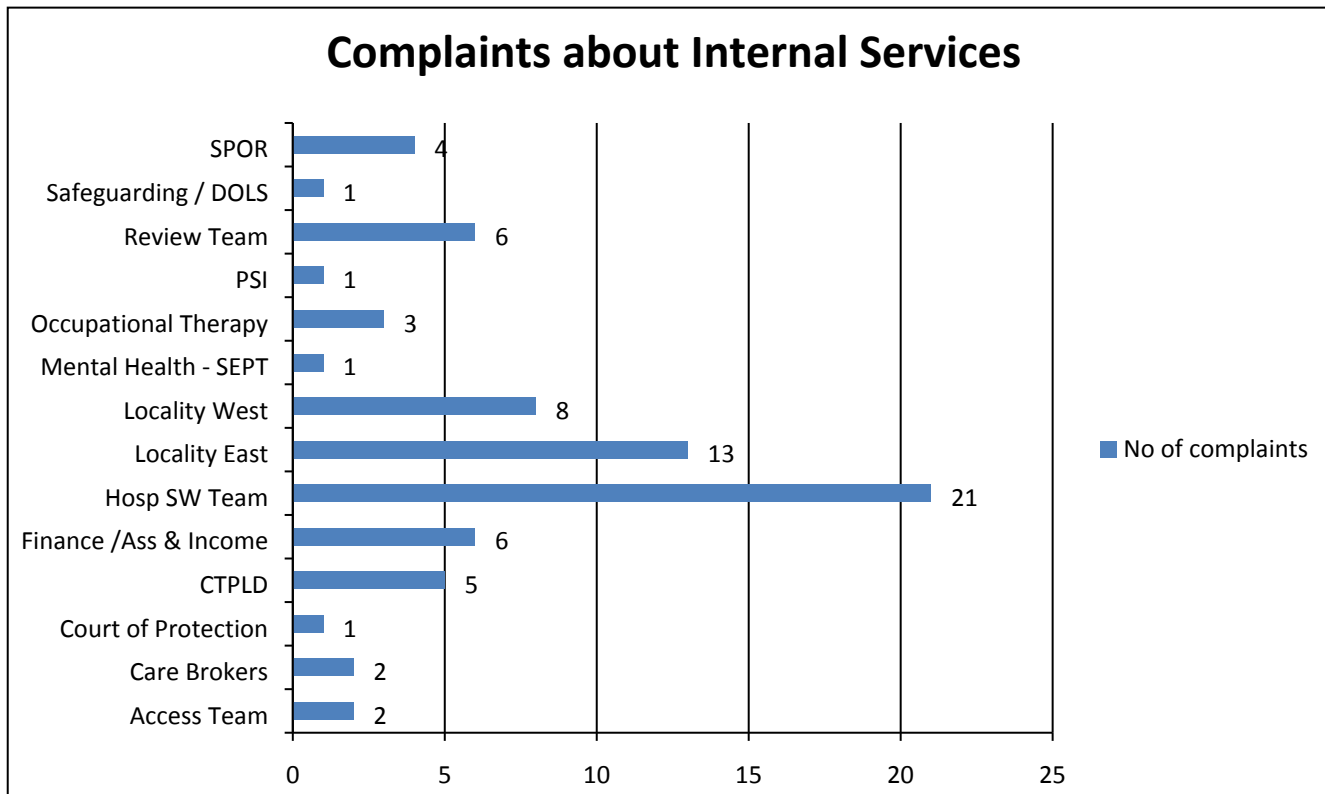


**Response Times**



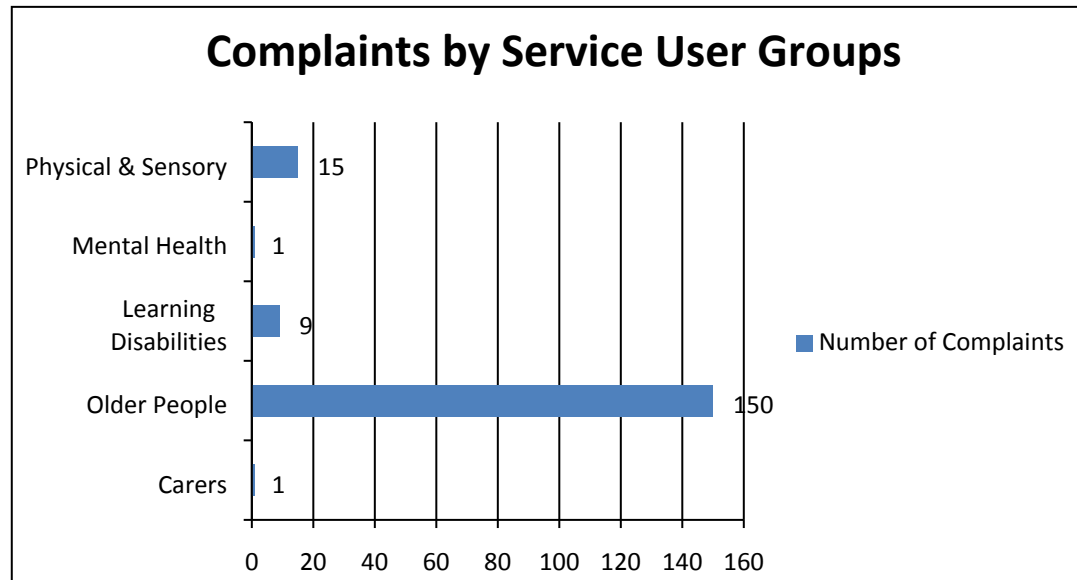
### Appendix 3

29



*Note : complaints about the Hospital Social Work Team were in the main about information provided about charges for services following discharge from hospital*

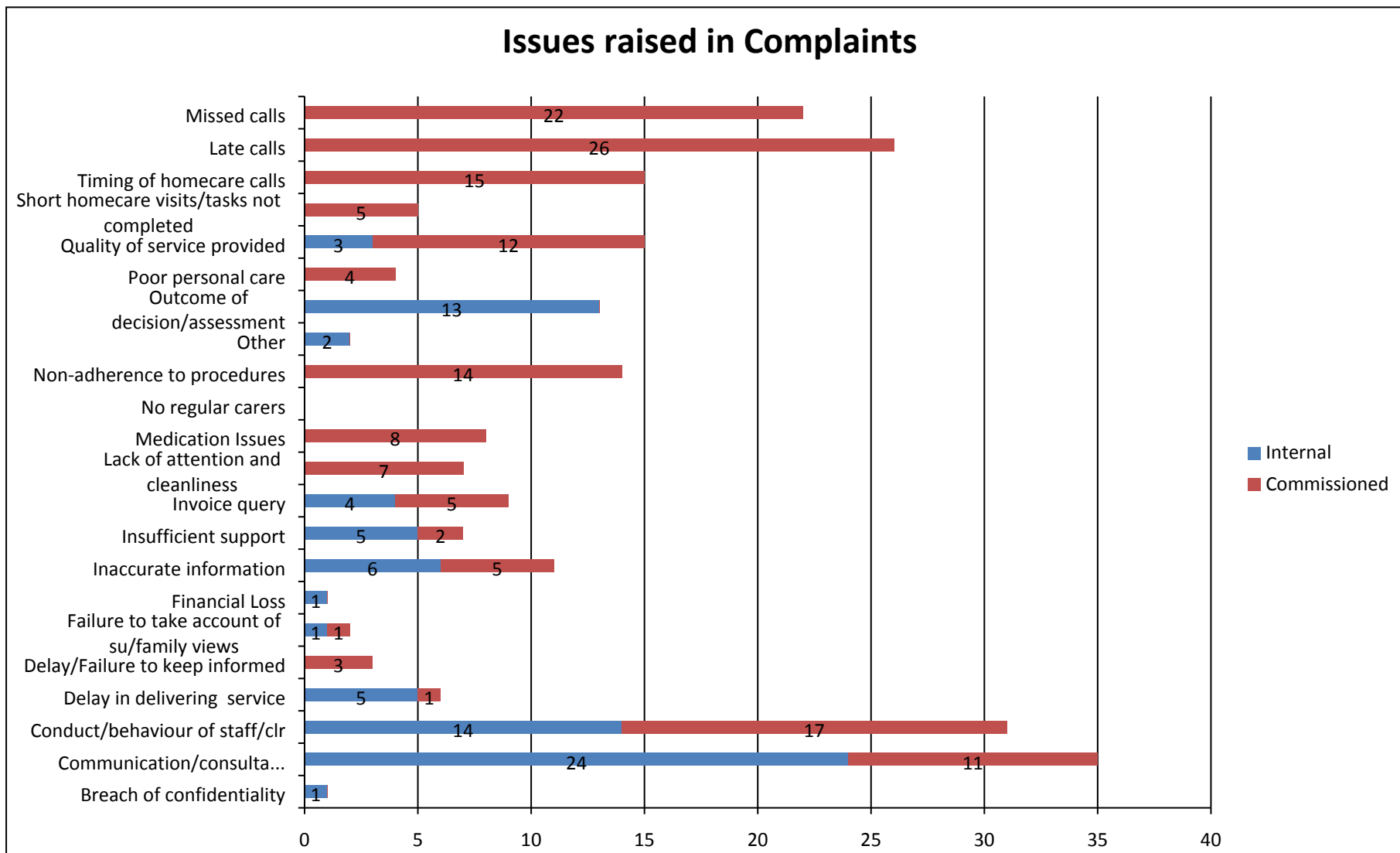
## Appendix 4



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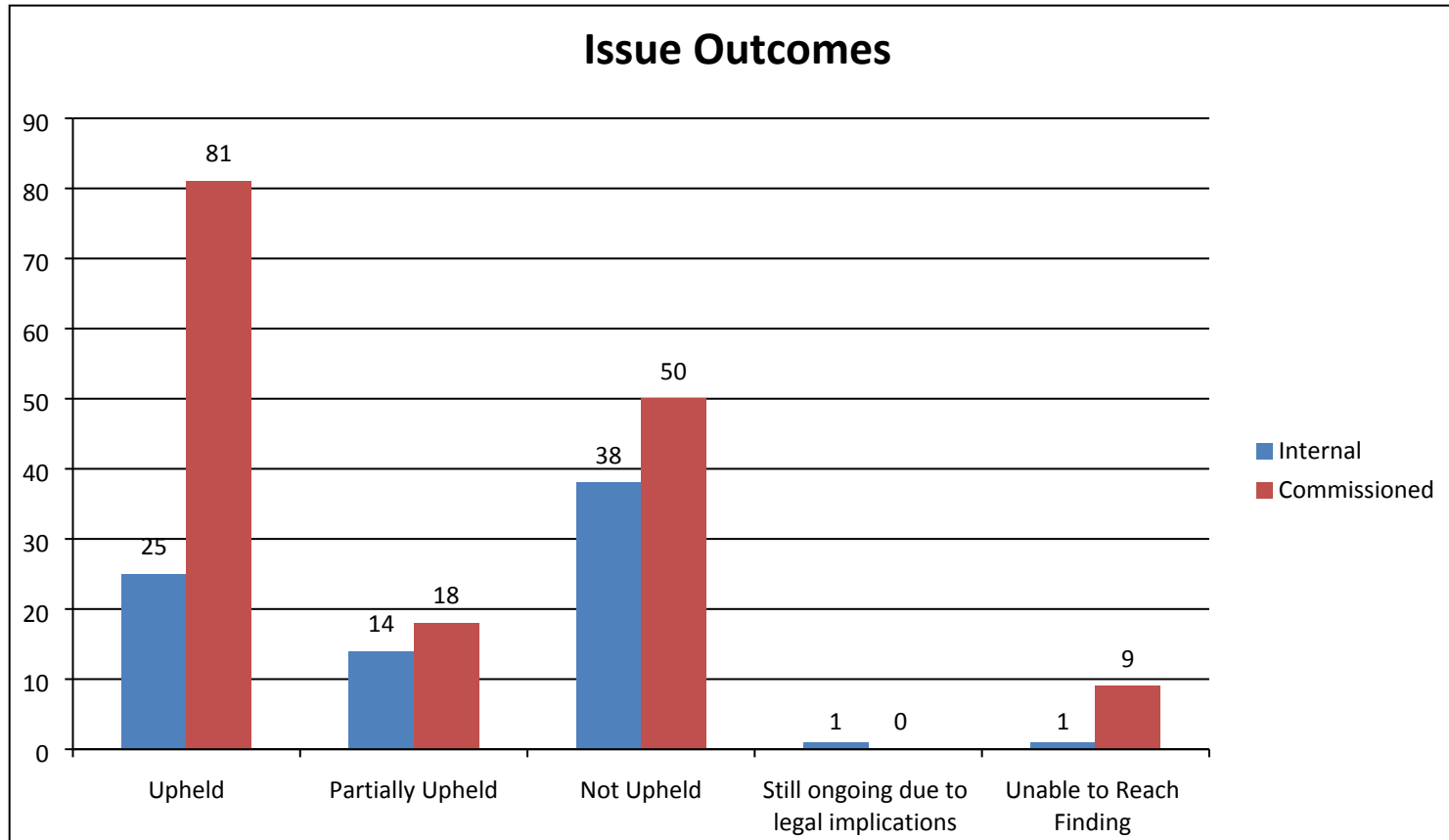
## Appendix 5



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Appendix 6

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## Appendix 7

<b>SBC Internally provided Services</b>	Upheld	Partially Upheld	Not Upheld	Unable to reach finding
Communication/consultation	11	4	9	0
Conduct/behaviour of staff	5	2	5	1
Outcome of decision/assessment	2	2	9	
Inaccurate Information	3	1	2	

<b>Commissioned Services (Homecare &amp; Residential)</b>	Upheld	Partially Upheld	Not Upheld	Unable to reach finding
Late calls	21	4	1	
Missed calls	14	1	7	
Conduct Behaviour of Staff	5	2	6	4
Timing of homecare calls	7	5	3	

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## Appendix 8

Issue	Improvements
Complaints regarding full cost invoices being sent to Service Users who receive a service via a 'spot' provider. These are providers who are not contracted to SBC and do not use the electronic monitoring system CM2000.	Requested that the 'spot' providers provide accurate timesheets for the care provided and the Business Support Team now collate the individual times to allow for more accurate invoicing to the Service User.
Confusion regarding information provided whilst in hospital regarding discharge and care options	Improved the discharge pack provided by the Hospital Social Work Team, this provides targeted advice and info regarding discharge planning and options as well as charging information
Complaints still being received where adults and/or families claim that they were told that care would be free following discharge from hospital and not made aware that there would be a charge for the ongoing care	<ul style="list-style-type: none"> <li>• Hospital Social work teams have been asked to document their discussion around finance in the body of the assessment as well as an observation on our care record system</li> <li>• Hospital Social Work Team Manager as spoken to the NHS staff to ensure they do not provide mis-leading information to patients.</li> </ul>
Complaints regarding missed / late calls	<ul style="list-style-type: none"> <li>• The contracts team have strengthened their contract monitoring to visit care providers in between the quarterly contract monitoring meeting. To ensure compliance with the electronic monitoring system, and highlight and address any issues earlier. Any trends in issues raised as part of complaints are fed back to the contracts team.</li> <li>• More focus within the contract monitoring meetings on late / missed visits and complaint response timescales</li> </ul>

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# Southend-on-Sea Borough Council

## Report of Corporate Director for People

to

## Cabinet

on

20th September 2016

Report prepared by: Charlotte McCulloch – Customer Service  
& Complaints Manager

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**Compliments and Complaints relating to Children's Social Care Services  
People Scrutiny Committee - Executive Cllr James Courtenay  
A Part 1 Public Agenda Item**

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### **1 Purpose of Report**

- 1.1 To fulfil the local authority's statutory duty to produce an annual report on compliments and complaints received about its children's social care function throughout the year.
- 1.2 To provide statistical and performance information about compliments and complaints received from April 2015 to March 2016 at all three stages of the statutory process.

### **2 Recommendation**

- 2.1 **That the Department's performance during 2015/2016 be noted.**

### **3 Summary**

- **8.3% increase in the number of compliments in 2015/16 compared to the previous year**
  - **39% increase in the number of Stage 1 complaints**
  - **87% of Stage 1 complaints responded to within 20 working days**
- 3.1 The number of statutory complaints has increased this year in comparison to 2014/15.
  - 3.2 The number of stage 1 complaints responded to within 20 working days was 87% compared to 93% in 2014/15 and 100% in 2013/14.
  - 3.3 6 complaints escalated to stage 2 in 2015/16, an increase from 4 in 2014/15.
  - 3.4 7 complainants approached the Local Government Ombudsman in 2015/16.

3.5 65 compliments were received in 2015/16, a percentage increase of 8.3% compared with last year.

**4 Background**

4.1 The statutory process has three stages. Stage 1 affords an opportunity to try to find a local resolution usually at team manager level. At stage 2, the Department commissions an independent investigation of the complaint and the response is made by the Head of Service. At the third stage, the complaint is referred to an Independent Review Panel of three independent panel members with one member acting as Chair. At each stage, a more senior officer responds on behalf of the Department, with the Corporate Director responding at the final stage.

4.2 Some complainants welcome an independent investigation of their concerns; for others, the process can seem unwieldy. The regulations encourage consideration of alternative dispute resolutions where appropriate and with the Complainant’s agreement. The Complaints Manager encourages and supports Team Managers to resolve complaints at the earliest stage.

4.3 If complainants remain dissatisfied at the end of the three stages, they may refer their complaint to the Local Government Ombudsman.

**5 Compliments received in 2015/2016**

5.1 The Department recorded 65 compliments about Children’s Social Care in 2015/16 compared with 60 in 2014/15. This is a percentage increase of 8.3% compared to last year.

The table below shows the number of compliments received in 2015/16 in comparison with previous years.

Apr 10 – Mar 11	Apr 11 – Mar 12	Apr 12 – Mar 13	Apr 13 – Mar 14	Apr 14 – Mar 15	Apr 15 – Mar 16
12	25	41	44	60	65

**6 Number of complaints received in 2015/2016**

**6.1 Stage 1**

6.1.1 The Department received 85 statutory complaints directly at stage 1 in 2015/16 compared with 61 in 2014/15. This represents an increase of 39%. Complaints are always welcomed and viewed as a way to improve practice and Managers are informed of any emerging trends in complaints.

6.1.2 93% of complaints were resolved at Stage 1 which is consistent with the figure from the previous year which was 93.3% in 2014/15

6.1.3 The table below shows the number of Stage 1 complaints received in 2015/16 in comparison with previous years.

Apr 10 – Mar 11	Apr 11 – Mar 12	Apr 12 – Mar 13	Apr 13 – Mar 14	Apr 14 – Mar 15	Apr 15- Mar 16
55	58	60	38	61	85

**6.2 Stage 2**

6.2.1 A total of 6 complaints reached Stage 2 of the complaints process in 2015/16, an increase from 2014/15 when 4 complaints were escalated to Stage 2.

**6.3 Stage 3**

6.3.1 2 complainants requested to pursue their complaint to Stage 3 this is an increase from 0 in 2014/15.

**7 Complaints by category**

7.1 Complaints were categorised by the main issues raised. Appendix 1 shows the causes of the complaints.

The 3 categories with the highest percentage of complaints were:

- Quality of service 29.4%
- Staff conduct / Employee Behaviour 23.5%
- Communication 21.2%

These 3 categories represent 75% of all complaints received. Appendix 2 shows the outcomes of these categories.

After thorough investigation of all complaints at stage 1, 69% of complaints were not upheld which has reduced in comparison to 79% in 2014/15 and 76.3% in 2013/14.

7.2 Complaints about Social workers/staff have reduced to 23.5% compared to 26.2% in 2014/15. However complaints regarding communication has increased from 13.1% in 2014/15 to 21.2% in 2015/16. Social workers have been reminded of their responsibilities about returning calls, responding to correspondence promptly and that regular communication is key to positive working relationships with service users.

It is apparent that many Complainants still believe that it is the Social Worker alone who makes decisions regarding child protection procedures and agreed actions. Work needs to continue to ensure that Service Users and their families understand the role of the Social Worker. There is a need for Social Workers to be more conscious of ensuring that explanations about difficult decisions are given in the context of wider policies when discussing and meeting with Service Users.

7.3 Complaints about the Quality of Service has increased from 24.6% in 2014/15 to 29.4% in 2015/16. The Complaints Manager has worked

closely with social work teams to highlight good practice and provide early identification of emerging trends.

7.4 The table below shows the percentage of complaints in each category in 2015-2016 in comparison with the previous year.

<b>Category</b>	<b>2013-14 % of total</b>	<b>2014-15 % of total</b>	<b>2015-16% of total</b>
1. Staff conduct / employee behaviour	7.9	26.2	23.5
2. Providing a service	5.3	6.6	4.7
3. Quality of service	34.2	24.6	29.4
4. Communication	31.6	13.1	21.2
5. Incorrect information given	10.5	0	0
6. Council decision making	5.3	11.5	8.2
7. Policy or procedure	5.3	14.8	8.2
8. Other	0	3.3	3.5
9. Service not provided	0	0	1.2
	100%	100%	100%

## **8 Response times and the Department’s performance**

### **8.1 Stage 1**

8.1.1 Stage 1 statutory response times: 10 working days, with a further 10 days for more complex complaints or additional time if an advocate is required.

8.1.2 The complexity of complaints has continued to increase, which has affected the ability to respond within the 10 working days. In 2015/16 51% of Stage 1 complaints were responded to within 10 working days compared to 73.7% in 2014/15 71% in 2013/14, 80% in 2012/13.

8.1.3 The percentage of stage 1 complaints responded to within the statutory timescale of 0–20 working days in 2015/16 was 88% compared with 93.4% in 2014/15 100% in 2013/14.

8.1.4 The table below shows response times for stage 1 complaints received in 2015/16 compared with 2014/2015 and 2013/2014

<b>Response Performance</b>	<b>2013/14 % of total</b>	<b>2014-15 % of total</b>	<b>2015-16 % of total</b>
Within 10 working days	71	73.7	51
10 – 20 days	29	19.7	37
Over 20 days	0	6.6	12
Withdrawn	0	0	0

Appendix 3 shows the response performance.



**8.2 Stage 2**

**8.2.1 Stage 2 statutory response times:**

A stage 2 complaint should be investigated and responded to within 25 working days, with a maximum extension to 65 working days.

6 complaints were pursued to Stage 2 in 2015/16. One of these complaints was not pursued by the complainant following the initial meeting with the Investigating Officer and Independent Person.

This year we were unable to complete any of the Stage 2 investigations within 65 working days. Due to the complexity of some of the cases and the availability of service users and investigating officers (who are now SBC staff) these timescales do represent a challenge.

Stage 2 complaints are subject to independent investigations involving interviews with the complainant and relevant members of staff, and the submission of a report to be responded to by the Head of Service.

Complaints at this stage are likely to involve an independent investigating officer, an independent person and an advocate.

The Stage 2 process starts as soon as a complainant decides to pursue their complaint to stage 2.

**8.2.2 The table below shows response times for Stage 2 complaints received in 2014/15 compared with 2013/2014 and 2012/2013**

Response Performance	2013/14 % of total	2014/15 % of total	2015/16 % of total
Within 25 days	0	0	0
Between 25 and 65 days	100	0	0
After 65 days or outstanding	0	100	100

**8.3 Stage 3**

**8.3.1 Stage 3 statutory response times:**

The recommendations from a Stage 3 Review Panel should be responded to within 15 working days of the date of the meeting.

**8.3.2 2 complaints were escalated to stage 3 during 2015/16 this is in comparison to none in 2014/15.**

**8.3.3 One Stage 3 complaint was responded to within the timescale of 15 days.**

**9 Outcome status of complaints**

**9.1 Stage 1**

The outcomes of these complaints are logged as either upheld or not upheld. In 2015/16, of the 85 complaints received, 26 were upheld (31%) and 57 were not upheld (67%) & 2 that are still ongoing.

**Stage 2 & 3**

There are often several points in a complaint at Stage 2 & 3 and these are addressed separately in a single response. The complaint may be upheld; not upheld; partially upheld, or inconclusive/no finding.

**Stage 2** – Of the 85 complaints received 6 were moved to Stage 2, 1 was not progressed and of the 5 remaining this involved 44 separate issues. The following shows the outcomes of these issues

Upheld	Partially Upheld	Not Upheld	No Finding
13	6	19	6

**Stage 3** – Of the 6 complaints that moved to Stage 2, 2 of these requested their complaint be escalated to Stage 3, this related to 10 separate issues.

Upheld	Partially Upheld	Not Upheld
1	3	6

**10 Local Government Ombudsman**

10.1 7 complainants approached the Local Government Ombudsman in 2015/16. However 5 of these complaints were either premature or out of jurisdiction of the LGO and referred back to the authority. The remaining 2 complaints were investigated by the LGO 1 was not upheld and 1 required no further action.

**11 Developments in the complaints process**

11.1 Senior Managers receive regular reports from the Complaints Manager which serve to highlight trends. There is also a facility available for managers to print Covalent reports at any time therefore ensuring that information is immediately available.

11.2 The Complaints Manager has delivered training to Children’s Social Care Staff to explain the complaints process and the role that the teams play in resolving complaints at the earliest possible stage.

11.3 The Complaints Manager offers ongoing support and advice to Team Managers on how to appropriately respond to complaints.

**12 Areas for improvement / Learning from Complaints**

- 12.1 Service Managers are being asked to ensure that families are aware of changes in their children's social worker. This will be achieved by implementing a new process whereby when a case is reallocated, a standard letter goes to the family giving the name of the new social worker, their phone number, name and phone number of admin[s] for the team and a duty number and the name of the team manager. The letter is sent to both parents
- 12.2 Team Managers have been reminded that when concerns are raised it essential that every effort is made to contact both parents to inform them of the concerns and offer an opportunity for them to be involved in the procedures.
- 12.3 All Social Workers and Team Managers have been reminded that all complaints should be brought to the Complaints Manager's attention as soon as possible to ensure that appropriate responses are provided within the statutory timescales.
- 12.4 Complaints about Communication issues continue to be an issue, it has been agreed for the Complaints Manager to attend the Service / Team Managers training day and to regularly attending the monthly management team meeting to help Service Managers identify areas for improvement in this area.

### **13 Corporate Implications**

- 13.1 Contribution to Council's Vision & Corporate Priorities.  
Complaints management is relevant to the Council's corporate priority of continuing to improve the outcomes for vulnerable children.

#### 13.2 Financial Implications

The commissioning of independent people to investigate Stage 2 complaints incurs additional cost. The Department seeks to provide efficient resolution to complaints at the earliest stage possible, as well as securing value for money from Investigating Officers. Costs are met from the existing base budget.

#### 13.3 Legal Implications –

The complaints process complies with statutory requirements.

#### 13.4 People Implications

None.

#### 13.5 Property Implications

None.

#### 13.6 Consultation

The Advocacy Services and Representations procedure (Children) (Amendment) Regulations 2004 confer a duty on local authorities to provide information about advocacy services and offer help to obtain an advocate to a child or young person wishing to make a complaint. The Authority has a contract with the National Youth Advocacy Service.

In 2015/16 3 complaints (3.5%) were made by children/young people. This is a decrease from 9 (14.8%) in 2014/15. All children and young people wishing to make a complaint in 2015/16 were offered the services of an advocate.

### 13.7 Equalities and Diversity Implications

Customer groups making complaints included parents, carers, advocates, family members and young person's themselves.

The majority of complaints are made by females and the high percentage reflects the nature of the service that the primary parent dealing with children's care issues is generally the mother. However the number of males making complaints has risen to 28% this year from 19.7% in 2014/15.

The gender of complainants is shown in Appendix 3.

### 13.8 Value for Money

The complaint process continues to be implemented by 1fte post. We have reduced the use of external Investigating Officers for Stage 2 complaints and now use SBC Managers to undertake this role.

Improving systems in relation to managing compliments and complaints is a factor in a trend toward improving value for money and improving operational practice from lessons learned.

### 13.9 Community Safety Implications – none

### 13.10 Environmental Impact – none

## 14 Background papers – none

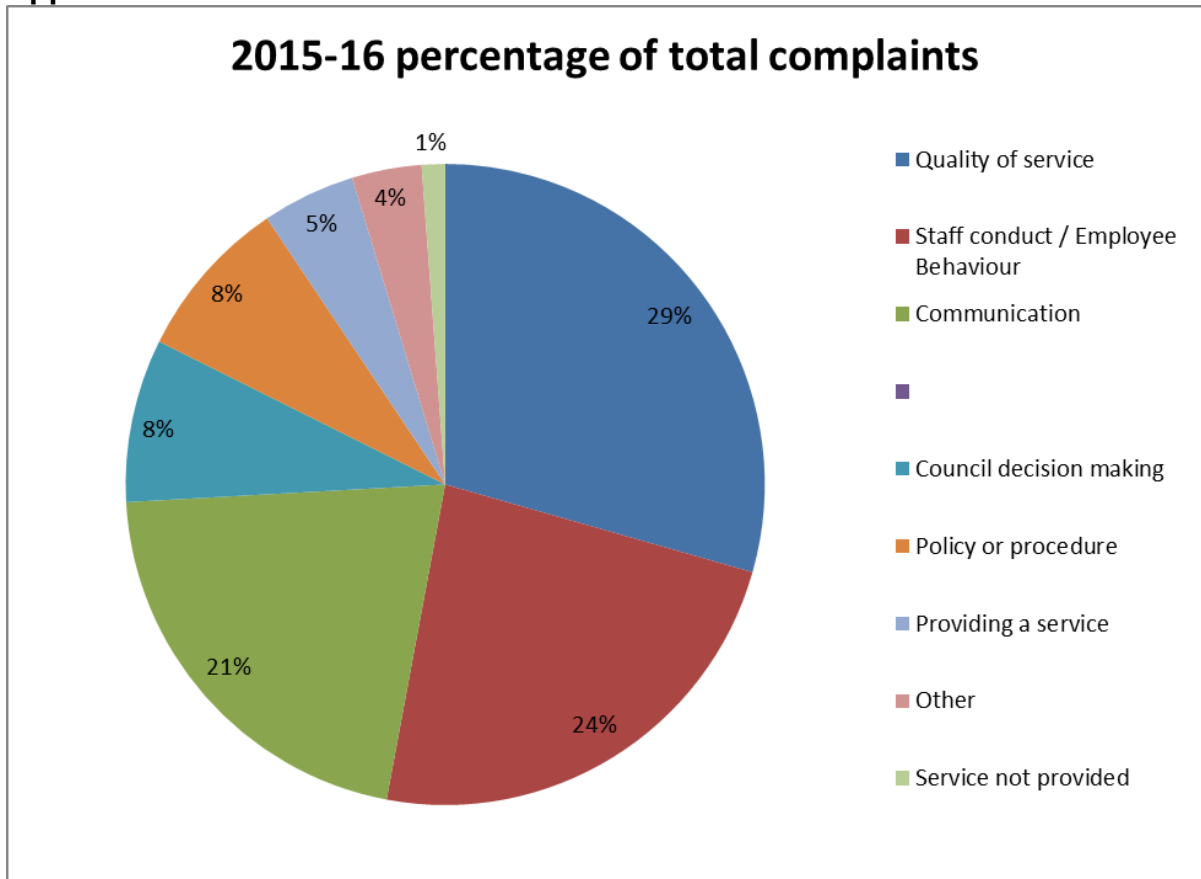
## 15 Appendices

15.1 Appendix 1: Percentage of complaints by cause

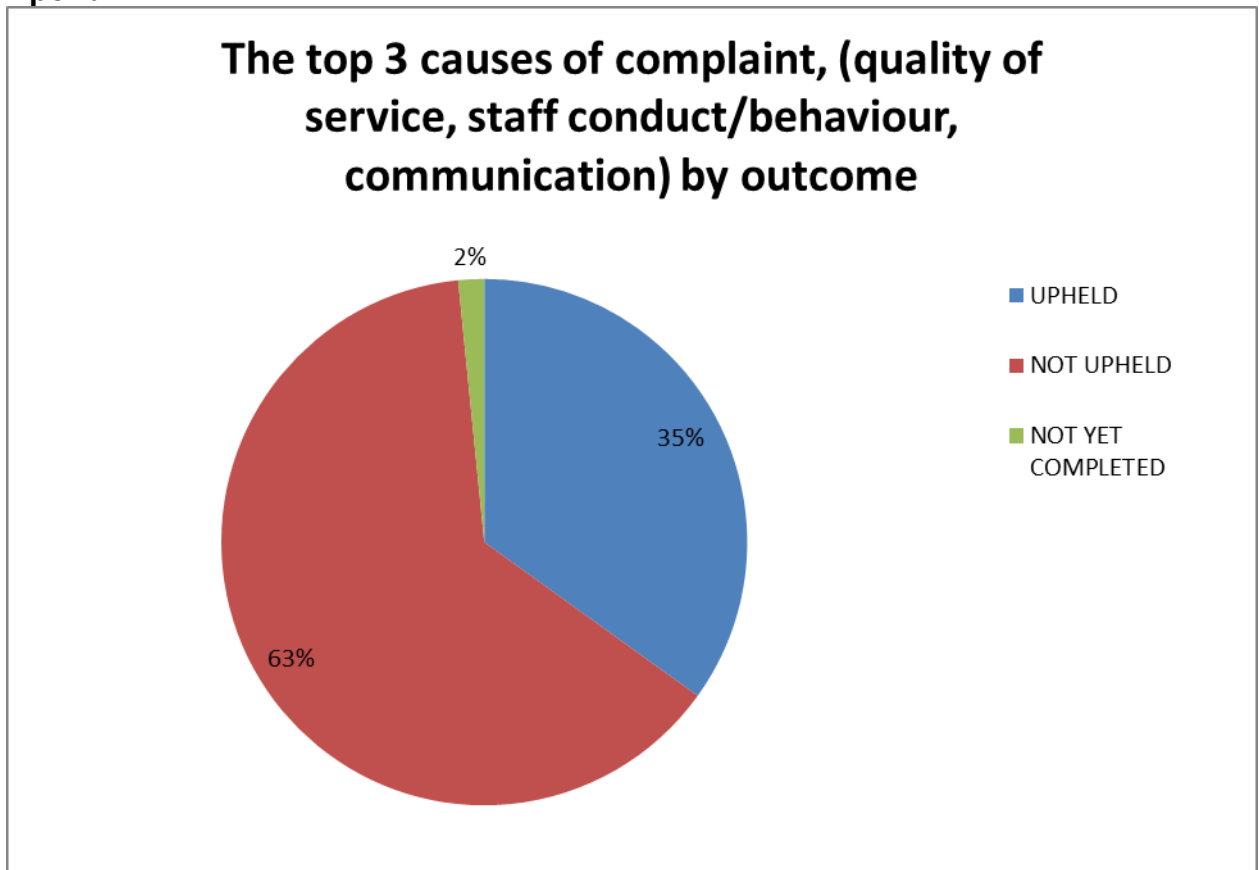
Appendix 2: Percentage outcome of the main causes of complaint

15.3 Appendix 3: Percentages of complaints responded to by timescale

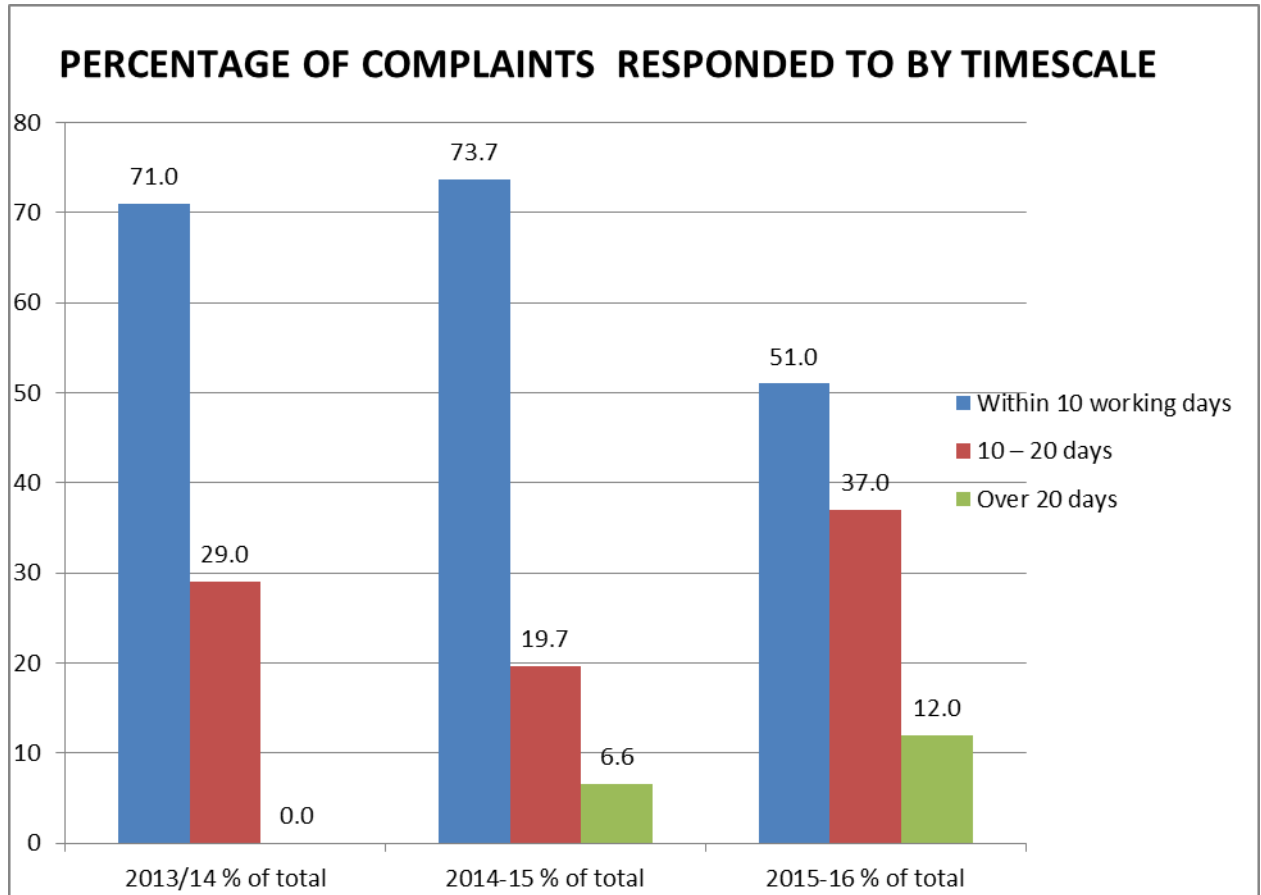
Appendix 1



Appendix 2



Appendix 3



# Southend-on-Sea Borough Council

Report of Corporate Director for Corporate Services

to  
**Cabinet**  
on  
**20 September 2016**

Report prepared by: Tim MacGregor, Team Manager, Policy  
& Information Management

<b>Agenda Item No.</b>
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## Corporate Comments, Complaints and Compliments - 2015-16

Place, People, Policy & Resources; Scrutiny Committee – Executive Councillor:  
Councillor Lamb

### A part 1 Public Agenda Item

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#### 1. Purpose of Report

- 1.1 To report on the performance relating to corporate comments, complaints and compliments process and to provide comparisons with previously reported results. Complaints and compliments in respect of adult and children's social care functions are subject to their own statutory processes and are not monitored by the corporate procedure. Their results are reported separately.

#### 2. Recommendations

- 2.1 **To note the performance of the corporate complaints process between April 2015 and March 2016.**

Refer the report directly to all Scrutiny Committees.

#### 3. Background

- 3.1. The three stage complaints procedure outlined in **Appendix 1** has been in place since 2009 and is well established throughout the organisation
- 3.2 The Council's corporate comments, complaints and compliments process deals with all general feedback about the Council. As well as the children and adult social care statutory complaints there are certain other functions which are outside of the corporate procedure which have their own processes. Examples include appeals against parking tickets and concerns about schools.
- 3.3 The benefits in operating a feedback process include:
- To learn lessons from the types of feedback made

- To help improve service delivery
- To improve the consistency and timeliness of responses
- To reflect sector wide and Local Government Ombudsman (LGO) best practice.

3.4 This report, therefore, provides an update on how the process is working and an analysis of customer feedback data.

#### 4. PERFORMANCE TO DATE

##### 4.1 Performance

Details of performance data for 2015/16 is set out in [Appendices 2 to 5](#).

402 Stage 1 complaints were received during 2015/16, reflecting a decrease of 5.7% compared to the 425 complaints reported for 2014-2015 and 420 complaints in 2013-14.

The monitoring system that is in place highlights trends and issues that are subject to complaints. Areas that have been of note, at all stages, for 2015/16 include:

- Quality of service – 32%
- Providing a service – 32%
- Staff conduct/employee behaviour – 14%

##### 4.2 Complaints by Directorate with Response Times

Department	Stage 1 & 2 Total Complaints Apr 2013-Mar 2014	Responded to in 10 working days	Stage 1 & 2 Total Complaints Apr 2014-Mar 2015	Responded to in 10 working days	Stage 1 & 2 Total Complaints Apr 2015-Mar 2016	Responded to in 10 working days
Corporate Services	40	80%	38	92%	62	81%
Department for People	48	79%	44	80%	41	68%
Department for Place	372	80%	379	92%	344	83%
Public Health					1	100%
<b>Grand Total</b>	<b>460</b>	<b>80%</b>	<b>461</b>	<b>87%</b>	<b>448</b>	<b>80%</b>

The number of stage 3 complaints received was 15, compared to 24 in 2014/15. Response times for Stage 3 complaints continue to be a challenge, taking an average of 44.5 days compared to 57 days in 2014/15. Complaints that escalate to Stage 3 are by their nature more complex and sometimes involve situations where it is not possible for the Council to meet complainants' wishes. However,



the response times are longer than desired and work will continue to reduce response times. It should also be noted that the process of early advice and assistance at Stage 2 by the corporate complaints member of staff resulted in less complaints progressing to the final stage of the process than would otherwise be the case.

### 4.3 Nature of Complaints

**Appendix 2** sets out the nature of all complaints under the following headings:

- Communication
- Decision making
- Discrimination
- Policy or procedure
- Providing a service
- Service not provided by council
- Staff conduct/staff behaviour
- Quality of service

The main areas of concern for 2015-16 were: providing a service (151); quality of service (154) and staff conduct/behaviour (67).

### 4.4 How the Complaints Were Received

The four year trend chart in **Appendix 3** shows that 65% of complainants contacted the Council by e-mail or the website, an increase from 61% for 2014/15 (and 50% in 2012/13) reflecting the work undertaken to encourage customers to use the website as their channel of choice. Complaints submitted by letter, phone and face to face are also on a downward trend.

### 4.5 Progression of complaints and satisfaction

- 89% of stage 1 complainants were satisfied with their response which is in line with 2014/15 reported figure of 90%.
- Of the 45 complaints that were addressed at stage 2 (there were 36 in 2014/15) 23 related to either quality of service or providing a service.
- The use of mediation between stages 2 and 3 will continue to be used, where appropriate, in an effort to further reduce the number of complaints reaching stage 3. Whilst the aim continues to be to resolve complaints at the earliest point it is worth noting that of the 15 stage 3 complaints investigated 2 were upheld.
- 4 complainants that completed the corporate process (in 2015-16) escalated their concerns to the Local Government Ombudsman. Of these 2 were closed after initial enquiries; 1 was not upheld as the claimed injustice was speculative and 1 was determined 'no fault' with the Council's action.

### 4.6 Complaint Resolution

The emphasis on learning from customer feedback continues to inform reviews to provide improved targeted services. This is evidenced by how complaints have been resolved:-

- Specific action has been taken in 44% of cases - by doing something that had not been done, carrying out work or putting something right.
- 30% of customers that complained received an apology when the Council had not got things right and no further action was needed, other than to apologise, or explain why the Council had taken a particular course of action.
- 24% of complaints required no action. This was where our actions were reviewed but deemed to be correct and no apology was required.
- 5 cases required a process review.

The chart in **Appendix 4** reflects the breakdown of complaints by resolution.

### 4.7 Learning Points

The continuous review of customer feedback, and resulting changes to service provision, will continue. A pro-active approach on learning points will also continue so that:-

- Previously identified trends will be monitored to ensure that the resulting service changes are reducing the number of related complaints.
- Continuous review of responses to ensure that identified service improvements that have been promised are implemented.

Examples of service improvements as a result of complaints and customer feedback include:

- A revised policy on dealing with abandoned vehicles, to make the process easier for those reporting incidents was agreed.
- Information on the rights of appeal for benefit claimants was revised on standard letters and the website.
- In response to a complaint about lack of transparency, the South Essex Homes Decant and Management Move Procedure was updated and made a public document.
- School transport appeals - reasoning is set out more in more detail both in appeal reports and letters to appellants;
- Reminders were issued to staff to replace food caddies appropriately, helped by a new category created on the Lagan customer service system to record where food cadies have not been replaced properly;
- Refuse vehicles are being fitted with 'on-board' systems to enable the direct reporting of service failures/requests, to refuse vehicles, enabling a swifter response.

### 4.8 Comments and Compliments

Govmetric, the customer satisfaction measurement tool used by the Council, specifically captures the provision of service by the Customer Service Centre and Council website and these figures are reflected in the Corporate Services analysis. It is anticipated that as the drive towards encouraging customer use of web based channels continues and results in less personal interaction between the Council and residents, so will the opportunities to receive compliments.

## Appendix C

Of the 1,673 comments and compliments recorded for Corporate Services in 814 were compliments, which compares to 505 recorded last year.

Comments received are responded to by the service and those making comments advised if their suggestion is to be taken up or not. Compliments are acknowledged and shared with the appropriate Head of Service to inform the service or member of staff. This may then inform the staff's performance review discussion.

The table below shows the 3 year comparison on total number of comments and compliments received by each Directorate.

Department	Total 2013/14	Total 2014/15	Total 2015/16
Corporate Services	1694	1326	1673
Department for People (excluding statutory complaints)	7	7	2
Department for Place	288	222	337
<b>Grand Total</b>	<b>1989</b>	<b>1555</b>	<b>2012</b>

### 5. LOCAL GOVERNMENT OMBUDSMAN (LGO)

56 complaints and enquiries about the Council were made to, and decided upon by the Ombudsman. This compared to 53 for 2014/15. Of the 56 complaints, 25 were categorised as 'referred back to the Council for local resolution\*', 12 were closed after initial enquiries, four were not referred on to the Council, and for one, advice was provided by the LGO to the complainant.

Of the remaining 14, seven were not upheld and seven were upheld. Of the 7 upheld, three were in adult services, one was in Benefits & Tax, one was in 'Corporate & other services' one in 'environmental services & public protection & regulation' and one was in planning.

[\*Prior to 2013/14 this category was known as 'premature complaints' and did not form part of the Ombudsman's statistics].

The LGO's annual review letter, including the breakdown of the results is attached at **Appendix 5**.

Alongside the statistical information the Ombudsman also publishes a yearly report on local government complaint handling. The report includes a summary of complaint statistics for every local authority in England which provides an opportunity for the Council to compare its performance against other Council's.

The table below shows comparisons with a small number of other similar authorities.

Local authority	Complaints/ enquiries made 14/15	Complaints upheld 14/15	Complaints/ enquiries made 15/16	Complaints upheld 15/16
Southend on Sea	58	5	54	7
Blackpool	50	9	47	7
Medway	137	19	97	19
Plymouth	90	10	102	19
Thurrock	82	5	82	9
Isle of Wight	70	6	60	14
Central Bedfordshire	58	6	65	10

## 6 MONITORING AND REPORTING

Regular reporting continues to inform Departmental Management Teams to coincide with their monthly report on performance.

## 7. CONCLUSIONS

The process continues to deliver improved performance and a more robust system of monitoring and real service improvements.

## 8 Corporate Implications

### 8.1 Contribution to Council's Vision & Corporate Priorities

Customer feedback and complaints management is directly relevant to the Council's corporate priorities.

### 9 Financial Implications

Service improvements and mediation continue to result in meaningful outcomes for customers and so ensure getting things right first time.

### 10 Legal Implications

This process is overseen by the Local Government Ombudsman

### 11 People Implications - See 14

12 Property Implications - None

13 Consultation - None

14 Equalities and Diversity Implications

The complaints process is open to all and has multiple methods of access for customers. Equality and diversity implications are a routine part of the process in recording customers details and are considered as part of any response. Mediation ensures people that may be vulnerable are able to access this service and receive the appropriate support.

15 Risk Assessment

16 Value for Money

The continued drive to resolving complaints as early as possible in the process reduce officer time spent dealing with concerns as well as providing the opportunity to improve service delivery.

17 Community Safety Implications - None

18 Environmental Impact - None

19 Background Papers – None

20 **Appendices**

Appendix 1 The Council's Comments, Complaints & Compliments Procedure

Appendix 2 Nature of Complaints

Appendix 3 How Stage 1 Complaints Were Received

Appendix 4 Complaint Resolution

Appendix 5 Local Government Ombudsman Annual Review Letter 2016

## **Appendix 1 – Southend-on-Sea Borough Council 3 Stage Process**

### **Complaints**

#### **Stage 1**

An initial complaint responded to by the respective service Group Manager.

#### **Stage 2**

A complaint that has been responded to but where the complainant is still unhappy and a response is required from the appropriate Director or Head of Service.

#### **Stage 3**

If the complainant is still unhappy after the Stage 2 process, they have a right of appeal to the Council's Chief Executive and Leader of the Council. In the first instance mediation is offered, if unsuccessful an investigation is undertaken and a report presented to the Chief Executive for consideration.

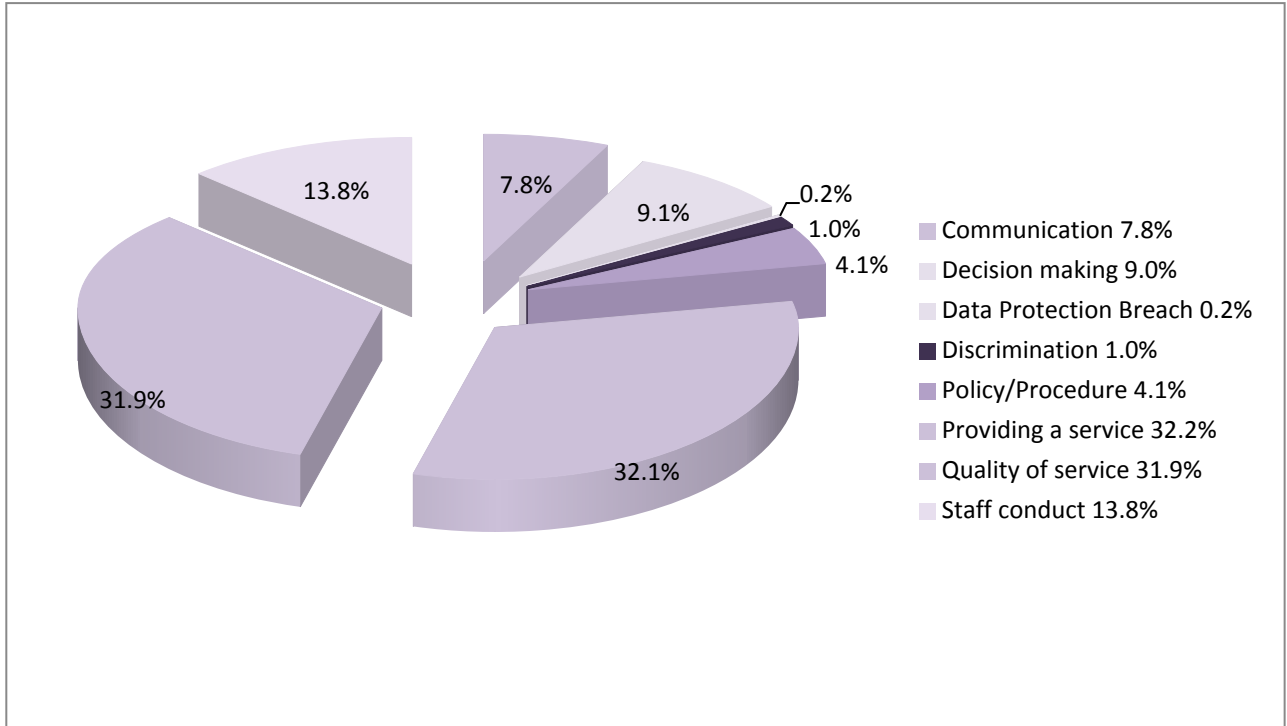
### **Comments and Compliments**

Any comment or compliment should be responded to within 10 working days. If a suggestion is not to be progressed then an explanation should be provided. Compliments must be gratefully acknowledged. Comments & Compliments should be logged into Covalent, the Council's system for recording feedback, by the Departmental Complaints Officer and reported as part of routine reporting to DMT.

Any compliments relating to staff should be shared with the appropriate Head of Service to share with his or her team member

Appendix 2 – Nature of Complaints – April 2015 to 2016

Total: 486



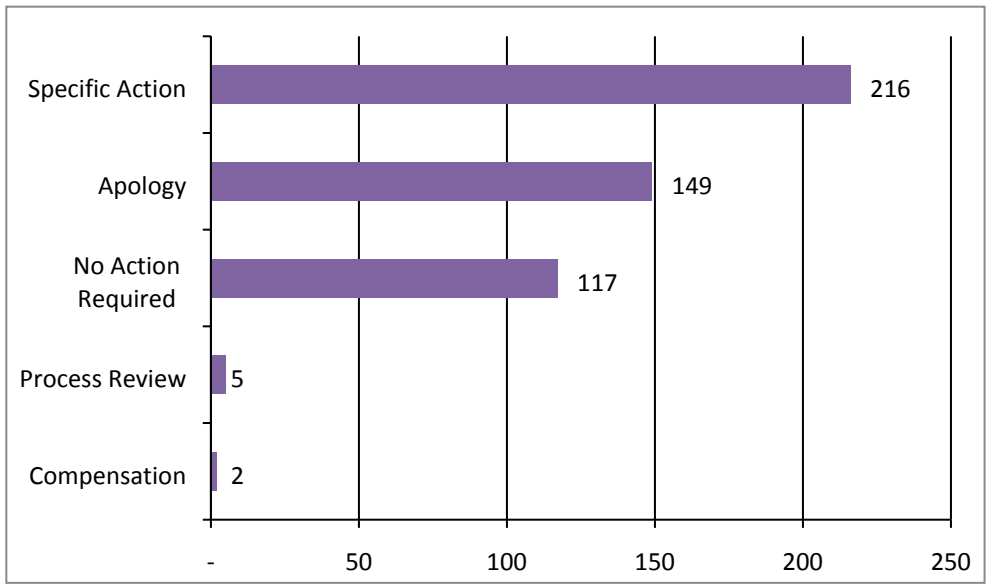
**Appendix 3 – 3 year Comparison of How Stage 1 complaints were received**

	<b>2012/2013</b>	<b>2013/2014</b>	<b>2014/2015</b>	<b>2015/2016</b>
Email	33%	29%	23%	34%
Internet	17%	18%	39%	31%
Telephone	26%	37%	30.5%	25%
Other	0%	1%	0.5%	0.5%
Letter / post	18%	10%	6%	9%
In person	6%	5%	1%	0.5%



**Appendix 4 Complaint Resolution April 2015 to 2016**

**Total 489**



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21 July 2016

*By email*

Rob Tinlin  
Chief Executive  
Southend-on-Sea Borough Council

Dear Rob Tinlin,

### **Annual Review Letter 2016**

I write to you with our annual summary of statistics on the complaints made to the Local Government Ombudsman (LGO) about your authority for the year ended 31 March 2016.

The enclosed tables present the number of complaints and enquiries received and the decisions we made about your authority during the period. I hope that this information will prove helpful in assessing your authority's performance in handling complaints.

Last year we provided information on the number of complaints upheld and not upheld for the first time. In response to council feedback, this year we are providing additional information to focus the statistics more on the outcome from complaints rather than just the amounts received.

We provide a breakdown of the upheld investigations to show how they were remedied. This includes the number of cases where our recommendations remedied the fault and the number of cases where we decided your authority had offered a satisfactory remedy during the local complaints process. In these latter cases we provide reassurance that your authority had satisfactorily attempted to resolve the complaint before the person came to us. In addition, we provide a compliance rate for implementing our recommendations to remedy a fault.

I want to emphasise that these statistics comprise the data we hold, and may not necessarily align with the data your authority holds. For example, our numbers include enquiries from people we signpost back to the authority, but who may never contact you.

In line with usual practice, we are publishing our annual data for all authorities on our website, alongside an annual review of local government complaints. The aim of this is to be transparent and provide information that aids the scrutiny of local services.

### **Effective accountability for devolved authorities**

Local government is going through perhaps some of the biggest changes since the LGO was set up more than 40 years ago. The creation of combined authorities and an increase in the number of elected mayors will hugely affect the way local services are held to account. We have already started working with the early combined authorities to help develop principles for effective and accessible complaints systems.

We have also reviewed how we structure our casework teams to provide insight across the emerging combined authority structures. Responding to council feedback, this included reconfirming the Assistant Ombudsman responsible for relationship management with each authority, which we recently communicated to Link Officers through distribution of our manual for working with the LGO.

## **Supporting local scrutiny**

Our corporate strategy is based upon the twin pillars of remedying injustice and improving local public services. The numbers in our annual report demonstrate that we continue to improve the quality of our service in achieving swift redress.

To measure our progress against the objective to improve local services, in March we issued a survey to all councils. I was encouraged to find that 98% of respondents believed that our investigations have had an impact on improving local public services. I am confident that the continued publication of our decisions (alongside an improved facility to browse for them on our website), focus reports on key themes and the data in these annual review letters is helping the sector to learn from its mistakes and support better services for citizens.

The survey also demonstrated a significant proportion of councils are sharing the information we provide with elected members and scrutiny committees. I welcome this approach, and want to take this opportunity to encourage others to do so.

## **Complaint handling training**

We recently refreshed our Effective Complaint Handling courses for local authorities and introduced a new course for independent care providers. We trained over 700 people last year and feedback shows a 96% increase in the number of participants who felt confident in dealing with complaints following the course. To find out more, visit [www.lgo.org.uk/training](http://www.lgo.org.uk/training).

## **Ombudsman reform**

You will no doubt be aware that the government has announced the intention to produce draft legislation for the creation of a single ombudsman for public services in England. This is something we support, as it will provide the public with a clearer route to redress in an increasingly complex environment of public service delivery.

We will continue to support government in the realisation of the public service ombudsman, and are advising on the importance of maintaining our 40 years plus experience of working with local government and our understanding its unique accountability structures.

This will also be the last time I write with your annual review. My seven-year term of office as Local Government Ombudsman comes to an end in January 2017. The LGO has gone through extensive change since I took up post in 2010, becoming a much leaner and more focused organisation, and I am confident that it is well prepared for the challenges ahead.

Yours sincerely



Dr Jane Martin  
Local Government Ombudsman  
Chair, Commission for Local Administration in England

**Local Authority Report:** Southend-on-Sea Borough Council  
**For the Period Ending:** 31/03/2016

For further information on how to interpret our statistics, please visit our website:  
<http://www.lgo.org.uk/information-centre/reports/annual-review-reports/interpreting-local-authority-statistics>

## Complaints and enquiries received

Adult Care Services	Benefits and Tax	Corporate and Other Services	Education and Children's Services	Environment Services	Highways and Transport	Housing	Planning and Development	Other	Total
11	11	1	10	5	6	6	3	1	54

## 59 Decisions made

				Detailed Investigations			
Incomplete or Invalid	Advice Given	Referred back for Local Resolution	Closed After Initial Enquiries	Not Upheld	Upheld	Uphold Rate	Total
4	1	25	12	7	7	50%	56

### Notes

Our uphold rate is calculated in relation to the total number of detailed investigations.

The number of remedied complaints may not equal the number of upheld complaints. This is because, while we may uphold a complaint because we find fault, we may not always find grounds to say that fault caused injustice that ought to be remedied.

The compliance rate is the proportion of remedied complaints where our recommendations are believed to have been implemented.

### Complaints Remedied

by LGO	Satisfactorily by Authority before LGO Involvement	Compliance Rate
4	0	100%

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# Southend-on-Sea Borough Council

Agenda  
Item No.

7

Report of Corporate Director for Place

to  
Cabinet

on

20<sup>th</sup> September 2016

Report prepared by:

Amanda Rogers (Senior S106 & CIL Officer, Planning)

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**Corporate Policy for Planning: Interim Affordable Housing Policy**  
**Place Scrutiny Committee**  
**Executive Councillor: Councillor Flewitt**

***A Part 1 Public Agenda Item***

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## **1. Purpose of Report**

- 1.1 To seek Members agreement to the Interim Affordable Housing policy being adopted as Corporate Policy.
- 1.2 The policy sets out the approach the Council will take in respect of Affordable Housing Policy and financial contribution calculations, until such time that any amendments are made to adopted planning policies as part of the Local Plan review, which will be informed by updates to the Strategic Housing Market Assessment (SHMA). It is in direct response to recent Government changes to national housing policy.
- 1.3 For the avoidance of doubt, this Interim Affordable Housing Policy will supplement but not supersede the following existing adopted planning policies:- Core Strategy Policy CP8 and Development Management Development Plan Document (DPD) policy DM7.

## **2. Recommendations**

- 2.1 That Members agree the following:
  - 2.1.1 **Agree the “Interim Affordable Housing Policy (September 2016)” document attached at Appendix 2 as Corporate Policy; and**
  - 2.1.2 **Delegate to the Corporate Director for Place in connection with the Portfolio Holder for Housing, Planning and Public Protection authority to make any minor amendments to the Interim Affordable Housing Policy to take into account the latest available evidence from the SHMA or its equivalent successor.**

### 3. Background

- 3.1 The Council secures affordable housing through adopted planning policies:- Core Strategy Policy CP8 and Development Management Development Plan Document (DPD) policy DM7.
- 3.2 Recent changes in national housing policy dealing with how Registered Providers operate and how development viability is considered, has led to developers increasingly seeking to deviate from adopted affordable housing policy in terms of mix of tenure and unit sizes. This presents significant risks in terms of the Council's ability to meet the Borough's affordable housing needs. This Interim Affordable Housing Policy seeks to minimise these risks as far as possible and assist in meeting the Borough's affordable housing needs. It also seeks to ensure that a fair and consistent affordable housing provision is required of all developers, avoiding any site being disadvantaged in comparison to any other due to the impact that changes in tenure and unit sizes have on residential sales values.
- 3.3 The Council is experiencing unprecedented levels of housing demand as a result of the changes in national housing and planning policies, which combined with welfare reforms is making it increasingly difficult for a household on a low income to meet their own housing needs.
- 3.4 In addition, since the publication of the Core Strategy in 2007 the Council has not published any further guidance in relation to the preferred arrangements for obtaining a financial contribution to fund off-site provision where, exceptionally, the Council is satisfied that on-site provision of affordable housing is not practical, feasible or appropriate. This will be formally addressed in the Local Plan review of affordable housing policies; however, this is a lengthy and formal statutory process that is prescribed in the Town and Country Planning Regulations (2012), so in the interim it is deemed appropriate to adopt a Corporate Policy describing in more detail how the Council intends to apply the existing adopted policies.
- 3.5 To ensure that the approach to securing financial contributions in lieu of on-site provision of affordable housing is fair, consistent and comparable with other local authorities, a report by BNP Paribas Real Estate (a leading consultancy specialising in this area) were commissioned to review and prepare a report. This report assessed the pros and cons of different approaches and concluded that the Council's current adopted approach allows for a simple, transparent and quick calculation of a payment in lieu, which is one of the Council's objectives in avoiding full viability assessments on each occasion an application is considered. A copy of this report is included in **Appendix 1**.
- 3.6 **Appendix 2 and 3** include copies of the proposed Interim Affordable Housing Policy (September 2016) and an example of a payment calculation where a payment is being accepted in lieu of on-site provision of affordable housing. This is set out in a table at **Appendix 3**. This table will enable planning and housing officers to establish the level of financial contributions for affordable housing where considered appropriate.



- 3.7 The payments in lieu of affordable housing are significantly lower than the open market value (OMV) of the residential properties being constructed within a development. However, the contributions that will be secured are reasonable when considered in the context of the values involved in on-site provision. The reason for this is because when completed affordable housing units are secured by a Section 106 agreement for transfer to a Registered Provider (RP), typically the RP will purchase the units from a developer at a price equivalent to their OMV less approximately 30% to reflect the land value. For example, if the OMV of a residential unit is £200,000 each, a reasonable offer from a RP would be £140,000 each. This would result in the developer making a contribution of £60,000 per unit. Hence, whether the Council receives a financial contribution in lieu of on-site provision or units are provided on-site, the financial contribution towards affordable housing secured from the developer is comparable. It is then proposed that 10% be added to the contribution to reflect the on-costs associated with the local authority delivering the affordable housing instead of the developer. Without the 10%, some of the affordable housing financial contribution would be lost to fees and acquisition costs.

#### **4. Options**

- 4.1 For the reasons set out in this report, officers recommend adoption of the Interim Affordable Housing Policy.
- 4.2 The only alternative option would be to not adopt the Interim Affordable Housing Policy as set out in this report. This would mean to continue developer affordable housing negotiations relating to housing mix, tenure and financial contributions in lieu of on-site provision on a more ad hoc basis. This presents significant risks in terms of the Council's ability to meet the Borough's affordable housing needs; may result in inconsistencies in planning negotiations; and may also delay delivery of development.

#### **5. Reasons for Recommendation**

- 5.1 The proposed Corporate Policy is intended to maximise the potential to deliver affordable housing and meet the Borough's local affordable housing need, through existing planning policy, planning applications and Section 106 agreements, whilst ensuring development remains deliverable.
- 5.2 The proposed Interim Affordable Housing Policy also seeks to ensure that a fair and consistent affordable housing provision is required of all developers, avoiding any site being disadvantaged in comparison to any other due to the impact changes in tenure and unit sizes have on residential sales values.
- 5.3 The proposed Corporate Policy will assist in meeting the key Corporate Priorities relating to affordable housing as set out in paragraph 6.1 below.

#### **6. Corporate Implications**

- 6.1 Contribution to Council's Vision & Corporate Priorities

Any financial contributions received through S106 agreements will be spent on enabling the Council to deliver affordable housing within the Borough. As such, this will support the following Corporate Priorities for 2016/17:-

*Prosperous: Maximise opportunities to enable the planning and development of quality, affordable housing.*

*Healthy: Work with the public and private rented sectors to provide good quality housing.*

## 6.2 Financial Implications

This policy will enable the Council to secure financial contributions in lieu of onsite provision and such financial contributions will support the delivery of affordable housing. The policy will enable a consistent and clear approach to be applied to the negotiation of financial contributions.

## 6.3 Legal Implications

In terms of planning legislation, a Corporate Policy will be given less weight than an adopted planning policy. However, a Corporate Policy will constitute a material consideration in the determination of any planning applications; and until such time that the affordable housing planning policies are formally reviewed as part of the Local Plan review; the Interim Affordable Housing Policy will provide clarity and consistency to developers. The Corporate Policy will also help maximise opportunities for delivering affordable housing.

## 6.4 People Implications

None.

## 6.5 Property Implications

None.

## 6.6 Consultation

None required.

## 6.7 Equalities and Diversity Implications

The proposed Corporate Policy seeks to ensure an equitable approach to affordable housing negotiations and delivery.

## 6.8 Risk Assessment

There is a risk that ad hoc negotiations on a site by site basis will result in less affordable housing being delivered in the Borough; and claims of the Council acting inconsistently.

## 6.9 Value for Money

The proposed Corporate Policy is an important means of ensuring value for money for the wider community from development.

## 6.10 Community Safety Implications

None.

## 6.11 Environmental Impact

None.

## 7. Background Papers/Reference Documents

Development Planning Document: Core Strategy December 2007

Development Planning Document: Development Management Document July 2015

## 8. Appendices

**Appendix 1** Report by BNP Paribas Real Estate

**Appendix 2** Interim Affordable Housing Policy (September 2016)

**Appendix 3** Example affordable housing in lieu payment calculation  
(calculator spread sheet)

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## **Commuted Sum In Lieu Of On-Site Affordable Housing Formula Study**

Prepared for  
Southend-on-Sea Borough Council

October 2015

## Contents

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# 1 Introduction and Terms of Reference

BNP Paribas Real Estate has been commissioned by Southend-on-Sea Borough Council ('the Council') to provide advice on potential approaches to securing payments in lieu of on-site affordable housing.

We set out below the Council's brief in respect of the requirements of this commission:

- An exploration of the right approach for the authority to affordable housing (AH) commuted sum calculations when on-site provision is deemed unfeasible;
- In practical terms, if developers are not claiming viability issues and are happy to pay a commuted sum in lieu of the policy compliant number of AH units, then we need a robust, simple and transparent formula to apply. Whilst it is more accurate to assess each site individually based on a viability appraisal we feel it would be a bit unfair to require an appraisal if the proportion of AH is not being disputed as our policy allows for a commuted sum in lieu of on-site provision;
- Considering Core Strategy Policy CP8 and the fact that it allows for commuted sums to be sought on schemes of any size we would like a formula that could be applied above and below the 10 unit AH threshold;
- Provide 2/3 different commuted sum formula options to consider.

This report evaluates the current approach adopted by the Council in addition to common approaches to securing payments in lieu adopted by other authorities. This report considers the benefits and issues associated with each approach and their subsequent suitability to the Council given their local circumstances.

## 1.1 BNP Paribas Real Estate

BNP Paribas Real Estate is a leading firm of chartered surveyors, town planning and international property consultants. The practice offers an integrated service from nine offices in eight cities within the United Kingdom and 150 offices, across 30 countries in Europe, Middle East, India and the US, including 15 wholly owned and 15 alliances.

BNP Paribas Real Estate has a wide ranging client base, acting for international companies and individuals, banks and financial institutions, private companies, public sector corporations, government departments, local authorities and registered social landlords.

The full range of property services includes:

- Planning and development consultancy;
- Affordable housing consultancy;
- Valuation and real estate appraisal;
- Property investment;
- Agency and Brokerage;
- Property management;
- Building and project consultancy; and
- Corporate real estate consultancy.

This report has been prepared by James Purvis MRICS under the supervision of Anthony Lee MRICS MRTPI, RICS Registered Valuer.

The Affordable Housing Consultancy of BNP Paribas Real Estate advises landowners, developers, local authorities and registered social landlords ('RSLs') on the provision of affordable housing.

In 2007 we were appointed by the GLA to review its Development Control Toolkit Model (commonly referred to as the 'Three Dragons' model). This review included testing the validity of the Three Dragons' approach to appraising the value of residential and mixed use developments; reviewing the variables used in the model; and advising on areas that required amendment in the re-worked toolkit. We were appointed again in 2012 by the GLA to review the Three Dragons model and our recommendations were carried forward to the 2014 version of the Toolkit.

Anthony Lee is a member of the RICS 'Experts in Planning Service' panel, which was established in March 2009 to support the Planning Inspectorate on major casework and local development plan work submitted for independent examination. He has assisted the inspectors examining the economic viability of housing policies within the Core Strategies of Stockton Borough Council; Hinckley and Bosworth Council; and East North Hants District Council. He was also a member of the working group which drafted guidance for planning authorities on viability, which was published by the Local Housing Delivery Group in June 2012 as 'Viability Testing Local Plans: Advice to Planning Practitioners'.

In addition, we were retained by the Homes and Communities Agency ('HCA') to advise on better management of procurement of affordable housing through planning obligations.

The firm therefore has extensive experience of advising landowners, developers, local authorities and RPs on the value of affordable housing and economically and socially sustainable residential developments.

## 1.2 Report Structure

This report is structured as follows:

- **Section two** evaluates the Council's current approach to securing payments in addition to approaches adopted by other local authorities;
- **Section three** considers how these approaches might be adopted in the Lambeth context; and
- **Section four** sets out our conclusions and recommendations.

## 1.3 Disclaimer

This report contains several appraisals of hypothetical development scenarios. These appraisals do not constitute valuations in accordance with PS 1.6 of the RICS Valuation – Professional Standards (January 2014 Edition) (the 'Red Book'), the provisions of VPS 1 to VPS 4 are not of mandatory application and accordingly this report should not be relied upon as a Red Book valuation. This report is addressed to Southend-on-Sea Borough Council only and its contents should not be reproduced in part or in full without our prior consent.



## 2 Approaches to securing payments in lieu

### 2.1 Background and Policy Position

Under section 106 of the Town and Country Planning Act 1990 (S106), the council can, in appropriate circumstances seek a legal agreement as part of a planning permission for a development. The Local Planning Authority must ensure that a S106 obligation meets the relevant tests set out in the Community Infrastructure Levy Regulations 2010 (CIL).

It must be satisfied that the obligation is necessary to make the development is:

- Acceptable in planning terms;
- Directly related to the development and fairly and
- Reasonably related in scale and kind.

Requirements may vary depending upon site specific conditions, but could include making a contribution to open space, affordable housing etc. This can either be physical provision on site or a commuted sum in lieu of on-site provision.

Government guidance on affordable housing commuted sums is set out in the National Planning Policy Framework ('NPPF'). The NPPF strongly recommends that affordable housing contributions be satisfied by building the homes on site, however, when there is robust justification, off-site provision in the form of a commuted sum may be acceptable. Any financial contribution should be of 'broadly equivalent value'.

The Council recognises the practical difficulties of securing affordable housing on-site on smaller schemes and therefore accepts that a payment in lieu might be preferable in many cases. The ability of schemes to make financial contributions in-lieu inevitably varies between sites and areas. It is therefore unlikely to be possible to arrive at a common formula that can be applied to all sites and there may therefore be a need to assess the level of financial contributions on a site by site basis.

Whilst it is recognised to be more accurate to assess each site individually based on a viability appraisal. The Council is of the opinion that a full appraisal would be onerous in the event that the proportion of AH is not being disputed as the Council's policies allow for a commuted sum payment in lieu of on-site provision. In practical terms, if developers are not claiming viability issues and are happy to pay a commuted sum in lieu of the policy compliant number of affordable housing units, the Council require a suitable formula.

The challenge for the Council is therefore to develop an approach that can be relatively simple and time efficient way of determining an appropriate payment in lieu of on-site affordable housing.

### 2.2 The Council's current approach

In this section we consider the current approach adopted by the Council which is has also been adopted by Mole Valley.

The Council through their SPD (2007) adopt formula based contribution for schemes of between 1 and 9 units. The Council has indicated that it may also use the formula in exceptional circumstances when sites of 10 or more units are to provide a payment in lieu. The current formula has also been amended to

reflect the National Space Standards which outlines required space standards for affordable housing units.

The formula is summarised as follows:

- The market value of each residential unit in the development is determined (by reference to comparable evidence);
- The value per square metre is calculated by dividing the total value by the Unit's floor area;
- The market value (rate per sq/m) of a market housing unit is applied to an equivalent sized affordable housing unit. If for example, a four bed unit is 173 square metres and an equivalent affordable 4 bed unit is 100 square metres, the market value on a per square metre basis would be applied to a 100 square metre unit;
- A 'residual value' or 'plot value' is determined by taking 30% of the 'market value' of an affordable-sized unit and adding 10% for acquisition fees. (30% is a broad 'rule of thumb' for land value as a percentage of Gross Development Value (GDV));
- The Council's policy requires 20% affordable, so the payment in lieu is based on 20% of the resulting 'plot value' figure applied across the scheme.

We set out below an example of how this formula works in practice.

#### Example of Formula

1 x 173m<sup>2</sup> (GIA) 4-bed house with reference to comparable evidence has a market value of £495,000

Guide size for a suitable affordable home – 130 m<sup>2</sup> (GIA).

**Step 1:** Open market value (OMV) of a relevant or comparative property divided by the size of the property and multiplied by the appropriate affordable housing size that would have been required on site.

$$£495,000 / 173 \text{ m}^2 = £2,861 \text{ per m}^2$$

$$£2,861 \text{ per m}^2 \times 100\text{m}^2 = £371,930$$

**Step 2:** Multiply the OMV (completed sale value, or GDV) by the residual land value percentage (30%)

$$£371,930 \times 30\% = £111,579 \text{ (base land / plot value)}$$

**Step 3:** Add 10% to the step 2 result to reflect site acquisition costs (this gives the per unit sum for that property type)

$$£111,579 + 10\% = £122,737$$

**Step 4:** Apply to the relevant number of units and affordable housing policy requirement (i.e. 20%)

$$£122,737 \times 20\% = \text{Payment in Lieu of } £24,547$$

We set out as appendix 1 a further worked example of this approach.

### 2.2.1 Evaluation of the Council's current approach

This is a fairly simple approach and lends itself well to an area with fairly homogenous developments. Key issues for the Council to consider if they were to continue to adopt this approach are set out in the following paragraphs.

**Issue 1:** Step 2 involves multiplying the GDV of the unit (adjusted for size) by 30% to arrive at a 'land value' or 'plot value'. An addition of 10% is then made for land acquisition costs.

The difficulty with this approach is that the 30% used to arrive at a land or plot value is a broad rule of thumb and may not be at all reflective of individual site circumstances. Given the range of locations within the borough it is unlikely that it would be possible to arrive at a percentage to reflect land value that would reflect all developments, even at a very high level. However, we have been informed by the Council that their recent experience has found this percentage to be reasonably accurate.

We would also question whether the addition of 10% to the land value to cover site acquisition costs is an appropriate method of calculation. It should be noted that if acquisition costs were reduced this would reduce the potential payment in lieu. However, if the Council has evidence to support the acquisition costs this would mitigate this issue.

**Issue 2:** If a plot size for a typical home in the area would allow for a home of 173 square metres (as per the example) but the calculations are based on a property of only 100 square metres, it is questionable as to whether the commuted sum will be sufficiently large to purchase land on an alternative site.

We consider that the developer should be in no better position in providing a payment in lieu of actually providing the unit on site as this would incentivise the developer to seek off-site payments.

**Issue 3:** The most significant issue for the application of this approach is that the model assumes that affordable housing is always self-financing. The model generates the ability for the Council to provide clean and serviced land to RPs, but the costs of building might exceed the capitalised rental income.

**Issue 4:** We would question whether it is sufficiently flexible insofar that the approach complies with the requirements of CIL Regulations 122. Although the approach is formulaic, there is clearly scope for adjustment in Step 3. If a particular scheme could not viably meet a payment based on 20% affordable housing, the percentage could be adjusted downwards. This would need to be supported by a viability appraisal. This viability appraisal would need to be undertaken as a separate exercise.

## 2.3 Alternative approaches adopted by other Councils

In this section, we consider the alternative approaches adopted by other Councils.

### 2.3.1 Basingstoke & Deane Borough Council

Basingstoke's methodology is set out in appendix 2 of the 'Draft Planning Obligations SPD' published in November 2014. We set out below the following extract from the document

*"Where the council agrees to accept a commuted sum towards affordable housing instead of either on-site or off-site affordable housing provision, the value of that financial contribution should be equivalent to the cost of providing 40% affordable housing on-site (i.e. the financial contribution will be the value of*

*private subsidy that the applicant, land-owner or developer would have been required to make, had the affordable housing been provided on-site).*

*The method that will be used to calculate the level of the commuted sum for affordable housing requires is very closely related to the method used in the main Community Infrastructure Levy evidence base work. It is a residual value approach sharing a viability methodology and development appraisal assumptions and is reliant on the same market evidence base.*

*The following inputs will be used to calculate the cost of the financial contributions towards off-site affordable housing provision:*

- *Development scenarios (number of dwellings, dwelling mix, site area, site coverage and density);*
- *Level of affordable housing;*
- *Affordable housing rents and shared ownership costs;*
- *Market values;*
- *Build costs;*
- *Development costs (including professional fees, finance costs, marketing, developers return and contractors return);*
- *S106 costs;*
- *Appropriate CIL charge; and*
- *Cost to registered provider of acquiring affordable housing from developer.*

*The values and costs used will reflect those used in the Basingstoke and Deane Viability Study (November 2013) and the Manydown and Golf Course Viability Report (July 2014). They will be updated over time.*

*The output will be expressed in terms of a cost per square metre. This cost will then be multiplied by the total gross internal floor area of all net additional dwellings to establish the total cost of the financial contribution towards affordable housing.”*

### **2.3.2 Evaluation of Basingstoke’s approach**

This approach relies upon the Council’s 2013 viability study and 2014 viability studies for 2 large strategic sites. From a high level perspective, this approach is simple and straight forward for the Council and/or developer to calculate. It provides a degree of transparency insofar that the Council and developer have full knowledge of the inputs that will be used to calculate and as a result there is likely to be minimal disagreement on the inputs. However, there are flaws with this approach, which we set out below.

If this approach was to be considered for future use by the Council the viability studies that inform the Council’s CIL charging schedule may be used as a basis for calculating such payments in lieu.

**Issue 1:** The approach relies upon inputs from historic viability studies and as a result it is unlikely that the appraisal inputs (i.e. costs and values) will be valid/relevant due to changes in the market. For example, the use of this data will not capture movement’s in house prices or cost inflation from the date of the viability studies to the date of the calculation of the payment in lieu. One method through which to address this flaw would be to update values on a regular basis or more simply to increase/decrease housing values in line with the land registry house price index or tender price inflation set out on the BCIS website.

The affordable housing values would also be set as a specific rate per sq/ft dependent upon tenure type the values would be unresponsive to changes in Government policy.

**Issue 2:** The viability studies provide a high level perspective of viability and appraisal inputs across a wide geographical area. This approach is unable to capture the value or cost implications associated with specific sites and emerging development proposals.

We consider that this approach would not suit Southend due to the diverse range of values that are currently being achieved within the Borough. For example, if apartments in Leigh-on-Sea were banded there would be complications with ascertaining appropriate values as values are extremely sensitive to location, height, views etc. As a result, developers could potentially gain benefit from paying a commuted sum that is artificially low in comparison to the market values they would be achieving for the units.

Notwithstanding the above issues, the approach fails to provide a means to calculate a payment in lieu on the basis of present day inputs that reflect the subject site.

### **2.3.3 London Borough of Richmond-Upon-Thames**

This approach attempts to directly tackle the question of compliance with Regulation 122 by adopting an 'opportunity cost' approach (i.e. calculating the cost to the developer, in terms of value that would have been forgone had the affordable housing been provided on site). Under this approach, the developer is no better (and no worse off) than they would have been had the affordable housing been provided on site.

The formula is calculated by using a simple Microsoft Excel spreadsheet. This calculates the benefit accruing to the developer of providing units that would otherwise have been affordable as private housing.

The commuted sum is calculated as follows:

**A** = Market Value of unit LESS profit (profit does not apply to affordable housing);

**B** = Value of affordable housing (capitalised net rent for rented units plus capitalised rent and equity sales for shared ownership units);

**A – B** = payment in lieu (equivalent to the 'opportunity cost' or value that would have been lost, had the affordable units been provided on site).

An example of the calculation is provided at Appendix 2.

### **2.3.4 Evaluation**

The Richmond approach is superior to the Council's current adopted approach, as the calculation reflects individual site circumstances and does not rely upon the broad rule of thumb 30% of GDV calculation. It provides a reasonably accurate reflection of the value uplift enjoyed by the developer resulting from the replacement of on-site affordable units as private.

The model addresses compliance with CIL regulation 122 by enabling the user to select the affordable housing percentage upon which the payment in lieu is to be calculated. If the developer has demonstrated that the scheme is only viable with a reduced quantum of affordable housing (or financial equivalent of), then the payment in lieu can be based on that agreed quantum.

The spreadsheet model is easy to replicate and amend so that it is suitable for use in Southend. However, there are several issues with the spreadsheet model that would need to be addressed if it were to be used by the Council in order to meet their requirements. These issues are outlined below. None of the issues

identified are sufficiently significant to warrant abandoning the approach altogether.

**Issue 1:** The information required to complete the model is somewhat onerous and could be simplified. In particular, determining the price a RP might pay for the units could be simplified by seeking a firm price. This would then reduce the need to determine weekly rent levels, management costs and yields. This would also help to address some of the other issues below.

**Issue 2:** The current calculations make no allowances for voids and bad debts, which has the effect of slightly over-valuing the affordable housing value. This could be addressed through an addition to the management costs, although it should ideally be entered separately to aid comparison of inputs.

**Issue 3:** RPs typically pay the developer the agreed purchase price during the build period. Having affordable housing on-site therefore provides a cash flow benefit, despite the reduction in value compared to private housing. However, this is unlikely to be so significant that the calculated sums are inaccurate. Arguably, there is potentially an uplift in value in the private housing values which is also not accounted for in the model, so the two factors may well balance each other out.

**Issue 4:** The model calculates the capital value of the affordable housing, but makes no account for the RPs deduction for on-costs (i.e. acquisition costs and employer's agent). On-costs are typically between 5% to 9% of value. The lack of a deduction for on-costs incorrectly enhances the affordable housing value, which in turn reduces the 'gap' between private and affordable values (and reduces the commuted sum).

**Issue 5:** Although full profit is deducted from private housing, there is no corresponding profit deducted from the affordable housing. It is widely recognised that developers typically apply a profit to both tenures, although at a considerably reduced rate to the affordable housing (circa 6%, compared to 20% on private).

**Issue 6:** A decision needs to be made as to the tenure assumptions on the 'rented' element used entered into the model. Clearly the decision as to which tenure would have been provided on-site has a profound impact on the commuted sum. For example, if the capital value of the affordable housing units is based on social rented tenure, the value will be considerably lower than would be the case if they were provided as affordable rent. The lower the affordable housing value, the higher the payment in lieu.

### 2.3.5 London Borough of Bromley

Bromley's approach is set out in the 'Addendum (June 2013) to the Council's Adopted Supplementary Planning Document Planning Obligations (2010)

Bromley's formula for calculating financial contributions in lieu of on-site affordable housing provision is as follows:

*"Difference between the open market value of the equivalent on-site affordable housing units and the maximum price that a Registered Provider (RP) would reasonably pay for those units, assuming nil grant (with limited adjustment reflecting potential cost variation for provision of units for private sale rather than affordable housing e.g. marketing costs)."*

In summary, the approach adopted by Bromley is a simplified version of the Richmond approach.

### 2.3.6 Evaluation of Bromley's approach

Whilst the approach is simplistic it relies upon the maximum price that a RP would be prepared to pay for the affordable housing units. However, in circumstances where a scheme is only required to provide a small number of units RPs do not tend to be interested in taking on a small number of units in a scheme due to management issues. As a result, the Council would need to have access to values for affordable units that can be applied to the formula to reflect a policy compliant tenure mix. In addition, the formula does not allow the ability to deduct profit.

### 2.3.7 London Borough of Wandsworth

Wandsworth Council's approach is essentially the same as Richmond's in that the Council indicates that where payments in lieu are to be agreed "*there can be no financial advantage to the developer in not delivering the affordable housing onsite*".

However, in contrast to Richmond, Wandsworth do not provide a specific formula to calculate the payment in lieu. Wandsworth's Planning Obligations SPD indicates that the Council will seek two appraisals from the developer. The first is to assume that the scheme incorporates the required percentage of affordable housing. The second assumes that the scheme is 100% private.

The payment in lieu is determined by deducting the residual land value generated by the second appraisal from the residual land value generated by the first.

As a principle for calculating a payment in lieu, the approach is identical to the approach adopted by Richmond. The only material difference between the two approaches is how the payment in lieu is calculated. The Wandsworth approach is arguably more onerous, as the developer is required to complete two appraisals (although in reality, the additional work required to turn an appraisal which includes some affordable housing into a 100% private housing scheme is relatively limited).

Completing two full appraisals also offers the advantage of addressing most of the modelling issues raised in relation to the Richmond model. The main advantage of the Wandsworth approach is that it can be used for dual purposes of (a) determining the overall level of affordable housing – if a policy compliant level is considered unviable and (b) determining the amount of a payment in lieu.

In terms of applying this approach in Southend, there may be instances where a subsequent appraisal of a scheme where affordable housing is not being contested may reveal that the scheme on a current day basis may be unviable and unable to support a payment in lieu.

Viability has been an issue in Southend in recent years and as a result this approach when applied specifically to Southend may result in a reduction in payments for off-site affordable housing.

## 2.4 Comparing the outcome of the approaches

We have tested a hypothetical 10 unit development to the approaches set out above (excluding Basingstoke) to provide an indication of the likely levels of payment in lieu that would be generated. For simplicity, we have assumed that all units in the scheme are two bed flats. We have also reflected a policy

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compliant level of affordable housing of 20% assuming a tenure mix split 70/30 between affordable rent and intermediate units.

### **Southend**

The current Southend approach generates a payment in lieu of £107,784. The calculations are attached as Appendix 1.

### **Basingstoke & Deane Borough Council**

We have not modelled this scenario due to the evidence base that informs the approach.

### **London Borough of Richmond-Upon-Thames**

The Richmond approach generates a payment in lieu of £144,502. The calculations are attached as Appendix 2. The higher payment in lieu in comparison to the current Southend approach reflects the cross-subsidy required from the private housing to the affordable housing. In contrast, the Southend approach assumes that the affordable housing is cost neutral (i.e. the price payable by the RP equals the development costs).

### **London Borough of Wandsworth**

The Wandsworth approach generates a payment in lieu of £96,586.

### **London Borough of Bromley**

We modelled this scenario assuming a blended capital value for the affordable units of £1,500 per sq/m. The payment in lieu generated is £129,500 and we attach a copy of this calculation as appendix 3.



### 3 Conclusion

In our view, payment in lieu structures should be tested against the three criteria, as follows:

- That the structure satisfies the tests contained within CIL Regulation 122;
- Ease of application to small schemes; and
- Provides a robust approach and is capable of reflecting the Council's policies and specific market conditions.

Basingstoke's approach is the least suitable approach and does not meet the criteria as the information base to be used for the calculation, although transparent, is unable to respond to market conditions or site specific factors that influence cost and value.

The Council's current approach is also unable to fully meet these three tests. Although it is a simple approach to apply, it is reliant on a percentage of GDV to arrive at a plot value. Whilst this approach could serve to distort the true value of the site, the Council have advised us that in their experience this method has been reasonably accurate.

We have previously discussed with the Council an approach that determines varying land values which would adopt different land values to reflect the three areas outlined in the Council's CIL Charging Schedule. However, there would be considerable difficulty in arriving at an appropriate method of adopting generic land values assessed for generic typologies and applying them to site specific scenarios. Furthermore, this approach would not allow for the payment in lieu to reflect movements in values of residential dwellings.

One positive of the current adopted methodology is that the payment in lieu will always be a function of current day sales as at the date of assessment and as a result as sales values increase the Council can capture higher payments in lieu.

The Richmond approach is based on the principle that replacing on site affordable housing provision with a payment in lieu should be financially neutral for the developer. In other words, the payment in lieu option should leave the developer no better, but no worse off. Consequently, the option meets the test of reasonableness in CIL Regulation 122; the approach is not punitive when considered alongside the Council's policies.

In terms of practical application, Bromley's approach is simple (comparing the value 100% market value of the units against what a RP is prepared to pay), but requires the developer to complete this exercise. However, there are potential issues for the Council to determine the value of the affordable units.

In summary, the Wandsworth approach is the most reliable and transparent way of ascertaining an appropriate payment in lieu formula in which two appraisals are modelled with 100% market housing and one with policy compliant affordable housing. The payment in lieu would therefore be the sum which is the difference between the land values.

However, we note from the brief that the Council feel it would be onerous to require a full appraisal from a developer if the level of affordable housing was not being disputed as the Council's policy allows for a commuted sum in lieu of on-site provision. We have also set out that if this approach was adopted it may serve to highlight that such schemes are unviable on a current day basis if a negative land value is generated and therefore a payment in lieu could not be justified.

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In summary, none of the approaches set out above sufficiently meet all of the Council's objectives and therefore the Council will need to accept a compromise position. In doing so, the Council will need to consider all of the pros and cons of each approach and choose the approach they consider will best fit their prioritised objectives

We highlight however that the Council's current adopted approach allows for a simple, transparent and quick calculation of a payment in lieu which is one of the Council's objectives in avoiding full viability assessments on each occasion.

The fact there are a wide range of approaches adopted by a number of Council's indicates that there is not a 'one size fits all' approach to ascertaining an appropriate formula that can tick every Council's requirements.

We would highlight that if the Council were to adopt an alternative approach it is likely that any approach would be subject to scrutiny and challenge from developers. We understand that the Richmond approach has in the past attracted opposition from developers and that there have been appeals in respect of the payment in lieu. However, Richmond's policy is subject to viability and we understand that appeals have focused upon agreeing the actual viability position of the scheme.

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## Appendix 1 - Southend-on-Sea Commuted Sum Formula

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**For each property type:**

**Step 1:** Market value (MV) of the relevant or comparative market property divided by the size of that property and multiplied by the affordable housing property size equivalent (to assess the market value of a suitably sized affordable home).

**Step 2:** Multiply by the residual land value percentage (30%) – to get to the base plot value for that home.

**Step 3:** Add 10% to the step 2 figure, to reflect site acquisition costs (this gives the per unit sum)

Then to get to the total contribution:

**Step 4:** Apply the resulting per unit sum(s) to the relevant site number and proportion (i.e. Step 3 per unit sum x number of dwellings in scheme x 20%).

**Calculation**

**Step 1:** Value of each property = £175,000

Size of each unit (private) 75 sqm = £2,333 per sqm

Size of equivalent affordable housing unit (70 sqm) = £163,310

**Step 2:** £163,310 x 30% = £48,993

**Step 3:** £48,993 + 10% = £53,892

**Step 4:** 10 units @ £53,892 each x 20% = £107,784

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## Appendix 2 - Richmond-upon-Thames Commuted Sum Formula



**LONDON BOROUGH OF RICHMOND UPON THAMES  
AFFORDABLE HOUSING SPD - ANNEXE A - COMMITTED SUM CALCULATION**

Site Name	Example	Date	Notes
Number of Units on proposed development	10 No.		
Level of Affordable Housing required	20% Per DMH 06		See SPD Para 2.8.3
Number of Affordable Units required	2.00 No.		
Percentage Affordable Rented required	70%		
Number of Affordable Rented Units required	1.40 No.		
Percentage Intermediate required	30%		
Number of Intermediate units required	0.60 No.		
<b>Less on Site provision</b>			
Affordable Rented Units provided on site	0 No.		
Net number of units of Affordable Rented off-site	1.40 No.		
Intermediate Units provided on site	0 No.		
Net number of Intermediate units off-site	0.60 No.		

**Off-Site Committed Sum calculation**

Unit type	Off Site Provision	OMV £	Profit 0.00%	Net Total Cost	Rent per week	Mgt Charge 25.00%	Yield 6.00%	Capitalised Rent	Committed Sum
1 Bed Flat			0	0		0	6.00%	0	0
2 Bed Flat	1.40	175,000	0	175,000	150	1,850	6.00%	97,500	108,500
3 Bed Flat			0	0		0	6.00%	0	0
3 Bed Hse			0	0		0	6.00%	0	0
4 Bed Hse			0	0		0	6.00%	0	0
5 Bed Hse			0	0		0	6.00%	0	0
<b>Total</b>	<b>1.4</b>								<b>108,500</b>

**Intermediate - Shared Ownership**

Unit type	Off Site Provision	OMV £	Profit 0.00%	Net Total Cost	Equity Rent 2.75%	Mgt Charge 0.50%	Yield 6.00%	Capitalised Rent	1st Tranche Committed Sum
1 Bed Flat			0	0	0	0	6.00%	0	0
2 Bed Flat			0	0	0	0	6.00%	0	0
3 Bed Flat	0.60	175,000	0	175,000	2,888	188	6.00%	44,997	70,000
3 Bed Hse			0	0	0	0	6.00%	0	0
4 Bed Hse			0	0	0	0	6.00%	0	0
5 Bed Hse			0	0	0	0	6.00%	0	0
<b>Total</b>	<b>0.6</b>								<b>36,002</b>

Total Units	2.00	TOTAL Committed Sum	144,502
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## Appendix 3 - Bromley's Commuted Sum Formula

Bromley's formula for calculating financial contributions in lieu of on-site affordable housing provision is as follows:

*“Difference between the open market value of the equivalent on-site affordable housing units and the maximum price that a Registered Provider (RP) would reasonably pay for those units, assuming nil grant (with limited adjustment reflecting potential cost variation for provision of units for private sale rather than affordable housing e.g. marketing costs).”*

### **Calculation**

#### **Step 1 – Market Value of Units**

Market Value of 2 bed units (75 sq/m) is £175,000 (£2,333 per sq/m)

10 units x £175,000 each = £1,750,000

Less 3% for sales and marketing fees = £1,697,500

#### **Step 2 – Scheme with 20% Affordable Housing**

##### **Affordable Values**

Policy compliant affordable housing is 20%. Affordable housing on a 10 unit scheme would be 2 units.

2 x 2 bed units = floor area of 140 sq/m

140 sq m x value of units of £1,500 per sq/m = £210,000

##### **Market Housing Values**

8 units x £175,000 each = £1,400,000

Less 3% for sales and marketing fees = £1,358,000

**Value of scheme with 20% affordable Housing = £1,568,000**

#### **Step 3 -**

##### **Deduct scheme with affordable from scheme with 100% market housing**

£1,697,500 - £1,568,000 = £129,500 payment in lieu



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## Appendix 4 - Wandsworths Approach

## Appraisal Model assuming 100% Market Housing

### APPRAISAL SUMMARY

**BNP PARIBAS REAL ESTATE**
**Wandsworth - Payment in Lieu example  
100% Market Housing Model**
**Summary Appraisal for Merged Phases 1 2**

Currency in £

**REVENUE**

Sales Valuation	Units	m <sup>2</sup>	Rate m <sup>2</sup>	Unit Price	Gross Sales
2 Bed Flats	10	750.00	2,333.00	174,975	1,749,750

**Rental Area Summary**

	Units	Initial MRV/Unit	Net Rent at Sale	Initial MRV
Ground Rents	10	200	2,000	2,000
<b>Totals</b>	<b>10</b>		<b>2,000</b>	<b>2,000</b>

**Investment Valuation**

Ground Rents					
Current Rent	2,000	YP @	5.5000%	18.1818	36,364
					<b>36,364</b>

**GROSS DEVELOPMENT VALUE**
**1,786,114**

Purchaser's Costs	5.80%	(2,109)	(2,109)
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**NET DEVELOPMENT VALUE**
**1,784,005**
**NET REALISATION**
**1,784,005**
**OUTLAY**
**ACQUISITION COSTS**

Residualised Price			154,644
Stamp Duty	3.00%		4,639
Agent Fee	1.00%		1,546
Legal Fee	0.80%		1,237
			<b>162,067</b>

**CONSTRUCTION COSTS**

Construction	m <sup>2</sup>	Rate m <sup>2</sup>	Cost
2 Bed Flats	882.35 m <sup>2</sup>	1,150.00 pm <sup>2</sup>	1,014,706
Contingency		5.00%	50,735
CIL			26,471
			<b>1,091,912</b>

**PROFESSIONAL FEES**

Architect	6.00%	60,882	60,882
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**MARKETING & LETTING**

Marketing	1.50%	26,246	26,246
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**DISPOSAL FEES**

Sales Agent Fee	1.50%	26,760	
Sales Legal Fee	0.50%	8,920	
			<b>35,680</b>

**FINANCE**

Debit Rate 7.000% Credit Rate 0.000% (Nominal)			
Land			13,673
Construction			36,322
Total Finance Cost			<b>49,994</b>

**TOTAL COSTS**
**1,426,782**
**PROFIT**
**357,223**
**Performance Measures**

Profit on Cost%	25.04%
Profit on GDV%	20.00%
Profit on NDV%	20.02%
Development Yield% (on Rent)	0.14%
Equivalent Yield% (Nominal)	5.50%
Equivalent Yield% (True)	5.69%

### Appraisal Model with 2 affordable housing units

<b>APPRAISAL SUMMARY</b>		<b>BNP PARIBAS REAL ESTATE</b>			
<b>Wandsworth - Payment in Lieu example Affordable Housing Model</b>					
Summary Appraisal for Merged Phases 1 2					
Currency in £					
<b>REVENUE</b>					
<b>Sales Valuation</b>	<b>Units</b>	<b>m<sup>2</sup></b>	<b>Rate m<sup>2</sup></b>	<b>Unit Price</b>	<b>Gross Sales</b>
2 Bed Flats Affordable	2	150.00	1,500.00	112,500	225,000
2 Bed Flats	8	600.00	2,333.00	174,975	1,399,800
<b>Totals</b>	<b>10</b>	<b>750.00</b>			<b>1,624,800</b>
<b>Rental Area Summary</b>					
	<b>Units</b>	<b>Initial MRV/Unit</b>	<b>Net Rent at Sale</b>	<b>Initial MRV</b>	
Ground Rents	2	200	400	400	
<b>Investment Valuation</b>					
Ground Rents					
Current Rent	400	YP @	5.5000%	18.1818	7,273
<b>GROSS DEVELOPMENT VALUE</b>				<b>1,632,073</b>	
Purchaser's Costs		5.80%	(422)	(422)	
<b>NET DEVELOPMENT VALUE</b>				<b>1,631,651</b>	
<b>NET REALISATION</b>				<b>1,631,651</b>	
<b>OUTLAY</b>					
<b>ACQUISITION COSTS</b>					
Residualised Price			58,118		
Stamp Duty		3.00%	3,146		
Agent Fee		1.00%	1,049		
Legal Fee		0.80%	839		
				63,152	
<b>CONSTRUCTION COSTS</b>					
<b>Construction</b>	<b>m<sup>2</sup></b>	<b>Rate m<sup>2</sup></b>	<b>Cost</b>		
2 Bed Flats Affordable	176.47 m <sup>2</sup>	1,150.00 pm <sup>2</sup>	202,941		
2 Bed Flats	705.88 m <sup>2</sup>	1,150.00 pm <sup>2</sup>	811,765		
<b>Totals</b>	<b>882.35 m<sup>2</sup></b>		<b>1,014,706</b>	<b>1,014,706</b>	
Contingency		5.00%	50,735		
CIL			26,471		
				77,206	
<b>PROFESSIONAL FEES</b>					
Architect		6.00%	60,882		
				60,882	
<b>MARKETING &amp; LETTING</b>					
Marketing		1.50%	20,997		
				20,997	
<b>DISPOSAL FEES</b>					
Sales Agent Fee		1.50%	21,100		
Sales Legal Fee		0.50%	8,158		
				29,258	
<b>FINANCE</b>					
Debit Rate 7.000% Credit Rate 0.000% (Nominal)					
Total Finance Cost					33,245
<b>TOTAL COSTS</b>				<b>1,299,446</b>	
<b>PROFIT</b>				<b>332,205</b>	
<b>Performance Measures</b>					
Profit on Cost%		25.57%			
Profit on GDV%		20.35%			
Profit on NDV%		20.36%			
Development Yield% (on Rent)		0.03%			
Equivalent Yield% (Nominal)		5.50%			

**Payment in lieu calculation =**

Residual Land Value of 100% Market Housing scheme = £154,644

Less

Residual Land Value of scheme with 20% affordable housing = £58,118

**= Payment in Lieu of £96,256**



# **Interim Affordable Housing Policy**

September 2016

## 1. Introduction

- 1.1 For the avoidance of doubt, this **Interim Affordable Housing Policy** will supplement and not supersede the following existing adopted planning policies: - Core Strategy Policy CP8 and Development Management Development Plan Document (DPD) policy DM7.
- 1.2 This represents the Corporate Policy position as of September 2016, and sets out the approach the Council will take until further notice in respect of affordable housing policy and financial contribution calculations. The position set out in this document will apply until such time that any amendments are made to adopted planning policies as part of the Local Plan review, which will be informed by any updates that are made to the Strategic Housing Market Assessment (SHMA).
- 1.3 This Corporate Policy is intended to maximise opportunities for the Council to meet the Borough's local affordable housing need, whilst ensuring development remains viable and deliverable.
- 1.4 In accordance with National Planning Practice Guidance, contributions to affordable housing will not be sought from developments of 10-units or less, and which have a maximum combined gross floor space of no more than 1000sqm. Therefore, this Interim Affordable Housing Policy only applies to schemes above these thresholds.
- 1.5 This document should be read in conjunction with using the "Example AH in lieu payment calculation Jan 2016" calculator spread sheet.

## 2. Planning Policy Background

- 2.1 Core Strategy Policy CP8 states that the Council will *"enter into negotiations with developers to ensure that a. all residential proposals of 10-49 dwellings or 0.3 hectares up to 1.99 hectares make an affordable housing or key worker provision of not less than 20% of the total number of units on site; and b. all residential proposals of 50 dwellings or 2 hectares or more make an affordable housing or key works provision of not less than 30% of the total number of units on the site"*. The policy goes on to state that *"For sites where, exceptionally, the Borough Council is satisfied that on-site provision is not practical, they will negotiate with developers to obtain a financial contribution to fund off-site provision. The Council will ensure that any such sums are used to help address any shortfall in affordable housing."*
- 2.2 Development Management Development Plan Document (DPD) policy DM7 sets out the dwelling mix, size and type of housing, both market and affordable, that is then required. This is based on the latest Strategic Housing Market Assessment (SHMA) information relating to housing need in the Borough, which was published at the time the DPD was adopted in July 2015 and may be subject to updates.

### 3. Application of policy CP8 and DM7

- 3.1 Unless it can be demonstrated that there are exceptional circumstances, policy CP8 and DM7 will be consistently applied, to ensure that an appropriate mix of dwellings are provided across the Borough. This will also ensure a fair and consistent affordable housing provision is required of all developers, avoiding any site being disadvantaged in comparison to any other due to the impact on sales values.
- 3.2 On-site affordable housing provision should include what is practical to comply with these policies. For example, it is clearly not possible to provide 0.4 of a dwelling; and it may also be impractical in some instances to provide large units on a site (although this would need to be justified). Therefore, in applying policy CP8 developers will be given the option of either rounding up the affordable housing number, and providing an additional affordable housing unit on site, or providing any proportion above a whole number (up to the 20/30% affordable housing requirement applicable) as an equivalent financial contribution. This approach is consistent with policy CP8, which states that a **minimum** of 20/30% affordable housing is required, and it is therefore inappropriate to round down if 0.49 or less. For example, if policy CP8 requires 6.4 units then 6 units could be provided on site (consisting of a mix to comply with policy DM7) plus 0.4% equivalent financial contribution; or 7 units could be provided (consisting of a mix to comply with policy DM7). And if, for example, it can clearly be demonstrated that units of a particular size are not practical to meet the requirements of policy DM7 (for reasons **not** including how the scheme has been designed or developer preference), then the units that cannot be provided could also be converted to a financial contribution.

### 4. Tenure Mix

- 4.1 The Council's current planning policy DM7 of the Development Management DPD states that a 60:40 tenure mix between social and/or affordable rented accommodation and intermediate housing respectively will be sought. This is based on the most recent SHMA evidence in respect of local affordable housing need, which may be subject to updates.
- 4.2 Due to changes in national housing policy and Government funding priorities developers and Housing Associations/Registered Providers consider it is no longer viable for them to provide rented accommodation and are only providing shared-ownership units. The Council is concerned that this will result in an over-provision of this tenure and a significant under-provision of affordable rented accommodation. There is no evidence at this point in time that there is greater than a policy compliant need for intermediate housing in the Borough, particularly if its supply threatens the Council's ability to meet the Borough's affordable rented housing need. Due to the impact on sales values, there is also the potential to unfairly disadvantage some sites when compared with others if tenure is varied on an ad hoc basis.
- 4.3 Taking into account the difficulties developers and Housing Associations/Registered Providers are encountering in delivering rented affordable housing, the Council's starting position is to invite developers to

consider retaining the policy compliant 40% intermediate accommodation and offering a financial contribution in lieu of on-site provision of the rented element (60%). If this is not feasible then 100% financial contribution will be considered. If this still remains unfeasible for financial viability reasons, then a Viability Assessment will be required to support any application to depart from an affordable housing policy compliant position.

## 5. Approach to Financial Contribution Calculations

### 5.1 Approach for new schemes and new Section 106 (S106) agreements

5.1.1 Depending on the number of affordable housing units required there may be a combination of on-site provision and financial contribution sought to reflect the dwelling and tenure mix set out in Development Management DPD policy DM7.

5.1.2 If a financial contribution is considered appropriate in lieu of on-site affordable housing provision, the following evidence/information will be required in support of any such request:

- a table of open market sales values for each of the residential units in a scheme together with comparable market evidence to support these values;
- details of the size of each of the residential units in a scheme;
- evidence in the form of correspondence from Registered Providers to demonstrate that there is no interest in the on-site affordable housing units in question (this must include explanations as to why the lack of interest to ensure terms of offer are reasonable).

5.1.3 If a financial contribution is sought, the methodology/formula is summarised as follows:

- The open market value (OMV) of **each** (i.e. all units including market and affordable) residential unit in the development is determined (by reference to comparable evidence);
- The value per square metre is calculated by dividing the total value by the unit's floor area;
- The market value (rate per sq/m) of a market housing unit is applied to an equivalent sized affordable housing unit (based on National Technical Housing Standards March 2015). If for example, a three storey four bed unit is 173 square metres and an average three storey affordable 4 bed unit is 116.5 square metres (based on the National Technical Housing Standards), the market value on a per square metre basis would be applied to a 116.5 square metre unit;
- A 'residual value' or 'plot value' is determined by taking 30% of the 'market value' of an affordable-sized unit and adding 10% on-costs associated with the local authority delivering the affordable housing (e.g. for site acquisition costs). 30% is a broad 'rule of thumb' for land value as a percentage of Gross Development Value (GDV);



- The Council's Core Strategy policy CP8 requires 20% or 30% affordable housing depending on the size, so the payment in lieu is based on 20% or 30% of the resulting 'plot value' figure applied across the scheme.

5.1.4 See Appendix 1 for a working example showing how to apply the above formula.

## **5.2 Approach for previously approved schemes and existing S106 agreements**

5.2.1 In most instances the above approach will be most suitable, particularly if on-site affordable housing is deemed unfeasible from the outset. However, **if affordable housing units are already specified** and detailed in a completed S106 and an applicant is seeking to renegotiate to secure a financial contribution in lieu of the on-site provision, **apply Steps 1-3 outlined above only to the units allocated as affordable housing properties**. If this approach is adopted, only the affordable housing units should be listed in the calculator spread sheet (i.e. not every unit in the scheme), and Column I in the spread sheet should be ignored with the total of Column H being used instead.

## Appendix 1: Working example of formula

1 x 173m<sup>2</sup> (GIA) 4 bed house with reference to comparable evidence has a market value of £495,000

Guide size for a suitable 4 bed, 2 storey affordable home – 110.5 m<sup>2</sup> (GIA)

**Step 1:** Open market value (OMV) of a relevant or comparative property divided by the size of the property and multiplied by the appropriate affordable housing size that would have been required on site (based on National Technical Housing Standards as detailed in Appendix 2).

$$£495,000 / 173 \text{ m}^2 = £2,861 \text{ per m}^2$$

$$£2,861 \text{ per m}^2 \times 110.5\text{m}^2 = £316,140.50$$

**Step 2:** Multiply the OMV (completed sale value, or GDV) by the residual land value percentage (30%)

$$£316,140.50 \times 30\% = £94,842.15$$

**Step 3:** Add 10% to the step 2 result to reflect on-costs (this gives the per unit sum for that property type)

$$£94,842.15 \times 1.10 = £104,326.37$$

**Step 4:** Depending on whether affordable housing policy requirement is 20% or 30%, apply to each of the units within the scheme

$$\text{e.g. } £104,326.37 \times 0.20 = \text{Payment in Lieu of } £20,865 \text{ in relation to this unit}$$

**Step 5:**

Apply steps 1 to 4 to **each** unit in the scheme then add together for a total financial contribution in lieu of on-site provision of affordable housing (total of Column I in the calculator spread sheet).

## Appendix 2: Affordable Housing National Technical Housing Standards March 2015 (NTHS)

Size averages are to be used when entering NTHS figures in the calculator spread sheet e.g. 1 bed 1 storey =  $(39+50)/2 = 44.5\text{sqm}$ ; 2 bed 1 storey =  $65.5\text{sqm}$ ; 3 bed 1 storey =  $85\text{sqm}$ ; 1 bed 2 storey =  $58\text{sqm}$ ; 2 bed 2 storey =  $74.5\text{sqm}$ ; 3 bed 2 storey =  $93\text{sqm}$

Affordable Housing National Technical Housing Standards					
		1 storey dwelling	2 storey dwelling	3 storey dwelling	Built-in storage
Number of Bedrooms	Number of bed spaces (persons)	NSS	NSS	NSS	NSS
1 bedroom	1	39			1
	2	50	58		1.5
2 bedroom	3	61	70		2
	4	70	79		
3 bedroom	4	74	84	90	2.5
	5	86	93	99	
	6	95	102	108	
4 bedroom	5	90	97	103	3
	6	99	106	112	
	7	108	115	121	
	8	117	124	130	

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# Appendix 3

## Example affordable housing in lieu payment calculation (calculator spread sheet)

Column E comment – unless Strategic Housing have specified the no. of bed spaces for each of the affordable units, enter average sizes (see accompanying guidance for figures)

Column I comment – formula needs changing depending on % to be applied whether this be 30% for larger schemes in accordance with CP8 or a lower % to take into account what cannot be provided on-site up to the required proportion of 20/30%

A	B	C	D	E	F	G	H	I
<b>[SITE ADDRESS]</b>								
Plot No:	Floor	Bedroom	m2	AH Min (based on National Technical Housing Standards)	Sq ft	Open Market Value (OMV)	Residual land value percentage 30%	Payment in lieu (based on 20% AH)
1		2	68.00503	65.5	732	£150,000.00	£47,676.62	£9,535.32
2		2	69.95599	65.5	753	£159,000.00	£49,127.82	£9,825.56
3		1	48.9599	44.5	527	£125,000.00	£37,492.42	£7,498.48
4		2	68.00503	65.5	732	£152,500.00	£48,471.23	£9,694.25
5		2	68.00503	65.5	732	£152,500.00	£48,471.23	£9,694.25
6		2	52.0257	65.5	560	£142,500.00	£59,204.15	£11,840.83
7		2	57.04247	65.5	614	£150,000.00	£56,839.23	£11,367.85
8		2	68.00503	65.5	732	£153,000.00	£48,630.16	£9,726.03
9		2	69.95599	65.5	753	£160,000.00	£49,436.80	£9,887.36
10		1	48.9599	44.5	527	£123,500.00	£37,042.51	£7,408.50
11		2	68.00503	65.5	732	£162,000.00	£51,490.75	£10,298.15
12		2	68.00503	65.5	732	£162,000.00	£51,490.75	£10,298.15
13		2	52.0257	65.5	560	£144,000.00	£59,827.35	£11,965.47
14		2	57.04247	65.5	614	£152,000.00	£57,597.09	£11,519.42
15		2	68.00503	65.5	732	£152,000.00	£48,312.31	£9,662.46
16		2	69.95599	65.5	753	£162,000.00	£50,054.76	£10,010.95
17		1	48.9599	44.5	527	£128,000.00	£38,392.23	£7,678.45
18		2	68.00503	65.5	732	£162,500.00	£51,649.68	£10,329.94
19		2	68.00503	65.5	732	£162,500.00	£51,649.68	£10,329.94
20		2	52.0257	65.5	560	£146,500.00	£60,866.02	£12,173.20
21		2	57.04247	65.5	614	£153,500.00	£58,165.48	£11,633.10
22		2	58.99343	65.5	635	£160,000.00	£58,623.48	£11,724.70
23		2	65.03213	65.5	700	£165,000.00	£54,841.74	£10,968.35
24		1	44.96507	44.5	484	£129,000.00	£42,129.70	£8,425.94
25		2	57.04247	65.5	614	£167,000.00	£63,281.01	£12,656.20
26		1	45.987	44.5	495	£129,500.00	£41,353.15	£8,270.63
27		2	52.95473	65.5	570	£152,250.00	£62,145.22	£12,429.04
		<b>Totals</b>	<b>1620.972</b>		<b>17448</b>	<b>£4,057,750.00</b>	<b>£1,384,262.57</b>	<b>£276,852.51</b>

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# Southend-on-Sea Borough Council

Agenda  
Item No.

8

Report of Corporate Director for Place

to

Cabinet

on

20<sup>th</sup> September 2016

Report prepared by: Mark Sheppard and Matthew Thomas  
(Strategic Planning)

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**Southend Central Area Action Plan (SCAAP) – Proposed Submission Document**  
**Place Scrutiny Committee**  
**Executive Councillor: Councillor Flewitt**  
***A Part 1 Public Agenda Item***

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## **1. Purpose of Report**

- 1.1 To seek Member agreement to the Southend Central Area Action Plan (SCAAP) for consultation, prior to consideration at Full Council on 20<sup>th</sup> October 2016.
- 1.2 To agree response to previous consultation comments on earlier iterations of the SCAAP, as set out in the accompanying Consultation Statement.
- 1.3 To seek agreement to carry out statutory public consultation for a minimum of 6 weeks on a Publication version of the SCAAP.
- 1.4 To seek agreement, following consultation, to then prepare and submit the SCAAP to the Secretary of State for examination in public.

## **2. Recommendation**

- 2.1 **Approve the Proposed Submission version of the SCAAP (Appendix 1) and associated Policies Map (Appendix 2) to be published for public consultation under Regulation 19 of the Town and Country Planning (Local Planning) (England) Regulations 2012.**
- 2.2 **Note and endorse response to the consultation comments received on earlier iterations of the SCAAP, as set out in the accompanying Consultation Statement at (Appendix 3).**
- 2.3 **Approve the SCAAP to be submitted to the Secretary of State, prior to Examination in Public, under Regulation 22 of the Town and Country Planning (Local Planning) (England) Regulations 2012.**

2.4 That the Corporate Director for Place in consultation with the Executive Councillor, for Housing, Planning and Regulatory Services, in conjunction with the Local Development Framework Working Party (if necessary), is authorised to:

- a) approve and make amendments to the SCAAP that may result from *inter alia* analysis of the representations made following public consultation, recommendations of the Sustainability Appraisal and any additional evidence considered and then consult on these changes, if required, before they are submitted to the Secretary of State; and
- b) agree and approve amendments that may be proposed by the Inspector during the Examination in Public process and to any further consultation that may be required as a result of this; and
- c) take all necessary steps to ensure compliance with the relevant statutory processes and procedures necessary for preparation and participation of the Council at the Examination in Public.

### 3. Background

#### *Preparation of the SCAAP to date*

3.1 In accordance with Southend Borough Council's Local Development Scheme timetable and the 'Strategic Objectives' and policies within the adopted Southend Core Strategy, the Borough Council has prepared an Area Action Plan for the Central Area of the town and associated policies map. It is a spatial plan that will provide:

- An up-to-date statutory basis for assessing planning applications within the Town Centre and Central Area; and
- A local planning framework for guiding development within the Town Centre and Central Area, within which the Council, other agencies and key stakeholders can coordinate their investment programmes.

3.2 The overarching vision and aim of the SCAAP is described as follows:

*"Our vision for Southend Central Area, which includes the Town Centre and Central Seafront Area, is a destination 'City by the Sea'. As a prosperous and thriving regional centre and resort, it will be an area that is vibrant, safe and hospitable, rich in heritage, commerce, learning and culture and an attractive, diverse place where people want to live, work and visit for both day trips, overnight and longer stays.*

*Our aim is to transform the perception and image of Southend through sustainable economic growth, high quality development and social provision and for it to be independently recognised as a popular location for businesses, residents, students and visitors."*



- 3.3 The SCAAP contains policies and proposals that in combination address: the development strategy for the Plan area (matters such as retail, transport, heritage); development principles for the 'Policy Areas' (including identification of appropriate land uses, access / public realm improvements); and a set of more detailed, site specific policies in the 'Opportunity Areas'.
- 3.4 The SCAAP is divided into Policy Areas, each having its own set of 'development principles' to guide development and investment. Within the Policy Areas the SCAAP only seeks to allocate sites where evidence suggests that they are deliverable by 2021, the end of the Core Strategy plan period. These sites are set out in Table 1 below. Sites that are likely to be delivered after 2021 will be taken forward as part of the preparation of the Southend Local Plan.
- 3.5 The non-allocation of a site within the SCAAP will not prevent the site from being implemented prior to 2021. The timescales provided reflect current published evidence and the understanding regarding the likely implementation of the sites. Any planning application proposed in the SCAAP area on any site would be determined on its merit, taking into account adopted and emerging planning policies and any other material considerations.

Table 1: SCAAP Policy Area's and Opportunity Site's

<b>Policy Area</b>	<b>Opportunity Sites</b>
PA1: High Street	-
PA2: London Road	-
PA3: Elmer Square	Elmer Square Phase 2 (PA3.1)
PA4: Queensway	Queensway (PA4.1)
PA5: Warrior Square	-
PA6: Clifftown	-
PA7: Tylers	Tylers (PA7.1)
CS1: Central Seafront	Southend Pier (CS1.1) Seaways (CS1.2) Marine Plaza (CS1.3) New Southend Museum (CS1.4)
PA8: Victoria Gateway Neighbourhood	Victoria Avenue (PA8.1) Baxter Avenue (PA8.2)
PA9: Sutton Gateway Neighbourhood	Sutton Road (PA9.1) Guildford Road (PA9.2)

- 3.6 The SCAAP Proposed Submission Document is being prepared as the final stage of public consultation, prior to submission to the Secretary of State.

- 3.7 Representations received at this stage of consultation on the SCAAP will be submitted to the Secretary of State, who will then arrange for an Examination in Public. The Examination will be undertaken by the Planning Inspectorate on behalf of the Secretary of State. Those parties who have made representations may be invited by the Inspector to appear.
- 3.8 The Examination will comprise a number of roundtable hearing sessions to discuss, in more detail, matters which the Planning Inspectorate considers necessary to make a decision on whether the SCAAP may be found 'sound'. The Planning Inspector will issue an Inspector's Report on the soundness and legal compliance of the SCAAP and make recommendations, which under current legislation are not binding on Councils. The outcome of the examination may require the Council to consult on modifications to the SCAAP to address outstanding matters which the Inspector considers can be resolved.
- 3.9 Following this and subject to the plan being sound, the SCAAP may be put before Full Council for adoption. The document, when adopted, will form part of the Development Plan for the authority. From previous experience, it is likely that the time from submission to adoption could take between 3 and 9 months, therefore, the date for adoption of the SCAAP (including Council approvals) is anticipated to be summer 2017.

#### ***Public Car Parking provision within the Town Centre***

- 3.10 The Council commissioned consultant Steer Davies Gleave (SDG) to prepare a Car Parking Report and Strategy for the SCAAP area. The Report has informed the 'Transport, Access and Public Realm' section and Policy DS5 of the SCAAP. The Study reviewed current and future public car parking provision in the SCAAP area, set out the current level of use of the existing car parking network and the potential impact of development proposals.
- 3.11 In respect of the impact of future development proposals, it forecast future supply of, and demand for, public car parking, based on information currently available. It reveals that additional parking, expected to be provided by development in the SCAAP area, supported by existing network provision, is likely to accommodate future demand for parking generated in the period up to 2021. This tended to focus on the month of August in particular.
- 3.11 Overall, the Report concludes that the SCAAP parking area network rarely exceeds 85% occupancy. However, the Report identified that there is a clear imbalance in the SCAAP parking network at peak periods of demand, where car parking 'south of the railway line' experiences much greater capacity issues than public car parking 'north of the railway line'. This occurs at peak weekend periods in the summer.
- 3.12 In conjunction with this, survey data analysed in the Report appears to suggest that the price of parking is prioritised by visitors, yet there is little differentiation in the pricing of parking between car parks south of the railway line, which are sometimes over capacity during peaks, and those north of the

railway line, where there is plenty of spare capacity. The Report therefore recommends that making better use of available spare capacity within a reasonable walking distance of key destinations should be a key priority in any parking strategy for the SCAAP area.

- 3.13 The Report also recommends a series of measures in the short and medium term (in the next 5 years) which will assist with managing this peak demand in the network. This includes the application of differential pricing, allocation of long and short stay parking, improved travel information with use of smart technology, encouraging more sustainable travel options, better signage and access, and improved Variable Message System technology.
- 3.14 Overall a combination of measures to encourage greater use of all the SCAAP area car parks for peak demand is put forward in the short term, which includes **no net loss of parking south of the railway line** i.e. those conveniently located for the seafront. This, together with a medium and longer term approach which adds to those short term measures, with a preference for creating new car park capacity that favours locations south of the railway line, within a 10 minute walk of the seafront, or locations slightly further away but still convenient for users that wish to access the seafront at peak periods.

### 3.15 ***Workshops, Drop in Sessions and Meetings***

During the public consultation on the Preferred Approach version of the SCAAP, which commenced on 18<sup>th</sup> December 2015, a number of events and meetings took place to discuss the Plan as a whole, as well as concentrating of specific elements including Parking.

- 3.16 Public workshops were held on 20<sup>th</sup> and 21<sup>st</sup> January 2016 at Park Inn, Palace Hotel located within the plan area. The workshops sought to further engage the local business community and local residents, and included a detailed look at the proposed Policy Areas as outlined in the SCAAP. Six separate sessions were held over the two days. Two sessions were targeted at each specified group, namely local businesses, the local community and Southend elected Members. These comments were all collated and taken into account in the preparation of the pre-submission version of the SCAAP.
- 3.17 In addition, meetings took place with the representatives of the Business Improvement District (BID) and representatives of the Seafront Traders who, as part of two separate groups, made representations on the Preferred Approach version of the SCAAP, detailed in the Consultation Statement (**Appendix 3**). These representations focussed substantially on the amount of public car parking available within the seafront area and the impacts during peak periods in particular. As a consequence the Council commissioned some additional survey work and analysis. The output of this work was shared with the BID and a further meeting was held with representatives from the BID on the 20<sup>th</sup> July to feedback on the findings and to seek additional views and comments. As a result of this meeting additional work was commissioned including looking at the nature of demand for car parking north and south of the railway as well as for the SCAAP areas as a whole, this more detailed

analysis has in turn informed the pre-submission version of the SCAAP. This Report reflects that additional work.

- 3.18 The final version of the Car Parking Study will be published alongside the pre-submission version of the SCAAP when it goes out to consultation in October/November, and will form part of the evidence base for the Plan to be considered by an independent Planning Inspector at examination.

#### **4. Other Options**

- 4.1 Not to prepare the SCAAP and consult on the Plan. This is not recommended as an up-to-date plan prepared using local evidence and guided by community consultation is the most appropriate guide for local planning decisions and regeneration of the town centre and central seafront area.
- 4.2 The absence of a robust plan and the resulting 'planning by appeal' scenario could result in the inability to control development in Town Centre and Central Area and an increase in service costs for Southend if the Borough Council has to respond to development appeals or public inquiries.

#### **5. Reason for Recommendation**

- 5.1 To ensure the expeditious production of the SCAAP under the terms of the recommendation, resulting in an anticipated adoption date of spring 2017 and to ensure the Borough has an up to date suite of planning documents as expected by Central Government.

#### **6. Corporate Implications**

##### **6.1 Contribution to Council's Vision & Corporate Priorities**

- 6.1.1 The successful delivery of the SCAAP will contribute to the fulfilment of a number of spatial elements of the Council's vision and priorities, for example, in relation to town centre and central seafront regeneration, improving economic prosperity, promoting green technologies and protecting and enhancing the natural and built environment.

##### **6.2 Financial Implications**

- 6.2.1 Financial input is necessary to fulfil the requirements of all statutory stages in the preparation and delivery of the SCAAP. Costs associated with this consultation will be met from existing agreed budgets.

##### **6.3 Legal Implications**

- 6.3.1 There are legal implications for the SCAAP as it will form part of the Council's development plan which will have implications for the assessment of planning applications in the Plan area.

6.3.2 As with any decision made by the Council there is also the potential for judicial review following the adoption of the document in the future at Full Council. The Council must therefore ensure that the correct procedures are followed to reduce the potential risk of such a challenge.

#### 6.4 People Implications

6.4.1 Staff resources from the Strategic Planning Team will be required in order to produce the SCAAP. Support from the Department for Place Business Support Unit will also be required particularly with regards to the public consultation.

#### 6.5 Property Implications

6.5.1 Southend Borough Council owns a number of the sites promoted for development within the SCAAP. The proposed Opportunity Sites are outlined in Table 1 above and can be viewed on the Policies Map (**Appendix 2**).

#### 6.6 Consultation

6.6.1 There has been a number of public consultation exercises carried out on the SCAAP. Previous representations received through the public engagement stages have been taken into account as the Plan has progressed. These consultations are referenced below. A separate technical paper, the Consultation Statement (**Appendix 3**), sets out the consultation stages, the key issues arising, and Council response to representations received on the Preferred Approach version of the SCAAP:

- Issues and Options Consultation 2010
- Pre-Submission Consultation 2011
- Preferred Approach 2015/ and early 2016

6.6.2 It is envisaged that the SCAAP will be published for consultation in October / November 2016. Consultation will be carried out in accordance with the Councils' Statement of Community Involvement. Unlike earlier stages of public participation, representations at this juncture can only be made on legal compliance and the soundness of the plan, as set out in planning regulations and reiterated in National Planning Policy which sets out that to be sound, a plan should be "justified, effective and consistent with national policy".

#### 6.7 Equalities and Diversity Implications

6.7.1 Public consultation has provided opportunity for different sections of the community to input into the plan making process. An Equality Impact Assessment (EqIA) has been completed in respect of the effects of the proposed policies and sites on equality and diversity issues (**Appendix 5**).

#### 6.8 Risk Assessment

6.8.1 If the SCAAP was not to be published and taken forward to adoption, the absence of the SCAAP policies may result in inappropriate or piecemeal development in Southend Central Area being allowed on appeal, which would not be aligned with the Borough Council's Vision and Priorities for the regeneration and growth of the town. Further, the Council may not be able to demonstrate deliverability of the Core Strategy's spatial approach to housing delivery, nor a five year land supply, and may have to consider the release of land elsewhere in the Borough.

6.8.2 The SCAAP has facilitated opportunity to draw upon additional funding, such as funding from the Growth Deal and City Deal, to deliver the planned growth within Southend Central Area. If the SCAAP were not to be published and taken forward for adoption, opportunity to leverage such funding in the future may be affected.

## 6.9 Value for Money

6.9.1 There will be significant beneficial impacts in terms of value for money from carrying out the work proposed using in-house resources wherever possible. This will bring benefits in terms of building in-house experience and expertise for officers, as well as utilising local knowledge and experience, within the Strategic Planning team which would not be gained otherwise. The SCAAP has also facilitated opportunity to draw upon additional funding, such as funding from the Growth Deal and City Deal.

## 6.10 Community Safety Implications

6.10.1 The SCAAP seeks to improve the natural and built environment thereby contributing towards improving community safety.

## 6.11 Environmental Impact

### 6.11.1 *Sustainability Appraisal*

All iterations of the SCAAP have been subject to Sustainability Appraisal, which is an assessment of the potential significant social, environmental and economic impacts of development and forms an integral part of the plan making process. It ensures that all policies and proposals are prepared with a view to contributing to the achievement of sustainable development. These appraisals have been used to assist with the identification of the most sustainable policies to take forward.

### 6.11.2 *Habitats' Regulations Screening Report*

Southend and the surrounding districts include a number of important designated sites for nature conservation. Habitats' screening is an assessment of the potential significant effects of a policy on European Sites designated for their nature conservation importance. These include Special Areas of Conservation, Special Protection Areas and international Ramsar sites. A policy should only be approved after determining that it will not

adversely affect the integrity of such sites. Each policy has been assessed for any significant impacts on European sites within or outside the Southend and it was considered that there was no significant effect.

## **7. Background Papers**

- 7.1 The Town and Country Planning (Local Development) (England) Regulations 2012
- 7.2 Planning and Compulsory Purchase Act 2004
- 7.3 Southend on Sea Local Development Scheme timetable 2015
- 7.4 Southend on Sea Statement of Community Involvement 2012
- 7.5 Southend on Sea Core Strategy DPD 2007
- 7.6 Central Area Master Plan 2008
- 7.8 Southend-on-Sea Borough Local Plan 1994
- 7.9 Southend-on-Sea Cabinet Report: Southend Central Area Action Plan (SCAAP) Preferred Approach Public Consultation 2015

## **8. Appendices**

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**Appendix 2:** SCAAP Policies Map

**Appendix 3:** Consultation Statement

**Appendix 4:** Sustainability Appraisal

**Appendix 5:** Equalities Impact Assessment

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Southend Central Area Action Plan DPD

Revised Proposed Submission Version 2016

Local Planning Framework



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  - Policy PA9: Sutton Gateway Neighbourhood Policy Area Development Principles
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# Part A: The Plan and its Context

## 1. Introduction

### Strategic Planning Context

The Southend Central Area Action Plan (SCAAP), when adopted, will form part of the Southend-on-Sea (hereafter referred to as 'Southend') Local Planning Framework.

The location and context of Southend Central Area is depicted on **Map 1: Strategic Context**. A more detailed boundary of the Southend Central Area is set out on **Map 2: SCAAP Boundary and Policy Areas** and on the accompanying **Policies Map**.

The SCAAP reflects the vision, strategic objectives and spatial strategy of the Southend Core Strategy (2007). The Core Strategy is a strategic level document that provides the framework for subsequent DPDs, including the SCAAP. **Appendix 1** provides a broad overview of the Core Strategy policies.

The Core Strategy establishes housing and job growth targets for the SCAAP area, over a plan period of 2001- 2021, as follows:

- 2,474 additional dwellings<sup>1</sup>
- 7,250 additional jobs<sup>2</sup>

Between 2001 and 2016, 1,087 dwellings have been built within the Southend Central Area. Employment data<sup>3</sup> for the entire Borough suggests that job numbers have declined over the plan period. However, more recently, since 2010, job numbers have increased and efforts to boost job creation is underway.

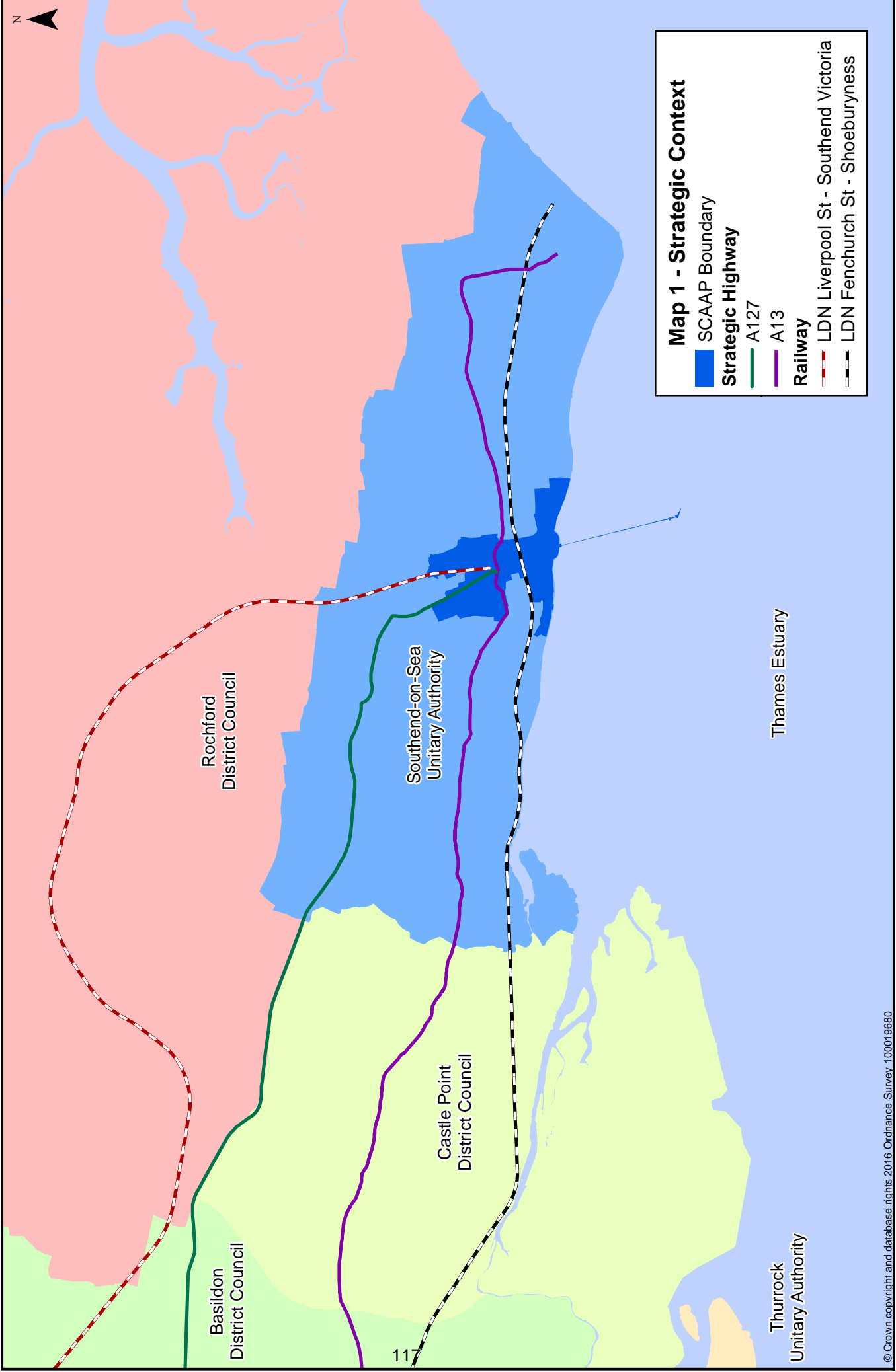
Building on this more recent resurgence and growth, the SCAAP is considered to be an important catalyst and driver for inward investment and for the delivery of the remaining proportion of planned regeneration and growth in the Southend Central Area to meet or exceed Core Strategy targets up to 2021.

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<sup>1</sup> Core Strategy CP8: Town Centre (2,000), plus Seafront (550), minus SHLAA sites identified in the rest of the seafront outside the SCAAP area (76) = 2,474

<sup>2</sup> Core Strategy Cp1: Town Centre (6,500), plus Seafront (750) = 7,250

<sup>3</sup> Southend Annual Monitoring Reports



## **Local Plan preparation and other planning policy and guidance for Southend**

It is also acknowledged that further work has been jointly undertaken to establish an objectively assessed need, in terms of jobs and housing, for Southend and surrounding housing market area. This will be a key evidence document in the preparation of the Southend Local Plan, which will set out new long term growth targets replacing those of the adopted Southend Core Strategy, including a review of unimplemented development sites within the SCAAP.

The Borough Council adopted the Development Management Document (DMD) in July 2015. The DMD sets out policies for positively managing development in Southend and will be used to assess and determine planning applications within the SCAAP area as well as throughout the Borough.

The Council's adopted Community Infrastructure Levy (CIL) charging schedule sets out where the levy may be payable, where exemptions apply, together with the CIL rates for development types and charging areas, including Southend Central Area. The proceeds can be spent on providing infrastructure to support the development within that authority's area.

The Borough Council has also adopted the following Supplementary Planning Documents (SPDs) which provide further guidance and advice:

- **Design and Townscape Guide SPD** (adopted 2009). This document provides clear contextual development and design guidance and should be referenced within all development proposals;
- **Planning Obligations Guide SPD** (revised 2015). This document provides guidance in relation to potential planning obligations or developer contributions in relation to development. The document also includes procedural information and contact details to assist in the negotiation of legal agreements.
- **Streetscape Manual SPD** (revised 2015). This document provides guidance to ensure a coordinated, high quality streetscape is sustainably achieved within the Borough.

### **South East Local Enterprise Partnership (SELEP)**

Southend, together with the areas of Essex, Thurrock, Kent, Medway and East Sussex, form part of the South East Local Enterprise Partnership (SELEP). The SELEP partnership has enabled the Council to secure a range of measures to support regeneration and growth within the Borough, including a City Deal which will link together a series of interventions including a newly formed business support facility and incubator space to aid business development, support for the regeneration of Victoria Avenue, and initial funding through the Growth Deal for the SCAAP area which will help to facilitate public realm enhancements.

The Council will continue to work with SELEP to generate public and private investment and support housing and jobs growth within Southend, with a particular focus on Southend Central Area.



South Essex Growth Partnership is driven by the private sector with support from the public sector and is part of the SELEP. The partnership seeks to draw upon the areas key assets to help unlock the growth potential of South Essex. The South Essex authorities work collaboratively to ensure the area achieves regeneration, economic growth, new infrastructure and skills for the population.

### **Southend Business Improvement District (BID)**

A Business Improvement District (BID) was established in Southend town centre and the seafront and began trading as the Southend BID Ltd in April 2013. This has helped to unlock £2.7m of investment for the town centre and seafront. The BID has introduced a number of measures to enhance the experience of visitors to the town, including the introduction of street wardens and planters to enliven the High Street experience.

### **Sustainable Development**

The Council's Low Carbon Energy and Sustainability Strategy 2015-2020 focuses on delivering low carbon growth, improving energy efficiency and providing a more sustainable future for residents, communities and businesses, with the aim of establishing Southend as Low Carbon City.

### **Southend Central Area Action Plan DPD**

#### **Context and Issues for the Southend Central Area**

- a. **Housing** – the **core** town centre has relatively fewer residential dwellings when compared to the rest of the Borough, where residential development dominates. The result of this is a relatively small town centre population, and lower levels of activity, particularly footfall in the evening, once shops and businesses have closed.
- b. **Offices** – Southend Central Area is characterised by concentrations of large, out-dated and often redundant office development<sup>4</sup>, which has been identified for mixed-use redevelopment in this Plan. Modern, fit-for-purpose, smaller-scale, flexible high-quality office accommodation, which better reflects demand, is therefore needed to support economic growth objectives.
- c. **Retail** – there has been limited investment in new retail development in recent years and there is a need for the town centre to diversify its offer, whilst maintaining its retail function, to ensure it remains attractive and competitive as high streets adapt to the market.
- d. **Education** – the higher and further educational offer of Southend has significantly improved in recent years, including a number of successful developments within Southend Central Area. There is also a need to ensure that any increase in the local population is accommodated in terms of school places.

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<sup>4</sup> Southend Employment Land Review (2010)

- e. **Leisure, Tourism, Recreation and Culture** – Southend has a vibrant offer in terms of leisure, tourism and cultural facilities, enhanced in recent years by a number of successful new developments. However, there is opportunity to further maximise Southend’s potential as a visitor destination, particularly in terms of the evening economy and overnight and longer stays<sup>5</sup>.
- f. **Central Seafront Area** – the central seafront is a valuable asset to the town, however, connections between it and parts of the town centre are disjointed and opportunities for ‘linked trips’ are not maximised<sup>6</sup>. With European and international environmental designations, it offers a unique form of open space, the biodiversity interests of which need to be sensitively balanced with regeneration and growth. The provision, and enhancement, of open and green spaces in Southend Central Area will be of benefit in terms of relieving pressure on these designations.
- g. **Transport, Access and Public Realm** – the car continues to dominate parts of Southend Central Area and the highway severs links between gateway neighbourhoods and the town centre. There is a need to build on the success of recent public realm and access improvement schemes to secure a more pleasant and accessible environment, encouraging more sustainable modes of transport including walking and cycling around the Central Area, whilst acknowledging the role the car plays in this balance.

The Parking Study for Southend Central Area identifies that the car parking network within Southend Central Area rarely exceeds 85% occupancy. It does identify however that there is a clear imbalance in the Southend Central Area parking network at periods of peak demand, with car parking south of the railway line experiencing overcapacity issues, while car parking north of the railway line has available spare capacity.

As a result of the peak capacity issues, as identified by the Parking Study and to support the vitality and viability of the central seafront area, it is expected that there will be no net loss of public car parking south of the railway line.

- h. **Heritage** – Southend Central Area boasts a wealth of heritage assets which will be celebrated as part of this Plan. However, there is a need to ensure that these assets and their setting are conserved and enhanced to ensure they continue to make a full contribution to the character of Southend Central Area.
- i. **Climate Change, Flood Risk Management and Sustainable Drainage** – Underpinning all these issues is the need for this plan, in association with the Core Strategy, to address the challenge of Climate Change and Flood Risk in the Central Area. Southend has been identified by the Environment Agency as susceptible to local surface water flooding under conditions of extreme rainfall. Additionally areas of the Borough are at risk from tidal flooding, as demonstrated by the Strategic Flood Risk

<sup>5</sup> Southend Local Economic Assessment (2013)

<sup>6</sup> Southend Local Economic Assessment (2013)

Assessment (SFRA). There is a need to manage development within areas of flood risk, particularly within the Central Seafront Area, and to incorporate properly designed Sustainable Drainage Systems (SuDS) into development proposals to reduce the rate and quantity of surface water runoff.

The SCAAP aims to address these issues by promoting land uses that support economic growth and housing delivery in order to create sustainable, vibrant communities.

### **The Purpose of this Document**

The purpose of the SCAAP is to give more detailed consideration to how and where regeneration and growth can sustainably be accommodated in the Southend Central Area, including the Town Centre, Central Seafront Area and gateway neighbourhoods.

It contains proposals for 'Policy Areas' and 'Opportunity Sites' aimed at strengthening and transforming Southend Town Centre's sub-regional role as a successful retail and commercial destination, cultural hub and educational centre of excellence, leisure and tourist attraction, and a place to reside.

The intention is also to seek to safeguard, conserve and enhance the significant biodiversity, green space and other environmental resources in the area and on the foreshore, as well as to bring about public realm and access improvements.

### **Relationship between Policies**

***All policies within this Area Action Plan should be read in conjunction with relevant national and local planning policies and guidance.***

***Please note that this Area Action Plan should be read as a whole, as the policies are cross-cutting and interrelate.***

***A policy linkage box is provided at the end of each section or policy, setting out links between policies within the SCAAP and other key documents within the Council's local planning framework.***

### **Supporting Documents**

The following documents together with other evidence base documents that have helped to inform the preparation of the SCAAP are available to view on the Council's website at [www.southend.gov.uk](http://www.southend.gov.uk).

### **Sustainability Appraisal**

A Sustainability Appraisal (SA) is an assessment of the potential significant social, environmental and economic impacts of development and forms an integral part of the plan making process. It ensures that all policies and proposals are prepared with a view to contributing to the achievement of sustainable development. The latest assessment of the

sustainability and the potential significant effects of this plan can be found in the SA Report, which is available for comment.

### **Habitats Regulations Screening Report**

Southend and the surrounding districts are home to a number of important designated sites for nature conservation. Habitats screening is an assessment of the potential significant effects of a policy on sites designated for their nature conservation importance. These include Special Areas of Conservation, Special Protection Areas, and international Ramsar sites.

The SCAAP was assessed for any significant impacts on European sites within or outside Southend. The screening report concluded that the proposed policies will have no significant impact on the European/ international designations, either alone or in combination with other plans and strategies.

### **Policies Map**

The SCAAP is accompanied by a Policies Map, which illustrates the boundary of the SCAAP area, its Policy Areas and Opportunity Sites, proposals and the extent of land use designations related to policy.

## 2 Vision and Objectives for Southend Central Area

The **Vision** for Southend Central Area is:

### Proposed Vision

- Our **vision** for Southend Central Area, which includes the Town Centre and Central Seafront Area, is a destination 'City by the Sea'. As a prosperous and thriving regional centre and resort, it will be an area that is vibrant, safe and hospitable, rich in heritage, commerce, learning and culture and an attractive, diverse place where people want to live, work and visit for both day trips, overnight and longer stays.
- Our **aim** is to transform the perception and image of Southend through sustainable economic growth, high quality development and social provision, and for it to be independently recognised as a popular location for businesses, residents, students and visitors.

Our objectives for achieving this are:

### Strategic Objectives

1. To improve and transform the economic vitality, viability and diversity of Southend Central Area by encouraging the establishment of a wider range of homes, businesses and shops whilst providing new opportunities for learning, recreation, leisure and tourism.
2. To promote design excellence and good quality development proposals and public realm improvements to reinforce a distinctive sense of place, complement new and existing development, and contribute towards the Council's aspirations to establish Southend as a Low Carbon City.
3. To increase the number and diversity of people living within Southend Central Area and its Gateway Neighbourhoods by building more homes, and ensure that living in the area becomes appealing to more families with children, supported by social and community infrastructure that contribute to reducing inequalities in health and wellbeing and support all ages to lead independent lives and live healthy lifestyles.
4. To encourage the establishment and expansion of businesses in Southend Central Area by identifying, promoting or actively bringing forward suitable sites for development to meet modern user and investor requirements.
5. To promote and enhance the tourism, cultural and leisure offer within the Central Area, including visitor accommodation, having regard to the assets

offered by the area, in order to attract greater visitor numbers and promote more overnight stays.

6. To promote the Central Area as a thriving learning quarter that provides state of the art facilities and well-designed student accommodation.
7. To improve accessibility to the area, ensuring streets, public and green spaces are well-connected, well-designed and safe, utilising a coordinated palette of materials and furniture that enhance the quality of the streetscape and improve opportunities for walking and cycling, and access to more sustainable modes of transport, such as rail and bus.
8. To promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre and central seafront area, managing the balance of parking provision to address peak demand and capacity, and provide good access to the seafront by encouraging improvements to the quality of access to and from parking areas that are convenient, well-signposted, safe and secure.
9. To address climate change matters and appropriately manage and mitigate flood risk and to encourage the provision of Sustainable Drainage Systems and urban greening measures in order to reduce surface water run-off.
10. To enhance the quality of, and access to, Southend Central Area's natural environment and open spaces, connecting to the green grid, and to improve connectivity between the Town Centre and Central Seafront Area in order to relieve pressure on the Site of Special Scientific Interest (SSSI), Ramsar site, Special Protection Areas (SPA) and other environmental designations, to protect and enhance local biodiversity and nature conservation, and to encourage opportunity for linked trips.
11. To celebrate and enhance the setting of Southend's unique heritage assets, such as the Grade II listed Pier, to ensure these assets are appropriately conserved and enhanced and continue to form an integral part of how Southend Central Area is experienced by those who live, work and visit the area.

## Part B: Development Strategy

### 3. Central Area Strategy and Criteria Based Policies

#### Central Area Strategy

The Central Area Strategy seeks to develop a ‘**City by the Sea**’ – a change in the function and transformation in the quality of the Town Centre and Seafront and renewal of Southend Central Area with additional residential development creating a new critical mass to support growth and inward investment.

Spatially, this concept embraces the vision of Southend as a prosperous regional centre defined by sustainable growth of its urban functions and the identification of Southend as a location of choice for businesses, residents and visitors. This will be achieved through the creation of Policy Areas and Opportunity Sites (see **Map 2: SCAAP Boundary and Policy Areas**), which will guide new development.

The SCAAP establishes Policy Areas which, to varying extents, take on a new mixed-use sustainable character. Development within these Policy Areas will be appropriate to their context, either seeking to strengthen the existing competitive advantage of current uses, encourage a greater mix of uses or defining new roles, contributing to the regeneration of the identified opportunity sites as well as Southend Central Area as a whole.

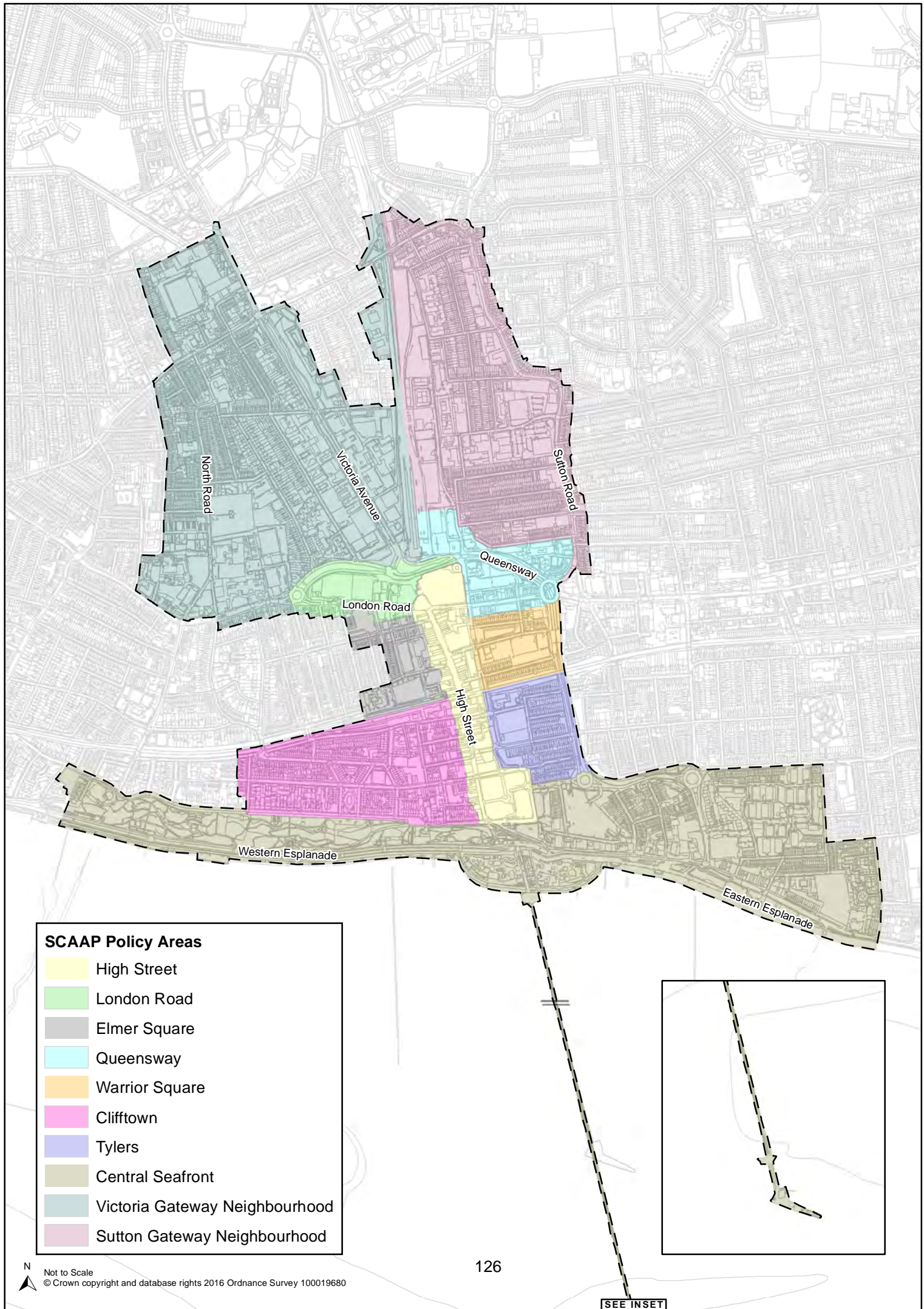
The introduction of new residential uses as part of a broader mix is a key element in achieving a vibrant, thriving Town Centre.

The Policy Areas have been identified as follows:

- **High Street**
- **London Road**
- **Elmer Square**
- **Queensway**
- **Warrior Square**
- **Cliff town**
- **Tylers**
- **Central Seafront**
- **Victoria Gateway Neighbourhood**
- **Sutton Gateway Neighbourhood**

Within the Policy Areas there are also a number of ‘Opportunity Sites’. The approach for managing these sites, and wider policy areas, is set out in Part C: Policy Areas and Site Allocations.

# Map 2 - SCAAP Boundary & Policy Areas





## Criteria Based Policies

### 4.1 Introduction

This section sets out the policy context for key uses and development within Southend Central Area. It includes a strategy and, where relevant, policies<sup>7</sup> for the delivery and management of:

- Retail
- Employment
- Housing
- Culture, Leisure, Tourism and Recreational facilities
- The Historic Environment
- Open and Green Spaces
- Key Views
- Landmarks and Landmark Buildings
- Flood Risk Management and Sustainable Drainage
- Transport, Access and Public Realm
- Infrastructure Provision (including education, health and social and community facilities)

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<sup>7</sup> Where there is not a policy sited in this section of the Plan there is a 'Policy Linkages' box which highlights another Plan where relevant policies may be found.

## 4.2 Retail

### *Southend Town Centre's Primary Shopping Area*

The Primary Shopping Area of Southend's town centre (see **Map 3** and **Policies Map**) is focused around a long, well established linear High Street, and anchored by The Victoria shopping centre to the north and The Royals shopping centre to the south.

New retail development should complement and strengthen the offer of the town centre, in accordance with **Core Strategy Policy CP2: Town Centre and Retail Development** and the provisions of the National Planning Policy Framework (NPPF), reinforcing pedestrian circuits around the two main shopping centres.

Opportunities for additional retail floorspace will be expected to arise from some incremental increases in existing floorspace through extending shop units or creating larger trading areas through internal unit reconfiguration<sup>8</sup>. Additionally, consideration and use of vacant floorspace, particularly in The Victoria shopping centre should be made.

The following Policy Areas and Opportunity Sites are located/partly located within the Town Centre Primary Shopping Area and have the potential to deliver additional retail floorspace where opportunities arise:

- P1 High Street;
- P2 London Road;
- P4 Queensway and Opportunity Site PA4.1 along Southchurch Road;
- P6 Clifftown (for small scale niche retail);
- P7 Tylers, including Opportunity Site PA7.1.

Public realm enhancements throughout the town centre, particularly within areas that have existing poor quality environments, would be expected to increase footfall and assist with letting vacant units. A schedule of access and public realm improvements is set out within the development principles of each Policy Area.

Outside the Town Centre Primary Shopping Area, the Council may permit additional small-scale convenience retail provision to meet the needs of residents. Details are contained in each of the relevant Policy Areas.

### **Town Centre Primary and Secondary Shopping Frontages**

The retail sector is crucial to the health of the local economy in terms of its attraction to visitors, business and investment. The changing nature of the 'High Street', facing competition from internet shopping, out-of-town retail parks and neighbouring centres, has impacted the level of trading in the Town Centre, and consequently there is a need to enhance and broaden its offer and function to possibly further include other complimentary uses.

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<sup>8</sup> Southend Retail and Town Centre Study (2011)

However, an over-concentration of non-retail uses within the primary frontage can detract from its shopping function and may prejudice its vitality and viability, create extensive lengths of 'dead' frontage and a lack of proper or conventional shop window displays. Maintaining a high concentration of retail uses ensures the attractiveness of the centre as an accessible, diverse shopping area; which is paramount to the vitality and viability of the local economy.

Town Centre Primary and Secondary Shopping Frontages, as defined on the Policies Map (and outlined on Map 3 below), perform a vital role by managing the shopping function of the town centre to ensure its long term vitality and viability is not significantly harmed as a sub-regional destination.

To ensure that a healthy balance of uses is maintained, the Council will actively manage the concentration of different Use Classes (under the Use Class Order) within the Primary Shopping Frontages, as depicted on the Policies Map. By designating and protecting key frontages it is possible to manage the proportion of retail and non-retail uses which will help to ensure that the town centre remains an attractive place to shop.

The Council recognises that shopping should be just one part of a rich mix activities within the Town Centre. Therefore, **Policy DS1** seeks to manage the designated primary frontages so that the proportion of retail use does not decrease below 60%. This approach not only seeks to secure the primacy of retail within these frontages by setting a percentage target, but also allows for an increase in supporting non-retail town centre uses when compared to the existing policy approach (80%)<sup>9</sup>. It also recognises that these primary frontages are crucially supported by adjacent secondary frontages, which do not include a retail percentage target, and therefore allows for further diversification of the town centre and its offer to further sustain its role of a retail, leisure and lifestyle destination.

It is also important to understand that Southend's town centre is perpendicular and well connected to the central seafront area. The central seafront area represents an important visitor destination in its own right, comprising a range of leisure uses, which together with the town centre supports a wider multifunctional Central Area within Southend that offers a unique and diverse visitor/ shopper experience.

The entire High Street length measures approximately 0.5 miles, a considerable distance for a high street. It is considered that the primary shopping frontage of the town centre comprises three inter-related distinct zones, the High Street, The Victoria Shopping Centre and The Royals Shopping Centre. Each of these zones should maintain a core retail function, but also be able to provide for a range of supporting town centre uses, particularly Class A3 restaurants and café uses, in order to enhance the experience for visitors and help towards achieving a low vacancy rate within the area.

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<sup>9</sup> Southend-on-Sea Borough Local Plan (1994)

The SCAAP substitutes all primary frontage not located on the main high street or the ground floor levels of the two respective shopping centres (The Victoria and The Royals) with secondary frontage. This reduces the length of primary frontage in the town centre by approximately 40% as compared to that designated by the Southend Borough Local Plan (1994).

Secondary shopping frontages located within the town centre, as defined on the policies map, are often located adjacent to a 'high street', and allow for a greater number and diversity of uses.

Where an empty unit has little prospect of being occupied within a primary or secondary shopping frontage in the short term, the Council will encourage the landowner/landlord to display local art within the windows to create visual interest from the public realm.

### **Permitted Development Rights and Temporary Uses**

Under permitted development rights<sup>10</sup> buildings in specific Use Classes, including A1 Retail, are able to change to a number of alternative Use Classes, either for a temporary period or indefinitely. However, there will remain instances where prior approval applies as part of the permitted development or the change of use of a building does not constitute permitted development and would require the granting of planning permission.

Where permitted development is sought and prior approval is applicable Policy DS1 will apply as follows:

- Within Primary Shopping Frontages the following will apply:
  - The 60% retail threshold will equate to an "adequate provision of services"
  - The marketing criteria in Appendix 8 will apply in determining whether "there is a reasonable prospect of the building being used to provide such services".
- A "key shopping area" is the same as the Primary Shopping Frontages and Secondary Shopping Frontages as defined on the Policies Map.

In respect to Policy DS1.4(a) vacant units could include units occupied for temporary or 'flexible' uses, permitted through a temporary planning permission or under permitted development rights.

For the purposes of calculating the proportion of retail in any given frontage (in respect to policy DS1 point 4a, any building operating under a permitted 'flexible use' at the time of assessment will be considered on the basis of the use class it had prior to the temporary use change. For example, a retail shop (A1) which has temporarily changed its use to a café or restaurant (A3) under the permitted development rights would still be considered as an A1 unit for the purposes of determining the overall percentage of retailing (A1) or whether there are more than two consecutive non-A1 uses.

### ***Development Management Policy***

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<sup>10</sup> Town and Country Planning (General Permitted Development) (England) Order 2016

Development Management Policy DM13 (Shopping Frontage Management outside the Town Centre) identifies 3 discreet areas of Secondary Shopping Frontage **within the central area**, which act as local centres and are located outside the Town Centre Primary Shopping Area. The boundaries of these are defined on the Policies Map.

<b>Policy DS1: A Prosperous Retail Centre</b>
1. Proposals for retail development inside or outside the Primary Shopping Area will be determined in accordance with <b>Policy CP2: Town Centre and Retail Development of the Southend-on-Sea Core Strategy</b> and the provisions of the National Planning Policy Framework (NPPF).
2. New retail development should be well integrated and closely linked with the Town Centre Primary Shopping Area, as defined on the Policies Map, in terms of proximity, continuity of function and ease of access.
3. The Council will seek to maintain a high level of retail use with at least 60% Class A1 retail use within town centre primary shopping frontage. Alternative appropriate non-retail uses, particularly Class A3 restaurants and café uses, will also be supported provided that they contribute to the vitality of the Town Centre and would result in: <ol style="list-style-type: none"> <li>a. no more than 40% of the town centre primary shopping frontage, measured in terms of length of frontage*, being used for non-retail purposes. Where non-retail uses would exceed 40% of the primary shopping frontage length, no further loss of Class A1 will be allowed**;</li> <li>b. an active frontage is retained or provided with a display function for goods and services rendered and the proposed use will provide a direct service to visiting members of the general public;</li> <li>c. no detrimental impact to those living or working nearby, for example by causing undue noise, odour and disturbance.</li> </ol> <p><i>* The measurement will be applied separately to each distinct Town Centre Primary Shopping Frontage zone, these being the High Street (944m), Victoria Shopping Centre (367m) and Royals Shopping Centre (405m)</i></p> <p><i>** exceptions to this will be considered if it can be demonstrated to the satisfaction of the Council that the A1 use is no longer viable through an effective 2 year marketing exercise where the vacant property has been offered for sale or letting on the open market at a realistic price and no reasonable offers have been refused. Appendix 8 sets out further information to be provided in relation to marketing of vacant floorspace.</i></p>
4. All proposals in the town centre secondary shopping frontage, as defined on the Policies Map, must ensure that: <ol style="list-style-type: none"> <li>i. an active frontage is retained or provided with a display function for goods and services rendered and the proposed use will provide a direct service to visiting members of the general public; and</li> <li>ii. it would not be detrimental to those living or working nearby, for example by causing undue noise, odour and disturbance.</li> </ol>
5. All new shop frontages will be of a high standard of design that is compatible with the architectural style, roofscape and character of the building and surrounding area. The design of new shop fronts should have regard to the Design and Townscape Guide SPD

and address the following design principles:

- i. The loss of traditional features and shop fronts, which make a positive contribution to the character and appearance of the building or surrounding area, will be resisted unless it can be demonstrated that the benefits of a proposal significantly outweigh their loss;
- ii. Blank frontages will be resisted on principal elevations and opportunities for exposing upper floor windows maximised.

6. Proposals for the use of upper floors in shopping frontages for retail, residential, leisure, office or other complementary uses which help to maintain or enhance the character and vitality of the centre will be supported. Where upper floors are currently in retail use, developers should seek to retain retail uses where viable and appropriate.

7. The Council will seek to maintain and enhance 'street market' provision within the Town Centre Primary Shopping Area, and will work with the private sector to promote the establishment of a new well designed street market within the pedestrianised London Road Policy Area. Proposal for street markets development elsewhere within the Town Centre Primary Shopping Area will be considered on their merits.

**POLICY LINKAGES - RETAIL**

**CORE STRATEGY DPD**

**Strategic Objectives:** 3, 5, 6, 8

**Policies:**

KP2, CP2

**SOUTHEND CENTRAL AAP**

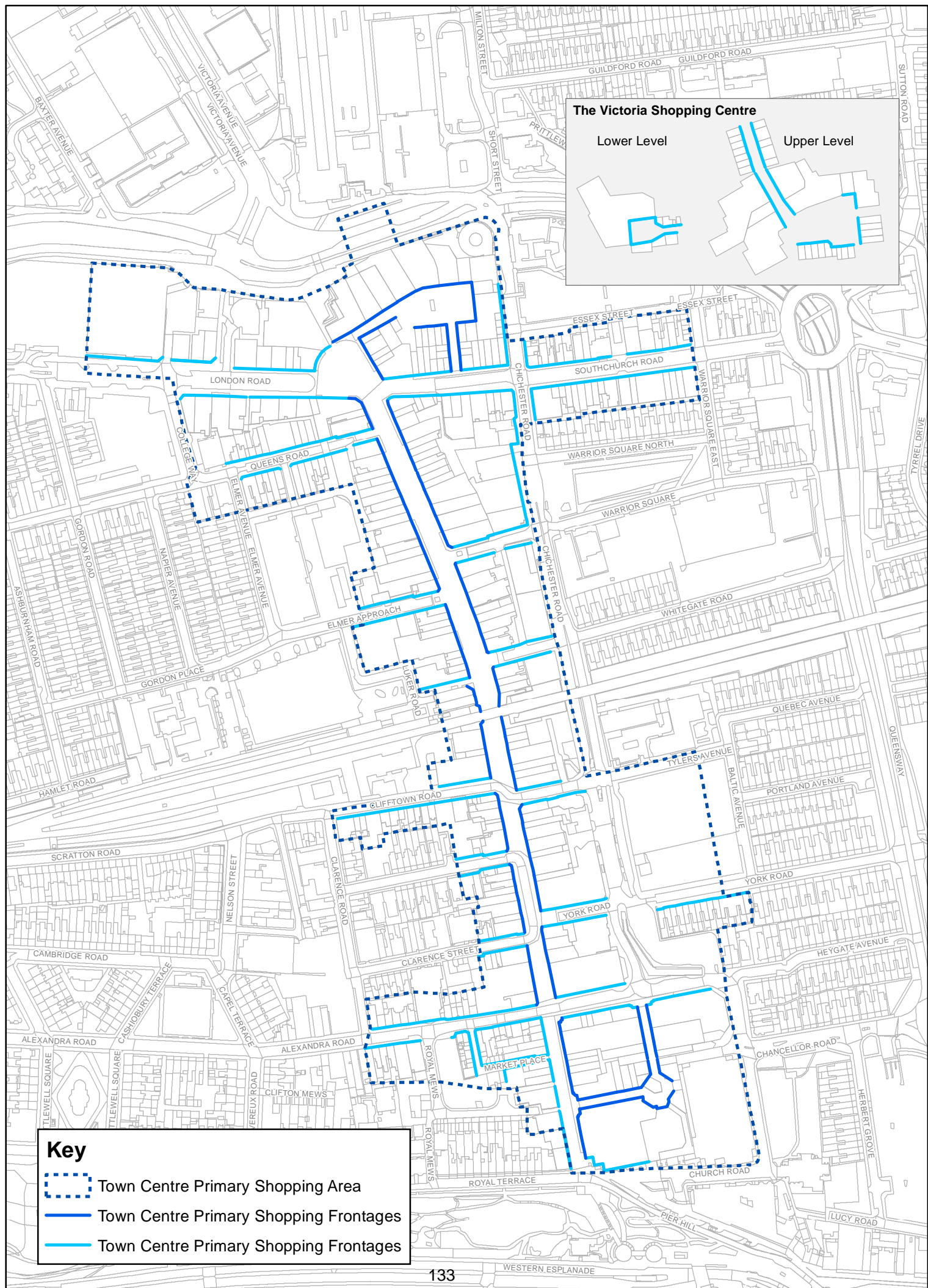
**Objectives:** 1, 2, 8

**Policies:**

PA1, PA2, PA4, PA6, PA7

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

# Map 3 - SCAAP Town Centre Primary Shopping Area & Shopping Frontages



**The Victoria Shopping Centre**

Lower Level

Upper Level

**Key**

- Town Centre Primary Shopping Area
- Town Centre Primary Shopping Frontages
- Town Centre Primary Shopping Frontages

### 4.3 Employment

The employment base of Southend as a whole has become increasingly diverse. The creative and cultural sectors, aviation and medical technologies are all growing and offer further potential for growth in the future. The Town Centre is a sustainable location for significant employment growth. This growth is concentrated in service sectors that require flexible and good quality offices, such as those for finance and business services as well as knowledge based creative industries.

It is recognised that delivery of the Core Strategy employment target (7,250 additional jobs to be delivered in the Town Centre and Central Area between 2001 – 2021<sup>11</sup>) is challenging, particularly following the impacts of the global economic downturn. In fact, monitoring of employment data across the Borough suggests that job numbers have declined over the plan period. However, more recently, since 2010, job numbers have begun to increase and efforts to boost job creation are underway. Clearly, the Borough-wide employment targets as set out in the Core Strategy will be reviewed as part of preparing a new Southend Local Plan. Nevertheless, the SCAAP will seek to maximise employment opportunities and the SCAAP is seen as an important catalyst in helping to deliver a sustainable balance of new jobs alongside housing within the Central Area.

The **South East Local Enterprise Partnership** (SE LEP), a business-led partnership, has enabled the Council to secure a range of measures to support regeneration and growth. One such initiative, **The Southend City Deal** provides support for small and medium-sized businesses, seeks to create new jobs, and attracts inward investment. An incubator system of one-on-one support ('The Hive'), based in the former Central Library on Victoria Avenue (in the **Victoria Gateway Neighbourhood Policy Area**), will help to improve business performance, safeguard jobs, and form part of the regeneration of Victoria Avenue.

#### Offices

The market for office space within the Central Area is oversupplied with outdated office stock, particularly within **Victoria Avenue Office Area (Opportunity Site PA8.1)**. Much of this stock is too large, underused/vacant, and unlikely to meet the changing requirements of small to medium sized occupiers - the focus of demand for office space in Southend. Providing a range of flexible, good quality, offices as part of mixed use developments will help to create viable proposals and a better balance of space than currently on offer.

Flexible, good quality office development is principally promoted in the following policy areas **High Street, London Road, Warrior Square, Clifftown, Tylers, and Victoria Gateway Neighbourhood Policy Area**. The policies for these areas, and opportunity sites are set out within **Part C: Policy Areas and Site Allocations** of this plan.

#### Southend as a Knowledge-Based Employment Centre

With the assistance of the university campus, Southend has significant potential to become a knowledge-based employment centre, utilising links with the A127 strategic corridor, the

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<sup>11</sup> Core Strategy CP1: Town Centre (6,500), plus Seafront (750) = 7,250



growth of London Southend Airport and the associated business parks, provision for which is made within the London Southend Airport and Environs Joint Area Action Plan (JAAP).

Southend has a high level of business start-ups. To date, business survival and therefore growth has struggled. Providing the support and infrastructure required to sustain and grow local businesses will be a crucial component in addressing this, and such activity will be supported by the **Southend City Deal** incubator hub (the Hive) at Victoria Avenue.

### **Southend’s Cultural and Creative Industries**

The Southend Cultural Strategy 2012-2020 sets out the vision for Southend: *‘To be recognised as the cultural and leisure capital of the East of England’*. The town has a significant concentration of creative and cultural businesses located across the Borough, particularly in the Town Centre.

The Local Economic Assessment (2013) outlines that whilst the creative and cultural industries have significant employment and wealth generating capacity, they also have the ability to create a step change in the economy, attracting new, ambitious people to Southend.

<b>POLICY LINKAGES - EMPLOYMENT</b>	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 1, 2, 3, 4, 5	<b>Policies:</b> KP1, CP1
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM10, DM11
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 9	<b>Policies:</b> PA2, PA5, PA6, PA8, PA9

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 4.4 Housing

There are a number of existing residential areas in the Central Area. However, when compared to the rest of the Borough, the core town centre has relatively few residential properties.

Delivering new homes within Southend Central Area, including residential above ground floor commercial, will contribute to creating sustainable communities that will add critical mass to support the vitality and vibrancy of the town centre, throughout the day and evening economy.

New housing development within the Central Area will be encouraged to provide a mix of housing types and sizes, including affordable housing, in accordance with Core Strategy CP8 (Dwelling Provision) and Development Management Policies DM7 (Dwelling Mix, Size and Type), DM8 (Residential Standards), DM9 (Specialist Residential Accommodation); although care will need to be taken to ensure there is a balanced housing offer, taking into account the existing tenure mix of a particular area.

### Scale of Residential Development

The SCAAP is considered to be an important catalyst and driver for inward investment and for the delivery of the remaining proportion of planned regeneration and growth in the Southend Central Area to meet, or exceed, Core Strategy targets up to 2021.

Further work has been jointly undertaken to establish an objectively assessed need, in terms of jobs and housing, for Southend and its surrounding housing market area. Following publication of this evidence, preparation of a Southend Local Plan will commence alongside delivery of the SCAAP. The Local Plan will set out new growth target replacing those of the adopted Core Strategy and will review unimplemented development sites within the SCAAP.

The Core Strategy requires at least 2,474<sup>12</sup> net additional new dwellings to be provided within Southend Central Area during the period from 2001 to 2021. According to the Southend Annual Monitoring Report (AMR), between 2001 and 2016, 1,087 dwellings have been built within the Southend Central Area. An additional 1,732 net dwellings have been identified through Opportunity Sites and 1,040 of these have planning permission as of 1 April 2016. A further 434 dwellings have been identified by outstanding planning permissions located outside of the opportunity sites and are predicted to be delivered by 2021, see table 1 below.

**Table 1: The Scale of new Residential Development to be delivered by 2021**

SCAAP Policy Area	Net additional dwellings identified in Opportunity Sites (of which committed)	Other committed at 1 April 2016 (net)	Total
PA1: High Street	n/a	171	171
PA2: London Road	n/a	1	1

<sup>12</sup> Core Strategy CP8: Town Centre (2,000), plus Seafront (550), minus SHLAA sites identified in the rest of the seafront outside the SCAAP area (76) = 2,474

PA3: Elmer Square	0	73	73
PA4: Queensway	380 (8)	0	380
PA5: Warrior Square	n/a	16	16
PA6: Clifftown	n/a	57	57
PA7: Tylers	150	4	154
CS1: Central Seafront	278 (278)	4	282
PA8: Victoria Gateway Neighbourhood	782 (662)	39	821
PA9: Sutton Gateway Neighbourhood	142 (92)	69	211
<b>TOTAL</b>	<b>1,732 (1,040)</b>	<b>434</b>	<b>2,166</b>

*Committed = with planning permission or prior approval*

Appendix 6 shows the amount of dwellings identified to be delivered by 2021 within the SCAAP area and relationship with the Core Strategy targets, further details are also included within each Policy Area, Part C: Policy Areas and Opportunity Sites.

### Student Accommodation

The University of Essex and South Essex College have a strong presence within the Central Area, reinforced in recent years by the delivery of phase 1 of Elmer Square with The Forum opening in 2013. The Core Strategy makes provision for the regeneration of the town centre and Central Area led by the development of the university campus, and it is anticipated that the higher and further education sector will continue to expand, with increases in student numbers.

There will be a need to provide student accommodation, much of which could be within the Central Area. The provision of student accommodation can often be met through purpose built development, such as the existing University Square development within the town centre, or through the private rented sector. The Council will support the provision of well-designed student accommodation in Southend Central Area, as it provides a sustainable location for students, with easy access to the university and college buildings. It will also help to contribute to the aim of increasing the residential population and potential spend in the Central Area.

The University of Essex has an accreditation scheme that all approved private landlords must meet, and this provides a measure to ensure student accommodation is of high quality and meets the needs of students. **Development Management Policy DM8: Residential Standards** sets out the internal space standards that all non-self-contained accommodation, such as student accommodation, will be required to meet.

POLICY LINKAGES – HOUSING	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 6, 7, 14	<b>Policies:</b> KP1, KP2, KP3, CP8
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM7, DM8, DM9
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 8, 10	<b>Policies:</b> PA1, PA2, PA4, PA5, PA6, PA7, CS1, PA8, PA9

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 4.5 Culture, Leisure, Tourism and Recreational Facilities

**'To be recognised as the cultural and leisure capital of the East of England.'**

*Southend-on-Sea Cultural Strategy 2012-2020*

Southend Central Area will continue to be the primary focus for further enhancement of cultural, leisure, tourism and recreational attractions and facilities. This will build on the town's role as a major resort and contribute to a stronger, more vibrant centre.

There have been a number of recent developments within the Central Area that have helped to progress Southend's cultural and tourism offer, including the Royal Pavilion cultural centre at the end of the Pier, the relocation of the Beecroft Gallery to the former central library building on Victoria Avenue, the relocation of the Focal Point Gallery to The Forum at Elmer Square, the regeneration of the former Palace Hotel as the Park Inn, and the new Premier Inn development on Eastern Esplanade, which has increased the quality as well as capacity of hotel offer.

Despite recent successes, the range of commercial leisure and recreational uses on offer in the town centre is moderate, and enhancing this could serve to diversify the centres offer overall as well as draw in additional visitors and investment. The strategy for the **Central Seafront Policy Area** within this Plan (see **Part C**) seeks to create a seamless connection between the Seafront and the Town Centre. Clearly the Seafront offers a considerable commercial leisure offer, and providing better connectivity between these areas may well be a positive way of maximising the attraction of the Southend Central Area.

Tourism is an important economic driver for the Town Centre. However, the tourism industry still tends to operate on low levels of overnight stays, and the relatively short supply of high quality hotels, bar those mentioned above, and current restaurant offer in the Central Area may not attract those visitors with more spending power. The refresh of the **Southend Local Economic Assessment (2013)** concludes that potential remains to improve and diversify the tourism offer to increase overnight and longer stays and add value, whilst at the same time, harness the spending power of visitors and out commuting residents alike.

The tourism and hotel sector is expected to grow in Southend over the next 20 years. The **Development Management DPD (Policy DM12: Visitor Accommodation)** seeks to manage this growth by focusing new visitor accommodation to the Central Area, London Southend Airport and at locations with good access and a clear and strong relationship with the seafront. The Central Seafront Policy Area, in particular, offers a good location for the development of visitor accommodation, given close proximity to both the Town Centre and Seafront.

Enhanced evening attractions, including provision of restaurants, have the potential to address concerns about the vitality of the evening and night-time economy through improved management and maintenance of the Town Centre, and by providing more pedestrian activity after shopping hours to help tackle the perception of safety after dark.

This could be complemented by increased public art provision and streetscape improvements to enrich the streetscape and enhance the quality of the public realm – purveying the Central Area’s cultural qualities and promoting legibility and way finding. The Council will seek to establish an increase in public art provision, where possible with local artists, within the Central Area, in line with its Public Art Strategy, to create a ‘Central Area Art Trail’.

<b>POLICY LINKAGES – CULTURE, LEISURE, TOURISM AND RECREATION</b>	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 1, 2 13, 14, 15, 18	<b>Policies:</b> KP1, KP2, KP3, CP1, CP4, CP6, CP7
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM6, DM10, DM12
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 3, 5, 6, 7, 8, 10	<b>Policies:</b> PA1, PA2, PA3, PA4, PA5, PA6, PA7, CS1, CS4, PA8, PA9

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 4.6 The Historic Environment

This Plan seeks to celebrate heritage and to conserve and enhance Southend Central Area's heritage assets in a manner appropriate to their significance, with the emphasis on high quality design in all development proposals. Heritage assets are an important component of the tourist economy and play a crucial role in the identity-making. Development Management Document **Policy DM5: Southend-on-Sea's Historic Environment** sets out the local approach to the management of the historic environment within the Borough.

Development proposals within the Central Area, including enhancements to the public realm, will be responsive to the setting of heritage assets and should seek to improve the quality of their environmental context. Heritage assets will be promoted and enhanced as part of the future development of the town.

Policy criteria regarding the historic environment are provided within the relevant **Policy Areas and Opportunity Sites** set out in **Part C** of this Plan, details of which are provided within the Policy Linkage box below. It should be noted that listed buildings, buildings in Conservation Areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Part L Building Regulations where compliance would unacceptably alter their character and appearance.

### Conservation Areas

There are a number of Conservation Areas within the Central Area, as depicted on the Policies Map. These include: Prittlewell, Eastern Esplanade, The Kursaal, Clifftown, and Warrior Square. Each has its own unique character which must be conserved and enhanced.

### Listed and Locally Listed Buildings

Southend Central Area contains a large number of listed and locally listed buildings, which help define the town's unique heritage. A list can be found on the Council's website [www.southend.gov.uk](http://www.southend.gov.uk)

### Frontages of Townscape Merit

Frontages of Townscape Merit are non-designated heritage assets and apply specifically to historic facades, many of which are shopping parades. This designation, as depicted on the Policies Map, will be a material consideration for planning applications affecting these frontages.

### Archaeology

Within this relatively small area there have been archaeological discoveries dating from the earliest evidence of humans in the area, to the medieval and later periods. The highest concentration of finds is in the Prittlewell area. Some of this area has been excavated for brickearth and other minerals but this remains the historic heart of the town and the potential for new finds is still significant.

Two Scheduled Ancient Monuments are located close to the Southend Central Area boundary. Prittlewell Priory just north of the area and Southchurch Hall to the south east. Immediately to the east of Prittlewell Priory are Roman and early Saxon cemeteries, which

included the chambered tomb of the ‘Prince of Prittlewell’, a discovery of international significance.

**Areas of Archaeological Potential in Southend Central Area**

Although most of Southend Central Area has been previously developed there are still areas of archaeological interest where there is potential for new finds. In particular, these sites include:

1. Seaways Car Park area (Opportunity Site CS1.2)
2. Roots Hall area
3. Nazareth House
4. Southend Cliffs (which includes Opportunity Site CS1.4)

Any additional areas that are subsequently considered to exhibit significant archaeological potential, should be assessed in line with national guidance and **Policy DM5** of the Development Management Document.

POLICY LINKAGES – THE HISTORIC ENVIRONMENT	
<b>CORE STRATEGY DPD</b> Strategic Objective: 14	<b>Policies:</b> KP2, CP4
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM4, DM5, DM6
<b>SOUTHEND CENTRAL AAP</b> Objective: 7	<b>Policies:</b> Transport and Access Strategy, DS3, DS5, PA1, PA5, PA6, CS1, PA8

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 4.7 Open and Green Spaces

Southend Central Area includes the Benfleet and Southend Marshes European Marine Site, encompassing both the SPA and Ramsar, which comprises the intertidal part of the Thames Estuary and also constitutes a Site of Special Scientific Interest (SSSI).

The Appropriate Assessment (AA) of the Core Strategy highlights that Core Strategy Policy KP1, which promotes development in the seafront area, is likely to result in increased recreational and development pressures on designated international and European sites. It is therefore imperative that Southend Central Area provides and enhances functional open and green space, such as pocket parks and play areas, linked to other attractive destinations in and around the Borough, in order to assist with relieving pressure on the Borough's designated sites. This builds on, and is embedded within, the South Essex Green Grid Strategy and Thames Gateway Parklands Initiative which seek to help promote South Essex as a green place, improving the local environment and access to it, linking all green spaces where possible.

The existing green spaces within Southend Central Area are depicted on the Policies Map. The Central Seafront Policy Area provides access to an abundance of green and open space. However, the Town Centre, in comparison, has relatively few areas of such space. This deficit will be addressed within the relevant Policy Areas and Opportunity Sites.

Particular attention will be paid to the improvement of existing public spaces and to the creation of new public and civic spaces. Existing and new green and open spaces will be linked together in a legible network. New green and open spaces should be linked through the green grid, and should seek to contribute to local biodiversity and, together with other urban greening measures such as green walls, improved landscaping, and tree planting, help mitigate the effects of climate change.

In order to ensure these open and green spaces are accessible they should be connected by an attractive network of accessible streets, and the quality of the public realm will be a key component in defining Southend Central Area as an urban environment where people want to live, work, visit and move around. The **Transport, Access and Public Realm Strategy** of this plan, the Council's **Design and Townscape Guide SPD** and **Streetscape Manual SPD** should be referenced for all street works within the Central Area.

POLICY LINKAGES – OPEN AND GREEN SPACES	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 14, 18	<b>Policies:</b> KP2, KP3, CP4, CP7
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 3, 6	<b>Policies:</b> DS5, PA1, PA3, PA4, PA5, PA6, CS1, PA8, PA9

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*



## 4.8 Key Views

There are a number of ‘Key Views’ from within, and of, Southend Central Area that further help to define its character, including links with the Thames Estuary. The Council will seek to ensure that Key Views, as identified below, are not adversely impacted by development:

- **The Seafront** – views to and from the seafront, with particular recognition given to views from: Westcliff Parade; Clifftown Parade; Clifton Terrace; Royal Terrace; Pier Hill; Pier Head; Queensway; Western Esplanade; Marine Parade; and Eastern Esplanade.
- **Southend Pier** – with particular recognition given to views from: the High Street in order to enhance the link between the town centre and seafront; Eastern Esplanade; Western Esplanade; Marine Parade; Royal Terrace; and Clifftown Parade.
- **The Kursaal** – with particular recognition given to views from: Marine Parade; Eastern Esplanade; Lucy Road; Queensway and Southchurch Avenue.
- **Royal Terrace and Clifftown Parade** – with particular recognition given to views from Western Esplanade.
- **All Saints Church** (outside of the SCAAP boundary) – with particular recognition given to enhancing the setting of this heritage asset, improving the quality of the public realm at Queensway dual carriageway;
- **Porters** (outside of the SCAAP boundary) – with particular recognition given to enhancing the setting of this heritage asset, improving the quality of the public realm and highway at Queensway dual carriageway;
- **St Mary’s Church** (outside of the SCAAP boundary) – with particular recognition given to improving the setting of this heritage asset, improving the quality of the public realm and highway junction at Victoria Avenue/East Street.

### Policy DS2: Key Views

New development within Southend Central Area will be expected to demonstrate that it is compatible with and/or enhances Key Views of:

- **The Seafront**
- **Southend Pier**
- **The Kursaal**
- **Royal Terrace and Clifftown Parade**
- **All Saints Church** (outside of the SCAAP boundary)
- **Porters** (outside of the SCAAP boundary)
- **St Mary’s Church** (outside of the SCAAP boundary)

### POLICY LINKAGES – KEY VIEWS

<b>CORE STRATEGY DPD</b> Strategic Objective: 14	<b>Policies:</b> KP2, CP4
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM4, DM5, DM6
<b>SOUTHEND CENTRAL AAP</b> Objectives: 2, 7	<b>Policies:</b> PA1, PA4, PA6, CS1, PA8

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 4.9 Landmarks and Landmark Buildings

***A Landmark Building is defined as one that has become, or may become, a point of reference because of its positive contribution to place making. This may include reference to its height, siting, distinctive design or use that sets it apart from surrounding buildings. Examples may include: churches, theatres and town halls.***

Landmarks or Landmark buildings provide orientation and aid way-finding. They are relatively limited in number and generally occupy strategic locations such as road junctions, terminations of vistas, and corners.

A building or feature will not be considered a landmark simply owing to its height or massing, indeed many of the existing landmarks within Southend Central Area are of a modest scale; essentially they must be of high quality, recognisable and distinctive. A landmark could also be represented by a significant piece of public art, a distinct architectural feature, or use of innovative and distinctive materials.

For the purposes of the SCAAP, the following have been identified as existing landmarks and landmark buildings (Table 2, and Appendix 3):

**Table 2: Existing Landmarks and Landmark Buildings**

Adventure Island, Western Esplanade <i>(Central Seafront Policy Area)</i>	Royal Hotel and Royal Terrace <i>(High Street and Clifftown Policy Areas)</i>
All Saints Church, Sutton Road <i>(outside of the SCAAP boundary)</i>	Seafront / Estuary <i>(Central Seafront Policy Area)</i>
Central Library (former), Victoria Avenue <i>(Victoria Gateway Neighbourhood Policy Area)</i>	South Essex College, Luker Road <i>(Elmer Square Policy Area)</i>
Central Museum, Victoria Avenue <i>(Victoria Gateway Neighbourhood Policy Area)</i>	St John's Church, Herbert Grove <i>(Central Seafront Policy Area)</i>
Civic Centre, Victoria Avenue <i>(Victoria Gateway Neighbourhood Policy Area)</i>	St Mary's Church, Victoria Avenue <i>(outside of the SCAAP boundary)</i>
Cliff Lift, Western Esplanade <i>(Central Seafront Policy Area)</i>	Swan Hall, Victoria Avenue <i>(Victoria Gateway Neighbourhood Policy Area)</i>
Cliffs Pavilion, Station Road <i>(Central Seafront Policy Area)</i>	The Forum, Elmer Square <i>(Elmer Square Policy Area)</i>
Clifftown Church/Studios, Nelson Street <i>(Clifftown Policy Area)</i>	The Kursaal, Eastern Esplanade <i>(Central Seafront Policy Area)</i>
Park Inn Palace Hotel, Pier Hill <i>(Central Seafront Policy Area)</i>	The Pier <i>(Central Seafront Policy Area)</i>
Pier Hill Observation Tower and Lift, Pier Hill <i>(Central Seafront Policy Area)</i>	University of Essex, Elmer Approach <i>(Elmer Square Policy Area)</i>

Porters, Southchurch Road (outside of the SCAAP boundary)	University of Essex Student Accommodation, London Road (Elmer Square Policy Area)
Prittlewell Chapel, North Road (Victoria Gateway Neighbourhood Policy Area)	

New development should not compete with existing landmarks in terms of bulk or height, and views of these buildings should not be compromised by new development.

The following (Table 3) have been identified as potential locations for new landmark buildings and features within Southend Central Area, as detailed in the relevant Policy Areas and Opportunity Sites. These are also depicted on the Policies Map.

**Table 3: Potential Locations for New Landmarks**

Opportunity Site (PA7.1): Tylers Avenue (Tylers Policy Area)
Opportunity Site (PA8.1): Victoria Avenue (Victoria Gateway Neighbourhood Policy Area)
Central House, Clifftown Road (Clifftown Policy Area)
Central Seafront Policy Area, including in particular Opportunity Site (CS1.2): Seaways, Opportunity Site (CS1.3) Marine Plaza, and Opportunity Site (CS1.4): New Southend Museum.

Where considered appropriate in principle, development proposals for new landmark buildings and landmark features within Southend Central Area should demonstrate a coherent design approach, based on an understanding of the character, form and function of the surrounding townscape. Opportunities to enhance the setting of landmark buildings with improvements to the public realm, provision of open space, will be encouraged in order to retain views, enhance way-finding and to reinforce a sense of place.

Table 4 set out below and the Policies Map depicts the location of existing and potential Landmarks and Landmark Buildings by Policy Area.

<b>Policy DS3: Landmarks and Landmark Buildings</b>
<p>1. The Council, through its role in determining planning applications, masterplanning, and other initiatives, will seek to conserve landmarks and landmark buildings as identified in Table 2 and Appendix 3 from adverse impact by:</p> <ul style="list-style-type: none"> <li>a. encouraging the provision of open spaces and public realm improvements which provide views to landmarks or landmark buildings or enhance their setting;</li> <li>b. resisting adverse impacts of new development by virtue of excessive height, massing or bulk;</li> <li>c. ensuring development proposals respect views, setting and character.</li> </ul>
<p>2. The Council will support and encourage the creation of new landmarks in the areas identified within Table 3, where development proposals must demonstrate that:</p> <ul style="list-style-type: none"> <li>a. design, detailing and use of materials are of exceptional quality and interest and will help to reinforce local character and distinctiveness;</li> <li>b. the location would provide a focal point for an existing vista/sight line or generate</li> </ul>

- a new one;
- c. the proposals do not adversely affect the amenity of local residents; and
- d. the proposals do not harm the setting of nearby heritage assets.

<b>POLICY LINKAGES - LANDMARKS</b>	
<b>CORE STRATEGY DPD</b> <b>Strategic Objective:</b> 14	<b>Policies:</b> KP2, CP4
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM4, DM5, DM6
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 2, 3, 7	<b>Policies:</b> DS5, DS2, PA1, PA3, PA4, PA6, CS1, PA8

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

**Table 4: Existing and Potential Landmarks and Landmark Buildings by Policy Area**

	Existing Landmarks within	Existing Landmarks near	Potential Landmarks Within	Potential Landmarks near
PA1 High Street	- Royal Hotel	- Royal Terrace - Seafront/ Estuary	N/A	- Central House - OS (PA7.1): Tylers Avenue - OS (CS1.2): Seaways
PA2 London Road	N/A	-UoE Student Accommodation - Central Museum	N/A	- OS (PA8.1): Victoria Avenue
Policy PA3 Elmer Square	- The Forum - SE College - UoE Building - UoE Student Accommodation	N/A	N/A	N/A
PA4 Queensway	N/A	- All Saints Church - Porters	N/A	N/A
PA5 Warrior Square	N/A	- All Saints Church - Porters	N/A	- OS (PA7.1): Tylers Avenue
PA6 Clifftown	- Clifftown Church/ Studios - Royal Terrace	- Royal Hotel - Seafront/ Estuary	- Central House	- OS (CS1.3): New Southend Museum
PA7 Tylers	N/A	N/A	- OS (PA7.1): Tylers Avenue	- OS (CS1.2): Seaways
CS1 Central Seafront	- Adventure Island - Cliff Lift - Cliffs Pavilion - Park Inn Palace Hotel - St John's Church - The Kursaal - The Pier	- Royal Hotel - Royal Terrace	- OS (CS1.2): Seaways - OS (CS1.3): Marine Plaza - OS (CS1.4): New Southend Museum	- OS (PA7.1): Tylers Avenue
PA8 Victoria Gateway	- Central Library (former) - Central Museum - Civic Centre - Prittlewell Chapel - Swan Hall	- St Mary's Church	- OS (PA8.1): Victoria Avenue Office Area	-London Road
PA9 Sutton Gateway	N/A	- All Saints Church	N/A	N/A

		- Porters		
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## 4.10 Flood Risk Management and Sustainable Drainage

Southend-on-Sea Borough Council, with the Environment Agency and Anglian Water Services Ltd. (Anglian Water) has formed a Local Flood Risk Management Partnership. The aim of this partnership is to work together to manage local sources of flooding.

The Core Strategy establishes a need to focus development within Southend Central Area, including the central seafront. The Southend Surface Water Management Plan (SWMP) and Strategic Flood Risk Assessment (SFRA) indicate that areas within the SCAAP are at risk from tidal and surface water flooding. A Level 1 SFRA was completed for Southend in September 2010 and a Level 2 SFRA in November 2010.

The Council has prepared a Local Flood Risk Management Strategy (LFRMS). The LFRMS outlines the priorities for local flood risk management across the Borough and provides a delivery plan to manage the risk over the next six years. The LFRMS complements and supports the National Flood and Coastal Erosion Risk Management Strategy published by the Environment Agency which outlines a National framework for flood and coastal risk. The SWMP, SFRA and LFRMS are available on the Council's website.

The extent of tidal flooding is limited to the Central Seafront Policy Area, Environment Agency Flood Zones 3a (higher risk) and Flood Zone 2 (lower risk). The SFRA indicates that sea levels are projected to rise so that more areas within the Central Seafront Policy Area will become increasingly affected by flooding over time.

To address this, the Essex and South Suffolk Shoreline Management Plan (2010) establishes an approach to hold the existing line of flood defence within the Central Area, which includes taking account of the effects of climate change. The Council will promote and help to deliver this strategic flood defence for the Central Area. It will do this by seeking Community Infrastructure Levy contributions from developers as well as seeking other sources of private sector and Government funding.

Given the long term timescales for implementing a strategic flood defence, the planning of individual new development sites also need to take into account the flood risk hierarchy as follows:

- Assess - a site specific flood risk assessment (FRA) may be required.
- Avoid (higher) flood risk areas – the Core Strategy establishes the need for new development within the SCAAP area. The sequential test will be applied within two separate areas: the Central Seafront Policy Area; and the remainder of the SCAAP area. The sequential test will also apply within individual Opportunity Sites.
- Substitute - more vulnerable uses should be located within parts of the development site at less risk of flooding. This will be balanced where necessary alongside other planning, design and deliverability objectives.
- Control and Mitigate – this will be a proportionate response taking account of the delivery of a strategic flood defence in the longer term, and the residual risk (that

the defence is breached or overtopped). This will ensure that individual developments achieve an appropriate degree of safety over their lifetime.

The Policies Map does not depict the areas at risk of flooding. This is contained in the Strategic Flood Risk Assessment and any future advice and/or new information that will be provided by the Environment Agency. This approach is necessary to enable the flood risk data to be updated when required. Users should contact both Southend Borough Council and the Environment Agency to confirm the most up to date information.

### **Site-specific Flood Risk Assessment**

A site-specific Flood Risk Assessment (FRA) will enable a developer to identify measures (if any) that are necessary to make a development safer and ensure it will not increase the risk elsewhere, to satisfy the Exception Test<sup>13</sup>.

In accordance with national planning policy a FRA will be required for development proposals:

- 1 hectare or greater in Flood Zone 1;
- for new development (including minor development or change of use) in Flood Zones 2/3, or in areas within Flood Zone 1 which have critical drainage problems; and
- where proposed development or change of use to a more vulnerable use class may be subject to other sources of flooding.

It is the responsibility of a developer to undertake the site-specific FRA, and they are strongly advised to agree the content with the Environment Agency prior to submission of it with the application. The FRA should be commensurate with the degree of flood risk posed to and by the proposed development, and take account of national planning practice guidance. Information from the SFRA should be used when developing the FRA.

### **Sustainable Drainage**

Sustainable urban drainage systems (SuDS) are designed to reduce the potential impact of new and existing developments with respect to surface water drainage discharges. SuDS try to replicate natural systems and use cost effective solutions with low environmental impact to drain away dirty and surface water run-off through collection, storage, and cleaning.

SuDS should be designed in accordance with the National Standards for Sustainable Drainage Systems (December 2011) guidance in the SuDS Manual (2007) published by Construction Industry Research and Information Association (CIRIA).

The 'core town centre' and central seafront policy area is characterised by a geology that exhibits low infiltration potential, although there are also surrounding areas where the

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<sup>13</sup> The Exception Test, as set out in paragraph 102 of the NPPF, is a method to demonstrate and help ensure that flood risk to people and property will be managed satisfactorily, while allowing necessary development to go ahead in situations where suitable sites at lower risk of flooding are not available.



geology offers greater permeability and potential for SuDs. The SCAAP area more widely is susceptible to localised surface water flooding, as indicated in the SFRA and Environment Agency online mapping. As such, all new development shall be drained via SuDs. It should be noted that SuDs must receive planning approval before construction is commenced and:

- For extensions and other single property developments the owner or developer will remain responsible for maintaining the system in good working order;
- For developments above single property scale, once the Council, as the Lead Local Flood Authority, is satisfied it has been constructed to an appropriate standard, the Council will adopt the SuDs for maintenance.

The design target will be to limit the discharge of the site run-off to green-field levels wherever possible. It may be found that this standard is not achievable, but any derogation will have to be approved by the organisation managing the receiving water system. For main rivers and ordinary watercourses this will be the Council, and for public surface water sewers Anglian Water.

**Developers are encouraged to consider the layout of their SuDS proposals prior to any other site masterplanning is undertaken, and to discuss them with the Council, as SuDs have specific requirements for location and construction.**

#### **Other Considerations**

In developing infrastructure schemes the Council will consider how these projects/improvements, such as highway, rail and public realm works, could be used to deliver flood risk/surface water management benefits. Similarly measures that would provide benefits to the environment, including the protection/enhancement of biodiversity, habitats, water quality and watercourses, will be considered.

<b>Policy DS4: Flood Risk Management and Sustainable Drainage</b>
<p>1. Development proposals which are or will be within a flood risk zone:</p> <ul style="list-style-type: none"> <li>a. Will be accompanied by a flood risk assessment that considers all sources of flooding;</li> <li>b. Will: <ul style="list-style-type: none"> <li>i. Locate more vulnerable uses in the area of the proposal least at risk; and</li> <li>ii. Provide a safe access and egress route away from the flood risk (i.e. to flood zone 1) during a design flood event;</li> <li>iii. Or provide a clear justification as to why these requirements are not practical, viable or appropriate in planning and design terms.</li> </ul> </li> <li>c. Will achieve an appropriate degree of safety over the lifetime of the development. The minimum safety standards are as follows: <ul style="list-style-type: none"> <li>i. For more vulnerable uses, the floor levels of habitable rooms will be above the design flood level, with an allowance for climate change*. Within Flood Zone 3 the floor level must be situated above the design flood level with allowance for climate change*, incorporating an allowance of at least</li> </ul> </li> </ul>

300mm for freeboard\*\*.

- ii. For all uses the development will:
  - 01. Remain structurally sound in an extreme flood event;
  - 02. Provide appropriate flood resistance / resilience measures to the extreme flood level;
  - 03. Not generate an increase in flood risk elsewhere;
  - 04. Provide a flood plan, which covers methods of warning and evacuation;
  - 05. Provide an appropriate safe refuge above the extreme flood level if criterion 2bii is not met.

*\* This is to ensure that floors must be set above the 1 in 200 annual probability event level plus climate change*

*\*\* Freeboard is an allowance to take account of: (i) physical processes that affect the defence level, that have not been allowed for in the design water level and (ii) adverse uncertainty in the prediction of physical processes that affect the defence level*

*Further technical information and definitions for this policy are included in Appendix 4*

2. For all new development, the Council will require new impermeable areas to be drained via SuDS. This will ensure the risk of surface water flooding is not increased onsite or elsewhere. Under no circumstances will surface water be permitted to discharge into a separate foul sewer or sewerage system. Surface runoff that cannot be discharged into the ground, a surface water body or a surface water sewer or local highway drain, must be discharged to a public, combined sewer system.

<b>POLICY LINKAGES – FLOOD RISK &amp; SUSTAINABLE DRAINAGE</b>	
<b>CORE STRATEGY DPD</b> <b>Strategic Objective: 15</b>	<b>Policies:</b> KP1, KP2, KP3, CP4
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM6
<b>SOUTHEND CENTRAL AAP</b> <b>Objective: 5</b>	<b>Policies:</b> PA1, PA2, PA3, PA4, PA5, PA6, PA7, CS1, PA8, PA9

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 4.11 Transport, Access and Public Realm

The level of regeneration and growth proposed for Southend Central Area will have an effect on the strategic transport network. The Transport, Access and Public Realm Strategy (Appendix 5), together with this Policy (DS5), seek to improve transport, access and connectivity, building on the approach set out within the Core Strategy and the Local Transport Plan (LTP).

This will be supported by a coordinated, sustainable public realm that addresses the principles of the Streetscape Manual SPD to create an attractive, inclusive environment for walking and cycling, improving the setting of, and links to, the Policy Areas and Opportunity Sites, and well-defined access points and gateways to the town centre, supported by accessible and well signed car parking servicing the entire SCAAP area. **Map 4: SCAAP Car Parking, Access and Public Realm** depicts the existing parking network, traffic information signs and information hubs, and as well as future public realm and access improvements to enhance connectivity within and through the Southend Central Area.

In respect of the public realm, there are instances in the Central Area where backs of buildings face onto the public areas, but have blank facades and are visually inactive thus creating a negative environment, particularly for pedestrians. Within the Central Area therefore, the Council will seek to encourage visually active frontages, particularly in specific locations identified on the Policies Map, in order to promote an enhanced urban environment.

The SCAAP has been used as a key evidence document to support the Council's priorities in the **South East Local Economic Partnership's (SELEP) Strategic Economic Plan and Growth Deal**, identifying funding priorities within Southend and the wider Thames Gateway South Essex (TGSE) sub-region.

There have already been positive outcomes from this, including funding for road and public realm improvements<sup>14</sup> within Southend Central Area to support the delivery of housing and economic growth, and as the SCAAP progresses it will be utilised to identify further opportunities for funding and partnership working. Specific schemes and projects are identified in the Implementation Section of this Plan.

There are also opportunities along access routes to allow other measures to be implemented that facilitate the use of sustainable transport modes, such as cycle lanes and bus priority measures, which will be implemented through the Local Transport Plan and associated strategies, together with linked improvements to the quality of the public realm. Improving signage will aid way-finding, and should encourage more linked trips, highlighting linkages between Southend Central Railway Station, Southend Victoria Railway Station and bus interchange, and the travel centre. **Map 5: SCAAP Public Transport** shows the public transport network within Southend Central Area.

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<sup>14</sup> South East Growth Deal

## Approach to Car Parking Management

Well informed and effective parking management techniques are likely to contribute towards an efficient transport network in and around the SCAAP area, ensuring that visitors are directed to convenient and accessible parking areas to access the many facilities and services that the town centre and central seafront areas have to offer and helping to mitigate the negative impacts of congestion. To assist with development of this approach, Southend Borough Council commissioned a Car Parking Study (the 'Study') for Southend Central Area.

The Study reviews current and future car parking provision in Southend Central Area. It sets out the performance of the existing parking network, and the potential impact of development proposals on the network. It also assesses the economic importance of parking in Southend Central Area based on a recent survey of shoppers. As a result it provides a good indication of modes of travel and associated spend within Southend Central Area. It reveals that all visitors, including those who travelled by car, bus, train, cycle or walk, contribute to the local economy by spending in Southend Central Area. It also shows that generally car users spend more but visit less often than other mode users.

The Study found that the Southend Central Area parking network rarely exceeds 85% occupancy. On the busiest day of 2015 (a Saturday in August) the peak network demand in Southend Central Area was 87% occupancy from 15:00 to 16:00. At all other times, demand was below 85%. It did identify however that there is a clear imbalance in the Southend Central Area parking network at periods of peak demand, with car parking south of the railway line experiencing overcapacity issues, while car parking north of the railway line has available spare capacity. Overall, the Study shows that parking areas to the south of Southend Central Area were busiest, and exceeded 85% occupancy on one in every ten days between May 2015 and April 2016.

The Study also considered the impact of the proposed indicative development outlined in the SCAAP Opportunity Sites, as well as approved planning applications (as of 1 April 2016), on car parking. It revealed that the additional parking expected to be provided by development in Southend Central Area, supported by existing network provision, is likely to accommodate future demand for car parking generated in the plan period up to 2021. However, this is indicative only and further work will be needed, in the light of the Study, to ensure that parking supply is carefully balanced between the car parks and development sites north and south of the railway line.

It is considered, therefore, that collectively the car parks located in Southend Central Area have the potential to serve both the Town Centre and Central Seafront, facilitating linked trips and increasing the potential for associated shared spend. Nevertheless it is also recognised that generally those car parks located south of the central railway line are better positioned to provide more direct and convenient access to the Central Seafront area.

There are currently around 2,600 publicly available car parking spaces south of the railway line serving both the seafront and southern parts Southend Central Area. As a result of the

peak capacity issues, as identified by the Parking Study, and to support the vitality and viability of the central seafront area, it is expected that there will be no net loss of public car parking south of the railway line, and, where viable and feasible, the Council will seek further provision in association with development

Furthermore the SCAAP will also seek to ensure that the existing traffic management network is as efficient and effective as possible to manage these periods of peak usage of the road network and car parks. Car parking demand within the SCAAP network will be managed through a combination of the following measures:

- ensure there is no net loss in car parking south of the central railway line, and to maintain overall capacity at a level that supports the vitality and viability of the SCAAP area, and enables the delivery of relevant Opportunity Sites;
- Seek to rebalance the discrepancies of parking supply and demand both sides of the railway line by acting on the outcome of the Study;
- application of Policy DM15 of the Development Management Document, particularly in respect to the requirements for transport assessments, travel plans and parking standards;
- the use of Smart technology providing up to date customer information and enhancing Variable Messaging Signs (VMS), detailed below, to direct visitors to the car parks with appropriate capacity within the overall car parking network for the SCAAP area;
- significant improvements to the public realm and car parks, to create a safe and pleasant environment in which to park a vehicle, in combination with well-signed routes, enhanced pedestrian links with good legibility to and from different parts of the SCAAP area, particularly between the Town Centre and Central Seafront;
- flexible, responsible and differential competitive pricing arrangements for car parking within the overall SCAAP network to provide consumer choice and options, taking account of demand, as well as convenient ways to pay for and top up parking;
- keep car parking capacity and demand under review to ensure that this capacity remains at a level to assist economic growth and regeneration;
- deliver strategic junction and transport improvements to improve vehicle circulation and the promotion and implementation of sustainable transport measures.

### **Variable Messaging Signs and Managing Car Parking and the Network**

An extension of the existing car park Variable Messaging Signs (VMS), or updated technology, is encouraged to direct drivers to the most convenient car park and avoid unnecessary circulating traffic. This may be particularly beneficial, together with the promotion of other sustainable travel modes, during seasonal visitor peaks where parking is at greater demand, such as the summer months and during December. Improving access to a number of the SCAAP car parks by managing the road network will provide enhanced access to and between SCAAP car parks and help reduce traffic circulating through the town centre and Central Seafront.

<b>Policy DS5 – Transport, Access and Public Realm</b>
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1. In order to improve access to, from and within Southend Central Area through the
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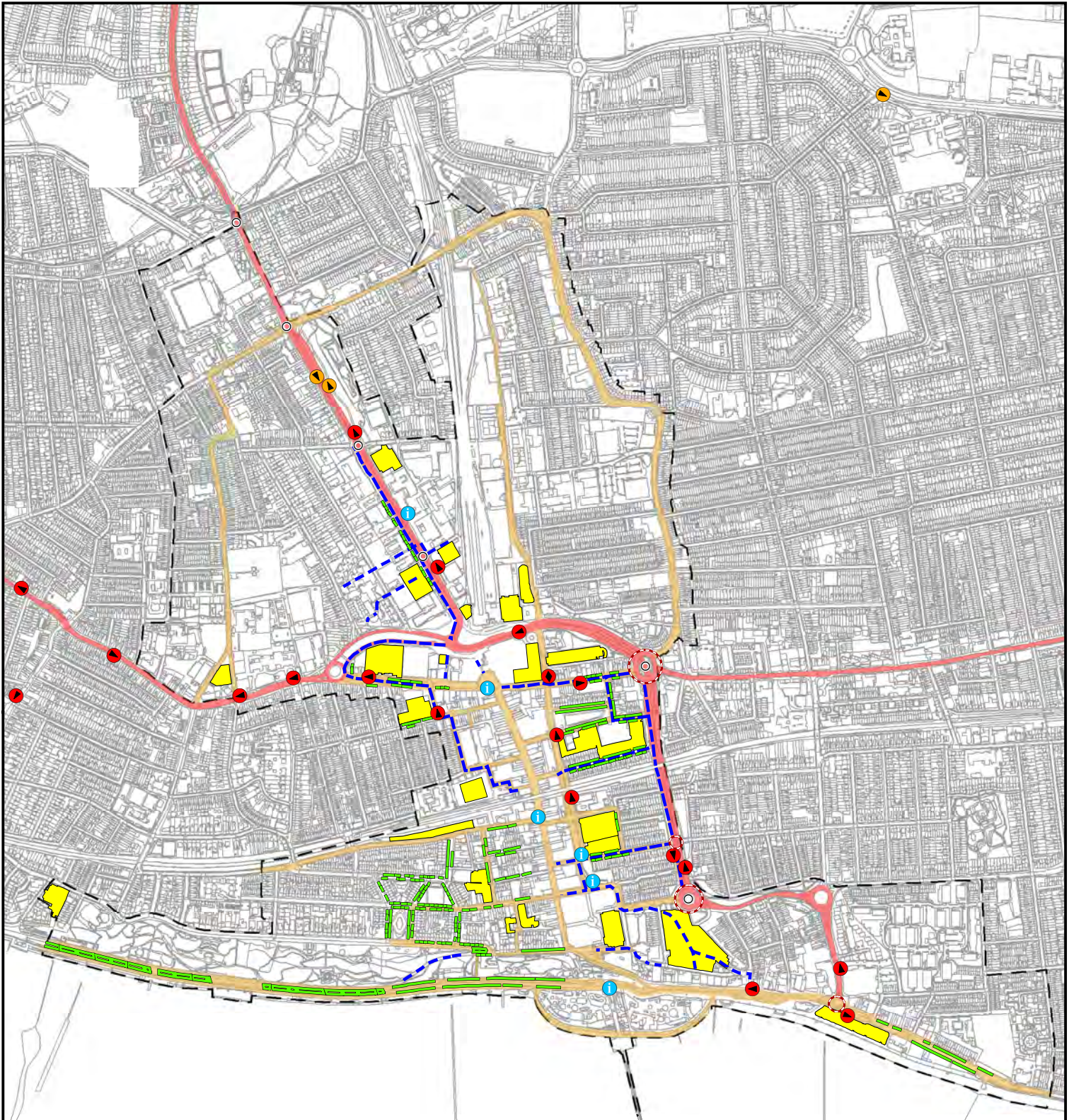
implementation of the Opportunity Sites and Policies within this Plan, the determination of planning applications and other initiatives, and partnership working, the Council will:

- a. Provide strategic junction improvements to improve vehicle circulation and to accommodate inward investment and growth;
- b. Seek to better manage demand on the road network leading to, from and within the SCAAP area safely, and balance this with the needs of other modes, particularly where this would give greater reliability to road users and priority to pedestrians, cyclists, public transport users and other vulnerable road users;
- c. Implement sustainable transport measures in line with the Transport, Access, and Public Realm Strategy as set out in Appendix 5;
- d. Have regard to Policy DM15 of the Development Management Document, particularly in relation to sustainable transport measures, travel plans, transport assessments, parking standards and the provision of facilities for charging electric vehicles and other ultra-low emission vehicles;
- e. Work with bus operators to encourage more users through a programme of bus priority measures, encouraging non-car trips to the SCAAP area, and enhance services later into the evening to serve the night time economy;
- f. Ensure bus priority measures enhance the A13 passenger transport corridor and the connections with London Southend Airport and the seafront, focusing on the Queensway dual-carriage way junctions at London Road, Southchurch Road and Seaways;
- g. Improve the quality of existing and promote the creation of new pedestrian and cycle priority routes to improve access to the SCAAP area, considering the potential for mixed-mode or segregated priority routes where appropriate;
- h. Improve gateway crossings for pedestrians at key locations on Queensway dual carriageway and routes into the SCAAP area from surrounding neighbourhoods;
- i. Encourage businesses to provide appropriate service and delivery arrangements and minimise their environmental impact; working with the freight industry and logistics to implement more efficient use of vehicles in terms of guidance, zoning and delivery timetables and this can be set out in a freight management plan;
- j. Review signage and implement an integrated signage strategy for vehicles, buses, freight, pedestrians and cyclists, including its integration with public art where possible, ensuring signage is kept to a minimum and appropriately scaled and sited to avoid cluttering the streetscape. Make full use of technology to facilitate the shift to sustainable transport modes;
- k. Ensure street lights are maintained, CCTV is prominently sited, and public transport and taxis operate after dark to help improve the perception of safety within Southend Central Area;
- l. In order to promote and reinforce local distinctiveness, ensure all public realm improvement works, including those outlined in the relevant Policy Areas, seek to provide a coordinated palette of materials, facilitate a reduction in street clutter, consider the needs of all users, including vulnerable and disabled users, the provision of additional seating where appropriate to provide resting places, and have regard to guidance within the Design and Townscape Guide and Streetscape Manual;
- m. Encourage visually active frontages through the installation of public art, green walls, well detailed signage, and appropriately placed windows and entranceways to

<p>enliven blank frontages, as defined on the Policies Map;</p> <p>n. Improve road safety and the quality of the environment by introducing traffic calming and related measures within predominantly residential areas as appropriate.</p>
<p>2. In order to support the vitality and viability of the SCAAP area the Council will:</p> <p>a. Maintain parking capacity within Southend Central Area, including for cars, motorcycles, taxis, bicycle and Blue Badge holder provision at a level that supports vitality and viability, whilst enabling the delivery of relevant opportunity sites;</p> <p>b. Ensure that there is no net loss in car parking south of the central railway line in Southend Central Area;</p> <p>c. Seek to rebalance the discrepancies of parking supply and demand both sides of the railway line by acting on the outcome of the Study and work with private car park owners and operators to ensure maximum usage of car park capacity;</p> <p>d. Assess the costs and benefits of an extension to the existing VMS scheme, or updated technology to enable real-time direction of drivers to the most appropriate car park for their destination based on proximity and available capacity, avoiding unnecessary circulating traffic, and by giving consideration to the management of the road network and access points to car parks;</p> <p>e. Improve the information available about the range of parking and sustainable travel options for visitors to Southend, including improvements to the Council website and through working with local businesses;</p> <p>f. Seek to relieve the pressure on the more well-used car parks at peak times and encourage use of less occupied car parks through a combination of dynamic signage, competitive pricing and pre-journey information;</p> <p>g. Ensure pedestrian routes to and from public car parks, railway stations and other public transport interchanges are direct, well-lit and signposted, benefiting from a high quality public realm that links well with main areas of interest;</p> <p>h. Ensure new and existing car parks add to the overall aesthetic quality of an area through such measures as landscaping, green walls, public art, pedestrian walkways and pedestrian permeability, as well incorporating innovative layouts to reduce visual impact and effect on key views within and to Southend Central Area.</p>
<p>3. The Council will work in partnership with key stakeholders to improve transport infrastructure and access in Southend Central Area, and to secure funding for transport and public realm improvements.</p>

<b>POLICY LINKAGES – TRANSPORT, PUBLIC REALM</b>	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 9, 10, 14	<b>Policies:</b> KP2, KP3, CP3, CP4
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM15
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 3, 4	<b>Policies</b> PA1, PA2, PA3, PA4, PA5, PA6, PA7, CS1, PA8, PA9
<b>Local Transport Plan 3 (refresh)</b>	<b>Policies:</b> LTP Policy 2, LTP Policy 4, LTP Policy 21

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*



**Southend Central Area Boundary**  
 [ - ] Southend Central Area Boundary  
**Parking**

- Off Street Payment Parking
- On Street Payment Parking

**Variable Message Signs**

- Parking Information
- Traffic Information

**Information**

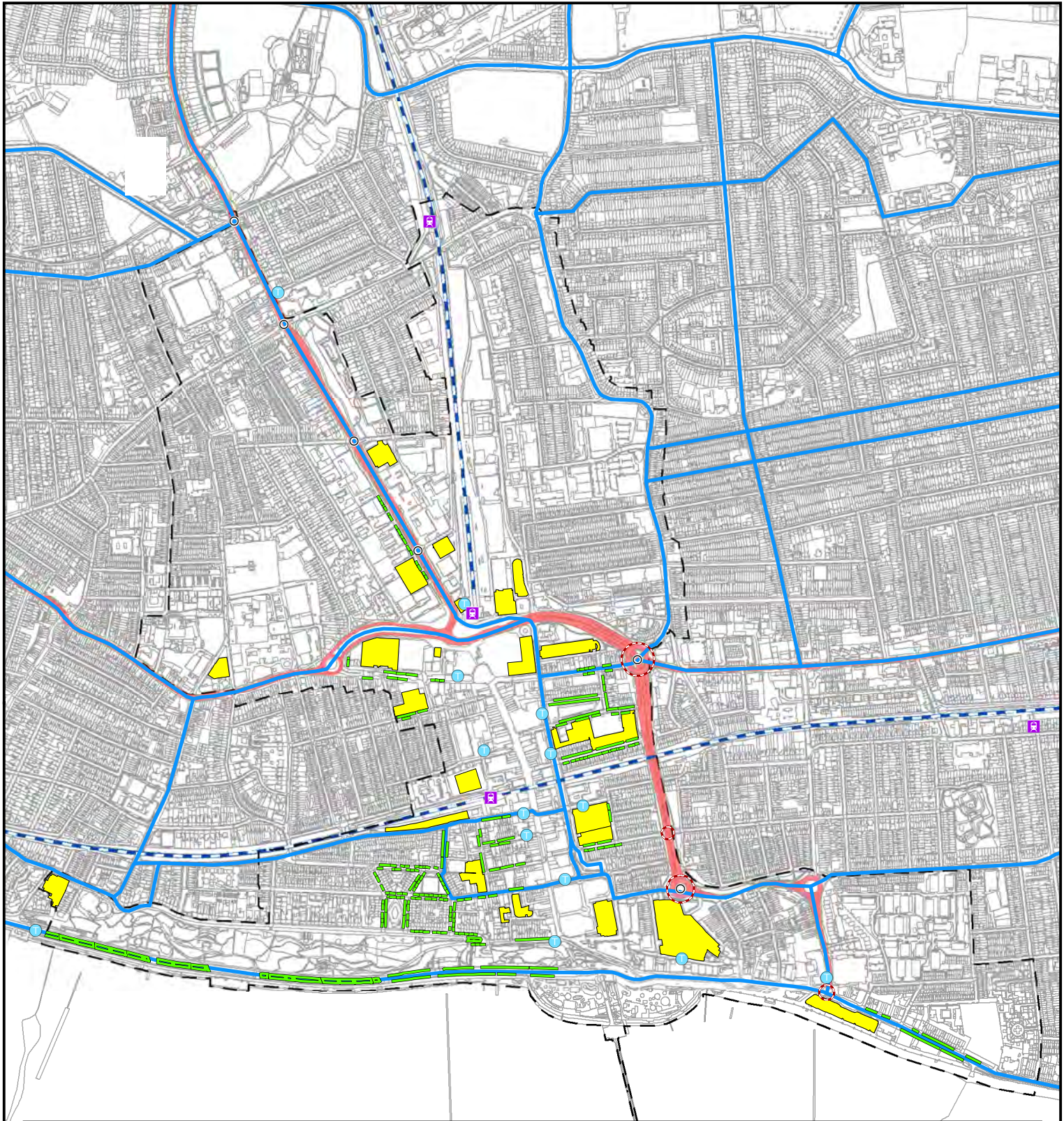
- Information Hubs For Pedestrians

**Movement and Activity**

- Key Public Realm Improvement
- Improved Pedestrian Routes
- Improved Gateway Access for Pedestrians
- Main Route Network - Crossing and Environmental Improvements
- Proposed Strategic Junction Improvement

*This map is intended to provide a visual overview only. Please refer to the accompanying SCAAP Policies Map for policy designation and proposals*





**Southend Central Area Boundary**

Southend Central Area Boundary

**Taxi Ranks**

Taxi Rank

**Parking**

Off Street Payment Parking

On Street Payment Parking

**Public Transport and Access**

Railway Station

Railway Line

Bus Stop

Bus Route - Road served at least 5 days a week

Improved Gateway Access for Pedestrians

Main Route Network - Crossing and Environmental Improvements

Proposed Strategic Junction Improvement

*This map is intended to provide a visual overview only. Please refer to the accompanying SCAAP Policies Map for policy designation and proposals*

## 4.12 Infrastructure Provision

### Central Area Infrastructure

Infrastructure will be required to support and, in some cases, enable the scale and location of growth set out in the Core Strategy. This includes the delivery of a significant number of new dwellings and jobs in the Central Area, which is expected to result in a notable increase in population. It is important that these residents have access to facilities and services and that these are in convenient locations to minimise the need to travel, creating sustainable communities.

The infrastructure types and projects vary, but include: education, health, transport, social and community facilities (such as museums, libraries and community buildings), flood risk management (see Policy DS4), utilities (such as water, gas and electricity), green infrastructure and open space/public realm (see section 4.7 and Policy DS5). All have been taken into account within the Southend Infrastructure Delivery Plan (IDP). Recent examples of new provision include a new Care Commissioning Group (CCG) facility at North Road, a new library at the Forum, the Royal Pavillion on the pier, the Beecroft art gallery and Prittlewell Chapel.

The IDP is produced to identify the range of infrastructure types and projects required to support growth. Importantly it identifies likely funding sources, delivery agents, timescales and priorities, and forms an important supporting document in relation to the Council's Community Infrastructure Levy (CIL) Charging Schedule. The document was produced through collaboration with key partners and infrastructure providers, and will be updated, where necessary, to reflect project delivery and change.

The mechanisms for requiring and encouraging infrastructure delivery include adopted planning policy, planning conditions, S106 agreements and planning contributions (via Southend Planning Obligation – A Guide to the Section 106 and Developer Contributions SPD); and the CIL. These mechanisms are broadly set out in the Core Strategy and CIL documents, and for this reason are not repeated.

Education: it is considered that the planned population growth in the Central Area will be accommodated via the expansion of existing schools, however, it is recognised that there may be a need for additional schools, and this will be kept under review. Consideration will be given to the provision of additional education facilities based on an assessment of expansion needs when and where appropriate development opportunities arise, particularly in the Sutton and Victoria Gateway Neighbourhoods.

Higher and further education is a key driver in providing economic and social benefits. The Council will support the expansion of higher education facilities in the Central Area through the continued development of South Essex College and The University of Essex's Southend campus to consolidate the role of Southend as an educational centre of excellence.

Health: the IDP identifies a need for new and enhanced GP floorspace provision in the Central Area, including extension, reconfiguration, refurbishment and re-equipping of surgeries.

Transport: opportunities identified in the IDP for local public transport measures (such as bus interchanges and stops, real time systems), local walking and cycling measures (such as cycle network upgrades and cycle parking), local traffic management and highway network measures (including minor junction modifications and local bus priority schemes), and local traffic control systems (upgrades to control systems and junctions).

Social and Community Facilities: all Policy Areas may provide opportunities for new and improved social and community facilities, particularly Victoria and Sutton Gateway Neighbourhoods. Regeneration in the Queensway and Warrior Square Policy Areas will also provide opportunity for further provision of social and community infrastructure, where feasible. The IDP identifies provision for a new museum on Western Esplanade (see Policy CS1 and Opportunity Site CS1.4).

Public Realm: future phases of the City Beach and Victoria Gateway public realm schemes are identified by the IDP as an infrastructure need within the Central Area (see Policy CS1). It also recognises the limited provision of children’s play equipment in the Central Area, identifying opportunity for future provision in Warrior Square Gardens.

Utilities: Water companies are subject to a statutory duty to ‘effectually drain’ their area. This requires them to invest in infrastructure suitable to meet the demands of projected population growth. It has been demonstrated to the satisfaction of the Environment Agency and Natural England, that Southend Waste Water Treatment Works has adequate capacity to accommodate the Core Strategy growth targets to 2021 and beyond. However, developers will need to consider the effect of their development on the capacity of the local waste water network. Proposals will need to demonstrate that they will not overload this.

There is statutory provision for developers to fund additional sewerage infrastructure required to accommodate flows from a proposed development. Adequate sewerage infrastructure should be in place to serve the area before development progresses. Developers should seek pre-planning advice from Anglian Water at the earliest opportunity to ensure appropriate provision is made. Further details and useful guidance can be found on Anglian Water’s website.

New development will require separate foul and surface water drainage/sewerage, as drainage of surface water to foul sewers is a major contributor to sewer flooding. Provision should be made for surface water to drain to SuDS systems (refer to Policy DS4).

In terms of water supply and sewerage, developers will be required to pay the infrastructure provider for any mains diversions resulting from development proposals.

POLICY LINKAGES – INFRASTRUCTURE	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 2, 4, 9, 13	<b>Policies:</b> KP1, KP2, KP3, CP3, CP4, CP6, CP7

<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM2, DM14
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 4, 5, 6, 8, 10	<b>Policies:</b> DS5, PA1, PA2, PA3, PA4, PA5, PA6, PA7, CS1, PA8, PA9
<b>COMMUNITY INFRASTRUCTURE LEVY</b>	Charging Schedule A guide to S106 and developer contributions Infrastructure delivery plan

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## Part C: Policy Areas and Site Allocations

### 5.1 Introduction

This section introduces the Policy Areas and their key Development Principles. Some of these Policy Areas contain allocated sites, comprising of Opportunity sites that establish parameters for development in specific locations. These sites are considered to be deliverable, at least in part, by 2021, the end of the SCAAP's plan period.

The boundary of each Policy Area and Site Allocation can be viewed on the Policies Map and Map 6.

Any planning application proposed in the SCAAP area on any site would be determined on its merit taking into account adopted and emerging planning policies and any other material considerations.

The policies are not explicit on the precise quantum of development, which leaves flexibility as development proposals come forward. Nevertheless, the scope for development, suitable uses and the deliverability of proposals is explored and presented in this section.

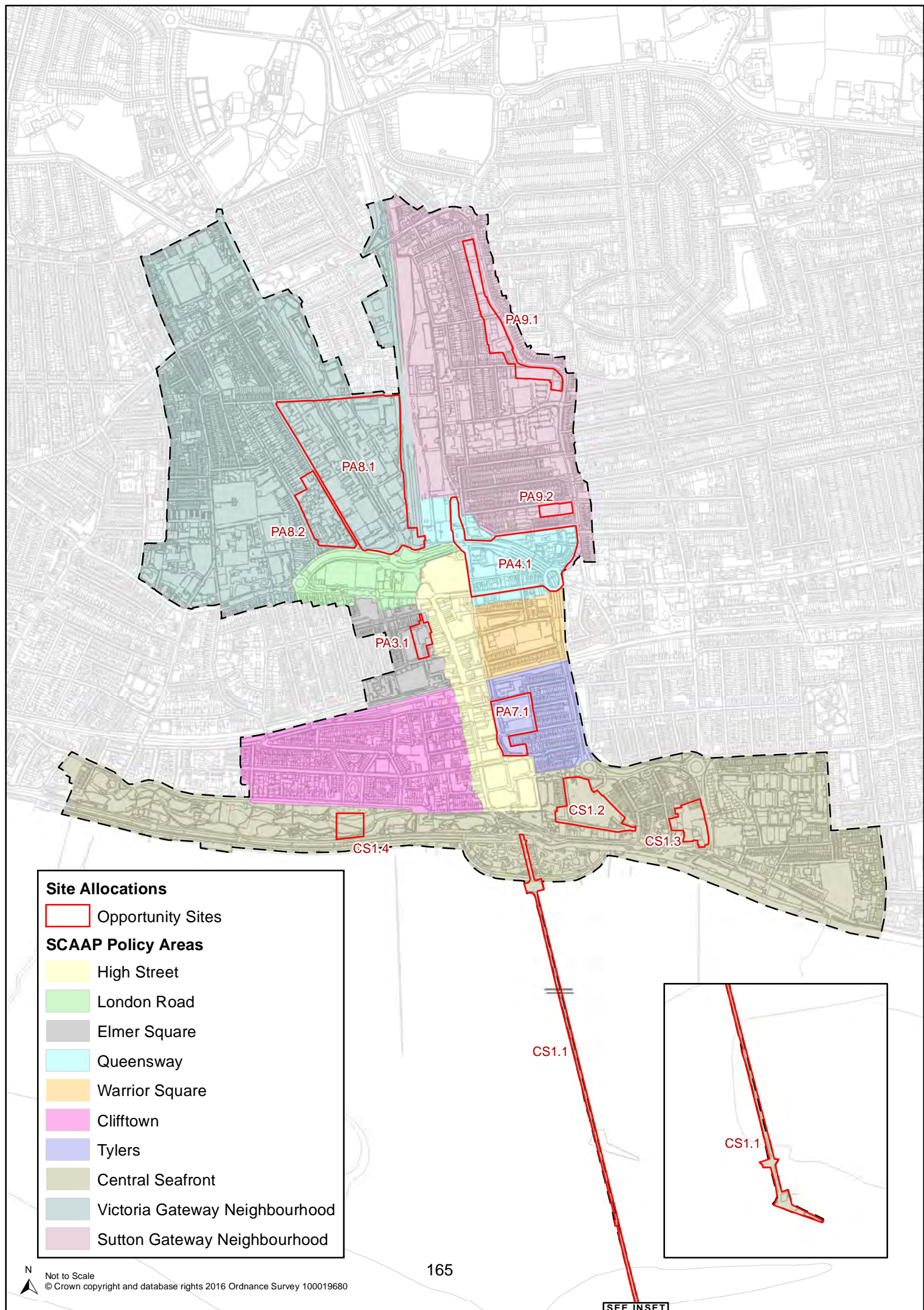
Applicants should demonstrate that they have considered and responded to the range of uses and site specific guidance identified in the policies in preparing their planning applications.

Informal planning guidance in the form of Master Plans or Development Briefs may be prepared for individual Policy Areas and Opportunity Sites, as necessary and appropriate, to provide greater clarity and further guide delivery and implementation.

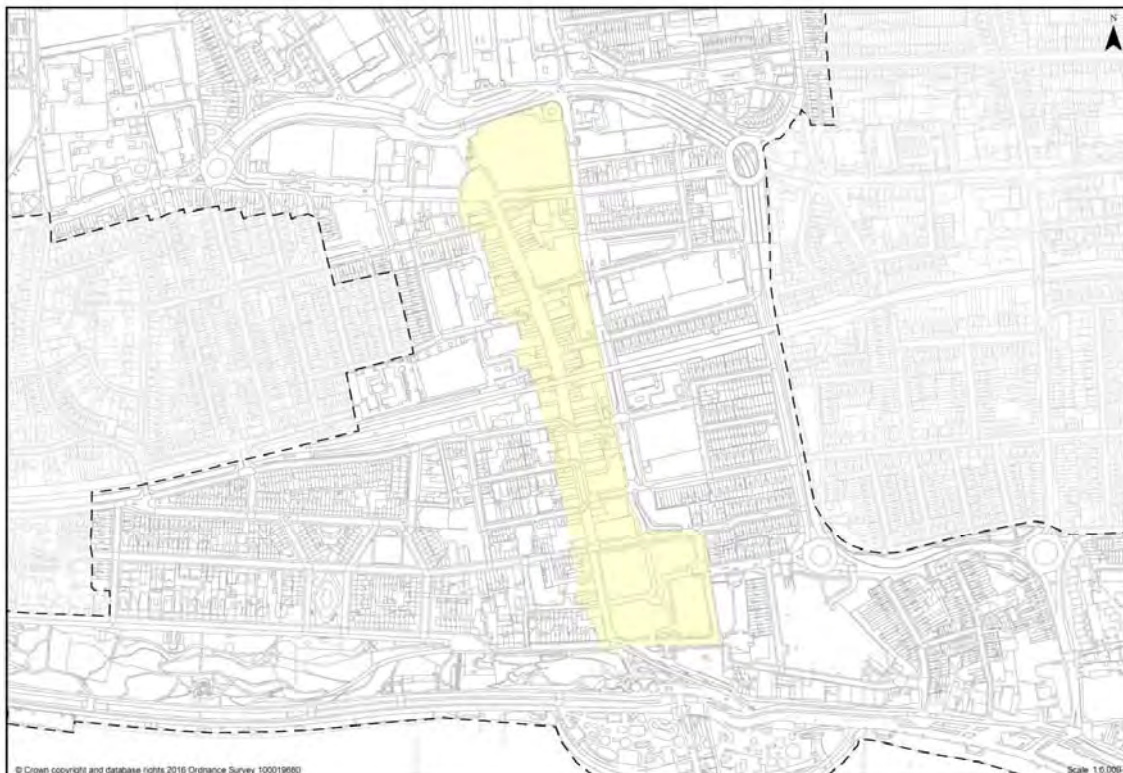
**Table 5: Opportunity Sites**

Opportunity Site		Proposed Land Use (summary)	Indicative Residential Capacity		Phasing
			Gross	Net	
PA3.1	Elmer Square Phase 2	Cultural and learning & supporting uses, including cafes, commercial workspace and studios.	-	-	Delivered by 2021
PA4.1	'Better Queensway' Project	Residential, social & community uses, secondary town centre uses, including small retail, cafes.	1200	760	Half of site delivered by 2021
PA7.1	Tylers	Retail, residential, public parking, transport interchange.	150	150	Delivered by 2021
CS1.1	Southend Pier	Leisure & cultural uses, including cafes, restaurants, small shops	-	-	Delivered by 2021
CS1.2	Seaways	Leisure, tourism, restaurants, cinema, hotel, public parking	-	-	Delivered by 2021
CS1.3	Marine Plaza	Residential, leisure, restaurants, local shops	282	278	Delivered by 2021
CS1.4	New Southend Museum	Cultural, leisure, public parking, supporting café, restaurant and shops	-	-	Delivered by 2021
PA8.1	Victoria Avenue	Residential, office, convenience retail, leisure, cafes, community facilities, public parking	1000	1000	Planning permissions delivered by 2021 (662 gross/net)
PA8.2	Baxter Avenue	Residential	500	240	Half of site delivered by 2021
PA9.1	Sutton Road	Residential, community uses	214	214	Planning permissions delivered by 2021 (92 gross/net)
PA9.2	Guildford Road	Residential, convenience retail	50	50	Delivered by 2021

# Map 6 - SCAAP Policy Areas and Opportunity Sites



## 5.2 High Street Policy Area



### Aims

The High Street, along with The Victoria and The Royals shopping centres, will form part of a vibrant and successful Town Centre Shopping Area, being the destination of choice within the sub-region.

A vibrant and viable Town Centre will be complemented by a variety of town centre uses, such as cafes and restaurants (particularly around new and improved public spaces), which enhance the experience for visitors, residents and workers and extend the economy throughout the day and into the evening.

The High Street will act as a spine for the Central Area, providing safe, quality pedestrian links to the adjoining policy areas, in particular the seafront.

### Overview

The High Street is almost 800 metres in length and contains multiple retail outlets interspersed with cafes, restaurants, coffee bars, banks and building societies. It is anchored in the north by The Victoria Shopping Centre and in the south by The Royals Shopping Centre where, via Pier Hill, there is a continuous link to the **Central Seafront Policy Area**. It is also linked to the London Road Policy Area and the opportunities identified within Policy



PA2 for street market provision and public realm enhancements will connect to the High Street.

The High Street, along with The Victoria and The Royals shopping centres, falls within the Town Centre Primary Shopping Area, which is a sub-regional comparison shopping destination and the first preference for all forms of retail and town centre uses within Southend.

<b>Policy PA1: High Street Policy Area Development Principles</b>
1. The High Street forms part of the Primary Shopping Area. The Council will support retail and Town Centre uses that contribute to the vitality and viability of the centre in accordance with Policy DS1.
2. Development proposals that would assist delivery of the following will be supported in principle: <ul style="list-style-type: none"> <li>a. a net increase in dwellings, including live-work units, above existing or new commercial development, where appropriate;</li> <li>b. the conservation and restoration of historic shopfronts (including frontages of townscape merit) in line with Policy DM5 of the Development Management Document;</li> <li>c. mixed-use development with active ground floor frontages;</li> <li>d. the provision of active frontage on the southern façade of The Royals Shopping Centre;</li> <li>e. the introduction of additional A3 cafes and restaurants, subject to the provisions of <b>Policy DS1: A Prosperous Retail Centre</b></li> <li>f. energy efficiency, as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy.</li> </ul>
3. The Council will seek to conserve existing landmarks and landmark buildings and ensure new development respects their views, setting and character, in line with <b>Policy DS2: Key Views</b> and <b>Policy DS3: Landmarks and Landmark Buildings</b> .
4. In order to enhance the High Street experience, the following public realm improvements will be promoted as development opportunities arise, addressing the principles of the Streetscape Manual where applicable: <ul style="list-style-type: none"> <li>a. transforming and enhancing the existing public space at Victoria Circus, enabling flexibility in its design and layout for public events;</li> <li>b. encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way;</li> <li>c. enhancing the public space to the High Street either side of the railway bridge, including a creative lighting scheme;</li> <li>d. further connect the Town Centre to the Central Seafront Policy Area: <ul style="list-style-type: none"> <li>i. by a series of multi-level public spaces, including an upper level public piazza (as referred to in Policy Area CS1.10.b);</li> <li>ii. through improved signage and public art provision;</li> </ul> </li> <li>e. maintain and improve the High Street as public space for pedestrians by providing quality landscapes and streetscapes, pursuing urban greening projects, including improved landscaping, green walls and roofs, tree planting, improved lighting and integrated signage, in order to create an attractive, coordinated public realm with</li> </ul>

opportunities for outside seating areas to cafes/restaurants to enliven the streetscene;

- f. pedestrianisation and enhancement of a number of the High Street’s inter-linking access (‘stub’) roads, supporting access to car parks, green space, retail and surrounding neighbourhoods;
- g. at key junction points, create a strong public realm to emphasise the intersection of east–west routes.

**POLICY LINKAGES – HIGH STREET**

<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 1, 4, 5, 8, 14, 15, 18	<b>Policies</b> KP1, KP2, KP3, CP1, CP2, CP4
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM5, DM7, DM8, DM15
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 4, 5, 6, 7, 8, 9, 10	<b>Policies:</b> DS1, DS2, DS3, DS4, DS5, PA2, PA3, PA4, PA5, PA6, PA7, CS1, PA8, PA9

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

### 5.3 London Road Policy Area



#### Aims

The London Road Policy Area will be a vibrant gateway to the Town Centre, providing high quality office space, shops, cafes/restaurants, and homes above street level. This will be complemented by public realm enhancements, public art installations, creative lighting, and landscaping to create a pedestrian-priority public space with opportunities for outside seating to cafes/restaurants.

Permeability for pedestrians and cyclists will be enhanced at this key gateway and interchange to the town centre, including improved links to/from the Victoria Gateway Policy Area as part of the 'Victoria Gateway' public realm improvement scheme.

#### Overview

The Policy Area, as defined on the Policies Map, is one of the main entry points to the Town Centre and High Street and is characterised by a varied architecture dominated by the Odeon Cinema and Sainsbury's food store, both of which present blank, 'inactive' frontages onto the Queensway dual carriage way. There is opportunity for this to be addressed within any development proposals to ensure that active frontages are created, including opportunities for public art and landscaping as well as enhancement to the facades of existing buildings.

London Road has transformed in recent years into a vibrant area of cafés and restaurants that, together with the cinema and shops, provides day time and evening attractions.

Consequently there is significant pedestrian activity in the area including movements to and from the High Street. Forming part of a future phase of the Victoria Gateway scheme, there is opportunity to improve pedestrian and cycle links to and from London Road and Queensway dual carriageway and the Queensway Policy Area, public transport interchanges, and links with Elmer Square Policy Area.

There is a principal taxi rank in London Road which serves the Town Centre. The width of the road also encourages the circulation of private cars; there is potential for this to be established as a pedestrian-priority space, retaining provision for the taxi rank within the Policy Area in consultation with taxi providers.

#### **Policy PA2: London Road Policy Area Development Principles**

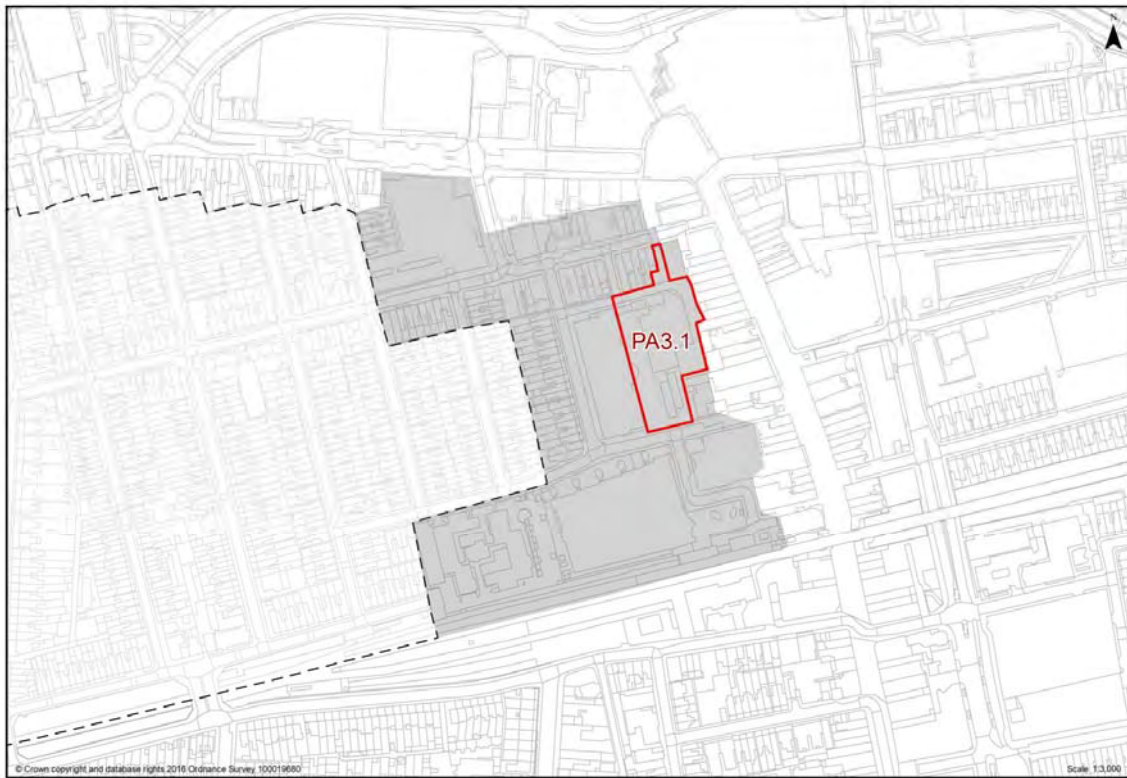
1. The Council, through its role in determining planning applications and other initiatives, will:
  - a. promote Town Centre uses that deliver the aims for the Policy Area and support the vitality and viability of the town centre, including leisure, retail floorspace, cafes/restaurants, office and residential (to upper floors);
  - b. encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way;
  - c. support development proposals that promote the provision of a street market on a new pedestrianised length of London Road, which connects well with the High Street;
  - d. support the provision of additional Higher and Further Education facilities based on an assessment of the expansion needs of the University of Essex and South Essex College;
  - e. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy;
  - f. seek to ensure that new development respects the views, setting and character of landmark buildings located near to the Policy Area, in line with **Policy DS3: Landmarks and Landmark Buildings**.
2. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual where applicable:
  - a. pedestrianisation of the eastern end of the London Road, linking with the pedestrianised High Street, to provide an attractive, coordinated public realm with opportunities for outside seating areas to cafes/restaurants to enliven the streetscene, with priority also given to cyclists;
  - b. relocation of taxi facilities to west of College Way on London Road, its location and facilities to be determined in consultation with taxi providers;
  - c. short and direct access maintained to the University Car Park, College Way, via London Road;
  - d. junction improvements at Queensway dual carriageway / London Road to improve pedestrian and cycle crossing;
  - e. provision for new/ improved pedestrian/ cycle priority link as identified on the Policies Map;
  - f. pursue urban greening projects including improved landscaping, green walls and roofs, and tree planting, with new/ improved open space at Queensway dual carriageway;
  - g. seek provision of public art and integrated signage that combine with more traditional signage to signal entry to the Town Centre from Victoria Gateway and facilitate clear

way-finding to improve legibility and pedestrian access, together with further improvements to the public realm and accessibility as part of the Victoria Gateway public realm improvement scheme.

<b>POLICY LINKAGES – LONDON ROAD</b>	
<b>CORE STRATEGY</b> <b>Strategic Objectives:</b> 1, 4, 5, 8, 14, 15	<b>Policies:</b> KP1, KP2, KP3, CP1, CP2, CP4
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM7, DM8, DM15
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 4, 5, 6, 7, 8, 9, 10	<b>Policies:</b> DS4, DS5, PA1, PA4, PA8, PA9,

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 5.4 Elmer Square Policy Area



### Aims

Elmer Square will be the heart of the educational hub in Southend, providing state of the art library and learning facilities set within a quality public realm with integrated pedestrian links with the High Street and London Road policy areas.

Ground floors of all buildings will be visually active, encouraging stronger engagement with the public space, the Forum and academic buildings and reinforcing Queens Road as a secondary shopping area.

Future development in this area will be well-designed, innovative and complementary to the Forum and phased to meet the expansion and growth needs of the University of Essex and South Essex College.

### Overview

Elmer Square is on the edge of the Town Centre in an area of transition between the High Street to the east and the residential areas to the west. The area contains a state of the art library 'The Forum' and learning facilities, and is complemented by the adjacent higher and further education campuses. Queens Road to the north of the Policy Area contains a range of popular cafes and small scale retail units and provides an important and vibrant link with the High Street.

The University Square student accommodation is situated at the northern extent of the Policy Area, providing a distinctive landmark, as well as high quality public car parking that will be maintained. The Forum, South Essex College and University of Essex Buildings are also identified as landmark buildings in Policy DS3.

Significant improvements have been made to the public realm; nevertheless, the backs of buildings on the High Street that front onto the public open space and associated service area have a detrimental visual impact.

**Opportunity Site (PA3.1): Elmer Square Phase 2** provides the opportunity to develop additional educational facilities, of a high quality design that complements the Forum and reinforces key links through the site.

**Policy PA3: Elmer Square Policy Area Development Principles**

1. The Council, through its role in determining planning applications and other initiatives, will:

- a. promote educational and supporting uses that deliver the aims of the Policy Area;
- b. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy.
- c. seek to conserve existing landmark buildings and ensure new development respects views to and from them, their setting and character, in line with **Policy DS3: Landmarks and Landmark Buildings**.

2. Planning applications for new student accommodation should be accompanied by a long term management and maintenance plan, to ensure the development has a positive impact on local amenity and the local environment for the lifetime of its use.

3. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual where applicable:

- a. creation of new shared space along Queens Road between Elmer Avenue and the High Street;
- b. provision for new/ improved pedestrian/ cycle priority link as identified on the Policies Map;
- c. provision of public art and integrated signage and artwork to building elevations that combine with more traditional signage to signal entry to the Town Centre, the High Street and Southend Central Railway Station, where appropriate, and enable clear way-finding;
- d. pursue urban greening projects including improved landscaping, green walls and roofs, and tree planting.

4. The following Opportunity Site, as identified on the Policies Map, is allocated primarily for educational use (Use Class D1):

Site Reference	Site Name	Planning Status*	Indicative number of dwellings	Other potential use classes
PA3.1	Elmer Square Phase 2	NA	N/A	D1, A3, B1

\*Planning Status as of April 2016. NA = New Allocation

i. Within **Opportunity Site (PA3.1): Elmer Square Phase 2**, planning permission will be granted for educational and supporting uses, such as commercial studios and workspace

and cafes/ restaurants to complement Phase 1 and to further reinforce Elmer Square as the heart of the learning hub.

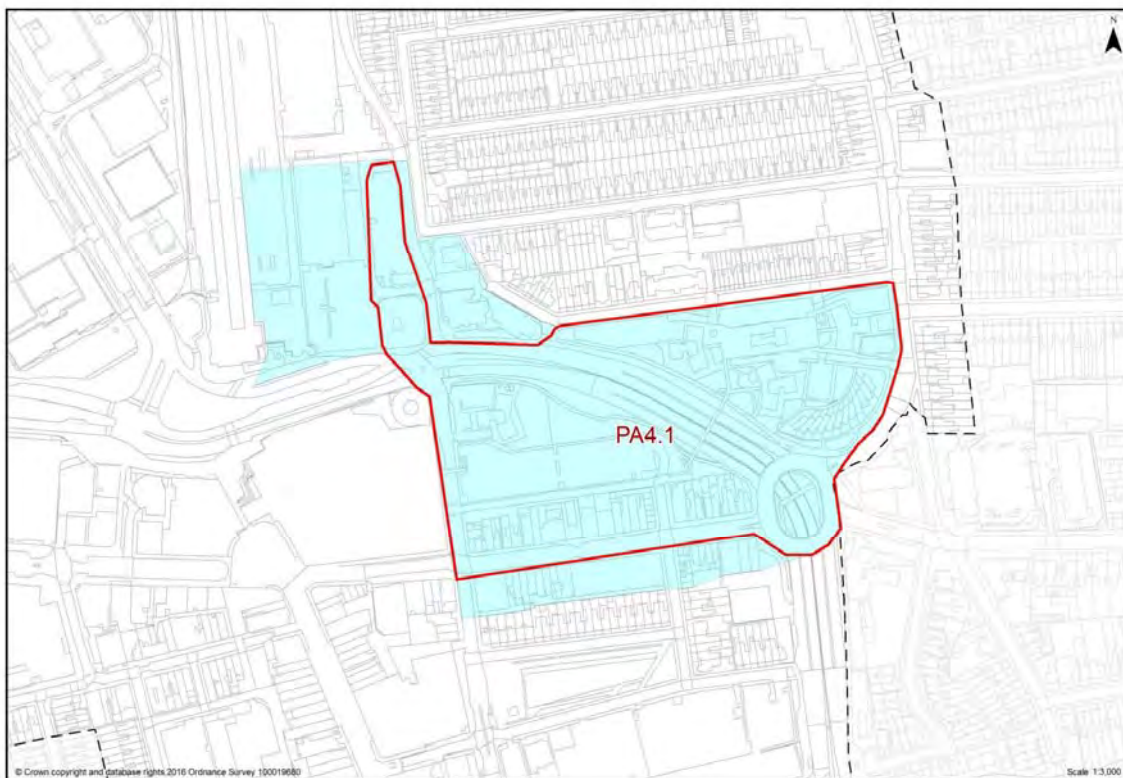
ii. Opportunities to improve the visual appearance of the rear of buildings on the High Street that front onto the public space, and associated public realm enhancements including surfacing, lighting, landscaping and the continued provision of high quality outside public space to complement Phase 1 will also be promoted within Opportunity Site PA3.1. Efforts to further connect this area and create new vistas with the high street area will be encouraged.

<b>POLICY LINKAGES – ELMER SQUARE</b>	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 2, 4, 13, 15, 18	<b>Policies:</b> KP1, KP2, KP3, CP4, CP6
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM15
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 4, 5, 6, 10	<b>Policies:</b> DS3, DS4, DS5

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*



## 5.5 Queensway Policy Area



### Aims

The development of the Queensway Policy Area will be based on a long term strategy that secures the regeneration of the area to create a balanced community, supported by social and community infrastructure, and complemented by active ground floor uses to Chichester Road and the secondary shopping frontage along Southchurch Road, an enhanced public realm and landscaping as well as well-defined public and private green open spaces.

It will be residential-led and create a vibrant, sustainable neighbourhood with a distinctive character and innovative housing typologies, providing opportunities for a range of building heights and densities suitable to the location. The development will be an exemplar of successful design-led estate regeneration, based on a partnership approach.

Queensway dual carriageway will be transformed, ensuring that vulnerable road users are prioritised and that the area is safely accessible by foot and bicycle. The environment will be more user friendly with appropriately sited pedestrian and cycle crossings.

Development will reinforce Southchurch Road as a secondary shopping area and provide new employment opportunities.

### Overview

The Queensway Policy Area is dominated by a swathe of 1960s residential tower blocks. These provide redevelopment potential (including the re-provision of social housing) and

associated opportunities to enhance the setting of All Saints Church (locally listed) and Porters (Grade 1 listed) heritage assets (situated just outside the SCAAP boundary). Locations such as Coleman Street will provide opportunity to re-establish urban grain (i.e. the physical form of street patterns and blocks) by providing residential development that complements existing dwellings in the streetscene.

To the north-west of the Policy Area is a retail outlet occupied by The Range on a long term lease, and the cleared site of the former Focus Youth Centre now utilised as a public car park. Given the prominence of this site if a redevelopment opportunity was to come forward there may be potential for it to be integrated within a comprehensive scheme for the redevelopment of the adjacent residential tower blocks (see Opportunity Site PA4.1 'Better Queensway' project below).

To the south of the Policy Area is Southchurch Road (secondary shopping frontage) containing a mix of older, low rise, buildings that have a poor visual appearance. Even so, Southchurch Road plays an important role as a secondary retail and commercial frontage, and is currently a principal route for traffic entering the Town Centre from the east in order to use the car parks in and around the Chichester Road area.

Queensway dual carriageway bisects the Policy Area and acts as both a major highway approach to the Town Centre and a ring road around it. Its scale and design acts as a barrier between the Town Centre and its outlying neighbourhoods, despite this its verges are amongst some of the most significant green wedges in the Town Centre, but as green spaces they are not useable given their nature and there is opportunity to enhance these spaces.

Chichester Road currently provides access through the Queensway Policy Area to the **Warrior Square** and **Tylers Policy Areas** and a number of Town Centre car parks, as well as being the major bus access to the Travel Centre in the Tylers Policy Area. The environmental quality of Chichester Road is very poor. Chichester Road is widely used by vehicles accessing the town centre car parks. Opportunity exists to enhance pedestrian links to the High Street Policy Area centre via Queensway and Chichester Road.

#### **Opportunity Site (PA4.1): 'Better Queensway' Project**

The Council has initiated the 'Better Queensway' project, which aims to regenerate the area that includes the large residential tower blocks and the site of the former Queensway House, now in temporary use as a public car park, to provide for modern purpose built social housing set within an enhanced local environment.

The project will improve this part of the Queensway Policy Area to form an integrated part of central Southend. It is envisaged that it will be an attractive area with a community focus that people want to visit, spend time in and live due to its improved and welcoming surroundings.

Regeneration and development of the 'Better Queensway' project will be the catalyst for wider regeneration in the Central Area, broadening the demographic and increasing the

number of residents living in the Central Area, generating more activity and demand for local services. Given the scale and nature of the redevelopment project it is envisaged that approximately half of the site will be delivered during the SCAAP plan period (by 2021). The site, including any outstanding phases of development, will be reviewed during the Local Plan preparation process.

The initiative provides significant opportunity for redevelopment and regeneration to re-establish the historic urban grain and uplift the image of the area, complemented by enhancements to the carriageway and public realm and re-provision of social housing.

The project also provides the opportunity to provide for improved landscaping, and the provision of a new park, the 'Queensway Urban Park.'

#### **Policy PA4 : Queensway Policy Area Development Principles**

1. The Council, through its role in determining planning applications, masterplanning, and other initiatives, will:

- a. promote residential and supporting uses that deliver the aims for the Policy Area;
- b. support well-designed, sustainable buildings appropriate to the location in terms of use, scale, massing and detailed design and contribute positively to successful place making;
- c. ensure that development will not result in a net loss of affordable housing provision, which includes the re-provision of social housing, as part of the regeneration of the area;
- d. support proposals for well-designed refurbishment or redevelopment of retail and commercial frontages to Southchurch Road, that are compatible with the Secondary Shopping Frontage designations;
- e. promote the provision of new social and community infrastructure, which may include facilities such as community centres and clubs, doctor and dental surgeries, and nurseries and childcare provision;
- f. support new commercial development and community uses that provide activity to ground floor including offices to upper floors, along Essex Street and Chichester Road where they contribute to the aims for the policy area;
- g. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy;
- h. ensure that new development respects the views, setting and character of all designated and non-designated heritage assets, including listed and locally listed buildings in line with Policy DM5 of the Development Management Document, and landmark buildings located near to the Policy Area, in line with **Policy DS2: Key Views** and **Policy DS3: Landmarks and Landmark Buildings**.

2. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual where applicable:

- a. improvements to the streetscape at Chichester Road opposite Victoria Shopping Centre to enhance the setting of new and existing buildings and improve the pedestrian experience, including improved pedestrian crossing points;
- b. improve connectivity and legibility to aid way finding and create a high quality pedestrian and cycling environment, enhancing links with the High Street, Elmer

Square, Warrior Square, Victoria Station, Victoria and Sutton Gateway Neighbourhood Policy Areas;

- c. provision of public art to enhance the urban environment, particularly to the Queensway carriageway frontage and at the junction with Sutton Road;
- d. provision for new/ improved pedestrian/ cycle priority link as identified on the Policies Map, together with improved crossings and gateway improvements at the Queensway/Sutton Road Junction, Queensway/Short Street/Chichester Road junction in association with capacity requirements for development on the Better Queensway Opportunity Site (PA4.1);
- e. Urban Greening, including improved landscaping, green walls and roofs, and tree planting and establish the Queensway Urban Park, which sensitively addresses and enhances the setting of Porters and All Saints Church, and links well with Warrior Square Policy Area;

3. The following Opportunity Site, as identified on the Policies Map, is considered suitable primarily for residential development, supported by social and community uses and retail provision:

Site Reference	Site Name	Planning Status*	Indicative number of dwellings	Timescale for delivery
PA4.1	'Better Queensway' Project	NA	1200**	D1, A1

\*Planning Status as of April 2016. NA = New Allocation

\*\*Half of site assumed to be delivered during SCAAP plan period (i.e. by 2021)

i. Within Opportunity Site (PA4.1): 'Better Queensway' Project, planning permission will be granted for comprehensive redevelopment of this site to transform it into a modern social housing-led development with supporting community and secondary town centre uses set within an enhanced local environment. The development will:

- a. re-establish the historic urban grain of the area;
- b. fully integrate with the surrounding area through the provision of pedestrian and cycle routes to improve access and linkages;
- c. incorporate climate change mitigation and sustainability measures;
- d. provide for comprehensive landscaping through the creation of linked public green space and the Queensway Urban Park;
- e. provide for new/improved open space fronting Chichester Road and at Coleman Street;
- f. provide for a comprehensive drainage system.

POLICY LINKAGES - QUEENSWAY	
<b>CORE STRATEGY DPD</b> Strategic Objectives: 3, 4, 6, 7, 10, 13, 14, 15, 18	<b>Policies:</b> KP1, KP2, KP3, CP1, CP4, CP6, CP8
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM3, DM7, DM8, DM15
<b>SOUTHEND CENTRAL AAP</b> Objectives: 1, 2, 3, 4, 5, 6, 8	<b>Policies:</b> DS1, DS2, DS3, DS4, DS5, PA1, PA3, PA5, PA8, PA9,

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken*

## 5.6 Warrior Square Policy Area



### Aims

Warrior Square will provide a tranquil contrast to the vibrant High Street area, focused on the predominantly small-scale residential character of the Conservation Area and the green quality of Warrior Square Gardens.

All buildings will be well designed to sensitively respect the setting of the Warrior Square Conservation Area in terms of detailing, scale and massing. Landscaping, tree planting and other urban greening techniques will be employed within new development to complement the existing green character of the area.

Access to Warrior Square from neighbouring policy areas and residential neighbourhoods will be enhanced and a legible network of new / improved pedestrian/ cycle priority links will be formed.

### Overview

Development within the Policy Area will need to sensitively address the setting of the Conservation Area in terms of scale, massing and detailed design. Additional residential-led development with car parking and the potential for appropriate supporting uses such as office development, particularly fronting Chichester Road, and community uses would be appropriate for the area. New development also provides the opportunity to bring activity and natural surveillance to Warrior Square Gardens.

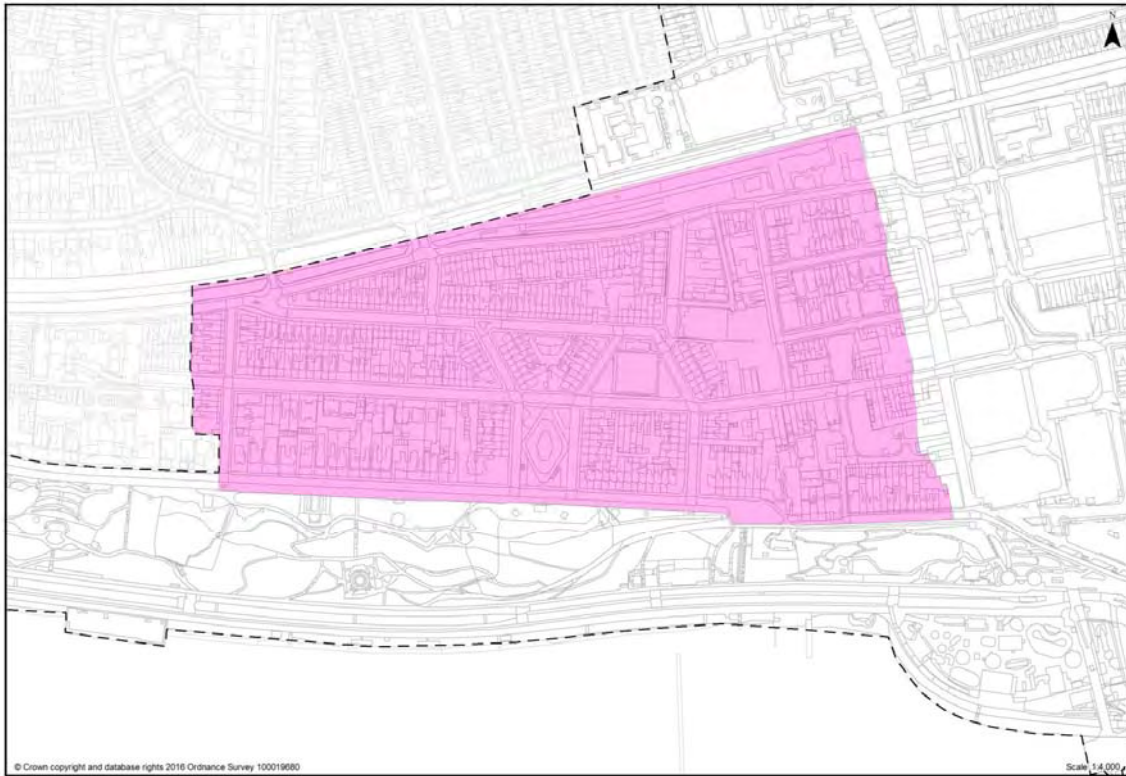
Chichester Road presents a main thoroughfare within the Policy Area yet the quality of the streetscape is poorly defined in part, and there is scope for enhancement to be made to the public realm, such as upgrading of the footway surfaces, tree planting and public art provision, as well as pedestrian and cycle links to and from the town centre and surrounding area, particularly the Queensway Policy Area via Chichester Road and Warrior Square East.

<b>Policy PA5 : Warrior Square Policy Area Development Principles</b>	
<p>1. The Council, through its role in determining planning applications and other initiatives, will:</p> <ul style="list-style-type: none"> <li>a. promote residential-led mixed-use development that re-establishes the urban grain and delivers the aims for the Policy Area, with active ground floor uses, including new community infrastructure such as doctor and dental surgeries, on Chichester Road with residential and offices above;</li> <li>b. conserve and enhance Warrior Square Conservation Area and its setting;</li> <li>c. reinforce the residential nature of Warrior Square East and Whitegate Road (east);</li> <li>d. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy.</li> <li>e. seek to ensure that new development respects the views, setting and character of landmark buildings located near to the Policy Area, in line with <b>Policy DS3: Landmarks and Landmark Buildings</b>.</li> </ul>	
<p>2. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual where applicable:</p> <ul style="list-style-type: none"> <li>a. maintain the environmental and design quality of Warrior Square Gardens and promote future public realm improvements and open space that respect and engage with the Gardens;</li> <li>b. provision for new/ improved pedestrian/ cycle priority link, as identified on the Policies Map, improving the access and linkages between Warrior Square Policy Area and Queensway, and appropriate crossing and footway improvements on Chichester Road as well as tree planting and other enhancements to the urban environment;</li> <li>c. environmental improvements to Queensway dual carriageway including planting to establish links to Queensway Urban Park and useable green spaces where appropriate;</li> <li>d. a restriction in the provision of hard landscaping, encouraging opportunities for soft landscaping to complement the character of the Gardens and assist drainage;</li> <li>e. public Art provision to buildings, public and private spaces.</li> <li>f. pursue urban greening projects, including improved landscaping, green walls and roofs, and tree planting.</li> </ul>	

<b>POLICY LINKAGES – WARRIOR SQUARE</b>	
<p><b>Core Strategy DPD</b>  <b>Strategic Objectives:</b> 1, 3, 4, 5, 6, 7, 9, 10, 13, 14, 15, 18</p>	<p><b>Policies:</b>            KP1, KP2, KP3, CP1, CP3, CP4, CP6, CP7, CP 8.</p>
<p><b>Development Management DPD</b></p>	<p><b>Policies:</b>            DM1, DM2, DM3, DM4, DM5, DM7, DM8, DM10, DM15.</p>
<p><b>Southend Central AAP</b>  <b>Objectives:</b> 1, 2, 3, 4, 5, 6, 7, 8, 9</p>	<p><b>Policies:</b>            DS4, DS5, PA1, PA4, PA7</p>

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 5.7 Clifftown Policy Area



### Aims

Clifftown will be a vibrant area, creating a lively setting for food, drink and small niche retail offer that is active throughout the day and into the evening, particularly in parts close to the high street.

The distinctive character and appearance of the Clifftown Conservation Area will be conserved and enhanced, and the setting of designated and non-designated heritage assets within the area will be respected. There will be much better access to information regarding the area's heritage.

The area will also have a strong cultural identity, capitalising on the fine grain of its historic streets, attractive character properties and links to the leisure and recreational resource of the Central Seafront.

Public realm improvements will enhance the setting of buildings and the experience for pedestrians and cyclists.

### Overview

The Policy Area is home to a variety of uses including small scale retail, food and drink premises, commercial, education and residential. There are opportunities to improve the promenade circuits, including linkages to the Cliffs and access to the Central Seafront Policy

Area, and to improve the retail and food and drink offer to reinforce the vibrancy of this area throughout the day and into the evening.

A significant proportion of the area is designated as Clifftown Conservation Area with associated concentrations of listed and locally listed buildings, and frontages of townscape merit closer to the High Street. The quality of these buildings and the historic fine grain of the area is one of its defining characteristics.

Noteworthy heritage assets within the Policy Area include: 1-15 Royal Terrace (Grade II listed), built in the 1870s as the first phase of the ‘New Town’, and Southend’s only surviving Georgian Terrace. These act as landmark buildings (see Section 4.9 of this Plan) within the Policy Area, aiding way-finding, occupying a visible location on top of the cliffs.

Southend Central Station, locally listed, is at present hidden away from the High street, with a very low quality forecourt and entrance which is dominated by cars. While public realm enhancements to the street in recent years have lifted the appearance of the area, there is potential to enliven this space and further improve the setting of the station and access to/from it. This could be complemented by the redevelopment of Central House on Clifftown Road for a new landmark building, which should also address the public space shared with the station (Refer to Table 4 and Policy DS3).

<b>Policy PA6: Clifftown Policy Area Development Principles</b>
<p>1. The Council, through its role in determining planning applications and other initiatives, will:</p> <ul style="list-style-type: none"> <li>a. promote independent small-scale retail, boutiques, cafés, restaurants, bars and small studio style workshops to create an area with a strong cultural identity together with residential uses above ground floor level to reinforce the fine grain historic street pattern and character;</li> <li>b. ensure that all development proposals affecting all designated and non-designated heritage assets, including Conservation Areas, listed and locally listed buildings conserve and enhance these buildings and their settings in line with Policy DM5 of the Development Management Document;</li> <li>c. require all development proposals, including replacement shopfronts, that impact upon ‘Frontages of Townscape Merit’ to have regard to the preservation and restoration of features which contribute to the special character of their frontage, in line with Development Management Policy DM5 (Historic Environment);</li> <li>d. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy;</li> <li>e. seek to conserve existing landmarks and landmark buildings and ensure new development respects views to and from them, their setting and character, in line with <b>Policy DS2: Key Views</b> and <b>Policy DS3: Landmarks and Landmark Buildings</b>.</li> </ul>
<p>2. The Council will pursue the upgrading and enhancement of this area with private sector land and property owners and developers by supporting applications that:</p> <ul style="list-style-type: none"> <li>a. regenerate the forecourt and entrance to Southend Central Railway Station as a signature public space designed in a way that respects the setting of the locally listed station building;</li> <li>b. redevelop Central House for new larger retail units with frontage on the High Street and Clifftown Road and office/residential development above. There is potential for a landmark building in this location and new public realm opportunities at the shared space/forecourt with</li> </ul>



Southend Central Railway Station;

c. regenerate the site of the Empire Theatre with uses that contribute to the Policy Area’s aim, including cultural uses such as galleries and performance space, particularly at ground floor to create an active frontage.

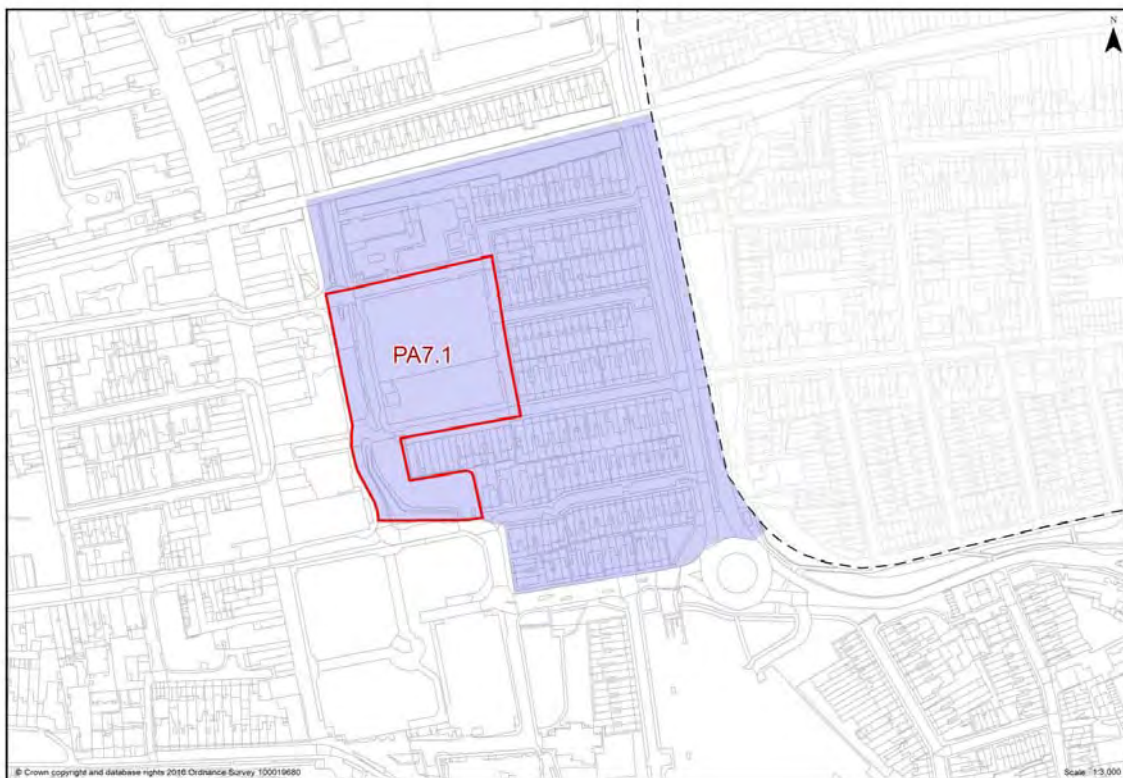
3. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual where applicable:

- a. seek a reduction in general vehicle circulation in residential streets by securing the most direct route to and out of the car parks at Alexandra Street and Clarence Road, and improving the quality of the public realm and cycle routes;
- b. streetscape and landscape design improvements, including urban greening and tree planting and improved way-finding signage, to create well lit walking circuits through Clifftown from a newly created public plaza at Southend Central Railway Station/ Central House, to Cliff Gardens and Pier Hill, facilitating better pedestrian access to the High Street;
- c. seek an improvement of soft landscaping and open space provision within the area;
- e. public art provision to buildings, public and private spaces.

<b>POLICY LINKAGES - CLIFFTOWN</b>	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 1, 2, 4, 8, 14, 15, 18	<b>Policies:</b> KP1, KP2, KP3, CP2, CP4, CP7
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM4, DM5, DM6, DM14
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 4, 5, 6, 7	<b>Policies:</b> DS1, DS2, DS3, DS4, DS5, PA1, CS1

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 5.8 Tylers Policy Area



### Aims

The policy area will benefit from a high quality public realm, complemented by landscaping, tree planting and public art, where pedestrians and cyclists are prioritised and bus travel is accessible, creating opportunities for shared public spaces, linked with opportunities for the future relocation and re-provision of the Travel Centre within the policy area.

A new public transport bus interchange would form part of an integrated development that allows for an area of public spaces to be created to the south western edge of the policy area, adjacent to its boundary with the High Street, improving links south to the Central Seafrost. It will be complemented by retail units at ground floor, with residential on the floors above, and may provide opportunity for houses to the rear of the site where these may re-establish the historic urban grain.

Car parking will be addressed within this integrated approach to development, which combines with other objectives for the policy area, and contributes to the vitality and viability of the town centre.

### Overview

The Tylers Policy Area has strong connections with the High Street, which lies to its west, and forms part of the link between the town centre and established residential communities to the east, as well as providing access to the Central Seafrost Policy Area, and key opportunity sites at Seaways and Marine Parade. It contains a fragmented area of office

blocks, Southend Travel Centre (the Town Centre public transport interchange), public surface level car parking at Tylers Avenue, and residential streets.

It is severed from the High Street by Chichester Road, which at present functions as a main access route for cars and service vehicles accessing the Town Centre and car parks, and for buses serving the Travel Centre. While it provides access to the Central Seafront Area, the quality of the public realm is poor and connections are weak. Queensway dual carriageway forms another barrier, severing links with and into the residential areas to the east.

Within this Policy Area there is one **Opportunity Site (PA7.1): Tylers Avenue**.

Redevelopment of the site could include the re-provision of the travel centre as a new public transport bus interchange, as part of a mixed-use scheme that includes retail uses at ground floor, and residential uses at upper floors, together with the provision of enhanced public space and parking.

Adjoining this fragmented area is an established residential area (comprised of Quebec Avenue, Portland Avenue, Baltic Avenue and Heygate Avenue) based on a traditional street pattern containing houses of different sizes and tenures. This residential area is somewhat isolated from the Town Centre because of the poor connectivity, given the barrier created by Chichester Road to the High Street. Measures to create a more cohesive and seamless transition through new development and public realm improvements will be welcomed.

#### **Policy PA7: Tylers Policy Area Development Principles**

1. The Council, through its role in determining planning applications, masterplanning and other initiatives, will:

- a. promote mixed-use development that delivers the aims for the Policy Area, which may include a new public transport interchange as part of the Tylers opportunity site;
- b. seek active frontages at ground floor on Chichester Road and York Road;
- c. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy.

2. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual where applicable:

- a. creation of a new public space in the location of the existing travel centre subject to this use relocating to the Tylers Opportunity Site, to provide enhanced connectivity to the Central Seafront and High Street Policy Areas;
- b. provision for new/ improved pedestrian/ cycle priority link as identified on the Policies Map, including appropriate crossings on Queensway dual carriageway and Chichester Road and improved linkages and access to the seafront;
- c. consider a 'Home Zone' style approach for the residential streets of Quebec Avenue, Portland Avenue, Baltic Avenue and Heygate Avenue including landscaping, tree planting, cycle parking and surface improvements.
- d. facilitate better pedestrian access to the High Street and Southend Central railway station;
- e. promote environmental improvements to Queensway dual carriageway including: crossing points to neighbouring residential areas; the removal of guardrails as appropriate; enhancing areas of landscaping and tree planting; and appropriately sited street furniture;
- f. junction improvements at Queensway dual carriageway/Seaway/Chancellor Road to

enhance access to the Central Seafront Area;  
g. public art provision to buildings and public spaces.

3. The following Opportunity Site, as identified on the Policies Map, is allocated for mixed-use development primarily comprising of residential and retail:

Site Reference	Site Name	Planning Status*	Indicative number of dwellings	Other potential use classes
PA7.1	Tylers Avenue	NA	150	A1, B1

\*Planning Status as of April 2016. NA = New Allocation

i. Within **Opportunity Site (PA7.1): Tylers Avenue**, planning permission will be granted for well-designed, sustainable buildings that provide a mix of uses compatible with the area, including active ground floor retail uses that connect well with the High Street and front Chichester Road, with residential uses and the potential for offices to upper floors;

ii. Any development of the Opportunity Site should address a need for replacement car parking provision in line with **Policy DS5: Transport, Access and Public Realm**, identifying how any displaced parking needs are to be met on the site or in this part of the town centre and explore the potential for relocating the travel centre on the northern extent of the site where applicable to provide for enhanced passenger transport facilities and improved pedestrian connectivity to the town centre and central railway station;

iii. Any development should incorporate a building design, form and massing that provides for a permeable environment that is pedestrian and cycle friendly, takes into account its setting and the proximity of neighbouring properties, with improved linkages to the High Street, Central Seafront Policy Area via St John's Church and Opportunity Site CS1.2: Seaways, with all servicing and deliveries from Chichester Road.

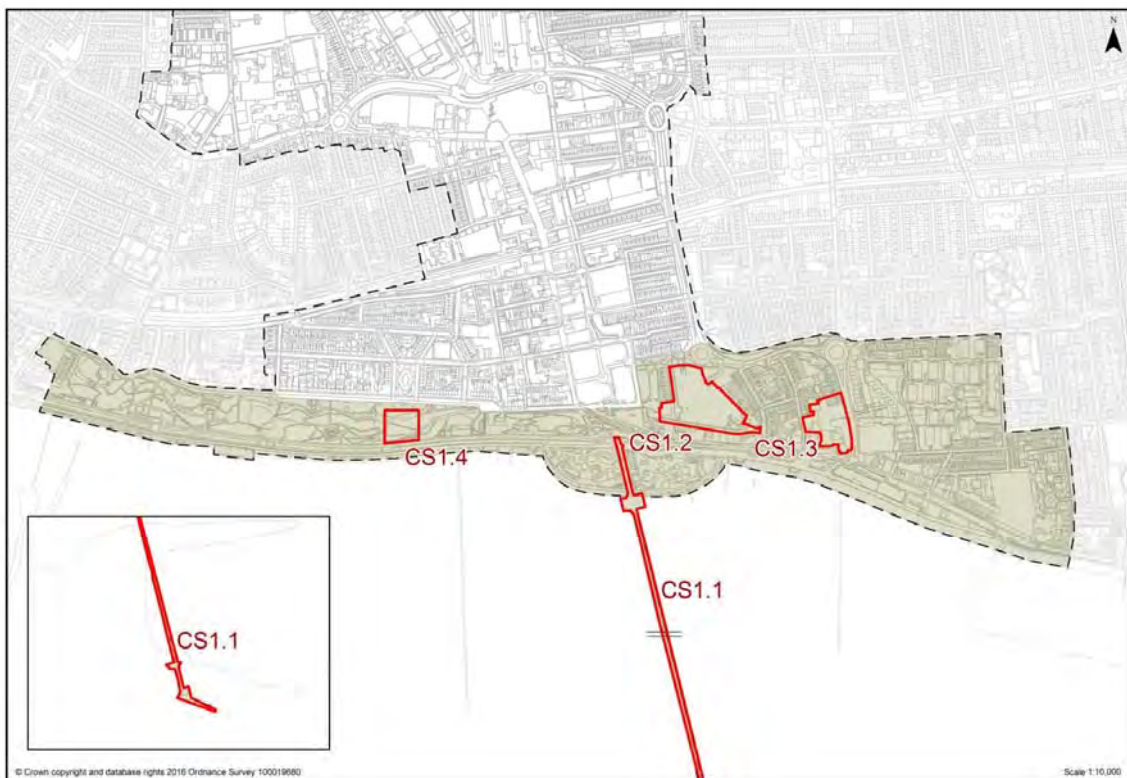
iv. The Council will encourage the application of masterplanning to guide development on this Opportunity Site.

#### POLICY LINKAGES - TYLERS

<b>CORE STRATEGY DPD</b> <b>Strategic Objective:</b> 1, 3, 4, 5, 6, 7, 8, 9, 10, 14, 15	<b>Policies</b> KP1, KP2, KP3, CP2, CP3, CP4, CP8
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM3, DM4, DM7, DM8, DM10, DM15
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 4, 5, 8, 9	<b>Policies:</b> DS1, DS4, DS5, PA1

\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.

## 5.9 Central Seafront Policy Area



### Aims

The Central Seafront will be a thriving and vibrant leisure, cultural and tourism area centred on the iconic Grade II listed Pier, which will be rejuvenated to reinforce its status as a key local landmark and attraction.

High quality mixed use schemes will be developed, including provision of hotel and visitor accommodation and heritage and natural assets conserved and enhanced. Innovative schemes such as 'Spanish steps,' providing pedestrian links to the seafront, beach, a seafront lido and new lagoon and a new world class museum will be promoted.

There will be seamless transition between the Central Seafront and the town centre. New and enhanced access points will create a network of routes that lead seamlessly to the estuary and foreshore from surrounding areas. This will increase permeability and encourage better functional links between the different policy areas, increasing footfall and opportunities to contribute towards the local economy.

Public realm improvements will be complemented by the placement of well-designed functional and creative lighting schemes and public art, which will visually enrich the area at night. The City Beach public realm scheme will be seamlessly extended facilitating improvements to Eastern Esplanade.

## Overview

*“The central area of the seafront is associated with a vibrant architectural style and sea-front leisure and pleasure. It provides a stark contrast to the orderly and mannered Victorian and Edwardian suburbs in the surrounding areas.”* Southend Borough Wide Character Study (January 2011)

The Central Seafront Policy Area, as defined on the Policies Map, is a thriving leisure and tourism area. Although the Pier Lift, a landmark building (Policy DS3) has helped to improve access between the Central Seafront Policy Area and Town Centre, if access was more straightforward and more pronounced in other locations such as Opportunity Site CS1.2 Seaways, there may be a better exchange of visitors between the Central Seafront and Town Centre and their functions. Its regeneration and successful integration with the town centre through improved and enhanced pedestrian links amongst other things, will therefore be key to increasing footfall and improving the areas vitality and viability.

The area encompasses a number of environmental designations, SSSI, SPA and Ramsar site. These designations highlight the estuary’s importance as a wildlife habitat. The policy approach to Nature Conservation and Biodiversity is set out within **Policy CS2** of this Plan.

While the estuary provides an important habitat for birds and wildlife, it has also attracted many human visitors. As such the area is under pressure from a number of competing influences. There is a need to strike a balance between the protection and conservation of natural and built assets, including Conservation Areas and listed buildings, with the needs of residents and visitors utilising these resources, and the regeneration ambitions for the wider Southend Central Area. In order to retain important views across the foreshore, development south of the sea wall will be restricted, and any acceptable proposed use will also have to be water compatible<sup>15</sup>, such as a lido or lagoon.

The seafront benefits from some notable buildings and structures. The Grade II listed Pier, the longest pleasure Pier in the world, for example, has recently benefitted from considerable investment, including the development of the Royal Pavilion and the Council will seek further opportunities for its enhancement.

In respect to leisure and tourism the Central Seafront draws in residents and visitors for a range of activities including use of the beach, water sports and other seafront attractions. Adventure Island is a major tourism asset to Southend, but its physical form tends to be inward looking and isolated from its urban context. It also obscures esplanade level views and routes to the sea. If redevelopment and expansion does occur options should be explored with the owners how changes within the site could simultaneously benefit the public realm around it by creating a more permeable boundary and incorporating active frontages to increase footfall around the site edges.

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<sup>15</sup> Planning Practice Guidance, paragraph 066, reference id: 7-066-20140306

The Cliffs Pavilion on the western edge of the Central Seafront Policy Area is another major visitor destination and is an important cultural asset which should be enhanced where possible to develop further the visitor offer.

### **Managing Flood Risk and Sustainable Drainage in the Central Seafront Policy Area**

The Central Seafront Policy Area is at risk of flooding from tidal and surface water flooding, including areas within the Environment Agency Flood Zones 3a (higher risk) and Flood Zone 2 (lower risk). Therefore, all development within the Central Seafront Policy Area, including within its Opportunity Sites, should have particular regard to **Policy DS4 Flood Risk Management and Sustainable Drainage**.

#### ***Opportunity Site (CS1.1): Southend Pier***

Southend Pier has the potential to be further rejuvenated as a landmark and destination, building on the success of recent developments such as The Royal Pavilion.

#### ***Opportunity Site (CS1.2): Seaways***

Seaways presents a major opportunity for mixed-use development, contributing to the leisure and cultural offer of Southend Central Area through the provision of uses such as restaurants and cinema as well as possibly a hotel or residential, car parking, public open and green spaces, improved access and connectivity through the creation of 'Spanish Steps' linking this opportunity site to the promenade of Marine Parade.

#### ***Opportunity Site (CS1.3): Marine Plaza***

The site offers potential for residential development, with supporting commercial uses and quality public open spaces, to reinforce the offer of the eastern end of Marine Parade. The site has potential for taller and larger buildings, creating a quality landmark to secure the regeneration of this part of the seafront. The site should have regard to the setting of the historic Kursaal building and the Conservation Area.

#### ***Opportunity Site (CS1.4): New Southend Museum***

A new museum within the cliff face will feature a range of displays from the collections of the Central Museum and Beecroft Gallery, including the internationally significant Saxon King find, in a new high quality iconic building for Southend. Other facilities will include: a planetarium, collections store with associated curatorial facilities, a museum shop, café and restaurant, and public car and cycle parking, complemented by and the creation of high quality green space, including amphitheatre within the cliffs, seating and good signage, linked to the High Street and Central Seafront via Cliff Gardens, Prittlewell Square and the wider Clifftown Policy Area.

The siting of the new building within the cliff face will minimise the potential impact on the Clifftown Conservation Area whilst improving accessibility between the Clifftown and Central Seafront Policy Areas, enabling good views of the estuary from the building and spaces around it.

### Policy CS1: Central Seafront Policy Area Development Principles

1. The Council through its role in determining planning decisions and other initiatives will:
  - a. consider favourably proposals which enhance or diversify the range of arts, culture, entertainment, tourism, leisure and recreational facilities, subject to an assessment of the scale, character, location and impact of the proposal on existing facilities and environmental designations, including protected green space;
  - b. promote the provision of hotels and visitor accommodation, subject to satisfactory access and parking provision;
  - c. secure high quality and sustainable redevelopment of poor quality, vacant or underused sites and buildings to improve the local townscape, including provision of active ground floor frontages to add to the vibrancy and vitality of the streetscene;
  - d. ensure that all development proposals affecting all designated and non-designated heritage assets, including Conservation Areas, listed and locally listed buildings, conserve and enhance these buildings and their settings in line with Policy DM5 of the Development Management Document;
  - e. ensure that all future sea defences and flood mitigation measures integrate seamlessly with the public realm;
  - f. seek to maintain foreshore views by restricting development south of the sea wall. Any proposed use will also have to be water compatible as defined in the Planning Practice Guidance;
  - g. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy;
  - h. seek to conserve existing landmarks and landmark buildings and ensure new development respects views to and from them, their setting and character, in line with **Policy DS2: Key Views** and **Policy DS3: Landmarks and Landmark Buildings**.
  - i. require all development within the Central Seafront Policy Area to:
    - i. have regard to **Policy DS4** in order to manage and mitigate against flood risk;
    - ii. safeguard, and where appropriate, enhance the biodiversity of the foreshore and respect the European designations in line with **Policy CS2: Nature Conservation and Biodiversity**

2. There is potential for archaeological deposits within the area of Southend Cliffs and Seaway Car Park and as such developers should have regard to Policy DM5 – Southend-on-Sea’s Historic Environment of the Development Management Document.

3. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual where applicable:
  - a. emphasise landmarks and landmark buildings (Policy DS3), orientation points, views and vistas, and improve accessibility between the seafront and town centre;
  - b. create a well-defined piazza area at the southern end of the High Street between The Royals, The Palace Hotel and Pier Hill and encourage new and existing uses to provide active frontages to face onto this space;
  - c. preservation and integration of the open spaces of the seafront and foreshore with the ‘green grid’ to create a series of linked, functional green spaces to relieve pressure on the seafront;
  - d. use creative lighting and public art to strengthen identity and connectivity;
  - e. remove unnecessary street furniture and improve and rationalise signage, with particular focus on public realm adjacent to listed and locally listed buildings and



- Conservation Areas, in particular around The Kursaal;
- f. provision of a more permeable boundary to Adventure Island to provide views in and through the site;
  - g. implement a rolling programme of improvements to the promenade and public spaces (further developing the City Beach scheme at Eastern Esplanade) and incorporating flood mitigation measures, such as SuDS;
  - h. junction improvements at Queensway/Seaways/Chancellor Road;
  - i. provision for new/ improved pedestrian/ cycle priority link as identified on the Policies Map;
  - j. pursue urban greening and provide additional seating, tree planting and landscaping, as well as enhanced links between neighbouring Policy Areas;
  - k. upgrade the Cliffs Pavilion outdoor space and improve its connection to the Cliffs;
  - l. improve traffic management with the aid of VMS and improved signage, parking, including taxi rank and coach drop off provision, and walking and cycling, including SUSTRANS route;
  - m. improvements to the Sealife Centre through redesign or redevelopment;
  - n. regeneration of redundant lavatories and the crazy golf site for kiosks or other small-scale seaside businesses;
  - o. explore development of a Seafront lido or new lagoon to compliment the seafront leisure offer.

4. The following Opportunity Sites, as identified on the Policies Map, are considered suitable for development for mixed-use purposes within the Central Seafront Policy Area:

Site Reference	Site Name	Planning Status*	Indicative number of dwellings	Other potential use classes
CS1.1	Southend Pier	NA	N/A	D2, D1, A3, A1
CS1.2	Seaways	NA	N/A	D2, A3, C1
CS1.3	Marine Plaza	NS	282	A1, A3, D2
CS1.4	New Southend Museum	NS	N/A	D1, A3, A1

\*Planning Status as of April 2016. NA = New Allocation. NS = Not started with full planning permission

i. **Opportunity Site (CS1.1): Southend Pier**, the Council will pursue with private sector partners and through other initiatives, sensitive redevelopment at both ends of the pier to provide a mix of cultural and leisure uses during the day and in the evening, including uses such as: cafes, restaurants, shops, events, and small scale moorings; deckchairs, telescopes, seating, cycle parking and improved pedestrian signage; facilities for traditional activities such as angling; creative lighting; and sensitively sited renewable technologies, where appropriate.

ii. **Opportunity Site (CS1.2): Seaways**, the Council will pursue with private sector partners, landowners and developers a high quality, mixed use development including the provision of leisure, cultural and tourism attractions, which may include: restaurants, cinema, gallery, hotel, public and private open spaces, and vehicle and cycle parking. The potential for residential development may also be explored. Design and layout solutions should allow for:

- a. remodelling of the urban form to create a north-south axis on the Seaway site, providing a clear sight-line from Queensway dual carriageway to the sea;
- b. a stronger relationship with the Town Centre through the provision of safe and legible pedestrian and cycle routes;

- c. opportunities for a new link to Marine Parade from the Seaway site designed around ‘Spanish Steps’ and in doing so ensure that development does not prejudice its future delivery as a new link between the seafront and town centre;
- d. addressing the need for replacement car parking provision in line with **Policy DS5: Transport, Access and Public Realm;**
- e. active frontages to all new and existing streets and spaces;
- f. a palette of good quality materials to reflect the vibrancy and colour of the seaside;
- g. relocation of a coach-drop off point within the site. The relocation of coach parking bays may be provided either on or off-site or a combination of both, provided off-site provision is well connected to the Seaway site and would not significantly adversely impact the local transport network;
- h. urban greening projects, including the creation of new public and private green space within new development;
- i. innovative design which allows the site to take advantage of the elevation and creates a legible environment with views of the estuary, respecting the amenity of neighbouring residential uses;
- j. the provision of appropriate seating, signage and way-finding aids to improve connectivity to the Town Centre, Seafront and Opportunity Site CS1.3: Marine Plaza.

iii. **Opportunity Site (CS1.3): Marine Plaza**, the Council will support the comprehensive redevelopment of the site for high quality/ iconic residential development with complimentary leisure and supporting uses that create activity at ground floor fronting Marine Parade, incorporating areas of public open space into the site which take advantage of views of the seafront and estuary. The provision of appropriate seating, signage and way-finding aids to improve connectivity to the seafront and town centre, including links to Opportunity Site CS1.2: Seaways, will also be promoted.

iv. **Opportunity Site (CS1.4): New Southend Museum**, the Council will promote the development of an exemplary, sustainable building that includes the new Southend Museum, gallery space, planetarium, conference/events spaces, and associated café/restaurant, together with public car and cycle parking and the creation of high quality green space, including amphitheatre within the cliffs, seating and good signage, linked to the High Street and Central Seafront via Cliff Gardens, Prittlewell Square and the wider Clifftown Policy Area. The design of new development will need to retain the open feel of this area and ensure that new planting includes native species and increases biodiversity in the area. Vehicular access should ensure that the primary road network, i.e. via Western Esplanade, is used to access the development and any new parking facilities.

POLICY LINKAGES – CENTRAL SEAFRONT	
<b>CORE STRATEGY DPD</b> Strategic Objective: 4, 6, 7, 9, 10, 12, 14, 15, 17, 18	<b>Policies:</b> KP1, KP2, KP3, CP3, CP4, CP7, CP8
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM3, DM4, DM5, DM6, DM7, DM8, DM9, DM10, DM12, DM14, DM15
<b>SOUTHEND CENTRAL AAP</b> Objectives: 1, 2, 3, 4, 5, 6, 7, 8, 10	<b>Policies:</b> DS2, DS3, DS4, DS5, PA1, PA6, PA7, CS2, CS3

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## **Nature Conservation and Biodiversity in the Central Seafront Policy Area**

The foreshore is designated for International and European sites for nature conservation. Particularly relevant to the Central Seafront Policy Area are Benfleet and Southend Marshes (SPA, SSSI and Ramsar site), which comprises the intertidal part of the Thames Estuary from Benfleet to Shoeburyness and cover the same land area within the SCAAP and wider Southend-on-Sea Borough.

The marshes also provide an attractive environment for both marine activities and more passive enjoyment of natural habitats. There may be opportunities to design high quality visitor facilities, giving visitors a better understanding of the ecosystems. All future activity and development will need to ensure that they do not adversely affect the interests of the nature conservation designations on the foreshore.

Developments which may affect a site of International or European nature conservation importance (SPA, Ramsar) will be subject to rigorous examination in consultation with Natural England and other relevant authorities.

Even if a development is located some distance from an International or European site it may still have a detrimental impact on the site and will need to be subject to a Habitats Regulations Assessment. Natural England should be consulted at an early stage of a planning application.

The applicant must submit appropriate biodiversity surveys, impact assessment and mitigation proposals to enable the Council to determine a planning application in addition to a Habitat Regulation Assessment, where required.

If it cannot be demonstrated that the application will not adversely affect an International or European site, then the application will be refused, unless there are no alternative solutions and the development has to be carried out for imperative reasons of over-riding public interest as set out in the Habitats Regulations.

In such cases compensatory habitat will be required. In addition, the Council will consider applying planning conditions or legal obligations to secure the integrity of the International or European site from any adverse impacts arising from the development.

### **Policy CS2: Nature Conservation and Biodiversity**

1. The Council will:

- a. Ensure that all development proposals within the Central Seafront Area are accompanied by a Habitats Regulations Assessment and associated documentation to ensure there will be no adverse effect on the European and International foreshore designations (SPA and Ramsar) either alone or in combination with other plans or projects;
- b. Not permit development proposals that will have an adverse impact, either directly or indirectly, on the foreshore designations;
- c. In exceptional circumstances make exceptions for development proposals on a Site of Special Scientific Interest (SSSI), only if it can be demonstrated that:
  - i. there are no alternative solutions; and

- ii. the reasons for the development clearly outweigh the nature conservation value of the site and is in the public interest;
- d. Apply planning conditions or legal obligations to secure the protection, conservation and enhancement of a Site of Special Scientific Interest (SSSI) from any harmful impacts arising from the development;
- e. Integrate the seafront and foreshore open space within a broader Southend ‘green grid’ of linked and functional green space to relieve visitor pressure on the seafront, and protect the sensitivities of the biodiversity interest. Areas of new/ improved public open space will be particularly promoted at Pier Hill, Seaways, Marine Parade and along Eastern Esplanade;
- f. Consider favourably the development of a high quality visitor facility close to the foreshore which will assist with interpretation of the natural habitat in the area, providing visitors a better understanding of the ecosystems and local biodiversity.

<b>POLICY LINKAGES – CENTRAL SEAFRONT NATURE CONSERVATION</b>	
<b>CORE STRATEGY DPD</b> Strategic Objectives: 12, 18	<b>Policies:</b> KP1, KP2, KP3, CP4, CP7
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM6
<b>SOUTHEND CENTRAL AAP</b> Objectives: 3, 5, 6	<b>Policies:</b> DS4, DS5, CS1, CS3

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

### **The Waterfront**

The Council will promote the waterfront for a wide range of sport, recreation and leisure activities, whilst also respecting the natural environment in line with Policy CS2.

Improving marine activity facilities will encourage more water based activities, users and visitors, and cater for leisure demand and tourism. Jetties, moorings and support facilities are vital components of an active and attractive waterfront. The Council will promote the beaches, foreshore and estuary for boat users, including through the maintenance and enhancement of existing facilities. There is opportunity to build on the success of water based sports and recreation in other seafront locations in the Borough and beyond to further broaden its offer within the Central Seafront Policy Area.

The provision of information and interpretation boards and other information-based facilities about the waterfront and wildlife, will be encouraged.

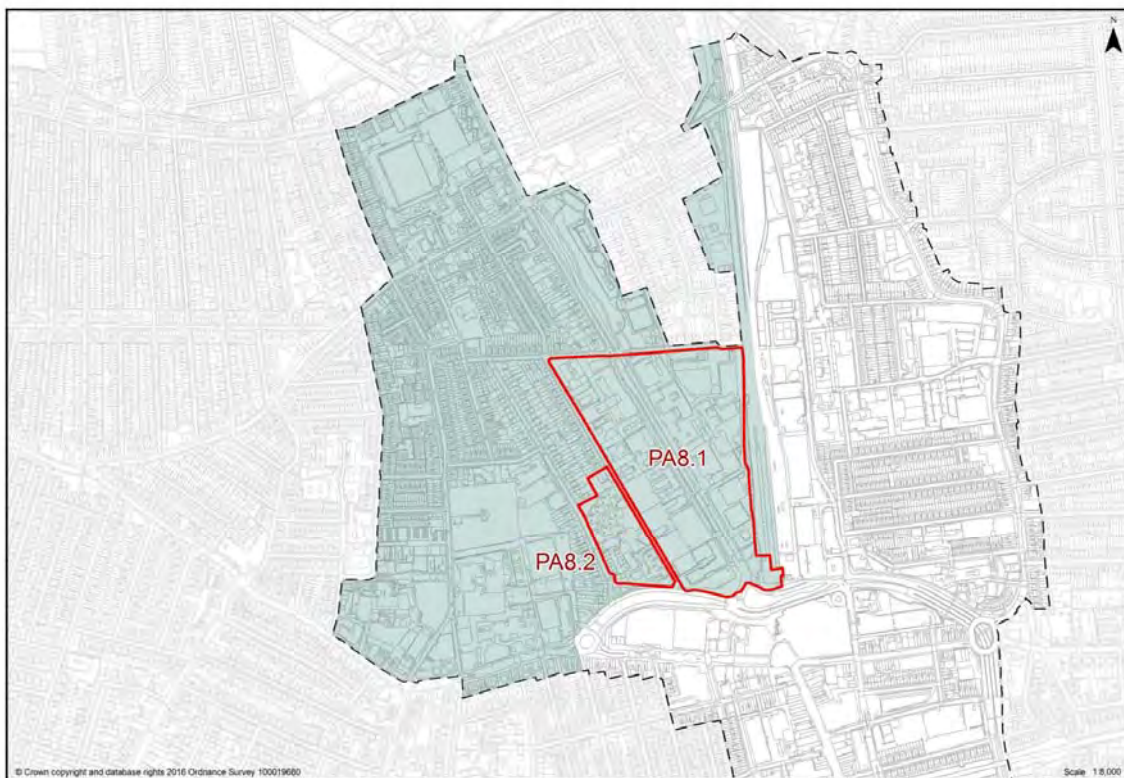
<b>Policy CS3: The Waterfront</b>
1. The Council, with private sector partners and through the exercise of its planning powers and other initiatives will: <ul style="list-style-type: none"> <li>a. promote the beach, foreshore and Estuary for appropriate cultural, leisure and tourism activities, including the maintenance and enhancement of facilities for:               <ul style="list-style-type: none"> <li>i. seafront, beach and water-based recreation activities and attractions including tidal paddling pools;</li> </ul> </li> </ul>

- ii. marine and boat users, including moorings, support facilities (wharfs, jetties, landing stages and slipways), club facilities and information.
- b. continue to maintain the quality and cleanliness of the beach and foreshore experience, including:
- i. pursuing a rolling programme of co-ordinated public realm improvements to the promenade;
  - ii. integrating the protection and interpretation of biodiversity interests, heritage assets and landscape features, including views across the Estuary, into the overall visitor experience through:
    - 01. provision of information boards/facilities including making increasing use of mobile phone and digital technology;
    - 02. themed walking and cycling signed routes, and links to a broader borough-wide network.
2. Proposals for waterfront development within the Central Seafront Area and improved facilities will need to demonstrate that there will be no unacceptable impact upon navigation, biodiversity, flood risk or the special character and designations of the area.

<b>POLICY LINKAGES – CENTRAL SEAFRONT WATERFRONT</b>	
<b>CORE STRATEGY DPD</b> Strategic Objectives: 12, 18	<b>Policies:</b> KP1, KP2, KP3, CP4, CP7
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM6
<b>SOUTHEND CENTRAL AAP</b> Objectives: 3, 5, 6	<b>Policies:</b> DS4, DS5, CS1, CS2

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 5.10 The Victoria Gateway Neighbourhood Policy Area



### Aims

A regenerated Victoria Avenue will create an attractive and vibrant gateway to the town centre, further enhanced by future phases of the Victoria Gateway scheme, which will see improved connections and accessibility. New development will be of a high standard of design, with urban greening techniques employed such as pocket parks, community gardens, tree planting and landscaping, together with a consistent palette of surface materials and street furniture.

Victoria Avenue will be an attractive area in which to live, where residents will benefit from the sustainability of the location, particularly with fast and convenient access to London and the City. Enhancements to the public realm will be undertaken throughout the neighbourhood, to encourage walking and cycling and improve access to the town centre, employment area and public transport interchanges.

The heritage of Prittlewell Conservation Area will be celebrated and enhanced, and the character of the area will be reinforced by sensitively restored buildings that positively contribute to a distinctive sense of place. Buildings will be set within a quality public realm, with improved access to information about the area's history.

The neighbourhood will benefit from a thriving local shopping parade on West Street.

## Overview

The Policy Area is home to Southend's traditional office zone at Victoria Avenue (**Opportunity Site PA8.1**), which includes the civic quarter. The significant challenge for this neighbourhood is to address the large amount of underused, vacant and outmoded office buildings. Some have gained prior approval to residential through recently changed permitted development rights introduced by central government, but others still provide opportunity for more comprehensive and holistic development. As the Opportunity Site is large and in multiple land ownership, requiring redevelopment of existing buildings, an assumption has been made that only development proposals with planning permission will be delivered during the SCAAP plan period (i.e. by 2021).

There is also the opportunity to regenerate an existing area of social housing at the junction of Queensway and Baxter Avenue close to the Victoria Avenue opportunity site for new high quality, mixed tenure residential accommodation, including additional sheltered and affordable housing (**Opportunity Site (PA8.2): Baxter Avenue**). Given the scale of the redevelopment it is envisaged that approximately half of the site will be delivered during the SCAAP plan period (i.e. by 2021). Both opportunity sites (PA8.1 and PA8.2) will be reviewed, including progress made with implementation, during the Local Plan preparation process.

Prittlewell Conservation Area to the north and the buildings, structures and spaces within it are important heritage assets, forming a key gateway on the main route into the Town Centre along Victoria Avenue. This Conservation Area contains some of the town's oldest and most important buildings, including St Mary's Church a landmark building located adjacent to the SCAAP boundary.

The neighbourhood also contains a local shopping parade at West Street, designated as secondary shopping frontage, which provides an important and convenient resource for local residents and businesses, and as the neighbourhood regenerates it is anticipated that this centre will continue to support the needs of the local population.

Some areas within this Policy Area are susceptible to surface water flooding. Therefore, development will need to have regard to local flood risk management policies, particularly regarding SuDS as set out in Policy DS4.

### **Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles**

1. The Council, through its role in determining planning applications and other initiatives, will:
  - a. look favourably on high quality developments and schemes which can demonstrate that they will contribute to the transformation of this area into a vibrant community, which is integrated with the surrounding neighbourhood and set within a remodelled built form of a quality that befits this key gateway to the Town Centre;
  - b. ensure all development within and adjacent to Prittlewell Conservation Area, seeks to conserve and enhance the heritage assets and repair gaps in the frontage along Victoria Avenue, realising the potential of the backland area to the rear of Victoria Avenue (west side 255-289) as a 'Lanes' style development, promoting specialist and independent industries, associated small scale businesses and ancillary

residential units;

- c. consider the provision of additional education facilities based on an assessment of expansion needs when and where appropriate development opportunities arise;
- d. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy;
- e. use its enforcement and other powers to reduce the damage to amenities and the environment resulting from long term vacant and derelict land and buildings;
- f. promote the provision of easily accessible new social and community infrastructure, such as doctor and dental surgeries, nurseries and community hubs;
- g. promote enhanced cultural facilities to complement the Beecroft Centre, the Central Museum Building and the former Water Board site on North Road;
- h. ensure that housing development including mix and tenure is delivered in line with Development Management Policy DM7 – Dwelling Mix, Size and Type;
- i. seek to conserve existing landmark buildings and ensure new development respects views to and from them, their setting and character, in line with **Policy DS3:**

**Landmarks and Landmark Buildings.**

2. There is potential for archaeological deposits within the area of Nazareth House and Roots Hall and as such developers should have regard to Policy DM5 – Southend-on-Sea’s Historic Environment of the Development Management Document.

3. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual SPD and any future masterplan for the area where applicable:

- a. a priority public transport route linking Southend Central Area with London Southend Airport and adjacent development areas;
- b. appropriate enhancements to North Road, including new civic space at the junction with Chelmsford Avenue, to improve the residential environment, provide for walking and cycling, and improve linkages to West Street local shopping centre, and cultural and community facilities on North Road, including Prittlewell Chapel;
- c. public realm improvements to the Victoria Avenue service road in association with development proposals within Opportunity Site PA8.1;
- d. public art provision to buildings, public and private spaces;
- e. full integration with the surrounding area through the provision of pedestrian and cycling routes, to improve access and linkages. Provision for mixed mode - pedestrian and cycle priority route along Victoria Avenue between Queensway dual carriageway and Harcourt Avenue;
- f. urban greening projects linked to the green grid, including planting and the creation of new public and private green space within new development;
- g. enhancement of the existing Civic Space (including the Holocaust Memorial) on the east side of Victoria Avenue between the Civic Centre and Law Courts, and its integration with the broader area;
- h. Junction improvements at along Victoria Avenue at Fairfax Drive, East Street/ West Street, Carnarvon Road and Great Eastern Avenue and provide an enhanced public realm complemented by soft landscaping and planting.

4. The following Opportunity Sites, as identified on the Policies Map, are considered suitable for mixed-use residential development:

Site	Site Name	Planning	Indicative number	Other potential
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Reference		Status*	of dwellings	use classes
PA8.1	Victoria Avenue	NA	1000**	D1, B1, A1, A3
PA8.2	Baxter Avenue	NA	500***	

\*Planning Status as of April 2016. NA = New Allocation.

\*\*Just sites with planning permission or prior approval (662 dwellings) assumed to be delivered during the SCAAP plan period (i.e. by 2021)

\*\*\* Half of site assumed to be delivered during SCAAP plan period (i.e. by 2021)

i. Within **Opportunity Site (PA8.1): Victoria Avenue Office Area**, planning permission will be granted for comprehensive redevelopment of this site, or incremental development within the area, to transform it into a sustainable mixed use community with high quality developments, this will include:

- a. an acceptable mix of uses focussed on residential uses to upper floors and small scale flexible office accommodation complemented by local convenience retail uses, leisure (cafes and bars) and community facilities to ground floors;
- b. full integration with the surrounding area through the provision of pedestrian and cycling routes, to improve access and linkages;
- c. limited strategic locations for taller buildings;
- d. urban greening projects, including:
  01. the use of green walls and roof gardens;
  02. comprehensive landscaping;
  03. the creation of a series of linked public green space within the area linked to a wider network of parks and gardens;
  04. a comprehensive sustainable drainage system;
- e. pursue, as appropriate, a full range of measures to enable delivery of the site, including Compulsory Purchase powers, application of masterplanning techniques to guide comprehensive redevelopment, partnership working with private sector landowners and developers;
- f. in the event of incremental redevelopment of individual sites, the Council will require each development site to demonstrate how it meets the policy criteria and development principles set out above and accords with any future masterplan related to the opportunity site;
- g. the grade II listed old museum building will be conserved and its setting enhanced as part of the proposals for the policy area.

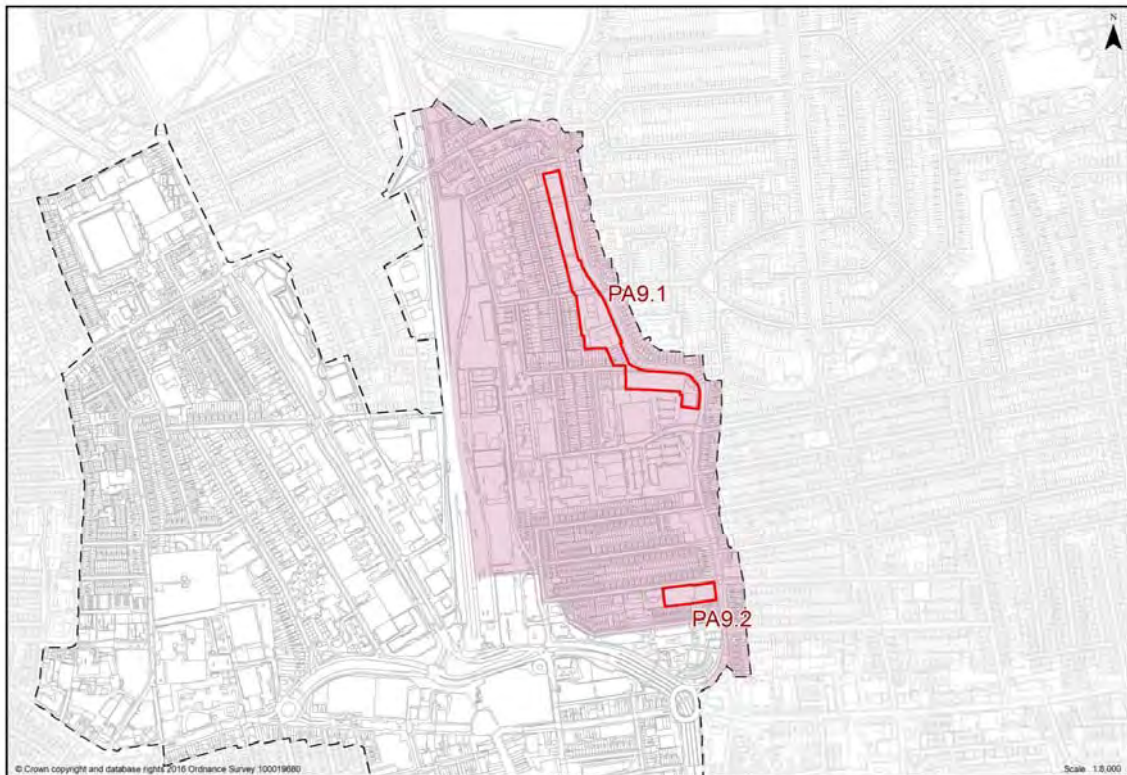
ii. Within **Opportunity Site (PA8.2): Baxter Avenue** the Council will promote the regeneration of the site for high quality mixed tenure residential development, including sheltered and additional affordable housing. Any scheme should incorporate amenity open space, urban greening and sustainability measures as well as providing pedestrian access and linkages between Victoria, Baxter and Boston Avenues.

POLICY LINKAGES – VICTORIA AVENUE	
<b>CORE STRATEGY DPD</b> Strategic Objectives: 1, 2, 3, 4, 5, 6, 7, 9, 10, 11, 13, 14, 15, 18	<b>Policies:</b> KP1, KP2, KP3, CP1, CP3, CP4, CP6, CP7, CP8
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM3, DM4, DM5, DM7, DM8, DM9,

	DM10, DM13, DM15
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 4, 5, 6, 7, 8, 9, 10	<b>Policies:</b> DS2, DS3, DS4, DS5, PA1, PA9

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## 5.11 Sutton Gateway Neighbourhood Policy Area



### Aims

The Sutton Gateway will be regenerated, with high quality, sustainable buildings helping to restore the urban grain, creating a distinctive sense of place where people are proud to live and work. It will be supported by a thriving local shopping parade on Sutton Road.

This will be achieved by repairing, unifying and knitting together the townscape to create a cohesive neighbourhood that retains a variety of uses, which relate well to each other and support the neighbourhood.

The policy area will provide employment opportunities, protecting, maintaining and promoting Grainger Road and Short Street Employment Growth Areas as locations for increased employment floorspace.

The area will be easily accessible by foot and by bike, with improved links between residential areas, the town centre, public transport nodes, retail and employments areas.

### Overview

The Sutton Gateway Neighbourhood Policy Area is defined on the Policies Map. It contains a diverse mix of uses including: residential, employment areas and a retail park. The southern section of Sutton Road comprises a local shopping parade with a mix of retail uses and services for the local community.

The SCAAP aims to maintain and reinforce the existing local shopping parade and in design terms, repair, unify and knit together the townscape to create a neighbourhood that retains a variety of uses but where these uses relate better to each other in an enhanced urban environment.

There is also scope to maintain and promote Grainger Road and Short Street Employment Growth Areas as locations for increased modern employment floorspace through a managed approach, using masterplanning as appropriate, which will set out the quantum of development and appropriate uses. Refer to Development Management Document Policy DM11 – Employment Areas.

Clearly access to and within this neighbourhood and to the Town Centre and public transport facilities are important, and there are opportunities to improve the environment of Short Street along its length, including continuing the footway north. Sutton Road is also a major multimodal route and environmental improvements here would support the local shopping parade.

**Opportunity Site (PA9.1): Sutton Road** is considered suitable for redevelopment for additional housing alongside an appropriate level of supporting uses, such as community facilities. Currently it contains a number of buildings, primarily in employment use, which front Sutton Road. In recent years change has slowly begun to take place, transforming the area to a residential-led community.

As Opportunity Site (PA8.1) is large and in multiple land ownership and will require redevelopment of existing buildings, an assumption has been made that only development proposals with planning permission will be delivered during the SCAAP plan period (i.e. by 2021). The site, including any outstanding phases of development, will be reviewed during the Local Plan preparation process.

**Opportunity Site (PA9.2): Guildford Road**, at the junction of Sutton Road and Guildford Road, is currently occupied by a three-storey convenience store, the upper two levels of which are vacant. The site offers opportunity for redevelopment to achieve a new enhanced convenience store and new residential accommodation. The façade of the current building fronting onto Sutton Road must be retained and incorporated into any proposals.

<b>Policy PA9: Sutton Gateway Neighbourhood Policy Area Development Principles</b>
<p>1. The Council, with private sector partners, through the determination of planning applications and other initiatives will:</p> <ul style="list-style-type: none"><li>a. maintain and promote Grainger Road and Short Street Employment Growth Areas as locations for increased employment floorspace in line with Development Management Policy DM11 – Employment Areas;</li><li>b. promote energy efficiency as appropriate, including opportunity for decentralised energy supply, and the retrofit of existing development in line with local policy;</li><li>c. consider the provision of additional education facilities based on an assessment of expansion needs when and where appropriate development opportunities arise;</li><li>d. seek to ensure that new development respects the views, setting and character of</li></ul>

landmark buildings located near to the Policy Area, in line with <b>Policy DS3: Landmarks and Landmark Buildings.</b>				
2. The Council will promote the following access and public realm improvements, addressing the principles of the Streetscape Manual where applicable: <ul style="list-style-type: none"> <li>a. appropriate enhancements to Sutton Road to uplift the residential environment and enhance provisions for walking and cycling;</li> <li>b. facilitate better pedestrian and cycle access along the length of Short Street, enhancing connections between East Street and Queensway dual carriage-way;</li> <li>c. streetscape and landscape design improvements, including urban greening and tree planting;</li> <li>d. public art provision to buildings, public and private spaces.</li> </ul>				
3. The following Opportunity Sites, as identified on the Policies Map, are considered suitable for redevelopment:				
Site Reference	Site Name	Planning Status*	Indicative number of dwellings	Other potential use classes
PA9.1	Sutton Road	NA	214**	A1, D1
PA9.2	Guildford Road	NA	50	A1
* <i>Planning Status as of April 2016. NA = New Allocation</i>				
** <i>Just sites with planning permission or prior approval (92 dwellings) assumed to be delivered during the SCAAP plan period (i.e. by 2021)</i>				
i. Within <b>Opportunity Site (PA9.1): Sutton Road</b> , the Council will support the redevelopment of this area for high quality housing and community facilities. The Council will require the building design, form and massing to: <ul style="list-style-type: none"> <li>a. have regard to residential buildings on the opposite side of Sutton Road and contribute positively to repairing the street scene and urban grain in this area;</li> <li>b. include enhancements to the public realm to create a coordinated, sustainable palette of materials and furniture in accordance with the Streetscape Manual.</li> </ul>				
ii. Within <b>Opportunity Site (PA9.2): Guildford Road</b> the Council will support the redevelopment of this site to achieve a replacement convenience store fronting Sutton Road that enhances the Secondary Shopping offer of this locality together with new residential accommodation. The façade of the current building fronting onto Sutton road must be retained and linked architecturally into any proposal. The scheme should also incorporate amenity open space, urban greening and sustainability measures. Site access will be via Guildford Road.				

POLICY LINKAGES - SUTTON	
<b>CORE STRATEGY DPD</b> <b>Strategic Objectives:</b> 2, 3, 4, 5, 6, 7, 9, 13, 14, 15, 17, 18	<b>Policies:</b> KP1, KP2, KP3, CP1, CP3, CP4, CP6, CP7, CP8
<b>DEVELOPMENT MANAGEMENT DPD</b>	<b>Policies:</b> DM1, DM2, DM3, DM4, DM7, DM8, DM9, DM10, DM11, DM13, DM14, DM15
<b>SOUTHEND CENTRAL AAP</b> <b>Objectives:</b> 1, 2, 3, 5, 6, 8, 9, 10	<b>Policies:</b> DS4, DS5, PA2, PA4, PA8

*\*This Policy Linkage Box provides a summary of key inter-related local policies. Other planning policy and guidance not listed here may also be applicable and, therefore, a full assessment should be undertaken.*

## Part D Implementation and Monitoring Framework

### 6. Delivery of the SCAAP

#### 6.1 Introduction

By setting out an approach to implementation, this section will assist with co-ordinating the delivery of SCAAP policies and proposals by identifying key partners as well as describing necessary infrastructure for creating sustainable developments in this location.

#### 6.2 Delivery

The SCAAP proposes 11 Opportunity Sites shown in Table 6. These have been allocated in terms of their perceived deliverability, based primarily upon viability, land ownership and alignment with key objectives in Council plans and strategies. These sites:

- have a positive impact on viability within the SCAAP area and will impact favourably on later or additional development within Southend Central Area;
- may require development of publicly owned land where the greatest control over outcomes may be applied;
- will be able to be delivered through private sector involvement or using Council budgets.

The SCAAP proposals are expected to come forward within 5 years, the plan period to 2021. Preparation of the Local Plan will act as an early review of the SCAAP and will consider all development sites over a longer time period.

In order for development to come forward as indicated, the Council will need to maximise its own town centre land assets, consider using, where necessary, statutory powers for land assembly and work with the private and other public sector land owners, where required, and explore all funding sources available.

It is acknowledged within the SCAAP boundary that other development will come forward in the Policy Areas which will contribute to the delivery of jobs and housing.

A key objective is to ensure that any change of use to residential and other value-generating uses must also provide wider benefits for the local area, such as helping to deliver access, public realm, employment, educational, health and other community related improvements either indirectly or directly.

**Table 6: Development Sites within SCAAP Policy Areas**

Policy Reference	Opportunity Site name	Ownership
<b>2016 to 2021</b>		
Policy PA3: Elmer Square	Elmer Square Phase 2 (PA3.1)	Southend Borough Council,

		University of Essex, South Essex College
Policy PA4: Queensway	Opportunity Site (PA4.1): Better Queensway Project	Southend Borough Council
Policy PA7: Tylers	Tylers Avenue (PA7.1)	Southend Borough Council
Policy CS1: Central Seafront	Southend Pier (CS1.1) Seaways (CS1.2) Marine Parade (CS1.3) New Southend Museum (CS1.4)	Southend Borough Council, Multiple Private Ownership
Policy PA8: Victoria Avenue Gateway Neighbourhood	Victoria Avenue (PA8.1) Baxter Avenue (PA8.2)	Southend Borough Council and Multiple Private Ownership
Policy PA9: Sutton Gateway Neighbourhood	Sutton Road (PA9.1) Guildford Road (PA9.2)	Multiple Private Ownership

### Indicative Figures for SCAAP Potential New Developments

Table 7 sets out an indicative total number/ floorspace of new development to be delivered within the Opportunity Sites by 2021. The figures provided will be subject to variation when each Opportunity Site comes forward and do not account for other development proposals that may come forward within the SCAAP area but outside the identified Opportunity Sites.

**Table 7: Indicative Scale of Development (Gross) within Opportunity Sites (2016 – 2021)**

<b>Residential</b>	<b>Commercial, including eating and leisure</b>	<b>Retail</b>	<b>Community</b>
1,732	17,500 m <sup>2</sup>	5,500 m <sup>2</sup>	18,500 m <sup>2</sup>

### 6.3 Implementation

This section outlines how the SCAAP and its policies will be implemented and monitored. It seeks to show how each policy will be delivered and by whom. Implementation of the SCAAP will be dependent on the provision of necessary infrastructure as described in the preceding chapters.

The Council has experience in working with private sector partners and securing funding, particularly recently with money secured in conjunction with the Local Enterprise Partnership. It is committed to working with agencies across the public, private and voluntary sectors to successfully realise a shared vision for the town centre and surrounding area.

The Implementation Plan sets out a 'rolling programme' of projects and tasks that is not exhaustive and will be kept under review. Potential delivery mechanisms identified are as follows:

- Planning Conditions or Obligations – site specific elements such as affordable housing or new open space provided by private or public developers and secured using planning conditions or planning obligations. Priorities for S106 agreements may be found in the adopted Planning Obligations SPD, however in particular regard will be given to:
  - Affordable Housing;
  - Sustainable Design and Development;
  - Open Space (and enhancement and management of the public realm); and
  - Transport Infrastructure
- Community Infrastructure Levy (CIL) – wider area improvement projects delivered by the Council. CIL is a charge on new development to spend on local and sub-regional infrastructure to support the development of the SCAAP.
- The SCAAP will also be used as an Investment Tool – to secure resources from funding bodies in support of the projects identified such as regional, national and European funding.
- Creation of a Limited Liability Partnership.

A Limited Liability Partnership (LLP) between the Council and a Private Sector Partner has been established that offers a mechanism to assist with delivery of SCAAP sites where appropriate. Under this arrangement the Council and a Private Sector Partner will each own 50% of the shares of the LLP whose main purpose will be to:

- Invest private sector funds in projects of mutual benefit.
- Facilitate comprehensive regeneration projects throughout the Borough.
- Provide potential capital receipts and/or revenue income streams to the Council from the on-going development of surplus land and buildings.



- Provide opportunities for any Council in-house surplus capacity to be deployed on development projects with a further fee income stream opportunity.
- Support the Council in the development of its strategic review of the property portfolio.

Access and public realm improvements will be delivered through a range of initiatives including the implementation of Opportunity Sites, partnership working, S106 planning obligations, CIL and through the Council's capital programme. The Implementation Plan for the Local Transport Plan 3 available on the Council's website sets out funding sources and a package of measures.

### **Monitoring**

To ensure that the vision and strategic objectives of the Southend Central Area Action Plan (SCAAP) are being met, the Council will monitor the implementation of the Plan's policies and proposals to gauge its overall performance.

To achieve this, a series of indicators and, where appropriate, targets, for the Plan's policies and proposals are set out below. Progress in implementing the Plan's provisions will be monitored against these indicators and targets. This will be carried out each year after the adoption of the SCAAP as part of the Council's Annual Monitoring Report. Where it is clear that any of the objectives of the Plan are not being met, appropriate action will be taken as part of the monitoring process or a review of the plan may be implemented.

In order to avoid duplication of policy provisions contained in other adopted plans and to keep the SCAAP plan concise and effective, the SCAAP does not contain specific policies relating to: employment, housing, culture, leisure, tourism and recreational facilities, and open and green spaces. These are contained within the Southend Core Strategy and Development Management Document and their provisions will be monitored as part of the annual monitoring process. They will be reported in the Southend Annual Monitoring Report (AMR) and will contribute to the process of gauging the effectiveness of the SCAAP.

Similarly, the Southend Local Transport Plan (LTP) contains a number of indicators for measuring the effectiveness of the LTP. The SCAAP does not repeat these but regard will be had to them in assessing the overall impact of the SCAAP on transport provision within the Central Area. Progress Reports on the Southend Local Transport Plan will be produced every two years starting with the period April 2015/16 to March 2016/17.

## Implementation and Monitoring

### Criteria Based Policies

#### Monitoring

Where feasible, the monitoring indicators as outlined in the Core Strategy will also be presented for the SCAAP area. This includes the indicators presented for the following Core Strategy Policies:

- Policy CP1: Employment Generating Development;
- Policy CP2: Town Centre and Retail Development;
- Policy CP3: Transport and Accessibility;
- Policy CP4: The Environment and Urban Renaissance;
- Policy CP6: Community Infrastructure;
- Policy CP7: Sport, Recreation and Green Space;
- Policy CP8: Dwelling Provision.

Specific reference to monitoring indicators is also provided below in reference to the SCAAP policies.

Policy DS1: A Prosperous Retail Centre			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council Public and private developers	Through the continual submission and determination of planning applications. Annual monitoring of retail floor space and refreshes of the retail study.	DS1.1 Proportion of frontage within Town Centre Primary Shopping Frontage that are in A1 Retail use – ensure compliance with policy target.  DS1.2 Proportion of units within Town Centre Primary and Secondary Frontage that are vacant - reduce As Core Strategy Policy CP2	Reliance on private sector funding and developer interest. Lack of Developer interest in retail sites. Out of centre developments reducing the capacity to support town centre retail. Growth of neighbouring and sub-regional town centre retail offer.

		As Indicator DM13.2	Changes to Central Government policy on Town Centre First. Further changes to Prior Approval or permitted development rights in town centres.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD: KP2, CP2 Development Management DPD: DM1, DM5, DM13</b>		

Policy DS2: Key Views			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council Public and private developers	Through the continual submission and determination of planning applications. Implementation of public realm improvements as set out by Policy Area Development Principles.	DS2.1 number of schemes that enhance visually important views – sight lines, access, open space and views improved to identified areas.	
<b>Other Relevant Policies</b>	<b>Core Strategy DPD: KP2, CP4 Development Management DPD: DM1, DM4, DM5, DM6</b>		

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Policy DS3: Landmarks and Landmark Buildings			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council Public and private developers	Through the continual submission and determination of planning applications. Implementation of public realm improvements as set out by Policy Area Development Principles.	DS3.1 number of appropriately located new landmark buildings – delivery of land mark buildings.	The new landmark building is not of a high quality design, and is poorly located in the townscape to the detriment of the local environment.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD: KP2, CP4 Development Management DPD: DM1, DM4, DM5, DM6</b>		

Policy DS4: Flood Risk Management and Sustainable Drainage			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council	Through the continual submission and determination of planning	DS4.1 Number of developments incorporating	A risk of low quality flood risk

Environment Agency Anglian Water Public and private developers	application. Site based flood risk assessments.	sustainable drainage systems (SuDS) - For all new development, new impermeable areas will be drained by SuDS. As Core Strategy Policy CP4.	assessments. Poorly designed SuDS.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD KP1, KP2, KP3, CP4: Development Management DPD: DM6</b>		

<b>Policy DS5 – Transport, Access and Public Realm</b>			
<b>Key Responsibilities</b>	<b>Implementation</b>	<b>Monitoring Indicators and Targets</b>	<b>Risks</b>
Southend Borough Council. Local Transport Operators. Public and private developers and owners.	Through the continual submission and determination of planning applications. Joint working with local transport operators. Joint working with private operators of car parks. Local Transport Plan and other funding mechanisms – £7m secured from first round of Local Growth Fund (LGF) to deliver transport and public realm improvements in the SCAAP area.	DS5.1 Providing a level of publically available car parking provision to support the vitality and viability of the central area – no net loss of permanent publically available car parking south of the central railway line. As Core Strategy Policy CP3. As Development Management Policy DM15.	Lack of funding for transport projects. Changes to rail or bus network, quality of service, number of services provided. Level of co-operation between operators and the local authority.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD KP2, KP3, CP3, CP4: Development Management DPD: DM15</b>		

## Policy Areas

Policy PA1: High Street Policy Area Development Principles			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council Public and private developers Landowners Business Improvement District English Heritage  211	Through the continual submission and determination of planning applications. Implementation of public realm improvements. LGF Funding. Stub end roads Pedestrianisation. Event space. Public realm improvements (greening). Town centre/ seafront connection (multi-level).	As Core Strategy Policies CP1, CP2, CP4, CP8  PA1.1 Total number of dwellings built, by size and tenure within Policy Area – 2,474 net additional dwellings by 2021 within SCAAP area.	Lack of interest in retail in the Town Centre. Growth of neighbouring and sub-regional town centre retail offer. Changes to Central Government policy on Town Centre First Further changes to Prior Approval or permitted development rights in town centres. Lack of inward investment opportunities. Effect of any out of town retail development.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD: KP1, KP2, KP3, CP1, CP2, CP4 Development Management DPD: DM1, DM2, DM5, DM15</b>		

Policy PA2: London Road Policy Area Development Principles			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council Public and private developers Street Market Operators University of Essex South Essex College	‘Victoria Gateway initiative’ Phase 2. Local Transport Plan 3. LGF Funding. Through the continual submission and determination of planning applications. Mixed-mode pedestrian and cycle priority route (LGF funding application). Tree planting.	As Core Strategy Policies CP1, CP2, CP3, CP4, CP8	Lack of funding for transport and public realm improvements. Lack of inward investment opportunities. Higher and further education establishments to not want to develop further in the town centre.

	landscaping/public art/integrated signage. Pedestrianisation/relocation of taxi rank.		
<b>Other Relevant Policies</b>	<b>Core Strategy DPD: KP1, KP2, KP3, CP1, CP2, CP4 Development Management DPD: DM1, DM2, DM15</b>		

<b>Policy PA3: Elmer Square Policy Area Development Principles</b>			
<b>Opportunity Site Elmer Square Phase 2 (PA3.1)</b>			
<b>Key Responsibilities</b>	<b>Implementation</b>	<b>Monitoring Indicators and Targets</b>	<b>Risks</b>
Southend Borough Council University of Essex South Essex College Public and private developers	Elmer Square Phase 2 project to complement the recently completed Forum public and academic library - Exploration of use of £6m notional allocation of LGF funding. Through the continual submission and determination of planning applications. Local Transport Plan 3. Mixed mode pedestrian and cycle priority route.	As Core Strategy Policies CP1, CP4, CP6	Lack of funding for transport and public realm improvements Lack of inward investment opportunities. Higher and further education establishments to not want to develop further in the town centre. Lack of funding for large scale projects .
<b>Other Relevant Policies</b>	<b>Core Strategy DPD: Development Management DPD:</b>		

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<b>Policy PA4: Queensway Policy Area Development Principles</b>			
<b>Opportunity Site 'Better Queensway' Project (PA4.1)</b>			
<b>Key Responsibilities</b>	<b>Implementation</b>	<b>Monitoring Indicators and Targets</b>	<b>Risks</b>
Southend Borough Council Public and private developers Registered Housing Providers	Better Queensway Project. Through the continual submission and determination of planning applications. Local Transport Plan 3. New community infrastructure. New public open space - Queensway Urban Park. Public realm improvements. Pedestrian and cycle crossing.	As Core Strategy Policies CP1, CP2, CP3, CP4, CP6, CP7, CP8	Lack of funding for transport and public realm improvements. Lack of inward investment opportunities. Additional cost of transport realignment, particularly in relation to the Queensway Dual Carriageway. Lack of interest from developers.

	Create mixed mode pedestrian and cycle priority route and shared priority route. Chichester Road improvements. Improvement to Southchurch Road retail area. Application made for Local Growth Funding specific to Better Queensway Project.		Change to political focus and priority for a large scale project of this size. Lack of support from local residents and wider community.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD:</b> KP1, KP2, KP3, CP1, CP4, CP6, CP8 <b>Development Management DPD:</b> DM1, DM2, DM3, DM7, DM8, DM15		

### Policy PA5: Warrior Square Policy Area Development Principles

Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council Public and private developers  213	Through the continual submission and determination of planning applications. Linked to Better Queensway Project. Public realm improvements. New pedestrian and cycle priority route and shared priority route. LGF Funding. Environmental improvements to Queensway and Chichester Road. Children's play facility at Warrior Square Gardens - £150,000 cost identified by IDP.	As Core Strategy Policies CP1, CP4, CP6, CP8	Lack of funding for transport and public realm improvements. Lack of inward investment opportunities. Lack of interest in office development. Lack of funding for children's play facility.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD:</b> KP1, KP2, KP3, CP1, CP3, CP4, CP6, CP7, CP 8 <b>Development Management DPD:</b> DM1, DM2, DM3, DM4, DM5, DM7, DM8, DM10, DM15		

### Policy PA6: Clifftown Policy Area Development Principles

Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council Public and private developers Landowners Transport Operators English Heritage	Through the continual submission and determination of planning applications. Tree Planting Programme. LGF Funding. Local Transport Plan 3. Public realm improvements. Regenerate the forecourt at Southend Central Station.	As Core Strategy Policies CP1, CP2, CP4, CP8	Lack of funding for transport and public realm improvements. Lack of inward investment opportunities. Lack of support from local community in relation to proximity to conservation area and noise.

	Redevelop Central House for retail, residential, offices. Regenerate site of Empire Theatre for cultural uses. Provision of information boards/digital technology to interpret historic assets.		Level of co-operation between rail operators and local authority to initiate public realm improvements
<b>Other Relevant Policies</b>	<b>Core Strategy DPD:</b> KP1, KP2, KP3, CP2, CP4, CP7 <b>Development Management DPD:</b> DM1, DM2, DM4, DM5, DM6, DM14		

<b>Policy PA7: Tylers Policy Area Development Principles</b>			
<b>Opportunity Site Tylers Avenue (PA7.1)</b>			
<b>Key Responsibilities</b>	<b>Implementation</b>	<b>Monitoring Indicators and Targets</b>	<b>Risks</b>
Southend Borough Council Public and private developers Travel Operators  214	Through the continual submission and determination of planning applications. Need for a masterplan. LGF funding. Local Transport Plan 3. Creation of a new public open space. Mixed mode pedestrian and cycle priority route. Home Zone. Improved walking and cycling linkages. Junction improvements at Queensway.	As Core Strategy Policies CP1, CP2, CP3, CP4, CP8	Lack of interest in retail or commercial space. Cost of parking re-provision and new travel interchange. Lack of funding for transport and public realm improvements Lack of inward investment opportunities. Lack of support from local community for home zone.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD:</b> KP1, KP2, KP3, CP2, CP3, CP4, CP8 <b>Development Management DPD:</b> DM1, DM2, DM3, DM4, DM7, DM8, DM10, DM15		

<b>Policy CS1: Central Seafront Policy Area Development Principles</b>			
<b>Opportunity Sites: Southend Pier (CS1.1); Seaways (CS1.2); Marine Plaza (CS1.3); New Southend Museum (CS1.4)</b>			
<b>Key Responsibilities</b>	<b>Implementation</b>	<b>Monitoring Indicators and Targets</b>	<b>Risks</b>
Southend Borough Council Public and private developers Environment Agency	Through the continual submission and determination of planning applications. Completion of committed sites at Marine Plaza and New Museum. Application made for LGF specific to museum. Continual maintenance and investment in the Pier, including the	As Core Strategy Policies CP1, CP2, CP3, CP4, CP7, CP8. As Development Management Policy DM12.	Lack of funding for transport and public realm improvements. Lack of inward investment opportunities. Outcome of screening under



	<p>installation of new digital technologies. Expansion of City Beach. Completion of committed new lagoon. Flood risk and mitigation measures. Improving connectivity from Town Centre and Central Seafront. Rationalise signage, street furniture, green grid. Upgrade the Cliffs Pavilion outdoor space. Improve traffic management, parking, walking and cycling. New frontage on the southern side of the Royals Shopping Centre. LGF funding.</p>		<p>Habitats Regulations. Lack of developer interest. Development of out of town retail and leisure attractions.</p>
<b>Other Relevant Policies</b>	<b>Core Strategy DPD:</b> KP1, KP2, KP3, CP3, CP4, CP7, CP8 <b>Development Management DPD:</b> DM1, DM2, DM3, DM4, DM5, DM6, DM7, DM8, DM9, DM10, DM12, DM14, DM15		

Policy CS2: Nature Conservation and Biodiversity			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
<p>Southend Borough Council Natural England</p>	<p>Through the determination of planning applications. Project-level Habitats Regulation Assessment where necessary. Development of visitor facility close to foreshore. Provision of public open space at Pier Hill, Seaways, Eastern Esplanade.</p>	<p>As Core Strategy Policies CP4, CP7</p>	<p>Outcome of screening under Habitats Regulations.</p>
<b>Other Relevant Policies</b>	<b>Core Strategy DPD:</b> KP1, KP2, KP3, CP4, CP7 <b>Development Management DPD:</b> DM6		

Policy CS3: The Waterfront			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
<p>Southend Borough Council</p>	<p>Through the determination of planning applications. Completion of committed new lagoon (Coastal Communities Fund). Public realm improvement. Provision of information boards/digital technology to interpret biodiversity of area.</p>	<p>As Core Strategy Policies CP4, CP7</p>	<p>Lack of funding</p>
<b>Other Relevant Policies</b>	<b>Core Strategy DPD:</b> KP1, KP2, KP3, CP4, CP7 <b>Development Management DPD:</b> DM6		

**Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles**

**Opportunity Sites: Victoria Avenue (PA8.1); Baxter Avenue (PA8.2)**

Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
<p>Southend Borough Council Public and private developers Land owners Registered Housing Provider</p> <p align="center">216</p>	<p>Through the continual submission and determination of planning applications. Completion of committed sites at Victoria Avenue and Carnarvon Road. Local Transport Plan 3. Recent completion of the Hive Southend Business Hub (Southend City Deal and £0.7m LGF match funding). Additional education facilities. Junction improvements at Victoria Avenue/ Fairfax Drive. Junction improvements at Victoria Avenue/ East Street/West Street (LGF funding). Junction improvements at Victoria Avenue/ Carnarvon Road (LGF funding). Junction improvements at Victoria Avenue/ Great Eastern Avenue (LGF funding). Enhancements to North Road including civic space at junction with Chelmsford Avenue. Enhancement of the Civic space on east side of Victoria Avenue/urban greening. Create mixed mode pedestrian and cycle priority route (LGF funding).</p>	<p>As Core Strategy Policies CP1, CP2, CP3, CP4, CP6, CP7, CP8.</p>	<p>Lack of funding for transport and public realm improvements Lack of inward investment opportunities. Lack of inward investment Further changes to Prior Approval or permitted development rights. Lack of interest for new office accommodation. Fragmented approach. Multiple site ownership.</p>
<p><b>Other Relevant Policies</b></p>	<p><b>Core Strategy DPD:</b> KP1, KP2, KP3, CP1, CP3, CP4, CP6, CP7, CP8 <b>Development Management DPD:</b> DM1, DM2, DM3, DM4, DM5, DM7, DM8, DM9, DM10, DM13, DM15</p>		

**Policy PA9: Sutton Gateway Neighbourhood Policy Area Development Principles**

Opportunity Sites: Sutton Road (PA9.1); Guildford Road (PA9.2)			
Key Responsibilities	Implementation	Monitoring Indicators and Targets	Risks
Southend Borough Council Public and private developers	Through the continual submission and determination of planning applications. Completion of committed sites at Sutton Road. Enhancements to Sutton Road – streetscape and landscape. LGF funding	As Core Strategy Policies CP1, CP2, CP3, CP4, CP6, CP8.	Lack of funding for transport and public realm improvements. Lack of inward investment opportunities. Multiple site ownership.
<b>Other Relevant Policies</b>	<b>Core Strategy DPD:</b> KP1, KP2, KP3, CP1, CP3, CP4, CP6, CP7, CP8 <b>Development Management DPD:</b> DM1, DM2, DM3, DM4, DM7, DM8, DM9, DM10, DM11, DM13, DM14, DM15		

## Appendix 1: Southend Core Strategy Policies

Core Strategy Policy KP1: Spatial Strategy	Sets out the spatial strategy for the Borough; the primary focus of regeneration and growth will be the Town Centre and Central Area, including the seafront.
Core Strategy Policy KP2: Development Principles	Sets out the ways in which new development in the Borough, including transport infrastructure, should contribute to economic, social, physical and environmental regeneration in a sustainable way, and to the regeneration of Southend's primary role within the Thames Gateway as a cultural and intellectual hub and higher education centre of excellence.
Core Strategy Policy KP3: Implementation and Resources	Makes provision for the preparation of Area Action Plans and Supplementary Planning Documents to help deliver the provisions of the Core Strategy, ensuring that development of an appropriate scale, mix and quality is brought forward in key areas of opportunity and change including the town centre and seafront.
Core Strategy Policy CP1: Employment Generating Development	Makes provision for an additional 6,500 jobs within the Town Centre and Central Area during the plan period 2001-2021.
Core Strategy Policy CP2: Town Centre and Retail Development	Promotes the development of the Town Centre, which will remain the first preference for all forms of retail development and other Town Centre uses. The Town Centre is afforded sequential preference for additional comparison and convenience floorspace.
Core Strategy Policy CP3: Transport and Accessibility	Seeks improvements to transport infrastructure and services by widening travel choice, and providing for high quality transport interchanges at Southend Victoria, Southend Central and Southend Travel Centre.
Core Strategy Policy CP4: The Environment and Urban Renaissance	Promotes sustainable development of the highest quality and innovation and excellence in design, recognising good quality urban design as a catalyst for regeneration and urban renaissance.
Core Strategy Policy CP5: Minerals and Soils Resources	Sets out the standards the Borough Council will require regarding the sustainable use of soil and mineral resources.
Core Strategy Policy CP6: Community Infrastructure	Supports improvements to existing, and the provision of new, facilities to support the needs of education, skills and lifelong learning strategies, including the Town Centre's higher education/university campus. Seeks to safeguard existing and provide for new leisure, cultural, recreation and community facilities.
Core Strategy Policy CP7: Sport, Recreation and Green Space	Promotes proposals that contribute to sports, recreation and green space facilities within the Borough for the benefit of local residents and visitors.
Core Strategy Policy CP8: Dwelling Provision	Makes provision for 2,000 net additional dwelling within the Town Centre and Central Area.

## Appendix 2 Glossary of Abbreviations

AA	Appropriate Assessment
AAP	Area Action Plan
AMR	Annual Monitoring Report
AVL	Advanced Vehicle Location
BBA	Better Bus Area
BID	Business Improvement District
c2c	Rail Operator of the London Fenchurch Street to Shoeburyness line
CAM	Central Area Masterplan
CCG	Care Commissioning Group
CCTV	Closed Circuit Television
CIRIA	Construction Industry Research and Information Association
DCLG	Department of Communities & Local Government
DFT	Department for Transport
DPD	Development Plan Document
EA	Environment Agency
ELR	Employment Land Review
FRA	Flood Risk Assessment
FWMA	Floods and Water Management Act 2010
HRA	Habitats Regulation Assessment
LDD	Local Development Document
LDS	Local Development Scheme
LEA	Local Economic Assessment
LLFA	Lead Local Flood Authority
LPA	Local Planning Authority
LSTF	Local Sustainable Transport Fund
LTP	Local Transport Plan
MSCP	Multi Storey Car Park
NHS	National Health Service
NPPF	National Planning Policy Framework
OAN	Objectively Assessed Need
PIP	Punctuality Improvement Partnership
PFRA	Preliminary Flood Risk Assessment
SAB	SUDS Approval Body
SE LEP	South East Local Enterprise Partnership
SFRA	Strategic Flood Risk Assessment
SHMA	Strategic Housing Market Assessment
SWMP	Surface Water Management Plan
TGSE	Thames Gateway South Essex

### Appendix 3 – Schedule of Existing Landmark Buildings



Adventure Island, Western Esplanade  
(*Central Seafront Policy Area*)



All Saints Church, Sutton Road  
(*outside of the SCAAP boundary*)



Central Library (former), Victoria Avenue  
(*Victoria Gateway Neighbourhood Policy Area*)



Central Museum, Victoria Avenue  
(*Victoria Gateway Neighbourhood Policy Area*)



Civic Centre, Victoria Avenue  
*(Victoria Gateway Neighbourhood Policy Area)*



Cliff Lift, Western Esplanade  
*(Central Seafront Policy Area)*



Cliffs Pavilion, Station Road  
*(Central Seafront Policy Area)*



Clifftown Church/Studios, Nelson Street  
*(Clifftown Policy Area)*



Park Inn Palace Hotel, Pier Hill  
*(Central Seafront Policy Area)*



Pier Hill Observation Tower and Lift,  
Pier Hill  
*(Central Seafront Policy Area)*



Porters, Southchurch Road  
*(outside of the SCAAP boundary)*



Prittlewell Chapel, North Road  
*(Victoria Gateway Neighbourhood  
Policy Area)*





Royal Hotel and Royal Terrace  
*(High Street and Clifftown Policy Areas)*



Seafront / Estuary  
*(Central Seafront Policy Area)*



South Essex College, Luker Road  
*(Elmer Square Policy Area)*



St John's Church, Herbert Grove  
*(Central Seafront Policy Area)*



St Mary's Church, Victoria Avenue  
*(outside of the SCAAP boundary)*



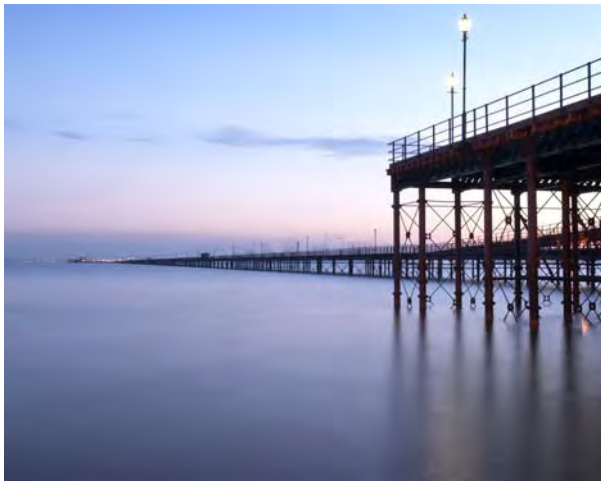
Swan Hall, Victoria Avenue  
*(Victoria Gateway Neighbourhood Policy Area)*



The Forum, Elmer Square  
*(Elmer Square Policy Area)*



The Kursaal, Eastern Esplanade  
*(Central Seafront Policy Area)*



The Pier  
*(Central Seafront Policy Area)*



University of Essex, Elmer Approach  
*(Elmer Square Policy Area)*



University of Essex, Elmer Approach  
*(Elmer Square Policy Area)*

## **Appendix 4: Flood Risk Management Technical information and Definitions**

### Flood Risk Definitions:

Flood risk zone 2 – medium risk, 1 in 1,000 to 1 in 200 annual probability

Flood risk zone 3 - high risk, 1 in 200 annual probability or more

Design flood event and flood level – based on 1 in 200 annual probability event at the end of the development's lifetime.

Extreme flood event and flood level – based on 1 in 1,000 annual probability event at the end of the development's life.

As an example, the SFRA2 predicts that within the Central Seafront Area, outside of Adventure Island, by 2110, the design and extreme maximum flood depth are approximately 1 metre and 4 metres. Adventure Island is characterised by 5m maximum flood depths under both scenarios.

Lifetime of development – assumed to be 100 years for residential, 60 years for commercial (unless circumstances indicate otherwise)

### Flood Risk Vulnerability Classification

#### Highly Vulnerable

- Emergency services
- Emergency dispersal points
- Basement dwellings

#### More Vulnerable

- Hospitals
- Residential institutions such as care homes, children's homes, and hostels
- Buildings used for: dwelling houses; student halls of residence; drinking establishments; nightclubs; and hotels
- Health services, nurseries and educational establishments

## Appendix 5: Transport, Access and Public Realm Strategy

### Transport, Access and Public Realm Strategy

In order to secure a 'step change' in Southend Central Area to achieve a modern integrated and accessible transport system that unlocks potential in opportunity sites and secures sustainable regeneration and growth, complemented by a quality, inclusive public realm, the Council will:

- Work in partnership with the **South East Local Enterprise Partnership (SELEP)** to deliver investment and improvements to the strategic road network and public realm, highlighting the importance of the A127 strategic corridor to delivering economic growth and housing, as set out in the Southend and Essex **A127 Corridor for Growth – An Economic Plan** (March 2014) .
- Continue the programme of public realm and access improvements, including a continuation of the Victoria's public realm improvement scheme at London Road, Queensway (west) and Victoria Circus, Queensway (east) (**Policy PA4**), Victoria Avenue (**Policy PA9**), and the Central Seafront Policy Area (including City Beach) (**Policy CS1**), addressing the principles established by the **Southend Streetscape Manual SPD3**.
- Continue to develop and support the cycle route network, provision of secure cycle parking and work with **Cycle Southend** in terms of promotion, marketing, Bikeability and other travel training, and the creation of cycle hubs.
- Seek to pedestrianise a number of the High Street's inter-linking access/'stub' roads and reduce the dominance of on-street parking and general vehicle circulation, creating one-way streets and 20mph zones.
- Improve the environmental quality of existing residential streets within the Central Area, particularly those to the east of the High Street, to create 'home zones' or pedestrian-priority areas that improve access and encourage walking and cycling.
- Continue to implement a quality signage and way-finding scheme for pedestrians and ensure that travel information better relay details to road users.
- Encourage the use of sustainable travel modes, ensuring that all forms of transport are equally accessible to all, through smarter choices techniques (including the promotion of a Borough-wide Smart Card ticketing system, and through the use of mobile phone technology) and mobility management measures, promoting opportunity for car sharing and the setting up of car and van clubs.
- Maintain and build upon existing bus stop improvements, real time information and bus prioritisation at signals, as well as targeted junction enhancements and highways improvement works.
- Improve public perceptions of safety within Southend Central Area particularly at night, by ensuring that street lights are maintained, CCTV is obviously sited, and public transport and taxis operate after dark to help secure a vibrant, safe evening economy.
- Work with local bus operators to further improve bus services to the town centre including evening and night time services, providing a more reliable and punctual bus service through the implementation of a Punctuality Improvement Partnership (PiP),

and improvements to the Advanced Vehicle Local (AVL) system.

- Work with train operators to achieve high levels of reliability and performance on all services, maintain and promote contra-flow inter-peak services for journeys to Southend, explore park and ride opportunities that provide quick and convenient access to the Town Centre and Central Seafront, continue active participation in the development and marketing of Station Travel Plans, and encourage pedestrian and cycle links between Southend Central and Southend Victoria Railway Stations and the Travel Centre.
- Work with taxi operators to improve the provision for taxi's at key locations to support access.
- Work with stakeholders to develop the work of the ***Ideas in Motion*** brand to encourage businesses, schools and colleges to implement managed travel plans and introduce a personalised travel planning service, and to support applications for funding.
- Seek to develop a priority route towards ***London Southend Airport***, together with corresponding junction improvements at A127 Victoria Avenue/Fairfax Drive/East Street.
- Manage car parking demand within the Southend Central Area network through a combination of measures to support the vitality and viability of the town centre and central seafront area; balance parking supply between the car parks and development sites north and south of the railway line; ensure there is no net loss of public car parking south of the central railway line.
- Improve access to car parks for vehicles and pedestrians.
- Encourage an extension to the existing VMS scheme, or updated technology, to improve information about car parking capacity within town centre car parks, direct drivers to the most convenient and accessible car park, and avoid unnecessary circulating traffic by improving access to town centre car parks from Queensway dual carriageway.
- Ensure that servicing and delivery arrangements meet the reasonable needs of businesses, and minimise their environmental impact; working with the freight industry and logistic to implement more efficient use of vehicles in terms of guidance, zoning and delivery timetables.

## Appendix 6 – Dwelling Figures for the Central Area - relationship with Core Strategy Requirements

	Core Strategy 2001 to 2021	Completed April 2001 to March 2016 (net)	Core Strategy (as adjusted 2016 to 2021)	Site Allocations (net) to be delivered by 2021*	Other commitments to be delivered by 2021	Total New Dwellings	Difference from Core Strategy (adjusted 2014 to 2021)
SCAAP	2,474	1,087	1,387	1,732	434	2,166	+779
Local Authority Area excluding SCAAP	4,026	3,694	332	To be determined	786	786	+454
All Borough (Total)	6,500	4,781	1,719	1,732	1,220	2,952	+1233

\* In relation to the amount of dwellings to be delivered via Opportunity Sites within the SCAAP area by 2021 the following assumptions have been made:

- Queensway (PA4.1): half of the site is assumed to be delivered by 2021
- Victoria Avenue (PA8.1): sites with planning permission assumed to be delivered by 2021
- Baxter Avenue (PA8.2): half of the site is assumed to be delivered by 2021
- Sutton Road (PA9.1): sites with planning permission assumed to be delivered by 2021

## Appendix 7: Replaced Saved Planning Policies<sup>16</sup>

Saved Planning Policies replaced by Southend Central Area Action Plan upon adoption

Policy/ Proposal Ref.	Policy Subject
L2	Central Seafront Area
L3	Southend Pier
C7	Shop and Commercial Frontages and Fascias
S5	Non-Retail Uses
S8	Improvements to Primary Shopping Frontages
S9	Retention of Secondary Shopping Frontages
P3a	Proposal Site: Former Gas Works Site, Eastern Esplanade
P3b	Proposal Site: Land at Burnaby Road
P3d	Proposal Site: Land West of Baltic Avenue
P3g	Proposal Site: Scrap Metal Yard, 215a North Road
P3j	Proposal Site: Industrial Uses, Roots Hall Avenue
P3k	Proposal Site: Industrial Uses between Roots Hall Avenue and Victoria Avenue
P4a	Proposal Site: Baxter Avenue
P4c	Proposal Site: Whitegate Road
P4d	Proposal Site: Pitmans Close
P4k	Proposal Site: Central Station, Clifftown Road
P5b	Proposal Site: Warrior Square (South side) and Whitegate Road (North side)
P5c	Proposal Site: Tylers Avenue and York Road
P6c	Proposal Site: Southchurch Avenue/ Marine Parade
P9b	Proposal Site: London Road
P9c	Proposal Site: Warrior Square
P9k	Proposal Site: Seaway Car Park, Queensway

<sup>16</sup> Saved planning policies were effective from 27th September 2007 - Direction under paragraph 1(3) of Schedule 9 to the Planning and Compulsory Purchase Act 2004.



## Appendix 8: Marketing Evidence

This appendix sets out requirements for applicants to produce evidence to demonstrate that A1 retail premises are no longer in demand, viable or suitable for their continued permanent authorised use.

The Council may seek independent advice, funded by the applicant at a reasonable cost, to test the veracity of any marketing exercise. This verification will assess the accuracy and robustness of the matters listed below.

In respect to Policy DS1.4(a) vacant units could include units occupied for temporary or 'flexible' uses, permitted through a temporary planning permission or under permitted development rights.

### PART A - Marketing

In relation to Policy DS1.4. the following details will be used to assess the acceptability, or otherwise, of the information submitted and any marketing undertaken.

Marketing evidence requires demonstration of an active marketing campaign for a continuous 2 year period, whilst the premises were vacant\*, which has shown to be unsuccessful.

Marketing must be through a commercial agent at a price that genuinely reflects the market value of the lawful use. It must be shown to the council's satisfaction that marketing has been unsuccessful for all relevant floorspace proposed to be lost through redevelopment or Change of Use.

Active marketing should include all of the following:

1. A visible advertisement board posted in a prominent location on site, including relevant contact information (subject to advertising consent, if required);
2. Registration of property with at least one commercial property agent and continuously advertised on the agent's website;
3. Property details and information available to enquirers on request;
4. Property marketed at a reasonable price reflecting market conditions, including in relation to use, condition, quality and location of the premises/ site;
5. Property marketed for the appropriate use or uses as defined by the relevant planning policy.

Sufficient detailed information is required to be submitted alongside any planning application to demonstrate compliance with the above criteria.

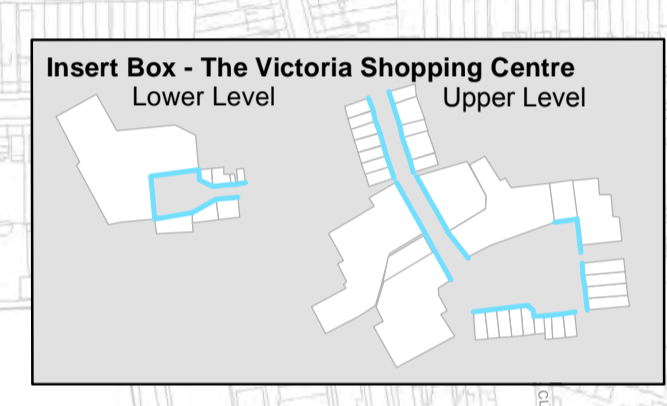
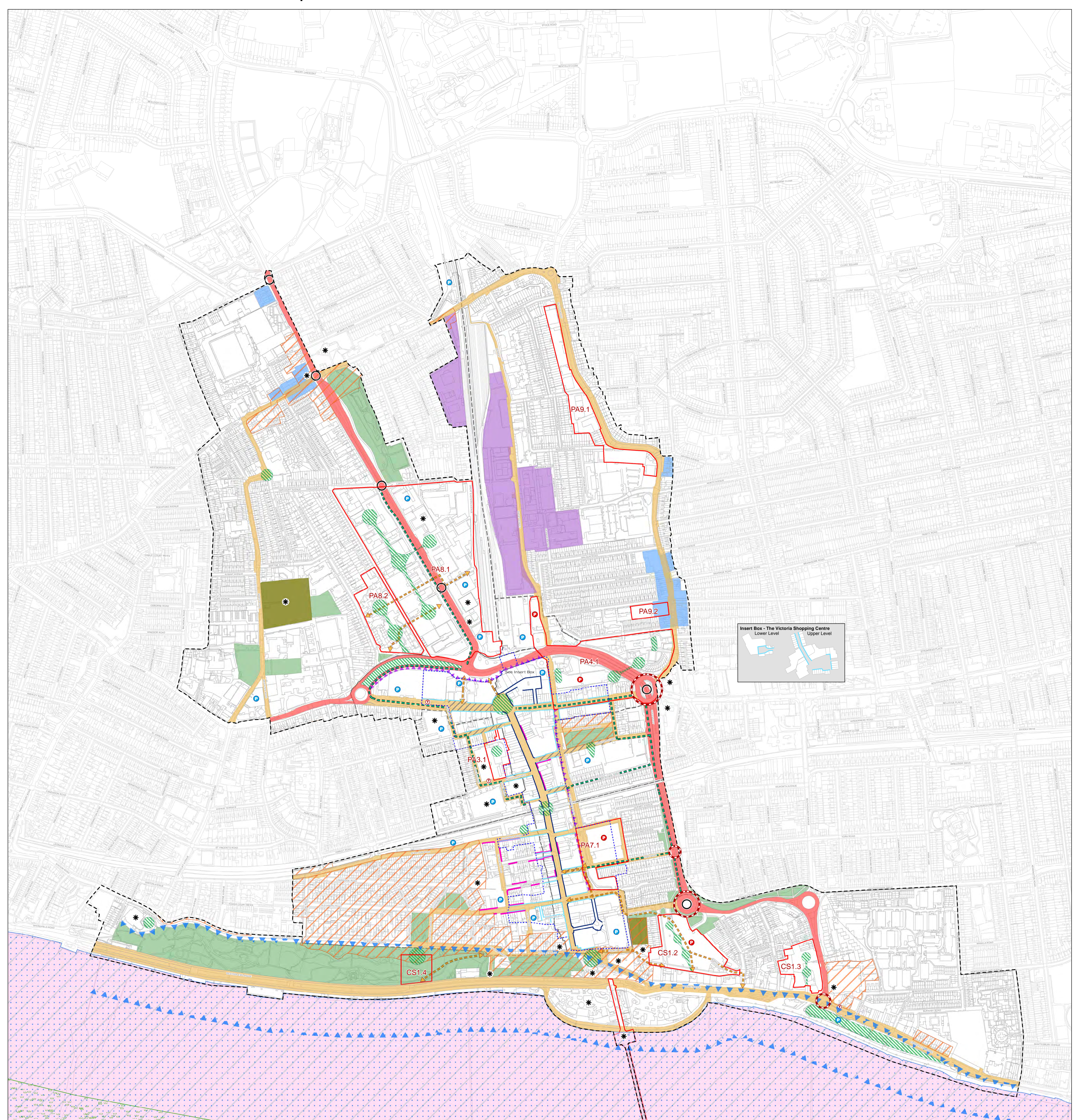
Additionally, information should be submitted regarding:

- i. the number and details of enquiries received;
- ii. the number of viewings;
- iii. the number, type, proposed uses and value of offers received;
- iv. reasons for refusal of any offer received, and/or reasons why any offers fell through;
- v. the asking price and/or rent that the site or property has been offered at, including a professional valuation from at least three agents to confirm that this is reasonable;
- vi. the length of marketing period, including dates, and

vii. the length of the vacancy period.

*\* - vacant units could include units occupied for 'meanwhile uses' or temporary uses, permitted through a temporary planning permission or under permitted development rights.*

# Southend Central Area Action Plan - Policies Map



**Southend Central Area Action Plan Policies Map**

**Southend Central Area Boundary**  
 - Southend Central Area Boundary

**Policy Area Boundaries**  
 - Policy Area Boundaries

**Site Allocations**  
 - Opportunity Site

**Employment**  
 - Employment Areas (KP1, CP1, DM10, DM11)  
 - Town Centre Primary Shopping Area (CP2, DS1)  
 - Town Centre Primary Shopping Frontages (DS1)  
 - Town Centre Secondary Shopping Frontages (DS1)  
 - Secondary Shopping Frontage - Local Centre (DM13)

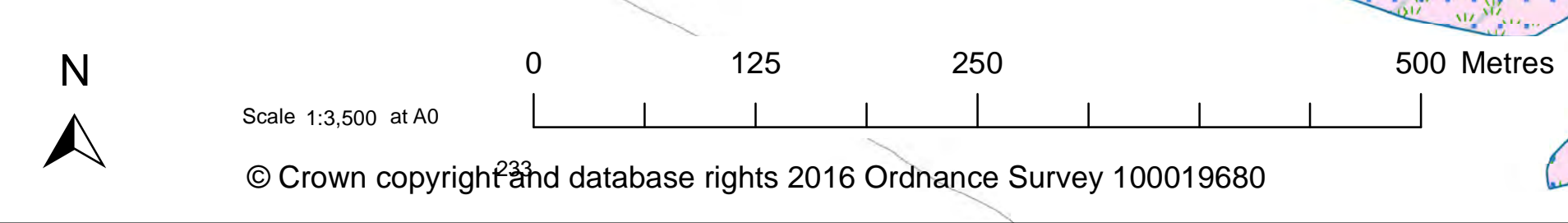
**Historic Environment**  
 - Conservation Area (KP1, CP4, DM5, PA5, PA6, PA8, CS1)  
 - Frontage of Townscape Merit (KP2, CP4, DM5, PA1, PA6)

**Townscape**  
 - Visually Active Frontage (DS5)  
 - New/Improved Open Space - Indicative Location (All Policy Areas)  
 - Seafront Landmark Area (DS3)  
 - Existing Landmark Building (DS3)  
 - Protected Green Space (KP2, CP4, CP7)  
 - Crematorium, Graveyard or Cemetery (KP2, CP4, CP7)

**Movement and Activity**  
 - Key Public Realm Improvement (All Policy Areas, DS5, CP4)  
 - New/Improved Pedestrian Links (All Policy Areas, DS5, DM15, CP3)  
 - New/Improved Pedestrian/Cycle Links (PA2, PA3, PA4, PA5, PA7)  
 - Proposed Pedestrianised Street (DS5, PA1, PA2, PA3, PA5)  
 - Improved Gateway Access for Pedestrians, Cyclists and Public Transport (DS5, CS1, PA2, PA4)  
 - Main Route Network - Crossing and Environmental Improvements (DS5, DM15, CP3)  
 - Proposed Strategic Junction Improvement (CP3, DS5, PA2, PA4, PA8, CS1)  
 - Existing Car Park (DS5)  
 - Reconfigured Car Park (DS5, PA4, PA7, CS1)  
 - Indicative Proposed Taxi Rank (DS5, PA2, CS1)

**Natural Resources**  
 - Local Nature Reserve (KP1, KP2, CP4, DM6)  
 - Ramsar (KP1, KP2, CP4, DM6, CS2)  
 - Site of Special Scientific Interest (KP1, KP2, CP4, DM6, CS2)  
 - Special Protection Area (KP1, KP2, CP4, DM6, CS2)  
 - Local Wildlife Site (KP2, CP4)

The Policies Map for the SCAAP does not include areas at risk of flooding. This information is contained within the Strategic Flood Risk Assessment and in any future advice / information provided by the Environment Agency. This approach enables flood risk data to be updated when required. Southend Borough Council and the Environment Agency can be contacted to confirm the most up to date information.



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**Southend Central Area Action Plan (SCAAP) Revised Proposed  
Submission  
Consultation Statement - Regulation 19**

**September 2016**

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<b>Appendix 5</b>	Summary of the Sustainability Appraisal for the Southend Central Area Action Plan Preferred Approach (December 2015)

## Section 1: Introduction

- 1.1 This statement has been prepared to comply with the requirements of Regulation 19 of the Town and Country Planning (Local Planning) (England) Regulation 2012.
- 1.2 This Consultation Statement provides a summary of the representations received on the Preferred Approach version of the Southend Central Area Action Plan (SCAAP) published in November 2015.
- 1.3 The process of producing the Southend Central Area Action Plan (SCAAP) has been informed by a number of public consultation events, namely:
  - Town Centre Area Action Plan Issues and Options 2007
  - Seafront Area Action Plan Issues and options version 2007
  - Central Area Masterplan 2007
  - SCAAP Issues and Options June 2010
  - SCAAP (Superseded) Proposed Submission September 2011
  - SCAAP Preferred Approach November 2015
- 1.4 Earlier SCAAP consultation statements published in December 2015 and September 2011, available on the Council's website, summarise the representations of these earlier events: [www.southend.gov.uk/scaap](http://www.southend.gov.uk/scaap).

## Section 2: Consultation under Regulation 19 Southend Central Area Action Plan Preferred Approach (2015)

- 2.1 The Southend Central Area Action Plan (SCAAP) Preferred Approach was published so that representations could be made between 18th December 2015 and 15<sup>th</sup> February 2016. This was extremely valuable and provided the Council with a number of helpful suggestions that would then improve the plan.
- 2.2 The Preferred Approach consultation was carried out in line with the Council's adopted Statement of Community Involvement (2013) and relevant planning regulations. During the 8 week consultation the plan was publicised in the local press, the council's website [www.southend.gov.uk/scaap](http://www.southend.gov.uk/scaap) and was available to view at the Council offices and all local libraries. Consultation response forms were also available.
- 2.3 **Appendix 1** set outs the list of consultees contacted and **Appendix 2** sets out a copy of the consultation material used during the Preferred Approach publication period. **Table 1** below sets out details of the consultation methods used to engage the resident and business community in the preparation of the Preferred Approach version of the Southend Central Area Action Plan.
- 2.4 In total 33 organisations and individuals made 543 representations on the Southend Central Area Action Plan Preferred Approach. However, one such submission represented the views of over fifty businesses located within the central seafront area. Of the 543 duly made representations, 44 were considered as 'objections' to the plan and 157 supported the plan or parts of it. There were also 342 general comments. There were no 'not duly made' representations received.
- 2.5 **Table 2** below summarises the type of representations made on the Southend Central Area Action Plan Preferred Approach. A detailed summary of the 543 representations received and the Council's response to these is set out in **Appendix 3**.
- 2.6 In addition to these representations received, a further 400 comments were made by organisations and individuals on the Preferred Approach following the holding of a series of workshops as part of the consultation methods used. A number of those making comments also made written representations on the Plan.
- 2.7 The workshops were held on 20<sup>th</sup> and 21<sup>st</sup> January 2016 at Park Inn, Palace Hotel located within the plan area. The workshops sought to further engage the local business community and local residents and included a detailed look at the proposed Policy Areas as outlined in the SCAAP. Six separate sessions were held over the two days. Two each were targeted at specific groups, namely local businesses, the local community and Southend elected Members.
- 2.8 **Table 3** below summarises the type of representations made at the workshops whilst **Appendix 4** provides a detailed summary of the comments and the Council's response to these.



**Table 1: Consultation Methods (Carried out during Southend Central Area action Plan Preferred Approach Consultation)**

Method	Action Taken
Direct Consultation with Specific, General and Other Consultees including hardcopies/electronic copies of the consultation document where appropriate	Letter sent on 18 <sup>th</sup> December 2015 to all contacts on the LDF database to inform them that the SCAAP Preferred Approach consultation document was published for consultation. The database contains 700 consultees representing Specific, General and Other Consultees.
	Hard copies of the document were printed and made available on request.
	Letters and hard copies of the SCAAP Preferred Approach consultation document were sent to all of the Southend-on-Sea Borough Councillors on 18 <sup>th</sup> , 21 <sup>st</sup> December and 15 <sup>th</sup> 20 <sup>th</sup> January 2016.
	Letters were sent to all residents living within or adjacent to the Opportunity Sites set out within the SCAAP Preferred Approach version on 13 <sup>th</sup> January 2016.
	An email was sent to all of Southend-on-Sea Borough Council's Corporate Directors informing them of Consultation and requesting dedicated officer for a response. Hard Copies were supplied on request.
	An email was sent to the Southend Tourism Partnership informing them of the consultation and public workshops.
	An email was sent to the BID partnership informing them of the consultation and public workshops.
Inspection copies were made available at all of the public libraries in the Borough and at the Civic Centre	Copies of the SCAAP Preferred Approach consultation document with posters and leaflets were placed at all libraries and Council Offices on 18 <sup>th</sup> December 2015.
Publish on the Southend-on-Sea Borough Council website	The SCAAP Preferred Approach consultation document was published on the Southend-on-Sea Borough Council website with a JDi on line consultation facility and ability to download document on 18 <sup>th</sup> December 2015. Information was provided on how to obtain hard copies and/or view at deposit points.
	Leaflets produced providing advice on the on-line consultation system and left at deposit points/exhibitions.
Publication of Newsletters and/or Leaflets as appropriate	21 <sup>st</sup> December 2015 consultation leaflets were printed advertising the public consultation workshops (see below).
	Poster and Leaflets deposited at all Doctors Surgeries on 21 <sup>st</sup> December 2015 in order to potentially target some of the harder to reach groups.
	Consultation information included within the Southend Business Partnership Newsletter, January 2015, and published on the Business on Sea website.
Press Release + newspaper notice	Press Release to local papers issued 18 December 2015 and 14 January 2016. Supported by Twitter and Facebook activity.
	Advert about public consultation and information about public workshop event in Town Centre (see below) placed in local press on Friday 15 <sup>th</sup> and 22 <sup>nd</sup> January 2016 [Yellow Advertiser].
Banners	Banners placed in the Civic Centre and at The Forum (public library in the Town Centre) on 18 <sup>th</sup> December 2015.
Area Forums/ Workshops/ Presentations	Public consultation workshop in Park Inn Palace Hotel on 20 <sup>th</sup> and 21 <sup>st</sup> January 2016 to target Residents, Business and Elected Councillors.
	Informed the BID Committee on 14 <sup>th</sup> January 2016 about the SCAAP Preferred Approach document.
Community Groups	Letter sent on 18 <sup>th</sup> December 2015 to all on LDF database to inform that the SCAAP Preferred Approach consultation document is published for consultation – includes comprehensive coverage of resident / tenants / community associations and societies across the Borough.
Councillors	Local Development Framework Working Party briefed about consultation on the SCAAP Preferred Approach consultation document on 15 <sup>th</sup> September 2015.
	An email was sent to all of Southend-on-Sea Borough Council's Councillors informing them of Consultation, Drop-in Sessions and Workshops on 18 <sup>th</sup> , 21 <sup>st</sup> December 2015, with follow-up emails sent on

Method	Action Taken
	15 <sup>th</sup> and 20 <sup>th</sup> January 2016. Hard Copies were supplied on request.
	Councillor Drop-in sessions 20 <sup>th</sup> and 21 <sup>st</sup> January 2016.
Feedback form to assess effectiveness of engagement activity	<p>The Council's online system for making representations also includes an equalities feedback form.</p> <p>Document placed on the Council's website (<a href="http://www.southend.gov.uk">www.southend.gov.uk</a>) for inspection and downloading. The Borough Council encourage comments online via our E-Consultation service in order to make commenting on documents easier and straightforward.</p>

**Table 2: Summary of Representations Received during the Consultation Period (18th December 2015 and 15<sup>th</sup> February 2016) on the Preferred Approach**

Total individual respondents was 33

Southend Central Area Action Plan DPD	Support	Object	Comment	Total Responses
Section 1 - Introduction				
Question 1: SA	0	0	1	1
Question 2: Policies Map	1	1	2	4
Section 2 - Visions and Objections				
Question 3: Vision	4	1	5	10
Question 4: Strategic Objectives	16	0	4	20
Section 3 - Central Area Strategy				
Question 5: Central Area Strategy	6	0	3	9
Section 4 - Criteria Based Policies				
Question 6: <b>Policy DS1</b> Retail	10	3	25	38
Question 7: <b>Policy Options DS1a, DS1b, DS1c</b>	3	1	4	8
Question 8: Employment Section	6	0	1	7
Question 9: Housing Allocation of residential sites with planning permission	1	0	0	1
Question 10: Housing Section	8	0	7	15
Question 11: Culture, Leisure, Tourism & Recreation Section	3	2	4	9
Question 12: Historic Environment Section	7	1	6	14
Question 13: Open and Green Space section	5	0	3	8
Question 14: <b>Policy DS2:</b> Key Views	6	0	2	8
Question 15: <b>Policy DS3:</b> Landmarks and Landmark Buildings	2	2	3	7
Question 16: <b>Policy DS4:</b> Floodrisk, SuDS	2	0	5	7
Question 17: <b>Policy DS5:</b> Transport, Access, Public Realm	6	13	46	65
Question 18: Infrastructure Section	4	0	9	13
Section 5 - Policy Areas and Site Allocations				
Question 19: Site Allocation Indicative Capacity Table	0	1	0	1
Question 20: <b>Policy PA1</b> High Street	9	1	24	34
Question 21: <b>Policy PA2</b> London Road	4	4	23	31
Question 22: <b>Policy PA3</b> Elmer	3	0	7	10
Question 23: <b>Policy PA4</b> Queensway	3	0	11	14
Question 24: <b>Policy PA5</b> Warrior Sq	1	0	13	14
Question 25: <b>Policy PA6</b> Clifftown	6	2	16	24
Question 26: <b>Policy PA7</b> Tylers	2	0	16	18
Question 27: <b>Policy CS1</b> Central Seafront	22	7	45	74
Question 28: <b>Policy CS2</b> Nature Conservation & Biodiversity	1	0	3	4
Question 29: <b>Policy CS3</b> Waterfront	2	0	3	5
Question 30: <b>Policy PA8</b> Victoria Gateway	3	2	18	23
Question 31: <b>Policy PA9</b> Sutton Gateway	3	1	4	8
Section 6 - Delivery of the SCAAP				
Question 32: Phasing of Development Table	1	1	1	3
Question 33: Useful to include indicative figures for potential development in this section	1	0	0	1
Question 34: Useful to set out a series of projects and tasks for the Plan – linked to funding	1	0	0	1
Question 35: Overall approach for the Implementation Plan	1	0	1	2
Question 36: Monitoring Framework	1	0	0	1
Question 37: General Comments	3	1	27	31
<b>Total</b>	<b>157</b>	<b>44</b>	<b>342</b>	<b>543</b>

Table 3: Type of Comments Received at Workshops Held on 20<sup>th</sup> and 21<sup>st</sup> January 2016

Policy Area/Representation		PA1: High Street	PA2: London Road	PA3: Elmer Square	PA4: Queens way	PA5: Warrior Square	PA6: Clifftown	PA7: Tylers	CS1: Central Seafront	PA8: Victoria Gateway	PA9: Sutton Gateway	Total
Support/ Like	Total	<b>9</b>	<b>14</b>	<b>8</b>	<b>9</b>	<b>4</b>	<b>8</b>	<b>16</b>	<b>20</b>	<b>8</b>	<b>2</b>	<b>98</b>
	Public	6	3	8	0	3	6	10	12	7	0	55
	Business	0	4	0	2	1	1	4	7	1	0	20
	Member	3	7	0	7	0	1	2	1	0	2	23
What is Missing	Total	<b>12</b>	<b>4</b>	<b>8</b>	<b>6</b>	<b>3</b>	<b>7</b>	<b>4</b>	<b>17</b>	<b>7</b>	<b>0</b>	<b>68</b>
	Public	7	1	6	4	3	4	3	5	7	0	40
	Business	3	1	0	0	0	0	1	11	0	0	16
	Member	2	2	2	2	0	3	0	1	0	0	12
What can be Improved	Total	<b>36</b>	<b>18</b>	<b>7</b>	<b>12</b>	<b>17</b>	<b>12</b>	<b>14</b>	<b>45</b>	<b>24</b>	<b>11</b>	<b>196</b>
	Public	19	14	4	9	8	9	10	25	16	5	119
	Business	7	1	1	2	3	0	2	14	5	0	35
	Member	10	3	2	1	6	3	2	6	3	6	42
Other Issues	Total	<b>5</b>	<b>5</b>	<b>4</b>	<b>4</b>	<b>2</b>	<b>3</b>	<b>1</b>	<b>8</b>	<b>6</b>	<b>0</b>	<b>38</b>
	Public	2	4	2	4	2	3	0	8	2	0	27
	Business	0	0	0	0	0	0	1	0	3	0	4
	Member	3	1	2	0	0	0	0	0	1	0	7
Total	Total	<b>62</b>	<b>41</b>	<b>27</b>	<b>31</b>	<b>26</b>	<b>30</b>	<b>35</b>	<b>90</b>	<b>45</b>	<b>13</b>	<b>400</b>
	Public	<b>34</b>	<b>22</b>	<b>20</b>	<b>17</b>	<b>16</b>	<b>22</b>	<b>23</b>	<b>50</b>	<b>32</b>	<b>5</b>	<b>241</b>
	Business	<b>10</b>	<b>6</b>	<b>1</b>	<b>4</b>	<b>4</b>	<b>1</b>	<b>8</b>	<b>32</b>	<b>9</b>	<b>0</b>	<b>75</b>
	Member	<b>18</b>	<b>13</b>	<b>6</b>	<b>10</b>	<b>6</b>	<b>7</b>	<b>4</b>	<b>8</b>	<b>4</b>	<b>8</b>	<b>84</b>

## Section 3: Key Issues Identified

- 3.1 The following information provides a list of some of the main issues raised by the representations on the preferred approach of the Plan and each of its policy provisions as part of the consultation process, including the workshops held with the local business and resident community and elected Members. This list is not intended to be exhaustive.
- 3.2 **Appendix 3** of this report provides a summary of each duly made representation and **Appendix 4** details those comments made at the workshops.
- 3.3 **Appendices 3 and 4** also provide a summary of how the Council responded to the issues raised through consultation on the Southend Central Area Action Plan Preferred Approach.
- 3.4 The following information provides a list of some of the issues raised by the representations on each proposed policy. This list is not intended to be exhaustive. Appendix 3 of this report provides a summary of each representation made. Full comments made during the consultation on the Preferred Approach can be viewed here: <http://southend.idj-consult.net/localplan/>

### **General Approach:**

- Vision, Strategic Objectives and Strategy well supported
- Approach to employment development supported
- Residential development in central area supported
- Approach to culture, leisure, tourism, historic environment and open space generally well supported
- Concern regarding lack of emphasis in Plan on importance of tourism to Southend and the importance of the areas historic past
- Need to ensure high quality design in new developments

### **Policy DS1: Maintaining a Prosperous Retail Centre**

- Support for maintaining High Street as a prosperous sub-regional centre
- Recognition that High Street needs to adapt to changing retail patterns and be more flexible in its approach and diversity to encourage restaurants, cafes and similar uses

### **Policy DS2: Key Views**

- Policy provisions well supported

### **Policy DS3: Landmarks and Landmark Buildings**

- Provisions welcomed but also concern that places emphasis on landmark buildings rather than best quality design

### **Policy DS4: Flood Risk Management and Sustainable Drainage**

- Policy provisions welcomed
- Concerns raised regarding surface water disposal – policy changes proposed as a result

### **Policy DS5: Transport Access and Public Realm**

- Provisions for sustainable transport welcomed
- Concern that additional residential development should make adequate provision for residents car parking
- Need to maintain car parking capacity at a level that supports the vitality and viability of centre
- Problems of accessibility to centre and limited car parking provision preventing further investment in tourism facilities
- Need for additional car parking provision in central seafront tourist areas
- Congestion/poor accessibility resulting in shoppers/visitors not returning to town

- Should be like for like car parking provision on Opportunity Sites which are currently used for car parking with additional provision for development proposed on site
- Concern whether 'mixed mode' transport provision is safe
- Needs of vulnerable road users, cyclists and motor cyclists must be taken into full account
- Road safety/connectivity improvements needed through improved road crossing facilities

#### **Policy PA1: High Street**

- Policy provisions generally supported
- Recognition of need to improve public realm, landscaping etc to create a quality pedestrian environment
- Need to improve signage and wayfinding
- Need to improve connectivity, particularly to seafront
- Need to improve High Street offer

#### **Policy PA2: London Road**

- Need to improve Victoria Circus
- No retail frontage to Queensway
- Pedestrianisation generally welcomed but concerns about mobility issues

#### **Policy PA3: Elmer Square**

- Policy provisions welcomed

#### **Policy PA4: Queensway**

- Policy provisions generally welcomed
- Concerns regarding road safety and access

#### **Policy PA5: Warrior Square**

- Need to improve connectivity and enhance urban greening

#### **Policy PA6: Clifftown**

- General support for policy provisions
- Concerns raised regarding traffic movement in area
- Need for greater consideration to be given to future use of Empire Theatre site
- Need for better connectivity to railway station

#### **Policy PA7: Tylers**

- Support for the relocation of the Travel Centre
- Need for better connectivity and facilities at Travel Centre
- Need for improved linkages to High Street and Seafront

#### **Policy CS1: Central Seafront**

- Good support for policy provisions
- Concerns relating to adequacy of car parking in area to support tourism facilities and level of traffic in area
- Need for improved signage and connectivity to High Street and surrounding areas
- Need to ensure Seaway Opportunity Site provides a quality gateway to the seafront
- Concerns regarding heights of buildings in new development

#### **Policy CS2: Nature Conservation and Biodiversity**

- Policy wording not considered adequate –policy changes proposed

#### **Policy CS3: The Waterfront**

- Policy provisions generally supported

**Policy PA8: Victoria Gateway**

- Regeneration of area welcomed
- Need to make adequate provision for residential parking
- Consideration should be given to school and health needs
- Improve connectivity to High Street
- Concern regarding potential redevelopment of Roots Hall Football Ground resulting in out of town retail development to detriment of central area
- Baxter Avenue site should be allocated as Opportunity Site – now proposed to be included

**Policy PA9: Sutton Road**

- General support for regeneration of area
- Guildford Road site should be allocated as Opportunity Site – now proposed to be included

## Section 4: Consultation under Regulation 19 Southend Central Area Action Plan (Revised Proposed Submission Document )

Planning and Compulsory Purchase Act 2004 (as amended) Town and County Planning (Local Planning) Regulations 2012: Regulation 19

- 4.1 Southend-on-Sea Borough Council has prepared a Revised Proposed Submission version of the Southend Central Area Action Plan (SCAAP) as part of its Local Planning Framework, which it proposes to submit to the Secretary of State under Regulation 22 of the above Regulations.
- 4.2 The SCAAP Revised Proposed Submission updates the Preferred Approach version of the document (published December 2015), taking into account representations made and additional evidence.
- 4.3 The SCAAP (Revised Proposed Submission Document) and accompanying documents have been published in order for representations to be made prior to the submission of the Southend Central Area Action Plan to the Secretary of State for independent examination.
- 4.4 The Plan sets out detailed policies for a wide range of issues for the Central Area of the town, against which planning applications will be assessed, including shopping, housing, transport and the natural environment. It also identifies a number of Opportunity Sites for development. These will replace a number of Saved Policies from the 1994 Borough Local Plan.
- 4.5 Representations can be made during the publication period which begins on **26<sup>th</sup> October 2016** and ends on **5pm 9<sup>th</sup> December 2016**.
- 4.6 Only representations received during this consultation period will be considered. Late responses will not be accepted.
- 4.7 Representations must relate to 'soundness' and legal compliance, and should be made using the Council's online interactive consultation system, which can be found at <http://southend.jdi-consult.net/ldf/>. Alternatively, representations may be submitted using the Response Form, available on request, by the following means:
  - e-mail to [ldf@southend.gov.uk](mailto:ldf@southend.gov.uk) or
  - in writing to the Corporate Director, Department for Place, PO Box 557, Civic Centre, Victoria Avenue, Southend-on-Sea, SS2 6ZF.



## Section 5: Statement of Fact - details of the Preferred Approach Consultation

5.1 The Revised Proposed Submission Southend Central Area Action Plan, Revised Policies Map and accompanying documents, alongside a statement setting out how representations can be made, are available for inspection from **26<sup>th</sup> October 2016 to 9<sup>th</sup> December 2016** at the following locations:

- Southend Council's website: [www.southend.gov.uk/scaap](http://www.southend.gov.uk/scaap)
- Southend Borough Council Contact Centre, Civic Centre, Victoria Avenue, Southend on Sea between 8.45am and 5.15pm (Monday to Friday); and
- All Southend Libraries during normal opening hours:
  - Southend Forum, Elmer Square, Southend
  - Southchurch Library, Lifstans Way, Southend
  - Kent Elms Library, Prince Avenue, Leigh
  - Thorpedene Library, Delaware Road, Shoebury
  - Friars Library, The Renown, Shoebury
  - Westcliff Library, London Road, Westcliff
  - Leigh Library, Broadway West, Leigh

5.2 Hard copies can be purchased for **£5** by **contacting the Business Intelligence Unit by telephone on 01702 215004 ext. 5408 or email [ldf@southend.gov.uk](mailto:ldf@southend.gov.uk)**

## Appendix 1: Consultees (Preferred Approach Stage, December 2015)

# LDF 2016 - Specific Consultees (ALL)

## Organisation

Aldi Foodstore Ltd

AMEC Environment & Infrastructure UK Ltd

Anglian Water Services

Arriva Southern Counties

Arriva Southern Counties Ltd

Asda Superstores

Barling Magna Parish Council

Basildon Borough Council

British Wind Energy Association

BUPA Wellesley Hospital

c2c Rail & National Express East Anglia

CAA Safety Regulation Group

Castle Point Borough Council

CPREssex

Dartford Borough Council

Defence Infrastructure Organisation

East of England Ambulance Service

EDF Energy

EDF Energy (Renewables)

EE

English Heritage East of England

Environment Agency

Environment Agency

Environment Agency

Essex Chambers of Commerce - South Essex Office

Essex Council Council

Essex County Council

Essex County Council

Essex County Council

## Organisation

Essex Fire & Rescue Service HQ

Essex Police

Essex Police

Essex Police Community Safety Dept

Essex Police, Headquarters

Essex Wildlife Trust

First Essex Buses Ltd

Foulness Parish Council

Friends, Families & Travellers & Travellers Law Reform Project Community Base

Great Wakering Parish Council

Guide Dogs for the Blind Association

H M Customs & Excise

Highways Agency

Highways Agency (Network Strategy)

Highways England

Historic England

Hockley Parish Council

Leigh Town Council

London Southend Airport

MOA (Mobile Operators Association)

National Grid

Natural England

Natural England Consultation Service

NHS England, Essex Area Team,

Planning Potential on behalf of Aldi Stores Ltd

Public Health

QinetiQ

Resident Association Watch

Rochford District Council

Rochford Parish Council

South East Local Enterprise Partnership

Southend University Hospital NHS Foundation Trust

SPORT ENGLAND

The Draughtsman

## Organisation

The National Federation of Gypsy Liaison Groups

The National Trust

The Planning Inspectorate

The Society for the Protection of Ancient Buildings

Three

Thurrock Council

Thurrock Unitary Council

Town Centre Partnership

Traveller Law Reform

UK Power Network

University of Essex

Vodafone and O2

# LDF - General Consultees (ALL)

## Organisation

A W Squier Ltd

AC Taxis

Age Concern

Arriva Southern Counties Ltd

Association of Jewish Refugees

Barton Wilmore

Belfairs Gardens Residents Association

Belfairs Gardens Residents Association

Braintree District Council

BRE Global

Brentwood Borough Council

British Hardware Federation

British Horse Society

Burges Estate Residents Association (BERA)

Bus & Rail User Group

c2c Rail

Campaign to Protect Rural Essex (CPREssex)

Canewdon Parish Council

Chalkwell Ward Residents Association

Chart Plan (2004) Ltd

Chelmsford Borough Council

COBRA (Coalition of Borough Residents Associations)

Conservation Association Westcliff Seaboard

County Hotel

CPRE Southend Area

Crest Nicholson

Crime Prevention Panel (Leigh)

Crown Estate Office

## Organisation

Cycling Touring Club (CTC)

Darby & Joan Organisation

DIAL Southend

English Sports Council (East)

Essex & Suffolk Water

Essex Amphibian & Reptile Group

Essex Badger Protection Group

Essex Biodiversity Project

Essex Bridleways Association

Essex Racial Equality Council

Essex Wildlife Trust

Essex Wildlife Trust - Southend and Rochford Group

Estuary Housing Association

Ethnic Minority Forum

Federation of Small Businesses

Fusion Online Ltd

GreenKeeper

Hamlet Court Road Business Association

Hamlet Court Road Business Association

Hanson Quarry Products

Harlow District Council

Hawkwell Parish Council

Heaton Planning

Herbert Grove Residents Association

Hindu Association (Southend & District)

Hobbs Parker

Home Builders Federation (HBF)

Horse Owners and Riders (SE Essex)

Hullbridge Parish Council

Iceni Projects

Iceni Projects Ltd

Iceni Projects Ltd

Indigo Planning

IPECO

## Organisation

J.C Gibb Chartered Surveyors

Januarys

John Grooms Association

Kent County Council

Lambert Smith Hampton

Lancashire Digital Technology Centre

Landmark Town Planning Group

Leigh Cliff Association

Leigh Seafront Action Group

Leigh Society

Leigh Traders Association

Leigh-on-Sea Crime Prevention Panel

Lidl UK Ltd

Maldon District Council

Milton Community Partnership

Milton Conservation Society

Milton Conservation Society

Moat Homes

National Express East Anglia

National Federation for the Blind

National Rivers Authority Anglian Region

Network Rail (Town Planning Team)

Network Rail Property

NIBS

North Crescent & Feeches Rd Residents Association

Older Peoples Federation

Olympus KeyMed

OPA

Paglesham Parish Council

Parklife

Pebbles 1

Persimmon Homes (Essex) Ltd

Peter Harris Associates

Phase 2 Planning and Development



## Organisation

Planning Perspectives LLP

Planning Perspectives LLP

Planning Perspectives LLP

Planning Potential

Planware Ltd

Port of London Authority

Powergen Plc

Prospects College

Qinetiq

Ramblers Association (Southend Unitary Authority)

Rayleigh Town Council

Residents Association of Westborough (RAW)

RIBA South East Chapter

Royal Association For Deaf People (RAD)

Royal Bank of Scotland (RBS)

Royal Mail Group Property

Royal National Lifeboat Institution - Southend Branch

SAEN

Sanctuary Group

Shoebury Residents Association

Shoebury Society

Shoebury Traders Association

Smart Planning Ltd

Smart Planning Ltd

Society for the Protection of Undercliff Gardens

SOS Domestic Abuse Projects

South East Essex Archaeological Society

South East Essex Archaeological and Historical Society

South East Essex College

South East Essex Friends of the Earth

South Essex Area Health Authority

South Essex Natural History Society

South Westcliff Community Group

Southend & District Aid Society

## Organisation

Southend & District Pensioners Campaign

Southend & Leigh Fishermans Association

Southend & Surrounds Cycling Campaign

Southend Adult Community College

Southend and Westcliff Hebrew Congregation

Southend Animal Aid

Southend Area Bus Users Group

Southend Association of Voluntary Services

Southend Blind Welfare Organisation

Southend Hospital NHS Trust

Southend Islamic Trust

Southend Mencap

Southend Mind

Southend Ornithological Group

Southend Primary Care Trust (PCT)

Southend Properties (Guernsey) Ltd

Southend Sports Council & Southend Wheelers Cycling Club

Southend Taxi Drivers Association

Southend Tenants and Residents Federation

Southend Town Centre Business Group

Southend University Hospital

Southend Wheelers

Southend YMCA

Southend Youth Council

Southend-on-Sea Arts Council

Southend-on-Sea Guild of Help and Citizens Advice Bureau

Southend-on-Sea Sports Council

Sport England East

SSA Planning

St. Matthew's Christian Spiritualist Church (1999) Ltd.

Stambridge Parish Council

Stephensons of Essex

Stewart Ross Associates

Stock Woolstencroft Architecture and Urbanism

## Organisation

Stockdale Group of Companies

Strutt and Parker

SUSTRANS Essex

Sutton Parish Council

Tarmac Southern Ltd

Tattersall Gardens Residents Group

Tendring District Council

Terence O'Rourke

Tesco Stores Ltd

Tetlow King Planning

Thames Gateway South Essex Partnership Ltd

Thames Water Property Services

The Guinness Trust

The Planning & Development Partnership

The Planning Bureau Ltd

The Salvation Army Leigh on Sea

The Southend Pier Museum Trust Ltd

The Southend Society

The Theatres Trust

The Victoria Shopping Centre

Tolhurst House Residents Association

Trust Links

University of Essex Southend

University of Essex Southend

Uttlesford District Council, Planning Department

Waitrose Ltd

West Leigh Residents Association

West Leigh Residents Association

Westborough Neighbourhood Action Panel

Westcliff & Leigh Neighbourhood Watch

**Appendix 2: Copy of Southend Central Area Action Plan Preferred Approach (December 2015) Consultation Material**

***To be inserted – Please refer to Table 1 above for Consultation Methods carried out during SCAAP Preferred Approach Consultation***



## Appendix 3: Southend Central Area Action Plan Preferred Approach (2015) – Detailed Summary of Representations Received During the Consultation Period (18th December 2015 to 15<sup>th</sup> February 2016)

The below provides a summary of each representation made on the Preferred Approach version of the Southend Central Area Acton Plan (SCAAP). Full submissions made during the consultation can be viewed on the Councils website.

### SCAAP – Representations for SCAAP Preferred Approach

Policy, Para, Section, or Question	Respondent (Name) [No]	Rep No	Object/Support	Summary of Representation	Response to Representation
<b>Part A: The Plan and its Context</b>					
<b>Sustainability Appraisal</b>					
Question 1: Sustainability Appraisal	Belfairs Garden Residents Association (Barbara Armitage) [511]	2330	Comment	Sustainability information not available	The SA was made available for public comment as an integral part of the SCAAP consultation process.
<b>Policies Map</b>					
Question 2: Policies Map	Essex Chambers of Commerce (Mr John Dallaway) [452]	1935	Support	Support the Policies map as set out	Noted.
Question 2: Policies Map	The Cooperative Group (Mr A Thompson) [473]	1974	Object	The Co-operative Group would wish to see the inclusion of land at 53-57 Sutton Road Southend within the SCAAP as an additional Opportunity Site.	The Council is considering inclusion of this site as an opportunity site. This will be assessed against information supplied by the owners of the site to demonstrate that development is deliverable by 2021.

Question 2: Policies Map	Burges Estate Residents Association [176]	1992	Comment	There is nothing in the document which justifies the Policies Map boundaries and they do seem somewhat arbitrary with the inclusion of vast swathes of residential areas which are unlikely to be the subject of significant change in the plan timescale. Large parts of areas of Victoria Gateway, Sutton Gateway and Kursaal estate are examples. On the other hand I would draw attention to the exclusion of St Marys Church from the Victoria area. The church and the adjoining properties on the corner of this important intersection are key elements in any junction improvement and should be included.	The SCAAP boundary has evolved through consultation and evidence, including the Central Area Masterplan, as well as previous versions of the SCAAP. St Marys Church is referenced in Policy DS2 – Key views, and Policy DS3 – Landmarks and Landmark Buildings, and therefore these policies will be taken into account in respect to any proposals that may impact upon it.
Question 2: Policies Map  261	Capita Property Infrastructure On behalf of Genesis Housing [465]	2030	Comment	<p>The SCAAP preferred approach is supported. However, Capita P&amp;I and Genesis consider that the OS11 site does not extend far enough, and that the adjacent Genesis site at Baxter Avenue should be incorporated within the OS11 site boundary. There are several reasons as to why, these are all explored in the supporting document. These considerations are:</p> <ul style="list-style-type: none"> <li>• The overall shortfall in housing supply and how the development of the site can help deliver the target;</li> <li>• The policy compliance of the proposal;</li> <li>• The removal of low quality housing;</li> <li>• Given the area of the site, a coherent regeneration masterplan approach should be adopted in accordance with OS11;</li> <li>• The site is well positioned on an access vista and therefore well located for a housing led regeneration initiative.</li> </ul> <p>The site is available, achievable and deliverable. The redevelopment of the site would allow for the residential density of the site to be optimised, whilst also providing a quality mixed use development with active frontages.</p>	The Council is considering inclusion of this site as an opportunity site. This will be assessed against information supplied by the owners of the site to demonstrate that development is deliverable by 2021.
<b>Vision</b>					
Question 3: Vision	Essex Chambers of Commerce (Mr John Dallaway) [452]	1936	Support	Support the proposed vision as set out	Noted.

Question 3: Vision	Burges Estate Residents Association [176]	1993	Comment	I have no problem with the Vision put forward for the centre, but I would question the rationale when it states that the regeneration of the centre will be led by the Uni campus. I have seen nothing in the document or elsewhere to suggest that the scale of activity, investment, etc. by the University would be such as to lead the way.	Noted, the rationale will not be included in the final version of the Plan. Growth of the university is regarded as one of the key elements which will lead to the successful regeneration of the town centre, as recognised by the Core Strategy DPD (Policy KP1).
Question 3: Vision	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2047	Support	Support the aspiration for Southend to be a City by the Sea and be a prosperous, vibrant, safe, thriving regional Centre as the cultural hub within the Thames Gateway and a great place to live, work and visit.	Noted.
Question 3: Vision 262	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2048	Comment	Would argue that in relation to the Strategic Vision the Sea is what gives Southend its Unique Selling Point (USP) and this needs to be a key theme in relation to future planning policy supporting the continued growth, regeneration and reinvestment.	Noted.
Question 3: Vision	Southend Bid (Mr S Kearney) [496]	2149	Support	Support the aspiration for Southend to be a City by the Sea and be a prosperous, vibrant, safe, thriving regional Centre as the cultural hub within the Thames Gateway and a great place to live, work and visit.	Noted.



Question 3: Vision	Southend Bid (Mr S Kearney) [496]	2150	Comment	Would argue that in relation to the Strategic Vision the Sea is what gives Southend its Unique Selling Point (USP) and these need to be a key theme in relation to future planning policy supporting the continued growth, regeneration and reinvestment.	Noted.
Question 3: Vision	Southend Bid (Mr S Kearney) [496]	2238	Support	Southend BID received a 100% consultation responses having sent the Pro Forma out to 50 plus business consultees. The results of these can also be found in the tables in Appendix 4. Overwhelmingly the business consultees supported the SCAAP aspiration for Southend to be a prosperous, vibrant, safe cultural hub and City by the Sea. 94% of respondents supported that. In relation to the SCAAP's aspirational growth in homes in the Central Area 74% of the consultees supports the Councils vision. In relation to the job growth within the SCAAP area 90% of the respondents supported the Councils aspiration.	Noted
Question 3: Vision  263	Belfairs Garden Residents Association (Barbara Armitage) [511]	2331	Comment	The vision is too vague to make a judgement. This 2015 document only mentions people as categories. It mentions building on car parks and creating more precincts without any consideration of how people of all ages and abilities including elderly and/or disabled, (blind, deaf, restricted mobility, learning disabled) mums with children and buggies are going to access and move in this changed and regenerated town centre or how it will cater for all kinds of visitors. Where is the statement of Equality duty?	The vision is considered to be an appropriate statement of what the Borough Council wishes to achieve in the central area of the town. Details of movement and function are contained in the policy provisions of the Plan. An Equalities Impact Assessment will be carried out for the Proposed Submission version of the Plan.
Question 3: Vision	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	2360	Object	We do not agree with the vision of Southend as a "City by the Sea". We resent the amount of public money being spent on Bids to become a City. There is no evidence that the residents desire this status or indeed that Her Majesty would be inclined to support it. The image of the town already attracts over 6 million visitors a year, and it will always be perceived as a 'down market seaside resort' . What needs changing is the economy. More well paid jobs in modern hi-tech industries. This we believe is planned	The vision sets out the Council's long term view and aspirations for the central area of the town. This is considered to be an ambitious and appropriate vision to work towards in the interests of improving the vitality and viability of the area. No changes proposed.

Question 3: Vision	Historic England (Dr Natalie Gates) [514]	2398	Comment	We would suggest that the issue of heritage is translated into the vision (paragraph 25) through this addition: "As a prosperous and thriving regional centre with a rich heritage, it will be an area..."	Noted. It is accepted that the vision does not make reference to heritage which is vitally important to the central area. <i>It is therefore proposed that the words 'heritage and' be added after the words 'rich in'. The vision in paragraph 25 would then read 'Our vision for Southend Central Area, which includes the Town Centre and Central Seafront Area, is for it to be a City by the Sea. As a prosperous and thriving regional centre <u>and resort</u>, it will be an area that is vibrant, safe and hospitable, rich in <u>heritage</u>, commerce, learning and culture and an attractive, diverse place where people want to live, work and visit for both day trips, overnight and longer stays.'</i>
<b>Strategic Objectives</b>					
Question 4: Strategic Objectives	Essex Chambers of Commerce (Mr John Dallaway) [452]	1937	Support	Support the Strategic Objectives as set out in the document	Noted.
Question 4: Strategic Objectives	Burges Estate Residents Association [176]	1994	Comment	Similarly I have no problem with the Strategic objectives with the exception of one fundamental addition. All efforts to promote design excellence, quality developments and use of sustainable materials will be for nothing without continuing effective maintenance and upkeep. Where the Council has the opportunity and that is especially work in the public realm, routine and timely maintenance to retain the intrinsic value of the work is essential. The designs of today are our heritage of tomorrow.	Noted.

Question 4: Strategic Objectives	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2054</b>	<b>Support</b>	Support the improvements to the transformation on economic, vitality, viability and diversity of Southend Central Area and the encouragement of a wide range of homes, businesses and retail. It would also support the opportunity for additional learning, recreation and leisure.	Noted.
Question 4: Strategic Objectives  265	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2055</b>	<b>Support</b>	Support the Councils aspiration for design excellence and good quality development proposals and significant public realm improvements to reinforce the sense of place to compliment the new and existing infrastructure and townscape of the Central Area.	Noted.

Question 4: Strategic Objectives	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2056</b>	<b>Support</b>	Supports the Councils aspiration to establish Southend as low carbon City providing that it doesn't have any adverse impact in terms of access, connectivity and parking allocations within the Central Area.	Noted.
Question 4: Strategic Objectives  266	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2057</b>	<b>Support</b>	Support the improvement to accessibility and the further encouragement of sustainable modes of transport.	Noted.

Question 4: Strategic Objectives	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2069</b>	<b>Support</b>	Support the Council in their aspiration to (i) improve Competitive Performance (ii) Reduce the cost base (iii) Diversify away from retail uses (iv) grow the local economy and/or population, which is a strong theme in the preferred option principle. Recognise that the High Street should be a social place that makes creative use of public space with a vibrant evening economy.	Noted.
Question 4: Strategic Objectives	Southend Bid (Mr S Kearney) [496]	<b>2156</b>	<b>Support</b>	Support the improvements to the transformation on economic, vitality, viability and diversity of Southend Central Area and the encouragement of a wide range of homes, businesses and retail. It would also support the opportunity for additional learning, recreation and leisure.	Noted.
Question 4: Strategic Objectives	Southend Bid (Mr S Kearney) [496]	<b>2157</b>	<b>Support</b>	Support the Councils aspiration for design excellence and good quality development proposals and significant public realm improvements to reinforce the sense of place to compliment the new and existing infrastructure and townscape of the Central Area.	Noted.
Question 4: Strategic Objectives	Southend Bid (Mr S Kearney) [496]	<b>2158</b>	<b>Support</b>	Supports the Councils aspiration to establish Southend as low carbon City providing that it doesn't have any adverse impact in terms of access, connectivity and parking allocations within the Central Area.	Noted.
Question 4: Strategic Objectives	Southend Bid (Mr S Kearney) [496]	<b>2159</b>	<b>Support</b>	Support the improvement to accessibility and the further encouragement of sustainable modes of transport.	Noted.
Question 4: Strategic Objectives	Southend Bid (Mr S Kearney) [496]	<b>2171</b>	<b>Support</b>	Support the Council in their aspiration to (i) improve Competitive Performance (ii) Reduce the cost base (iii) Diversify away from retail uses (iv) grow the local economy and/or population, which is a strong theme in the preferred option principle. Recognises that the High Street should be a social place that makes creative use of public space with a vibrant evening economy	Noted.

Question 4: Strategic Objectives	Southend Bid (Mr S Kearney) [496]	2239	Support	Southend BID received a 100% consultation responses having sent the Pro Forma out to 50 plus business consultees. The results of these can also be found in the tables in Appendix 4. Overwhelmingly the business consultees supported the SCAAP aspiration for Southend to be a prosperous, vibrant, safe cultural hub and City by the Sea. 94% of respondents supported that. In relation to the SCAAP's aspirational growth in homes in the Central Area 74% of the consultees supports the Councils vision. In relation to the job growth within the SCAAP area 90% of the respondents supported the Councils aspiration.	Noted
Question 4: Strategic Objectives	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2471	Comment	A number of strategic objectives are set out within the SCAAP which include improving and transforming the economic vitality, viability and diversity of Southend Central Area by encouraging the establishment of a wider range of homes, businesses and shops whilst providing new opportunities for learning, recreation and leisure. We suggest that a further strategic objective be included that seeks to maintain and protect existing shops and town centre uses in the Southend Central Area.	Strategic Objective 1 seeks to improve and diversify the town centre to ensure its future economic vitality and viability. To protect existing shops and other uses per se is not considered appropriate if a flexible approach to the future development of the central area is to be achieved. No changes are proposed.
Question 4: Strategic Objectives	Bowhill Planning Partnership (Anthony Bowhill) [474]	2321	Support	Objective 1 - Currently the High Street is run-down. This is partly due to the recession (2008-2014) but more fundamentally to the restricted hinterland of The Centre which only has two main sides. This means that the shopping draw is limited as testified by the growing number of vacancies. Thus a wider range of uses in the High Street providing diversity and assisting viability and vitality is to be welcomed. I, therefore support Objective 1 with its emphasis on "a wider range of..." which would help to increase the draw of the shopping centre.	Noted.
Question 4: Strategic Objectives	Bowhill Planning Partnership (Anthony Bowhill) [474]	2322	Support	Objective 8 is welcomed by bringing more people into the centre to live who will be able to supports its vitality.	Noted.

Question 4: Strategic Objectives	Belfairs Garden Residents Association (Barbara Armitage) [511]	2332	Comment	Too generalised for comment.	The strategic objectives set out the main direction for the Plan. The details are contained within its policy provisions.
Question 4: Strategic Objectives	Historic England (Dr Natalie Gates) [514]	2397	Support	Welcome the identification of Southend's heritage in the context and issues (paragraph15) and its inclusion as Strategic Objective 7.	Noted
Question 4: Strategic Objectives	Environment Agency (Miss Lizzie Griffiths) [334]	2419	Comment	Strategic Planning Context We are pleased that Flood Risk Management and Sustainable Drainage is included within the Context and Issues for the Southend Central Area, and in particular, that reference is made to the key challenge of addressing climate change. This section discusses the risk of tidal flooding to the Borough, although it should be noticed that there are other sources of flood risk which will need to be considered too.	Noted, additional text will be included in Issues I of 'Context and Issues for the Southend Central Area' to outline that the SCAAP area is also susceptible to surface water flooding as follows: <b><i>'Southend has been identified by the Environment Agency as susceptible to local <u>surface water</u> flooding under conditions of extreme rainfall.'</i></b>
Question 4: Strategic Objectives	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	2361	Support	Yes we agree	Noted.

#### Part B: Development Strategy

#### Central Area Strategy

Question 5: Central Area Strategy	Essex Chambers of Commerce (Mr John Dallaway) [452]	1938	Support	Agree with the proposed Central Area Strategy as set out	Noted.
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Question 5: Central Area Strategy	Mr Michael Davies [493]	<b>2036</b>	<b>Comment</b>	An important question is- Will current and future generations thank us for the new plans in years to come? High-rise developments like these may be typical of a large city, but I'm not sure that many residents of Southend want to live in a 'City by the sea'. I think they'd want Southend to retain some of its 'seaside town' charm. London is only an hour's train ride away; let's keep it that way, and not let London engulf the area. A vibrant, but charming town is the best thing to aim for, in my view.	Noted. The Plan puts in place a number of policy provisions to protect the central areas heritage assets and the character and setting of the area. Furthermore, Policy DM4 of the Development Management Document sets out provision for managing tall and large buildings. No changes are proposed.
Question 5: Central Area Strategy	Mr Michael Davies [493]	<b>2039</b>	<b>Comment</b>	Have the Council considered a Park & Ride for Southend, to ease the long traffic queues along the A127 at busy times? It works well in Chelmsford and other towns.	Park and Ride schemes have been considered a number of times in recent years but have not been considered feasible given the limited land available and linear peninsula geography of the town. The provision of Park and Ride would only be feasible outside the SCAAP boundaries. Such options will be kept under review as part of the on-going Local Transport Plan provisions and development of the Southend Local Plan. No changes are proposed.
Question 5: Central Area Strategy  270	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2070</b>	<b>Support</b>	Support the Council in their aspiration to (i) improve Competitive Performance (ii) Reduce the cost base (iii) Diversify away from retail uses (iv) grow the local economy and/or population, which is a strong theme in the preferred option principle. Recognise that the High Street should be a social place that makes creative use of public space with a vibrant evening economy.	Noted.
Question 5: Central Area Strategy	Southend Bid (Mr S Kearney) [496]	<b>2172</b>	<b>Support</b>	Support the Council in their aspiration to (i) improve Competitive Performance (ii) Reduce the cost base (iii) Diversify away from retail uses (iv) grow the local economy and/or population, which is a strong theme in the preferred option principle. Recognises that the High Street should be a social place that makes creative use of public space with a vibrant evening economy	Noted.



Question 5: Central Area Strategy	Southend Bid (Mr S Kearney) [496]	<b>2240</b>	<b>Support</b>	Southend BID received a 100% consultation responses having sent the Pro Forma out to 50 plus business consultees. The results of these can also be found in the tables in Appendix 4. Overwhelmingly the business consultees supported the SCAAP aspiration for Southend to be a prosperous, vibrant, safe cultural hub and City by the Sea. 94% of respondents supported that. In relation to the SCAAP's aspirational growth in homes in the Central Area 74% of the consultees supports the Councils vision. In relation to the job growth within the SCAAP area 90% of the respondents supported the Councils aspiration.	Noted
Question 5: Central Area Strategy	Bowhill Planning Partnership (Anthony Bowhill) [474]	<b>2323</b>	<b>Support</b>	The Strategy is supported and every effort should be made to bring forward various identified sites particularly for new residential	Noted.
Question 5: Central Area Strategy 271	Belfairs Garden Residents Association (Barbara Armitage) [511]	<b>2333</b>	<b>Comment</b>	Too broad to comment	The central area strategy is a broad statement of what the Plan is trying to achieve.
Question 5: Central Area Strategy	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2362</b>	<b>Support</b>	Yes we agree	Noted.

#### Criteria Based Policies

#### Maintaining a Prosperous Retail Centre – Policy DS1

Question 6; Policy DS1	Essex Chambers of Commerce (Mr John Dallaway) [452]	<b>1939</b>	<b>Support</b>	Agree with the proposed approach to maintaining a prosperous retail centre	Noted.
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Question 6; Policy DS1	Mr Jason Pilley [469]	1965	<b>Comment</b>	I would like to make the comment that attempting to move shops out of the High Street towards outlying areas of the town would be a bad idea on many levels. For one thing it would increase people's reliance on cars, which isn't just environmentally unsound but is also an example of poor land-use planning, we ought to be making it easier for people to get to shops, not harder; we ought to be building up a strong central community, not dissipating it.	Policy DS1 makes no provision for out of centre retail. Wider retail policy for the Borough is set out within the adopted Core Strategy within the framework of which the SCAAP has been prepared. Retail development outside the SCAAP area will be considered against the adopted Core Strategy and national planning policy. These contain a town centre first approach to retail and other town centre proposals. Significant out of town retail development proposals will have to satisfy a sequential test (i.e. looking at town centre sites first) and be subject to an impact assessment.
Question 6; Policy DS1	Mr Jason Pilley [469]	1966	<b>Comment</b>	The character and soul and reputation of towns and cities are determined by their centre, not by their outskirts. A High Street full of pawn shops and cheapo stores and closed-down restaurants won't be doing anyone any favours	See comments in relation to Rep 1965
Question 6; Policy DS1	Burges Estates Residents Association [176]	1995	<b>Comment</b>	Maintaining a prosperous retail centre begs the question as to how prosperity is measured. Does the retail turnover statistics for the centre show a level of prosperity that is considered adequate since the policy options seek only to maintain the current prosperity not improve or enhance it? Anecdotal evidence suggests otherwise with vast sums of disposable income from Southend residents increasingly spent at Bluewater, Lakeside, Westfield and even Chelmsford as Southend has slowly declined with many poor quality, here today gone tomorrow, shops.	Noted, it is proposed to remove the word 'maintaining' from the title of Policy DS1 and to amend this to read ' <b>A Prosperous Retail Centre</b> '.
Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2044	<b>Comment</b>	Based on various research and commissioned reports there is substantive evidence to indicate that the High Street is mainly singular, and due to change in customer expectations and behaviour the type of High Street is no longer viable.	Policy PA1 provides for a flexible approach to the future development of the High Street incorporating mixed use development and public realm improvements that contribute to the vitality and viability of the centre. Furthermore, Policy DS1 allows for a greater mix of town centres uses, such as cafes and restaurants. No changes are proposed.

Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2045	Comment	Support the Council's view that spatially the High Street and connections to the seafront are an inappropriate configuration for regeneration and commercial vitality.	Noted.
Question 6; Policy DS1  273	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2046	Support	Support idea of the Central Seafront Area achieving a compact defined prime retail core in the Town Centre with a mixture of uses and peripheral areas made over to complimentary uses and in particular support the intensification of the growth of housing in the Central Area.	Noted.
Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2059	Support	Recognise the importance of the Town Centre in viability and vitality terms and the need to enhance the existing markets where appropriate and introduce and create new markets.	Noted.

Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2062	Comment	High Streets Adapting for Change (RICS) introduces the Governments recognition that our High Streets have to offer something new and different that neither the shopping centres nor the internet can match. They need to offer an experience that goes beyond retail and they need to be a destination for the socialising culture, health, well being, creativity and learning. Offices alongside shops, alongside housing, alongside eateries.	Noted. The Plan seeks to achieve this.
Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2065	Comment	Changes in consumer behaviour, the growth in car ownership and its impact on accessibility of in and out of Town Centre shopping are reasonably well understood. This is particularly likely to be an issue should the Council grant the Fossetts Farm application to subsequently increase a retail offer in an out of town centre location.	Noted.
Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2067	Comment	The growth of out of town shopping centre has been widely blamed for Town Centre decline and planning policies have attempted to restrict this growth, which the Stockvale Group wish to see reflected in the Council's determination of planning applications at Fossetts Farm. (The outcome of Planning Applications for extensive retail at Fossetts Farm will determine the value of the Council progressing the SCAAP process. If Fossetts Farm retail development is approved the Stockvale Group feel the SCAAPs aspirations will be undeliverable.	The Fossetts Farm proposals are located outside the SCAAP boundary. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact was taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study). Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy provisions and require a further retail impact assessment. No changes are proposed.

Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2071</b>	<b>Support</b>	Support the Council in their aspiration to (i) improve Competitive Performance (ii) Reduce the cost base (iii) Diversify away from retail uses (iv) grow the local economy and/or population, which is a strong theme in the preferred option principle. Recognise that the High Street should be a social place that makes creative use of public space with a vibrant evening economy.	<b>Noted.</b>
Question 6; Policy DS1  275	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2073</b>	<b>Comment</b>	Given that there is a shortage of housing and surplus retail property, there is no doubt that conversion to residential use should form part of the future of the Town Centre.	<b>Noted. The Plan seeks to achieve this. In relation to ground floor conversion, this would be outside of designated shopping frontage and in accordance with national policy.</b>
Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2075</b>	<b>Comment</b>	SCAAP should be seeking diversification of some of the retail uses on the ground floor as conversion to residential uses, providing attractive exit strategies for the asset managers and investors. This needs careful consideration in terms of how spatially to organise the retail uses	<b>Policy DS1 seeks to protect retail and town centre uses on the ground floor in identified shopping areas in order to maintain the vitality and viability of the centre. No changes are proposed.</b>

Question 6; Policy DS1	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2077	Comment	Many of our High Streets have been shaped by their past; however they are now trapped in their current configurations and often in poor shape to face the future. In relation to Southend on Sea, this is certainly the case. The High Street in particular has a linearity with no social space for congregation, interaction and the alternative commercial uses that would reactivate these spaces such as cafes, coffee shops, office space and importantly a high intensification of residential uses both at ground level and above The SCAAP and the Stockvale Group recognise that the High Street in particular requires a restructuring on a significant scale.	The SCAAP recognises the need to enhance and broaden the offer in the High Street and seeks to do this by providing a more flexible approach in the determination of planning applications to encourage a mix of retail, cafe and restaurant uses. The Plan also seeks to enhance and promote new public spaces within the centre. No changes are proposed.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2145	Comment	Based on various research and commissioned reports there is substantive evidence to indicate that the High Street is mainly a singular and due to change in customer expectations and behaviour the type of High Street is no longer viable.	Policy PA1 provides for an approach to the future development of the High Street that incorporates mixed use development and public realm improvements that contribute to the vitality and viability of the centre. No changes are proposed.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2146	Comment	Support the Council's view that spatially the High Street and connections to the seafront are an inappropriate configuration for regeneration and commercial vitality.	Noted.
Question 6; Policy DS1 276	Southend Bid (Mr S Kearney) [496]	2148	Support	Support idea of the Central Seafront Area achieving a compact defined prime retail core in the Town Centre with a mixture of uses and peripheral areas made over to complimentary uses and in particular support the intensification of the growth of housing in the Central Area.	Noted.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2161	Support	Recognise the importance of the Town Centre in viability and vitality terms and the need to enhance the existing markets where appropriate and introduce and create new markets	Noted.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2164	Comment	High Streets Adapting for Change (RICS) introduces the Governments recognition that our High Streets have to offer something new and different that neither the shopping centres nor the internet can match. They need to offer an experience that goes beyond retail and they need to be a destination for the socialising culture, health, well being, creativity and learning. Offices alongside shops, alongside housing, alongside eateries.	Noted. The Plan seeks to achieve this.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2167	Comment	Changes in consumer behaviour, the growth in car ownership and its impact on accessibility of in and out of Town Centre shopping are reasonably well understood. This is particularly likely to be an issue should the Council grant the Fossetts Farm application to subsequently increase a retail offer in an out of town centre location.	Noted.

Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2169	Comment	The growth of out of town shopping centre has been widely blamed for Town Centre decline and planning policies have attempted to restrict this growth, which the Stockvale Group wish to see reflected in the Council's determination of planning applications at Fossetts Farm. (The outcome of Planning Applications for extensive retail at Fossetts Farm will determine the value of the Council progressing the SCAAP process. If Fossetts Farm retail development is approved the Stockvale Group feel the SCAAPs aspirations will be undeliverable).	The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact was taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study).  Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy and require a further retail impact assessment. No changes are proposed.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2173	Support	Support the Council in their aspiration to (i) improve Competitive Performance (ii) Reduce the cost base (iii) Diversify away from retail uses (iv) grow the local economy and/or population, which is a strong theme in the preferred option principle. Recognises that the High Street should be a social place that makes creative use of public space with a vibrant evening economy	Noted.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2175	Comment	Given that there is a shortage of housing and surplus retail property, there is no doubt that conversion to residential use should form part of the future of the Town Centre.	Noted. The Plan seeks to achieve this. In relation to ground floor conversion, this would be outside of designated shopping frontage and in accordance with national policy.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2177	Comment	SCAAP should be seeking diversification of some of the retail uses on the ground floor as conversion to residential uses, providing attractive exit strategies for the asset managers and investors. This needs careful consideration in terms of how spatially to organise the retail use.	Policy DS1 seeks to maintain retail uses or other town centre uses that provide an active frontage and contribute to the vitality of the town centre. No changes are proposed.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2179	Comment	Many of our High Streets have been shaped by their past, however they are now trapped in their current configurations and often in poor shape to face the future. In relation to Southend on Sea, this is certainly the case. The High Street in particular has a linearity with no social space for congregation, interaction and the alternative commercial uses that would reactivate these spaces such as cafes, coffee shops, office space and importantly a high intensification of residential uses both at ground level and above. The SCAAP and the Stockvale Group recognise that the High Street in particular requires a restructuring on a significant scale.	The SCAAP recognises the need to enhance and broaden the offer in the High Street and seeks to do this by encouraging a mix of retail, cafe and restaurant uses. The Plan also seeks to enhance and promote new public spaces within the centre. No changes are proposed.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	2244	Comment	Retail provision achieved 82% top score high priority.	Noted.

Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	<b>2261</b>	<b>Comment</b>	Whilst the BID do not object or have any particular concern regarding the moving of the Southend Football Club the move is predicted on the suggested development requirement to combine a significant number of retail outlets. This is being presented as a financial necessity to allow the Club to move to new premises, however, if this is supported many if not all the High Street chains are likely to follow.	Noted.
Question 6; Policy DS1	Southend Bid (Mr S Kearney) [496]	<b>2263</b>	<b>Object</b>	In relation to the Fossetts Farm development. Proposals to have a large quantum of A1 retail provision would have a major impact on the Town Centre which is highly likely to lead to a further decline of an already struggling retail offer within the High Street and surrounding environs. Furthermore, the highway connection and infrastructure would not support the level of traffic journeys that the proposals at Roots Hall are likely to generate.  The BID would ask that the Council ensure that in accordance with advice in the National Planning Policy Framework (NPPF) a sequential test is undertaken and would like to be informed of the conclusions in relation to the impact on the vitality and viability of the Town Centre. The BID are rightly concerned that the Fossetts Farm proposals will have negative impact on the future of the High Street and the existing retail economy of the SCAAP area.	The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact was taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study).  Any new proposal at Fossetts Farm, which includes a significant amount of retail development, will require planning permission, be subject to planning policy, satisfy a sequential test and require a retail impact assessment. No changes are proposed.
Question 6; Policy DS1 78	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	<b>2300</b>	<b>Support</b>	Valad (Europe) largely agree to the proposed approach to maintaining a prosperous retail centre, however , a number of amendments are suggested:	Noted.
Question 6; Policy DS1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	<b>2301</b>	<b>Object</b>	Part 7 of Policy DS1 states that the Council will encourage the landowner/landlord of a unit with little prospect of being occupied in the primary or secondary frontage to display local art. This should be removed. If this situation arises, the Council should liaise with the landowner/landlord and ask if this could be provided. It is not appropriate to set this out in policy.	The policy wording is considered to be appropriate as it seeks to 'encourage' landlords. This would necessitate consultation with the landlord/landowner. The policy merely sets out the Council's intent in such matters. However, it is considered that this statement can be moved to the supporting text.



Question 6; Policy DS1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	<b>2302</b>	<b>Object</b>	Policy DS1 seeks to ensure that new retail development is well integrated and closely linked with the Town Centre Primary Shopping Frontage and that proposals for retail development inside or outside the Primary Shopping Area will be determined in accordance with Core Strategy Policy CP2 (relating to Town Centre and Retail Development). The policy should be amended to state that any out of centre retail will be determined in accordance with the NPPF and Policy CP2 of the Core Strategy (in so far as it conforms with the NPPF). Policy CP2 was drafted before the publication of the NPPF and is out of date in some respects, referring to the needs test, for example.	It is accepted that the Core Strategy was adopted before the publication of the National Planning Policy Framework (NPPF). It is therefore proposed that the following words are added to the end of Policy DS1 point 2 as follows: <b>'and the provisions of the National Planning Policy Framework (NPPF)'</b> .
Question 6; Policy DS1	Belfairs Garden Residents Association (Barbara Armitage) [511]	<b>2334</b>	<b>Support</b>	Yes if improvements to shopping area are made. A bright and clean shopping area will attract custom but much of the shopping area is uneven and dirty.	Noted.
Question 6; Policy DS1  279	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2363</b>	<b>Support</b>	Yes – need reliable buses	Noted.
Question 6; Policy DS1	Procurement Consulting (Mr Barrie Evans) [513]	<b>2385</b>	<b>Comment</b>	The city centre lacks large retailers such as John Lewis etc and the shopping centres are outdated and house little of use to the population. Smaller retailers should be housed in the more traditional road side areas as opposed to shopping centres. A regular farmers market should be promoted further in the pedestrian centre and local Essex produce promoted. This should run over the weekend to allow workers to take advantage of this useful and enhancing function. Chelmsford has a new John Lewis and a thriving farmers market and the town centre is better for it.	The SCAAP in Policy DS1 seeks to provide for a prosperous retail centre and promotes the provision of street markets. No changes proposed.
Question 6; Policy DS1	Procurement Consulting (Mr Barrie Evans) [513]	<b>2386</b>	<b>Comment</b>	Shop fronts should have strict planning permission on them and rid the town of dilapidated and tacky cheap plastic oversized advertising frontage. This will enhance the areas look (Bury St Edmunds), assist with job creation and the local economy.	Policy DS1 6. Seeks to ensure that shop fronts are of a high standard of design. The adopted Design Guide provides for appropriate shop front design. Policy DM5 sets out provision for frontages of townscape merit. No changes proposed.

Question 6; Policy DS1	Historic England (Dr Natalie Gates) [514]	<b>2399</b>	<b>Comment</b>	Note that the current wording does not explicitly set out the importance of roof scape as a part of overall building frontages in paragraph 6.	Noted. It is proposed that the word 'roofscape' be inserted into Policy DS1 6. So that it reads: <b><i>'All new shop frontages will be of a high standard of design that is compatible with the architectural style, <u>roofscape</u> and character of the building and surrounding area....'</i></b>
Question 6; Policy DS1	National Federation for the Blind (Mrs Jill Allen-King) [516]	<b>2427</b>	<b>Comment</b>	All shops in the High street should have flat entrances and therefore be totally accessible for all customers including disabled people. Shops should not have A-boards or other obstacles outside them, restricting the safe passage of pedestrians especially Blind people. If restaurants and cafes want to have tables outside then they must have a metre high barrier, preventing blind people from walking in to them.  When market stalls are positioned in the high street it is very difficult and dangerous for blind and partiality sighted people to walk.	Noted. Access arrangements to shops are considered as part of the design stage of planning applications to ensure accessibility for all users. No changes to policy are proposed.  The Council seeks to discourage the use of 'A' Boards as outlined in the Streetscape Guide SPD.
<b>Managing Primary Shopping Frontages - Policy Options DS1a, DS1b and DS1c</b>					
Question 7: Managing Primary Shopping Frontages; Policy Options DS1a, DS1b and DS1c	Burges Estate Residents Association [176]	<b>1996</b>	<b>Comment</b>	On balance Option B is to be preferred for providing the greatest flexibility in what is a fast changing situation. There is also the question mark about one or two centres. The plan makes implicit reference to reinforcing routes to the two main areas which suggests that the middle might become less of a Class A1 retail use. Overall while there is a case for upgrading and improving the shopping environment both in terms of public space and retail stores, the overall volume of retail space should not increase.	Noted.
Question 7: Managing Primary Shopping Frontages; Policy Options DS1a, DS1b and DS1c	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	<b>2303</b>	<b>Object</b>	Three policy options are presented which seek to maintain a prosperous retail centre. In the first instance, further clarity is required as to how the length of frontage should be calculated when assessing the percentage of A1 units.	The Policy options refer to length of measured frontage which is depicted on the Policies Map. This is considered to be clear in its intent and has been successfully implemented as Council policy for over 20 years. No changes are proposed.

Question 7: Managing Primary Shopping Frontages; Policy Options DS1a, DS1b and DS1c	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	<b>2304</b>	<b>Support</b>	<p>Valad (Europe) support Option C as it will allow for more restaurant (A3) uses which the town centre is currently lacking. More A3 uses will increase footfall and linked trips and support the night time economy thus adding to the vitality and viability of the town centre. Further flexibility should be built into this policy to allow other town centre appropriate uses to be permitted providing there is not an over concentration of these uses within a certain length of the frontage.</p> <p>It is widely acknowledged that the nature of retail is changing. It must be acknowledged that retail frontage policy needs to change, to allow capacity for other, new innovative uses, as well as other leisure and supporting uses which will create vitality in the borough's centres.</p>	<b>Noted.</b>
Question 7: Managing Primary Shopping Frontages; Policy Options DS1a, DS1b and DS1c	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	<b>2305</b>	<b>Comment</b>	<p>The Council should also consider directing new large comparison retail developments onto existing car parks in order to help strengthen the town centre and prevent it from going into decline. This would achieve the town centre first approach to retail of the NPPF. Car parking could then be re-provided in the form of undercroft or multi storey parking facilities. This would assist in relieving pressure on existing parking facilities whilst bolstering the town centre, thus enhancing its vitality and viability.</p>	<p>The policy provisions in the Plan promote mixed use developments on the existing car parks which would not preclude retail development if this were to come forward for consideration. No changes are proposed.</p>
Question 7: Managing Primary Shopping Frontages; Policy Options DS1a, DS1b and DS1c; para 47, para 48	Bowhill Planning Partnership (Anthony Bowhill) [474]	<b>2319</b>	<b>Comment</b>	<p>It is clear (paras 47 &amp; 48) that vacancy is higher than the average national town centre rate. While this may partly be as a result of the high level of vacancies in the Victoria Shopping Centre, inspection shows that there are also many vacant units in the High Street itself. In relation to this the increasing flexibility with regard to non-retail floor space set out in policy options DS1a-c provides a pragmatic approach to ensuring vacant units are used in an appropriate way, particularly for restaurant use.</p>	<b>Noted.</b>

Question 7: Managing Primary Shopping Frontages; Policy Options DS1a, DS1b and DS1c	Bowhill Planning Partnership (Anthony Bowhill) [474]	2320	Support	DS1a-c provides a pragmatic approach to ensuring vacant units are used in an appropriate way, with each option providing greater flexibility. Policy DS1c is to be supported as it provides the greatest flexibility, thus allowing more restaurants. The increase in the number of restaurants and cafes are to be welcomed as they will encourage shoppers to remain longer.	Noted.
Question 7: Managing Primary Shopping Frontages; Policy Options DS1a, DS1b and DS1c	Belfairs Garden Residents Association (Barbara Armitage) [511]	2335	Support	Option B <i>supported</i> .	Noted.
Question 7: Managing Primary Shopping Frontages; Policy Options DS1a, DS1b and DS1c	Procurement Consulting (Mr Barrie Evans) [513]	2387	Comment	Quality restaurants should be promoted in the area and quiet bars should also be promoted instead of chain sports bars which degrade the area. Bars etc should not be concentrated in one area as this will again cause degradation and poor maintenance. This never works and if you look at union street in Plymouth and Botchergate in Carlisle a concentration of Lively bars ruins an area, promoting drugs, prostitution and dilapidated buildings.	Policy DS1 seeks to retain a balanced mix of uses within the town centre.
<b>Employment</b>					
Question 8	Essex Chambers of Commerce (Mr John Dallaway) [452]	1940	Support	Agree with proposed approach to employment development as it seeks to take advantage of the important opportunities that the central area has over the coming years.	Noted.

Question 8	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2040	Support	The aspirations and preferred options are supported in relation to the further regeneration, renewal and economic growth in the SCAAP area.	Noted.
Question 8 283	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2084	Support	Wholeheartedly support the encouragement and expansion of businesses in the Southend Central Area, although note that the issues around transport, access and parking need further consideration and understanding.	Noted.
Question 8	Southend Bid (Mr S Kearney) [496]	2141	Support	The aspirations and preferred options are supported in relation to the further regeneration, renewal and economic growth in the SCAAP area.	Noted.
Question 8	Southend Bid (Mr S Kearney) [496]	2187	Support	Wholeheartedly support the encouragement and expansion of businesses in the Southend Central Area, although note that the issues around transport, access and parking need further consideration and understanding.	Noted.

Question 8	Southend Bid (Mr S Kearney) [496]	2241	Support	Southend BID received a 100% consultation responses having sent the Pro Forma out to 50 plus business consultees. The results of these can also be found in the tables in Appendix 4. Overwhelmingly the business consultees supported the SCAAP aspiration for Southend to be a prosperous, vibrant, safe cultural hub and City by the Sea. 94% of respondents supported that. In relation to the SCAAP's aspirational growth in homes in the Central Area 74% of the consultees supports the Councils vision. In relation to the job growth within the SCAAP area 90% of the respondents supported the Councils aspiration.	Noted
Question 8	National Federation for the Blind (Jill Allen King) [516]	2428	Comment	With reference to employment opportunities, consideration should be given to the employment of people with disabilities. The Council does have a responsibility under the Equality Act to take the needs of disable people in to account.	Noted
<b>Housing</b>					
Question 9: Residential Development (site allocations) 284	Basildon Borough Council (Amanda Parrott) [492]	2033	Support	It is recognised that additional work has been undertaken by Southend-on-Sea Borough Council to identify opportunity sites with the potential to deliver additional housing supply within the Southend Central Area, over and above that initially proposed in the Core Strategy. This is welcomed by Basildon Borough Council in terms of meeting housing needs arising within the South Essex Housing Market Area.	Noted.
Question 10: Residential Development (proposed approach)	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2074	Comment	Given that there is a shortage of housing and surplus retail property, there is no doubt that conversion to residential use should form part of the future of the Town Centre.	Noted. The Plan seeks to achieve this. In relation to ground floor conversion, this would be outside of designated shopping frontage and in accordance with national policy.

Question 10: Residential Development (proposed approach)	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2080</b>	<b>Support</b>	As noted in the British Property Federation Report 'Meeting the Town Centre Challenge' Town Centres are accessible places suitable for densification and accommodating more housing. In this regards the Stockvale Group supports the aspirations of the Local Planning Authority through the SCAAP.	<b>Noted.</b>
Question 10: Residential Development (proposed approach)	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2087</b>	<b>Support</b>	Support a higher intensification of residential uses in the Central Area together with a broader mix of commercial uses ranging from A1 retail, A3 restaurant café, D2 leisure and B1 offices to provide a more diverse multi-use High Street and Central Seafront.	<b>Noted.</b>

Question 10: Residential Development (proposed approach)	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2094</b>	<b>Support</b>	The STOCKVALE GROUP understand the concept of a much greater residential intensification of the SCAAP area and would wholeheartedly support the Councils aspirations for an additional 4000+ homes however, this must be in the context of insuring there is suitable amenity and infrastructure. The intensification together with a greater mix of uses in the Town Centre and Central Seafront create a much more buoyant and sustainable economy and the STOCKVALE GROUP welcome the Councils proposals as part of the SCAAP planning document.	Noted.
Question 10: Residential Development (proposed approach)	Southend Bid (Mr S Kearney) [496]	<b>2176</b>	<b>Comment</b>	Given that there is a shortage of housing and surplus retail property, there is no doubt that conversion to residential use should form part of the future of the Town Centre.	Noted. The Plan seeks to achieve this. In relation to ground floor conversion, this would be outside of designated shopping frontage and in accordance with national policy.
Question 10: Residential Development (proposed approach)	Southend Bid (Mr S Kearney) [496]	<b>2182</b>	<b>Support</b>	As noted in the British Property Federation Report 'Meeting the Town Centre Challenge' Town Centres are accessible places suitable for densification and accommodating more housing. In this regards the BID supports the aspirations of the Local Planning Authority through the SCAAP.	Noted.
Question 10: Residential Development (proposed approach)	Southend Bid (Mr S Kearney) [496]	<b>2190</b>	<b>Support</b>	Support a higher intensification of residential uses in the Central Area together with a broader mix of commercial uses ranging from A1 retail, A3 restaurant café, D2 leisure and B1 offices to provide a more diverse multi-use High Street and Central Seafront.	Noted.
Question 10: Residential Development (proposed approach)	Southend Bid (Mr S Kearney) [496]	<b>2197</b>	<b>Support</b>	The BID understand the concept of a much greater residential intensification of the SCAAP area and would wholeheartedly support the Councils aspirations for an additional 4000+ homes however, this must be in the context of insuring there is suitable amenity and infrastructure. The intensification together with a greater mix of uses in the Town Centre and Central Seafront create a much more buoyant and sustainable economy and the BID welcome the Councils proposals as part of the SCAAP planning document.	Noted.



Question 10: Residential Development (proposed approach)	Southend Bid (Mr S Kearney) [496]	<b>2242</b>	<b>Support</b>	Southend BID received a 100% consultation responses having sent the Pro Forma out to 50 plus business consultees. The results of these can also be found in the tables in Appendix 4. Overwhelmingly the business consultees supported the SCAAP aspiration for Southend to be a prosperous, vibrant, safe cultural hub and City by the Sea. 94% of respondents supported that. In relation to the SCAAP's aspirational growth in homes in the Central Area 74% of the consultees supports the Councils vision. In relation to the job growth within the SCAAP area 90% of the respondents supported the Councils aspiration.	Noted
Question 10: Residential Development (proposed approach)	Bowhill Planning Partnership (Anthony Bowhill) [474]	<b>2324</b>	<b>Support</b>	It is crucial that more housing be provided in and close to the town centre. This is because Southend is ringed by the green belt and thus there is no room for outward expansion. Every effort should be made to find suitable new sites, including the use of redundant office blocks which are now no longer required. Owners and developers should be encouraged to bring these forward with the emphasis on the lower end of the market.	Noted.
Question 10: Residential Development (proposed approach)	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2364</b>	<b>Comment</b>	We must have more affordable family homes in the Borough of two storeys with gardens	Noted.
Question 10: Residential Development (proposed approach)	Procuresure Consulting (Mr Barry Evans) [513]	<b>2376</b>	<b>Comment</b>	Southend-On-Sea central residential area is dilapidated due to poor land lord management. Houses should be returned from multiple occupancy to private family homes. The property management companies and landlords in Southend are in it for pure profit, and many don't even live in Southend. This culture has been proven to bring down the standard of living in an area which is demonstrated all over Southend. The planning department should be promoting family owned homes in central Southend allowing private money to turn the dilapidated properties back in to quality family homes steering away from flats and multiple occupancy. This would attract London professionals who have the disposable income to significantly invest in their own properties, providing employment for local tradesmen.	A key aspect of the SCAAP is to promote residential development in the central area to provide a range of dwelling types suited to housing needs. Policy DM7 of the Development Management Document also seeks to promote family accommodation. No changes proposed.

Question 10: Residential Development (proposed approach)	Procurement Consulting (Mr Barry Evans) [513]	<b>2381</b>	<b>Comment</b>	Southend-On-Sea central area should make use of more student halls as opposed to multiple occupancy private landlord run accommodation. The multiple occupancy student accommodation in residential areas has been studied elsewhere and is proven to bring down the area in which it is situated. Students living in residential areas do not do anything for that area. Students should be accommodated in halls which should be funded by the university. The current university halls are a complete eye sore and do nothing to enhance the local area. This architectural design is not sensitive to the culture in Southend and not built to last. Private landlords should be strictly controlled and forced to maintain properties to a high standard, which is currently not happening.	Noted. The Plan seeks to enhance and promote improved educational facilities and to provide opportunities for the provision of additional student accommodation, No changes proposed. Policy PA3.4. outlines that new student accommodation should be accompanied by a long term management and maintenance plan, to ensure the development has a positive impact on local amenity and environment for the lifetime of its use.
Question 10: Residential Development (proposed approach)	National Federation for the Blind (Mrs Jill Allen-King) [516]	<b>2429</b>	<b>Comment</b>	In section 65 you talk about new housing in the Central area. This should include housing for elderly and disabled people. I think you should also have sheltered housing and homes for elderly and disabled people. This would enable elderly and disabled people to walk to shops and take an active part in the life of their community.	The Plan seeks to provide for a range of housing types to meet housing needs.
Question 10: Residential Development (proposed approach)	National Federation for the Blind (Mrs Jill Allen-King) [516]	<b>2430</b>	<b>Comment</b>	<i>In paragraph 75</i> all student accommodation should also be made fully accessible, so that not only disabled students can live there but they can be visited by their friends and family.	Noted. Access arrangements to residential accommodation are considered as part of the design stage of planning applications and will meet building regulations to ensure accessibility for all users.
<b>Culture, Leisure, Tourism and Recreation</b>					
Question 11	Essex Chambers of Commerce (Mr John Dallaway) [452]	<b>1941</b>	<b>Support</b>	Agree with the proposed approach to culture, leisure and recreation as it recognises its importance to the local economy	Noted.
Question 11	Burges Estate Residents Association [176]	<b>1999</b>	<b>Support</b>	The drive towards enhanced culture, leisure and recreational activities is supported and indeed is essential if we are to take advantage of the huge potential for visitors from the continent. This must be coupled with more and better hotel accommodation to encourage longer stays.	Noted

<p>Question 11</p> <p style="text-align: right;">289</p>	<p>Stockvale Group representing Sands &amp; Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]</p>	<p>2049</p>	<p><b>Object</b></p>	<p>Many of the Seafront businesses consulted as part of the Stockvale Group's own consultation have identified their concerns that leisure and tourism is not a strong focus of the SCAAP.</p>	<p>The Central Seafront Policy Area aims and policy provisions (Policy CS1) seek to actively promote Southend as a 'thriving and vibrant leisure, cultural and tourism area' (page 110). However, it is recognised that this approach is not strongly reflected and identified in the vision, the strategic objectives or section 4.5 of the Plan (Culture, Leisure, Tourism and Recreational Facilities). It is therefore recommended that the words 'and resort' is added in the vision after 'regional centre'. The vision would then read:  <b><i>'Our vision for Southend Central Area, which includes the Town Centre and Central Seafront Area, is for it to be a City by the Sea. As a prosperous and thriving regional centre <u>and resort</u>, it will be an area...'</i></b></p> <p>Strategic Objective 10 (page 18) would be amended and split to address these issues, and placed further up the ordering:  <b><i>' To promote and enhance the tourism, cultural and leisure offer within the central area, including visitor accommodation, having regard to the assets offered by the area, in order to attract greater visitor numbers and promote more overnight stays.  To promote the central area as a thriving learning quarter that provides state of the art facilities and well-designed student accommodation'.</i></b></p> <p>Amend the last sentence of paragraph 76 (page 39) to read:  <b><i>'This will build on the town's role as a major resort and contribute to a stronger, more vibrant centre.</i></b></p>
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Question 11	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2151	<b>Object</b>	Many of the Seafront businesses consulted as part of the Stockvale Group's own consultation have identified their concerns that leisure and tourism is not a strong focus of the SCAAP.	<p>The Central Seafront Policy Area aims and policy provisions (Policy CS1) seek to actively promote Southend as a 'thriving and vibrant leisure, cultural and tourism area' (page 110). However, it is recognised that this approach is not strongly reflected and identified in the vision, the strategic objectives or section 4.5 of the Plan (Culture, Leisure, Tourism and Recreational Facilities). It is therefore recommended that the words 'and resort' is added in the vision after 'regional centre'. The vision would then read:</p> <p><b><i>'Our vision for Southend Central Area, which includes the Town Centre and Central Seafront Area, is for it to be a City by the Sea. As a prosperous and thriving regional centre and resort, it will be an area...'</i></b></p> <p>Strategic Objective 10 (page 18) would be amended and split to address these issues, and placed further up the ordering:</p> <p><b><i>'To promote and enhance the tourism, cultural and leisure offer within the central area, including visitor accommodation, having regard to the assets offered by the area, in order to attract greater visitor numbers and promote more overnight stays. To promote the central area as a thriving learning quarter that provides state of the art facilities and well-designed student accommodation'.</i></b></p> <p>Amend the last sentence of paragraph 76 (page 39) to read:</p> <p><b><i>'This will build on the town's role as a major resort and contribute to a stronger, more vibrant centre.'</i></b></p>
Question 11	Southend Bid (Mr S Kearney) [496]	2243	<b>Comment</b>	Leisure and Tourism received 70% response as a top scoring 10 priority.	Noted.
Question 11	Belfairs Garden Residents Association (Barbara Armitage) [511]	2336	<b>Comment</b>	Much better promotion of the Pier and its Museum and better Quality building on the Pier is required. Tourist information is tucked away on the Pier and promotions at the railway stations and airport are needed as well as some direction in the town for information. The new Beecroft Art Gallery is bare inside and does not announce what it is outside with any colour	Noted.

Question 11	Belfairs Garden Residents Association (Barbara Armitage) [511]	2337	Comment	Public Art should not be a factor in planning permission.	Public art provision is considered essential to improving the public realm and environment.
Question 11	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	2365	Support	Yes, but we need later running transport, and a concert hall.	Noted.
Question 11 291	Procurement Consulting (Mr Barry Evans) [513]	2389	Comment	Southend-On-Sea Council need a clearer tourist and seafront strategy. It is no good just stating that the pier is open to development and the seafront enhanced. Building high rise flats will not enhance the seafront and any developer that wishes to build should be prepared to enhance the infrastructure including car parks and access.	The vision and strategy is considered to be forward looking and ambitious. No changes proposed. Policy CS1 sets out the development principles that will be used to assess development proposals within the central seafront area. Policy DM4 of the Development Management Document sets out policy to manage tall and large buildings. Specific tourist and cultural strategies are prepared by other Council services outside of the SCAAP.
<b>The Historic Environment</b>					
Question 12	Milton Conservation Society (Mr Andy Atkinson) [488]	1977	Support	The broad intentions, including statutory obligations, are supported.	Noted.
Question 12	Milton Conservation Society (Mr Andy Atkinson) [488]	1978	Object	Far too little importance is given to our historic past, both designated and un-designated and instead it is seen as something of the past, to be preserved rather than part of our aggregated and improved future.	Noted. Detailed policy on the historic environment is contained within the Development Management Document (Policy DM5). It would be inappropriate and repetitive to include such policy provisions within the SCAAP. To emphasise the importance of the historic environment it is proposed to add a new sentence after 79 to read: <b><i>'Heritage assets will be promoted and enhanced as part of the future development of the town'</i></b> .

Question 12	Milton Conservatio n Society (Mr Andy Atkinson) [488]	1980	Comment	In particular we would like to see far more recognition given to historic Southend, including the non-designated building frontages 'of townscape merit' in the High Street. This 'townscape merit' should not just be a 'material consideration' in future planning decisions but should be woven into the future planning of our town centre. This is not because of some sort of nostalgic affection for these buildings (although this does strongly exist) but because these are amongst the best buildings in our town and future construction should aggregate from these with the best of human scaled, modern or traditional 'living' architecture.	Frontages of Townscape Merit are identified on the Policies Map and in Policy PA1. Detailed policy on the historic environment, including 'frontages of townscape merit', is set out in the Development Management Document (Policy DM5). No changes are proposed.
Question 12  292	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2081	Support	Supports the enhancement and quality of the Green and Open spaces along the Central Seafront and the protection of Southend's unique heritage including the nationally important Grade II Listed Southend Pier.	Noted.
Question 12	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2102	Support	The STOCKVALE GROUP note the townscape improvements and guidance on design quality and Heritage preservation and enhancement are inextricably linked to improvements to Public Realm and pedestrian connectivity. The STOCKVALE GROUP like the majority of the Town support the continued regeneration and reinvention of the Towns greatest icon Southend's Pleasure Pier. As there are a number of opportunity sites outlined in the SCAAP document, the STOCKVALE GROUP would suggest that the Council (through the SCAAP document) develop design codes and development briefs to ensure that the townscape improvements and quality of design of future developments meet the aspirational high standard to create a coherent and consistent Central Area. This needs to reflect on the Towns Heritage and look towards the future to create Southend as unique place and destination for leisure, shopping, living and working.	Noted. The Plan seeks to achieve this and will be updated to identify a number of proposal sites that could be subject to a masterplanning approach.

Question 12	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2136	Support	There is wholeheartedly a support for regeneration and renewal of the SCAAP Area, the STOCKVALE GROUP would support the improvements to the Victoria Gateway through to the top end of London Road, down the High Street including Tylers Avenue, connection through to the Seafront from the High Street, improvements to the Seafront, the Public Realm, landscaping of the Public Realm, enhancement of key views and connectivity, preservation enhancement of the iconic Southend Pier and the general approach to intensification of the Town Centre	Noted.
Question 12	Southend Bid (Mr S Kearney [496]	2183	Support	Supports the enhancement and quality of the Green and Open spaces along the Central Seafront and the protection of Southend's unique heritage including the nationally important Grade II Listed Southend Pier.	Noted.
Question 12 293	Southend Bid (Mr S Kearney [496]	2205	Support	The BID townscape improvements and guidance on design quality and Heritage preservation and enhancement are inextricably linked to improvements to Public Realm and pedestrian connectivity. The BID like the majority of the Town support the continued regeneration and reinvention of the Towns greatest icon Southend's Pleasure Pier. As there are a number of opportunity sites outlined in the SCAAP document, the BID would suggest that the Council (through the SCAAP document) develop design codes and development briefs to ensure that the townscape improvements and quality of design of future developments meet the aspirational high standard to create a coherent and consistent Central Area. This need to reflect on the Towns Heritage and look towards the future to create Southend as unique place and destination for leisure, shopping, living and working.	Noted. The Plan seeks to achieve this and will be updated to identify a number of proposal sites that could be subject to a masterplanning approach.
Question 12	Southend Bid (Mr S Kearney [496]	2250	Comment	Townscape Improvements and Guidance on Design, Quality and Heritage Preservation were given a top 10 priority by 36% of respondents.	Noted.

Question 12	Southend Bid (Mr S Kearney [496])	2265	Support	There is wholeheartedly a support for regeneration and renewal of the SCAAP Area, the BID would support the improvements to the Victoria Gateway through to the top end of London Road, down the High Street including Tylers Avenue, connection through to the Seafront from the High Street, improvements to the Seafront, the Public Realm, landscaping of the Public Realm, enhancement of key views and connectivity, preservation enhancement of the iconic Southend Pier and the general approach to intensification of the Town Centre	Noted.
Question 12: Management of the historic environment	Belfairs Garden Residents Association (Barbara Armitage) [511]	2338	Comment	The policy is well stated but the delivery of it is questioned.	Noted.
Question 12 294	Procuresure Consulting (Mr Barry Evans) [513]	2382	Comment	Southend-On-Sea conservation areas should be extended and cover the majority of Southend's Georgian and Victorian buildings, both residential and commercial. Shop owners and retailers should have strict planning guidelines and be forced to maintain shop fronts. Shops such as Bargain buy with their over use of on street advertising and garish and tacky shop fronts should be banned and in place smaller and more traditional shop fronts should be used. Hitchin, Bury St Edmunds etc have good planning control which maintains the heritage look and feeling of pride in those towns.	Conservation Area reviews are undertaken periodically to assess whether there is merit in seeking to extend/promote new areas in the town. Shop front design is covered in the Council's adopted design guide. Policy DS1 also seeks to ensure shop frontages are of a high standard of design. Policy PA1.2.b. supports the conservation and restoration of historic shop fronts. No changes proposed.
Question 12	Historic England (Dr Natalie Gates) [514]	2400	Comment	We note the rationale to not duplicate the policies contained elsewhere. We would suggest deleting "...and much of the archaeology in these locations is likely therefore to have been destroyed" from paragraph 91 as even previously developed sites have potential for archaeology and the focus should be on those sites of high potential.	This point is accepted. It is therefore proposed to delete the words ' <i>...and much of the archaeology in these locations is likely therefore to have been destroyed</i> ' from paragraph 91.
Question 12	Historic England (Dr Natalie Gates) [514]	2401	Comment	We would recommend including Policy Linkages to Policies DM1, DM4 and DM6 in the Development Management DPD and Policy DS3 in the SCAAP itself.	Noted. It is proposed to include references in the policy linkages box to <i>Policies DM1, DM4 and DM6 in the Development Management DPD and Policy DS3 in the SCAAP itself.</i>

**Open and Green Space Provision**



Question 13	Essex Chambers of Commerce (Mr John Dallaway) [452]	1942	Support	Agree with proposed approach to open and green space provision in Southend Central Area	Noted.
Question 13	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2082	Support	Supports the enhancement and quality of the Green and Open spaces along the Central Seafront and the protection of Southend's unique heritage including the nationally important Grade II Listed Southend Pier.	Noted.
Question 13	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2137	Support	There is wholeheartedly a support for regeneration and renewal of the SCAAP Area, the STOCKVALE GROUP would support the improvements to the Victoria Gateway through to the top end of London Road, down the High Street including Tylers Avenue, connection through to the Seafront from the High Street, improvements to the Seafront, the Public Realm, landscaping of the Public Realm, enhancement of key views and connectivity, preservation enhancement of the iconic Southend Pier and the general approach to intensification of the Town Centre	Noted.
Question 13	Southend Bid (Mr S Kearney) [496]	2184	Support	Supports the enhancement and quality of the Green and Open spaces along the Central Seafront and the protection of Southend's unique heritage including the nationally important Grade II Listed Southend Pier.	Noted.

Question 13	Southend Bid (Mr S Kearney [496])	2266	Support	There is wholeheartedly a support for regeneration and renewal of the SCAAP Area, the BID would support the improvements to the Victoria Gateway through to the top end of London Road, down the High Street including Tylers Avenue, connection through to the Seafront from the High Street, improvements to the Seafront, the Public Realm, landscaping of the Public Realm, enhancement of key views and connectivity, preservation enhancement of the iconic Southend Pier and the general approach to intensification of the Town Centre	Noted
Question 13	Mr Alan Grubb [59]	2296	Comment	There is also a need to create green spaces in each of the new developments with semi matured trees, this will then invite the wild life (birds and squirrels)	Such provisions for urban greening are included in the various policies of the Plan. No changes are proposed.
Question 13	Belfairs Garden Residents Association (Barbara Armitage) [511]	2339	Comment	There is a plan to build on Blenheim Park an overlarge sports building. It is difficult to understand why the policies for the above culture and green spaces have been 'rationalised and removed. Does this mean that their importance has been allowed to downgrade?	Outside the Plan area.
Question 13 296	National Federation for the Blind (Mrs Jill Allen-King) [516]	2431	Comment	There are many guide dog owners who live in the Southend district and close to the high street as well as many that visit the high street for shopping and holidays. Currently there is no safe free running areas for our dogs and nowhere is there an area where our dogs can go to the toilet. Up to the time when the Odeon cinema was built in Victoria Circus area, there was always some grass where our dogs could go. Although our dogs are trained to go in a gutter there are very few of these now except in a few side streets. So please plan for designated areas close to bus and train stations and to shops. You cannot complain about dog mess when no areas are provided. When building blocks of flats and other housing this should also be provided, not only for guide dog owners but for other dog owners. Green areas should be provided with seating and shelters. The shelters to protect people from the rain and hot sun. There is no mention of Public Toilets in the document and they should be available throughout the town in shopping areas and green space areas.	The Plan seeks to enhance and provide new areas of open and green throughout the Central Area. No changes are proposed.

Question 14; Policy DS2	Essex Chambers of Commerce (Mr John Dallaway) [452]	<b>1943</b>	<b>Support</b>	Important to protect and enhance the management of Key Views in Southend Central Area.	Noted.
Question 14; Policy DS2	Burges Estate Residents Association [176]	<b>2000</b>	<b>Comment</b>	The management of key views is acceptable but I have tried and failed to see Porters other than from inside the grounds. It is so well screened by trees and shrubs I doubt many people know it's there. In that sense it can hardly rank as a landmark building.	Policy DS3 not only sets out criteria protecting the views to and from landmark buildings, but the policy also seeks to conserve landmarks and enhance their setting. It is considered that the setting of Porters and links to the Queensway Policy Area can be improved. Furthermore, maintaining and enhancing key views to Porters is considered important and beneficial to the aesthetic quality of the local area. No change required.
Question 14; Policy DS2  297	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2043</b>	<b>Support</b>	Wholly support enhancement and retention of key views	Noted.

Question 14; Policy DS2	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2138</b>	<b>Support</b>	There is wholeheartedly a support for regeneration and renewal of the SCAAP Area, the STOCKVALE GROUP would support the improvements to the Victoria Gateway through to the top end of London Road, down the High Street including Tylers Avenue, connection through to the Seafront from the High Street, improvements to the Seafront, the Public Realm, landscaping of the Public Realm, enhancement of key views and connectivity, preservation enhancement of the iconic Southend Pier and the general approach to intensification of the Town Centre	Noted.
Question 14	Southend Bid (Mr S Kearney) [496]	<b>2144</b>	<b>Support</b>	Wholly support enhancement and retention of key views	Noted.
Question 14; Policy DS2  298	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2248</b>	<b>Comment</b>	Key Views were given a 20% top 10 priority.	Noted.

Question 14; Policy DS2	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2267</b>	<b>Support</b>	There is wholeheartedly a support for regeneration and renewal of the SCAAP Area, the BID would support the improvements to the Victoria Gateway through to the top end of London Road, down the High Street including Tylers Avenue, connection through to the Seafront from the High Street, improvements to the Seafront, the Public Realm, landscaping of the Public Realm, enhancement of key views and connectivity, preservation enhancement of the iconic Southend Pier and the general approach to intensification of the Town Centre	Noted
Question 14; Policy DS2	Historic England (Dr Natalie Gates) [514]	<b>2402</b>	<b>Support</b>	Welcome the identification of a number of key views, from within and of the central area, with the aim that they will not be adversely impacted by development.	Noted.
<b>Landmarks and Landmark Buildings – Policy DS3</b>					
Question 15; Policy DS3	Essex Chambers of Commerce (Mr John Dallaway) [452]	<b>1944</b>	<b>Support</b>	Agree with the proposed approach to landmarks/landmark buildings in Southend Central Area	Noted.

Question 15; Policy DS3	Milton Conservatio n Society (Mr Andy Atkinson) [488]	1979	<b>Object</b>	The document seems to place landmarks and landmark buildings (section 4.9) above the best quality aggregated urban design. This attitude of <i>landmark</i> (or 'iconic' building to use the popular language) is becoming discredited so it seems rather odd that it so strongly features in our forward looking planning. As an example, the Sainsbury site was, not many years ago, hailed by the planners of our town as an important and focal town centre development. Not many years passed before the folly of this development was then realised so that the site has been proposed for re-development, should Sainsbury's relocate, and this is included in the document. Your document feels like it will lead to similar, repeated mistakes in future. The student housing building, now proposed as nothing less than a new potential landmark building demonstrates exactly what we are claiming. This building is largely disliked and ridiculed because it was built as an iconic or landmark building that paid virtually no relation to its urban surroundings. This type of arrogant 'look at me' building should not be the focus of future urban development in the town centre.	Landmarks and landmark buildings provide orientation and aid way-finding, being recognisable and distinctive, and it is important that they are conserved. Policy DS3 sets out the provision for the development of new landmark buildings to ensure they are well designed and detailed to help reinforce local character and distinctiveness.  The Sainsbury's site will not be included in the final version of the SCAAP as there is insufficient evidence that it will be redeveloped by 2021.
Question 15; Policy DS3	Southend Bid (Mr S Kearney) [496]	2247	<b>Comment</b>	Landmarks and Landmark Buildings 36% of respondents gave that top 10 priority.	Noted.
Question 15; Policy DS3	Belfairs Garden Residents Association (Barbara Armitage) [511]	2340	<b>Support</b>	Yes to the list of Landmark Buildings.	Noted.
Question 15; Policy DS3	Belfairs Garden Residents Association (Barbara Armitage) [511]	2341	<b>Object</b>	No to Potential Locations as already publicised at Opportunity Site 8: Seaway Car Park, Marine Parade and Opportunity Site 9 : New Southend Museum.	Seaway car park, Marine Parade and the New Southend Museum are key development sites identified in the SCAAP and are considered appropriate for the provision of new landmark buildings. Design and detailing will be important in such provision as set out in Policy DS3. No changes proposed.

Question 15; Policy DS3	Historic England (Dr Natalie Gates) [514]	2403	Comment	Recommend that a bullet point d) is added to paragraph two of Policy DS3 stating: "d. the proposals do not harm the setting of nearby heritage assets."	These points are accepted. It is therefore proposed to include an additional criteria in paragraph 2 of Policy DS3 stating: ' <b><i>d. the proposals do not harm the setting of nearby heritage assets.</i></b> '
Question 15; Policy DS3	National Federation for the Blind (Mrs Jill Allen-King) [516]	2432	Comment	In paragraph 99 which refers to Landmark buildings, these buildings can help blind and partially sighted people to locate where they are, so long as they are well lit and have good colour contrast with their surroundings. Also tactile information should be given and provided. For example, a water fountain or chiming clock can help to find a building.	Noted.
<b>Flood Risk Management and Sustainable Drainage</b>					
Question 16; Policy DS4	Essex Chambers of Commerce (Mr John Dallaway) [452]	1945	Support	Agree with the proposed approach to flood risk management and sustainable drainage in Southend Central Area	Noted.
Question 16; Policy DS4 101	Anglian Water (Sue Ball) [37]	2028	Comment	For Surface water disposal we would expect a SuDS solution to be utilised where at all viable and under no circumstances will surface water be permitted to discharge into the foul sewerage system. (Infrastructure Provision 4.12 paragraph 140)	Noted. It is proposed to add the following text to Policy DS4 point 2 as follows: '...Under no circumstances will surface water be permitted to discharge into a separate foul sewer or sewerage system. Surface runoff that cannot be discharged into the ground, a surface water body or a surface water sewer or local highway drain, must be discharged to a public, combined sewer system.'
Question 16; Policy DS4	Southend Bid (Mr S Kearney) [496]	2246	Comment	Flood Risk Management and Sustainable Drainage 48% of respondents gave that a top 10 priority.	Noted.
Question 16; Policy DS4	Belfairs Garden Residents Association (Barbara Armitage) [511]	2342	Comment	The Council persisted in its shared space along the sea front where flooding has damaged commercial properties and even put at risk the business of the owners.	Policy DS4 seeks effective flood risk management and sustainable drainage within new development. The maintenance and improvement of existing flood defence and mitigation is administered through complimentary Council services. <b>It is proposed to include reference in Central Seafront policies to flood mitigation measures.</b>

Question 16; Policy DS4	Environment Agency (Miss Lizzie Griffiths) [334]	<b>2420</b>	<b>Support</b>	<i>Paragraph 105</i> - We are pleased to note reference is made here to the Strategic Flood Risk Assessment and emerging Surface Water Management Plan, which clearly form a key part of your evidence base.	Noted.
Question 16; Policy DS4	Environment Agency (Miss Lizzie Griffiths) [334]	<b>2421</b>	<b>Comment</b>	<i>Paragraph 116</i> - With regards to surface water management, we wish to remind you that, whether or not the receiving water body is a main river, the Environment Agency is no longer the statutory consultee in the planning process. All surface water management scheme proposals and their associated discharge rates must therefore be approved by Southend Borough Council in its role as Lead Local Flood Authority.	Noted. Amend paragraph 116 last sentence to read, ' <b><i>For main rivers and ordinary water courses, this will be the Council, and for public surface water sewers Anglian Water,</i></b> '



<p>Question 16; Policy DS4</p> <p style="text-align: center;">303</p>	<p>Environment Agency (Miss Lizzie Griffiths) [334]</p>	<p><b>2422</b></p>	<p><b>Comment</b></p>	<p>We consider that further clarity could be provided within this policy. Our suggested changes are <i>as follows</i></p> <p>1 a. Will be accompanied by a flood risk assessment that considers all sources of flooding.</p> <p>1 c. i. For more vulnerable uses, the floor levels of habitable rooms will be above the design flood level, <i>with an allowance for climate change</i>. Within Flood Zone 3 the floor level must be situated above the design flood level <i>with climate change</i>, incorporating an allowance of at least 300mm for freeboard.</p> <p>(This is to ensure it is clear that floors must be set above the 1 in 200 annual probability event level plus climate change).</p>	<p>Noted, the following amendments are therefore proposed to DS4: <b><i>'1a. Will be accompanied by a flood risk assessment that considers all sources of flooding'. And '1ci. For more vulnerable uses, the floor levels of habitable rooms will be above the design flood level, with an allowance for climate change*. Within Flood Zone 3 the floor level must be situated above the design flood level with allowance for to climate change*, incorporating an allowance of at least 300mm for freeboard.</i></b></p> <p><b><i>* This is to ensure that floors must be set above the 1 in 200 annual probability event level plus climate change</i></b></p>
<b>Transport, Access and Public Realm</b>					
<p>Question 17; Policy DS5</p>	<p>Rev. Phyllis Owen [456]</p>	<p><b>1929</b></p>	<p><b>Object</b></p>	<p>Insufficient allowance for parking to take into account the number of residential units proposed.</p>	<p>The Councils parking standards are set out in the Development Management Document and these have been found sound by a planning inspector and subsequently adopted.</p>

Question 17; Policy DS5	Essex Chambers of Commerce (Mr John Dallaway) [452]	1946	Comment	Agree with the proposed approach to the management of transport, access and the public realm in the Southend Central Area, with the proviso that it should be amended to state that the Council 'will maintain car parking capacity at a level that supports the vitality and viability of the town centre' rather than 'seeking to maintain car parking capacity....'	Agree; the amendment is appropriate in the context of Policy DS5.2.a. <b>Remove the word 'seek'</b> . The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
Question 17; Policy DS5	Mr David Batley [479]	1975	Support	I strongly support the introduction of bus priority measures along the A13 (London Road). Most of Westcliff and Leigh near this road consist of high-density housing with no off-street parking, a land-use pattern which works well with high-frequency public transport.	Noted.
Question 17; Policy DS5	Mr Harry Chandler [219]	1988	Comment	It might be worth considering the creation of a tramway based on the bus station to connect Southend airport, Victoria Avenue, Southend Victoria railway station, Southend Central, the High Street and the sea front. It is likely that the creation of a tramway would encourage more visitors to come to Southend by train and help reduce our car parking problems.	Policy PA8 identifies the need for a priority route to be provided linking Southend Central Area with London Southend Airport. This does not preclude innovative transport schemes to link these points. Such improvements will be pursued mainly through the provisions of the Southend Local Transport Plan. A tramway is not considered viable or deliverable by 2021, and therefore is not included within the SCAAP. No changes are proposed.
Question 17; Policy DS5  304	Burges Estate Residents Association [176]	1990	Comment	There is an anti- car feeling about some of the comments. A reluctance to acknowledge its importance in sustaining the central area's economy and over emphasis on suppressing it in favour of other modes. Car parking is only mentioned in terms of capacity ignoring the issue of pricing which is one of the major disincentives that the centre faces.	The SCAAP seeks improvements to the transport network for all users. There have already been a number of major junction improvements. Further reference will be included in Policy DS5 to highlight proposed strategic junction improvements as outlined on the Policies Map. There will also be a review of signage and implementation of an integrated signage strategy to assist road users around the transport network and direct them to the most convenient car parks.  The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
Question 17; Policy DS5	Burges Estate Residents Association [176]	2001	Comment	Car parking on the sea front is mostly kerbside and it is difficult to believe that on- street parking duration is only 5mins. However the capacity needed to support the vitality of the town centre is not just a function of demand as it stands but the price mechanism. It must be a significant factor where choosing where to shop especially when so much of the competition has free parking.	The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP. It is proposed that specific reference to the 5 minute on-street parking duration will be removed.

Question 17; Policy DS5	Burgess Estate Residents Association [176]	2002	Comment	The only other point on transport is mixed mode priority routes. Mixed in the sense of ped/cycle routes are not working because too many cyclists now have it in their heads that any footway or footpath is fair game. I know this is an enforcement issue but if it cannot or will not be enforced effectively then it is bad policy.	The implementation of new pedestrian and cycle routes will have regard to national guidance and best practice.
Question 17; Policy DS5	Mr Michael Davies [493]	2037	Comment	Serious thought needs to be given to what <i>to</i> do to the local traffic situation. If the plan includes a large number of residential flats, how many cars will that mean? Which way will they go to get out of town? Along the 'Golden Mile' and seafront towards Westcliff, then up Chalkwell Avenue to the London Road, or up Southchurch Avenue to try to join the A127 arterial road via Bournemouth Park Road, Sutton Road, or Victoria Avenue? Either way, it will mean added congestion, frustration, and stress for road users in an already very congested town.	Noted. Policy DS5 and related Policy Area policies make provision for a number of transport improvements, particularly junction improvements and the promotion of passenger transport. These will be actioned through the Local Transport Plan and partnership working. No changes are proposed.
Question 17; Policy DS5  305	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2041	Comment	Transport and Access into the Town is a key theme and in order to deliver the aspired number of new dwellings and new jobs in the Central Area the Stockvale Group wish to see this appropriately addressed through the SCAAP documentation. At present Stockvale Group does not believe that the Transport, Access and Parking Issues have been given enough consideration. Nor the highway infrastructure on existing businesses let alone the aspirational growth.	Policy DS5 together with the Policy Area policies provide for a number of transport and highway improvements within Southend Central Area to improve accessibility and provide for more sustainable methods of transport. The Local Transport Plan will develop these further in line with planned growth.  The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.

Question 17; Policy DS5	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2042	Support	Wholly support townscape improvements, improvements to the public realm, vastly improved connectivity from car parks to the Seafront, car parks to the high street and the creation of active public spaces in an otherwise linear High Street.	Noted.
Question 17; Policy DS5  306	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2050	Comment	As part of this representation we have reviewed Blackpool's Local Plan Making as a similar seaside town and comparable seaside resort. As part of the Blackpool Core Strategy Consultation and examination in public, the seafront businesses made succinct clear representation regarding the impact of traffic and parking on the sustainability and future growth of Blackpool as a tourist resort and destination of choice. The [Blackpool] Seafront business representation noted that the major attractions that make Blackpool a tourist destination rely on easy access to car parking and good access from car parks to the attractions by foot and public transport. The Seafront businesses further noted that this matter is often not well understood by councils, who generally consider that it is not necessary to plan car parking for peak periods only. In most industries, for example planning the levels of parking for shopping areas based only on the Christmas peak, this a reasonable approach but for the businesses which are seasonal and need to meet visitor targets to survive (or at least to continue at the present scale), this approach can have far reaching consequences.	Noted.

Question 17; Policy DS5	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2051	Object	The Southend Seafront businesses feel this particular issue [related to peak periods for car parking] is not understood by the Local Authority and as such the level of tourism and investment has peaked. Many of the Seafront businesses have expressed their view as part of this consultation that they cannot invest further in the town due to the issue of access and parking and as such they already have a declining customer base.	<p>Policy DS5 and related Policy Areas include a number of proposals for improving transport accessibility. These policies will sit alongside the Local Transport Plan which aims to address issues of congestion, circulation and accessibility to Southend to assist economic growth.</p> <p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
Question 17; Policy DS5  307	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2052	Comment	Members of the Stockvale Group together with representatives of local businesses within the SCAAP area discussed highways and parking issues with Southend on Sea Borough Council's Head of Planning and Transport. As the Chief Officer responsible for transport he was recorded as saying 'the issue with parking is if you create more parking spaces, more people will come and they will create congestion i.e. there will be greater numbers of visitors to the Town meaning greater business! This exasperates the concerns of local businesses that parking and transport issues are not fully understood and have no serious consideration as part of the Council's preferred option and SCAAP Framework.	Noted, no agreed minutes are recorded of this meeting. The purpose of the SCAAP is to plan for regeneration, growth and inward investment whilst taking account of impacts on matters such as amenity and the local environment. This is planned for within a range of travel mode options and the infrastructure necessary to support them.

Question 17; Policy DS5	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2053	Comment	Interestingly the representations made by the Seafront businesses in Blackpool persuaded the Government Inspector of their position. The Inspector concluded in their report that 'Car Parks need to accommodate peak weekend/bank holiday parking'.	Noted.  The Council is unable to identify this direct quote in the Inspector's Report that has been cited.
Question 17; Policy DS5  308	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2076	Object	RICS paper 'High Streets Adapting for Change' discusses parking changes and out of town retail which provides free parking. Since 2007 many local authorities have increased parking charges significantly. In the SCAAP area this is a key issue which requires essential review. This is in contrast to the smaller districts of wider Southend on Sea, Leigh and Southchurch where the Council have extended free parking to 2 hours. Compared to Central Southend and the SCAAP area where parking for 2 hours is in excess of £3.30. In Stockvale Group's view this is a deterrent for people coming into Southend particularly for shopping. This combined with the poor spatial and environmental quality is a contributing factor to the decline of Southend's Town Centre.	The SCAAP recognises the importance of car parking provision to the vitality and viability of the centre. The wider implications of car parking charges are a matter which will be kept under review by the Borough Council as part of its overall approach to car parking for the Borough. The SCAAP is a planning policy document and does not directly cover parking charges. No changes are proposed.

Question 17; Policy DS5	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2085	Support	Wholeheartedly support the encouragement and expansion of businesses in the Southend Central Area, although note that the issues around transport, access and parking need further consideration and understanding.	Noted.
Question 17; Policy DS5  309	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2086	Object	Transport, access and parking issues need further consideration and are a particular issue for the Seafront businesses and the tourist economy. The highway infrastructure makes journeys into the town prolonged and difficult. Many visitors and customers simply don't return.	<p>Policy DS5 and related Policy Areas include a number of proposals for improving transport accessibility.</p> <p>The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>

Question 17; Policy DS5	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2090	Comment	Southend's Unique Selling Point is the Seafront which is a destination of choice. The Seafront and High Street inter relate on each other for business with the major attractions of Southend relying on easy access to car parking and good access from the car parks to the High Street and the Seafront attractions by foot or public transport.	Noted. The SCAAP seeks to improve and enhance connectivity between the seafront and town centre. No changes are proposed.
Question 17; Policy DS5  310	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2091	Comment	Access into the Town is problematic certainly on peak days, but also in evenings when there are events on in the Town and Central Seafront. There is a view that from the Victoria Gateway junction to the Raleigh Weir on days of high visitation and sunny days the key route is completely grid locked between these two key points. This represents somewhere in the region of 3840 cars parked nose to tail across the main artery into the Town which is mainly a dual carriageway.	Accessibility improvements are on-going as part of the Local Transport Plan and other regeneration initiatives. No changes are proposed.



<p>Question 17; Policy DS5</p> <p style="text-align: center;">311</p>	<p>Stockvale Group representing Sands &amp; Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]</p>	<p><b>2092</b></p>	<p><b>Comment</b></p>	<p>Some members of the STOCKVALE GROUP and representatives of the Seafront businesses believe that one way in which the congestion into the Town could be improved is for an additional 3840 parking spaces to be made accessible and available within close proximity to the Seafront and core High Street Area. This is due to the day visitor attraction industry, particularly family attractions such as the Seafront receiving the vast majority of its income in a few weeks of the year. These generally coincide with the school holidays. During this peak period a visitor attraction business needs to be able to accommodate every visitor that wants to visit as these peak days effectively subsidise the operation for the rest of the year.</p> <p>If the access to the main attractions is limited on peak days by the availability of car parking spaces, this could and does have serious impact on the viability of the Seafront businesses. The main parking areas are generally at capacity on peak holiday periods. Any loss of capacity as a result of the SCAAP proposals would result in a cap of visitors during these peak periods. This limits the amount of investment within the Seafront to the current status quo.</p> <p>Transport and access is not just limited to the Seafront and does have a huge impact on the High Street, combined with parking tariffs, access and egress, and poor legibility around the Town Centre. Whilst the changes outlined in the SCAAP from a space and use perspective will do an awful lot to reinvigorate and regenerate the High Street, this must be inclusive of a renewed and fresh approach to parking provision within the SCAAP Area.</p>	<p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
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<p>Question 17; Policy DS5</p>	<p>Stockvale Group representing Sands &amp; Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]</p>	<p><b>2096</b></p>	<p><b>Comment</b></p>	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there have been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The STOCKVALE GROUP would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	<p>Noted. The Plan seeks to achieve these aspects through its policy provisions. No changes are proposed.</p>
<p>Question 17; Policy DS5</p> <p>312</p>	<p>Stockvale Group representing Sands &amp; Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]</p>	<p><b>2104</b></p>	<p><b>Comment</b></p>	<p>Before the SCAAP document is enshrined, the STOCKVALE GROUP would ask that the Council carry out thorough research, analysis and investigation into the capabilities of the existing infrastructure and the capabilities of utility suppliers to meet the aspirational growth. This is essential and will need some degree of consideration in terms of new sub stations around the SCAAP area. This directly links to townscape and Public Realm improvements as these sub stations represent an opportunity to not create a negative space in the overall townscape. Many of the Members of the STOCKVALE GROUP are continuously seeking to improve their offer and find that the limitation of the existing utilities coming into the SCAAP area prohibit their future plans and proposals. This has not been at all addressed in the SCAAP document.</p>	<p>Infrastructure provision is addressed in the Plan. Such provisions have been subject to consultation with utility companies as part of the Plan preparation process. No changes are proposed.</p> <p>Further consultation with the National Grid will reveal whether further capacity is required to support the additional development in the Central Area. There was no objection from the National Grid to the housing and job targets in the Core Strategy.</p>

Question 17; Policy DS5	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2110</b>	<b>Comment</b>	There is the further opportunity to reopen the deepening alleviating some of the traffic stress that has resulted of the Highway alterations.	Noted. The Deeping was closed a number of years ago as part of transport improvements to the area. Its future use will be kept under review as part of on-going transport monitoring.
Question 17; Policy DS5  313	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2126</b>	<b>Comment</b>	There are a couple of issues that the STOCKVALE GROUP want to ensure are adequately addressed through the SCAAP. The first of those is the potential of having residents parking zones, this could have a negative effect on the existing Town Centre on and off street parking and consume spaces that are vital for visitors.	Noted. Such aspects will be kept under review as part of the on-going transport monitoring of the area.

Question 17	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2139	<b>Object</b>	Major concerns relating to all supported aspirations being achieved falls into a number of categories - transport, access and parking is a key theme and at present the existing parking provision is woefully inadequate. The access route into the Town is often unable to cater for the number of visitors on sunny days and this is likely to be detrimental to economic sustainability and the projected growth of 6,000 jobs within the SCAAP Area.	<p>Policy DS5 and related Policy Areas make a number of proposals for the improvement of transport and accessibility in the central area. These policies will sit alongside the Local Transport Plan which aims to address issues of congestion, circulation and accessibility to Southend to assist economic growth. No changes are proposed.</p> <p>The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
Question 17  314	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2140	<b>Object</b>	The identified opportunity development sites are mainly existing public car parks. Through the SCAAP the Local Authority should seek a minimum of a replacement like for like number of public spaces on each of the sites whilst also meeting the development requirements in accordance with the Council's Development Management Policy.	<p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p> <p>The Development Management Document establishes maximum parking standards for commercial development and appropriate standards for residential development in the Central Area. The amount of parking provided for a development scheme will be assessed against these policy standards, together with a consideration of the sites local context, location and distance from public transport links.</p>

Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2142</b>	<b>Comment</b>	<p>Transport and Access into the Town is a key theme and in order to deliver the aspired number of new dwellings and new jobs in the Central Area the Stockvale Group wish to see this appropriately addressed through the SCAAP documentation. At present Stockvale Group does not believe that the Transport, Access and Parking Issues have been given enough consideration. Nor the highway infrastructure on existing businesses let alone the aspirational growth.</p>	<p>Policy DS5 together with the Policy Area policies provide for a number of transport and highway improvements within the Central Area to improve accessibility and provide for more sustainable methods of transport. These policies will sit alongside the Local Transport Plan which aims to address issues of congestion, circulation and accessibility to Southend to assist economic growth.</p> <p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2143</b>	<b>Support</b>	<p>Wholly support townscape improvements, improvements to the public realm, vastly improved connectivity from car parks to the Seafront, car parks to the high street and the creation of active public spaces in an otherwise linear High Street.</p>	<p>Noted.</p>
Question 17; Policy DS5  315	Southend Bid (Mr S Kearney) [496]	<b>2152</b>	<b>Comment</b>	<p>As part of this representation we have reviewed Blackpool's Local Plan Making as a similar seaside town and comparable seaside resort. As part of the Blackpool Core Strategy Consultation and examination in public, the seafront businesses made succinct clear representation regarding the impact of traffic and parking on the sustainability and future growth of Blackpool as a tourist resort and destination of choice.</p> <p>The [Blackpool] Seafront business representation noted that the major attractions that make Blackpool a tourist destination rely on easy access to car parking and good access from car parks to the attractions by foot and public transport.</p> <p>The Seafront businesses further noted that this matter is often not well understood by councils, who generally consider that it is not necessary to plan car parking for peak periods only. In most industries, for example planning the levels of parking for shopping areas based only on the Christmas peak, this a reasonable approach but for the businesses which are seasonal and need to meet visitor targets to survive (or at least to continue at the present scale), this approach can have far reaching consequences.</p>	<p>Noted.</p>

Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2153</b>	<b>Object</b>	The Southend Seafront businesses feel this particular issue [related to peak periods for car parking] is not understood by the Local Authority and as such the level of tourism and investment has peaked. Many of the Seafront businesses have expressed their view as part of this consultation that they cannot invest further in the town due to the issue of access and parking and as such they already have a declining customer base.	Policy DS5 and related Policy Areas include a number of proposals for improving transport accessibility. These policies will sit alongside the Local Transport Plan which aims to address issues of congestion, circulation and accessibility to Southend to assist economic growth.  The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2154</b>	<b>Comment</b>	Members of the Stockvale Group together with representatives of local businesses within the SCAAP area discussed highways and parking issues with Southend on Sea Borough Council's Head of Planning and Transport. As the Chief Officer responsible for transport he was recorded as saying 'the issue with parking is if you create more parking spaces, more people will come and they will create congestion i.e. there will be greater numbers of visitors to the Town meaning greater business! This exasperates the concerns of local businesses that parking and transport issues are not fully understood and have no serious consideration as part of the Council's preferred option and SCAAP Framework.	Noted, no agreed minutes are recorded of this meeting. The SCAAP is planning for growth and inward investment and seeks to attract greater visitor numbers.
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2155</b>	<b>Comment</b>	Interestingly the representations made by the Seafront businesses in Blackpool persuaded the Government Inspector of their position. The Inspector concluded in their report that 'Car Parks need to accommodate peak weekend/bank holiday parking'.	Noted. This quotation could not be cited within the Blackpool Inspectors Report. The Council has nevertheless noted the modifications made by the Inspector.
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2178</b>	<b>Object</b>	RICS paper 'High Streets Adapting for Change' discusses parking changes and out of town retail <b>which</b> provides free parking. <b>In contrast to this</b> Since 2007 many local authorities have increased parking charges significantly. In the SCAAP area this is a key issue which requires essential review. This is in contrast to the smaller districts of wider Southend on Sea, Leigh and Southchurch where the Council have extended free parking to 2 hours. Compared to Central Southend and the SCAAP area where parking for 2 hours is in excess of £3.30. In Stockvale Group's view this is a deterrent for people coming into Southend particularly for shopping. This combined with the poor spatial and environmental quality is a contributing factor to the decline of Southend's Town Centre.	The SCAAP recognises the importance of car parking provision to the vitality and viability of the centre. The SCAAP is a planning policy document and does not directly cover parking charges. The wider implications of car parking charges are a matter which will be kept under review by the Borough Council at part of its overall approach to car parking for the Borough. No changes are proposed.

Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2188</b>	<b>Support</b>	Wholeheartedly support the encouragement and expansion of businesses in the Southend Central Area, although note that the issues around transport, access and parking need further consideration and understanding.	Noted.
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2189</b>	<b>Object</b>	Transport, access and parking issues need further consideration and are a particular issue for the Seafront businesses and the tourist economy. The high way infrastructure makes journeys into the town prolonged and difficult. Many visitors and customers simply don't return.	<p>Policy DS5 and related Policy Areas include a number of proposals for improving transport accessibility. These policies will sit alongside the Local Transport Plan which aims to address issues of congestion, circulation and accessibility to Southend to assist economic growth.</p> <p>The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2193</b>	<b>Comment</b>	Southend's Unique Selling Point is the Seafront which is a destination of choice. The Seafront and High Street inter relate on each other for business with the major attractions of Southend relying on easy access to car parking and good access from the car parks to the High Street and the Seafront attractions by foot or public transport.	Noted. The SCAAP seeks to improve and enhance connectivity between the seafront and town centre. No changes are proposed.
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2194</b>	<b>Comment</b>	Access into the Town is problematic certainly on peak days, but also in evenings when there are events on in the Town and Central Seafront. There is a view that from the Victoria Gateway junction to the Raleigh Weir on days of high visitation and sunny days the key route is completely grid locked between these two key points. This represents somewhere in the region of 3840 cars parked nose to tail across the main artery into the Town which is mainly a dual carriageway.	Accessibility improvements are on-going as part of the implementation of the Local Transport Plan and other regeneration initiatives. No changes are proposed.

<p>Question 17; Policy DS5</p> <p style="text-align: center;">318</p>	<p>Southend Bid (Mr S Kearney) [496]</p>	<p><b>2195</b></p>	<p><b>Comment</b></p>	<p>Some members of the BID and representatives of the Seafront businesses believe that one way in which the congestion into the Town could be improved is for an additional 3840 parking spaces to be made accessible and available within close proximity to the Seafront and core High Street Area. This is due to the day visitor attraction industry, particularly family attractions such as the Seafront receiving the vast majority of its income in a few weeks of the year. These generally coincide with the school holidays. During this peak period a visitor attraction business needs to be able to accommodate every visitor that wants to visit as these peak days effectively subsidise the operation for the rest of the year. If the access to the main attractions is limited on peak days by the availability of car parking spaces, this could and does have serious impact on the viability of the Seafront businesses. The main parking areas are generally at capacity on peak holiday periods. Any loss of capacity as a result of the SCAAP proposals would result in a cap of visitors during these peak periods. This the amount of investment within the Seafront to the current status quo. Transport and access is not just limited to the Seafront and does have a huge impact on the High Street, combined with parking tariffs, access and egress, and poor legibility around the Town Centre. Whilst the changes outlined in the SCAAP from a space and use perspective will do an awful lot to reinvigorate and regenerate the High Street, this must be inclusive of a renewed and fresh approach to parking provision within the SCAAP Area.</p>	<p>The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
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Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2199</b>	<b>Comment</b>	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there has been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The BID would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions.
Question 17; Policy DS5  319	Southend Bid (Mr S Kearney) [496]	<b>2207</b>	<b>Comment</b>	<p>Before the SCAAP document is enshrined, the BID would ask that the Council carry out thorough research, analysis and investigation into the capabilities of the existing infrastructure and the capabilities of utility suppliers to meet the aspirational growth. This is essential and will need some degree of consideration in terms of new sub stations around the SCAAP area. This directly links to townscape and Public Realm improvements as these sub stations represent an opportunity to not create a negative space in the overall townscape. Many of the Members of the BID are continuously seeking to improve their offer and find that the limitation of the existing utilities coming into the SCAAP area prohibit their future plans and proposals. This has not been at all addressed in the SCAAP document.</p>	Infrastructure provision, particularly flood risk management, which has been a major issue in the central seafront area, is addressed in the Plan. Such provisions have been subject to consultation with utility companies as part of the Plan preparation process. No changes are proposed.
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2213</b>	<b>Comment</b>	<p>There is the further opportunity to reopen the deepening alleviating some of the traffic stress that has resulted of the Highway alterations</p>	Noted. The Deeping was closed some years ago as part of transport improvements to the area. Its future use will be kept under review as part of on-going transport monitoring.
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2229</b>	<b>Comment</b>	<p>There are a couple of issues that the BID want to ensure are adequately addressed through the SCAAP. The first of those is the potential of having residents parking zones, this could have a negative effect on the existing Town Centre on and off street parking and consume spaces that are vital for visitors.</p>	Noted. Such aspects will be kept under review as part of the on-going transport monitoring of the area.

Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2245</b>	<b>Comment</b>	Transport and Access and Public Realm also received 70% of respondents giving this a score of 10 and a top priority.	Noted.
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2268</b>	<b>Object</b>	Major concerns relating to all supported aspirations being achieved falls into a number of categories, transport, access and parking is a key theme and at present the existing parking provision is woefully inadequate. The access route into the Town is often unable to cater for the number of visitors on sunny days and this is likely to be detrimental to economic sustainability and the projected growth of 6,000 jobs within the SCAAP Area.	<p>Policy PA5 and related Policy Areas make a number of proposals for the improvement of transport and accessibility in the central area. No changes are proposed.</p> <p>The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
Question 17; Policy DS5	Southend Bid (Mr S Kearney) [496]	<b>2269</b>	<b>Object</b>	The identified opportunity development sites are mainly existing public car parks. Through the SCAAP the Local Authority should seek a minimum of a replacement like for like number of public spaces on each of the sites whilst also meeting the development requirements in accordance with the Council's Development Management Policy.	<p>The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p> <p>The Development Management Document establishes maximum parking standards for commercial development and appropriate standards for residential development in the Central Area. The amount of parking provided for a development scheme will be assessed against these policy standards, together with a consideration of the sites local context, location and distance from public transport links.</p>
Question 17; Policy DS5	Mr Rod Levin[497]	<b>2271</b>	<b>Comment</b>	Provision of pavement seating throughout the borough	The Plan seeks to improve existing and provide new public spaces within Southend Central Area. Seating provision will be considered on a scheme by scheme basis in line with the Streetscape Manual Supplementary Planning Document. No changes are proposed.
Question 17; Policy DS5	Mr Rod Levin [497]	<b>2276</b>	<b>Comment</b>	Upgrade the Street lighting from the current dismal effect	A programme of street lighting improvements is being implemented as part of the Local Transport Plan provisions. No changes are proposed.
Question 17; Policy DS5	Mr Rod Levin [497]	<b>2279</b>	<b>Comment</b>	Reduce / eliminate all car-parking charges on Sundays and Bank holidays and, hospital car parks completely	The SCAAP recognises the importance of car parking provision to the vitality and viability of the centre. The wider implications of car parking charges are a matter which will be kept under review by the Borough Council as part of its overall car parking strategy for the Borough. No changes are proposed.

Question 17; Policy DS5	Mr Rod Levin [497]	<b>2280</b>	<b>Comment</b>	Ensure all new houses are provided with car parking room for at least two cars	Residential car parking is set out in the Council's adopted car parking standards in the Development Management Document. No changes are proposed.
Question 17; Policy DS5	Mr Rod Levin [497]	<b>2284</b>	<b>Comment</b>	Plan to improve Road access to Southend (Additional to A127) by 2020	A number of road improvements to the strategic highway network have been completed in recent years and further improvements are proposed as part of the Local Transport Plan provisions. No changes are proposed.
Question 17; Policy DS5	Mr Rod Levin [497]	<b>2285</b>	<b>Comment</b>	Plan week-end Park and ride scheme for visitors by road to leave their cars Out-of-Town	Park and Ride schemes have been considered a number of times in recent years but have not been considered feasible given the limited land available and linear peninsula geography of the town. The provision of Park and Ride would only be feasible outside the SCAAP boundaries. Such options will be kept under review as part of the on-going Local Transport Plan provisions and development of the Southend Local Plan. No changes are proposed.
Question 17; Policy DS5	Mr Rod Levin [497]	<b>2286</b>	<b>Comment</b>	As in towns abroad, make commercial deliveries to be during Night hours only - eg: Monaco	Commercial delivery times are kept under review as part of on-going traffic management proposals. The SCAAP Transport, Access and Public Realm Strategy and Policy DS5 seeks to ensure the efficient and effective servicing and delivery arrangements. No changes are proposed.
Question 17; Policy DS5	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	<b>2306</b>	<b>Comment</b>	Policy DS5 states that the Council will work with the freight industry and logistics to implement more efficient use of vehicles in terms of guidance, zoning and delivery timetables and suggests that this can be set out in freight management plans. Valad (Europe) Ltd suggest that the requirement for freight management plans is not set out in policy but dealt with on a case by case basis.	Policy purely sets out intent to provide for a freight management plan in the interests of efficient traffic management. No changes are proposed.

<p>Question 17; Policy DS5</p>	<p>Belfairs Garden Residents Association (Barbara Armitage) [511]</p>	<p><b>2343</b></p>	<p><b>Object</b></p>	<p>Vulnerable groups' need transport and the statement that car travel is to be discouraged is discriminatory against those of us who cannot get on public transport and need cars <i>for</i> accessibility to all areas of the town and the profoundly disabled who use nothing else, not only motability cars but blue badge users and those taken by friends or taxis. <b>Also need to be</b> relief areas for guide and assistance dogs. The Southend Local Transport Plan 3 to 2026 notes as a Key Fact p87 the expected rise in population over 65 and that all public transport should be accessible by 2017 which does not appear likely. It also notes the lack of buses along the seafront.</p>	<p>The SCAAP does not seek to discourage car travel, rather it seeks to encourage and promote better public transport. The Local Transport Plan seeks to promote public transport for all, including concessionary fares for those of retirement age.</p> <p>Site occupiers with reference to national parking guidance and legislation are responsible for providing an adequate number of spaces for people with disabilities. The SCAAP seeks to promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre and access to the seafront by encouraging improvements to the quality of access to parking so that it is convenient, well-signposted, safe and secure. It is considered that reference to a range of parking types, including for disabled people, should be made within Policy DS5.</p>
<p>Question 17; Policy DS5</p> <p>322</p>	<p>Belfairs Garden Residents Association (Barbara Armitage) [511]</p>	<p><b>2344</b></p>	<p><b>Object</b></p>	<p>Reducing car parking space, particularly in the streets, is nonsense. Some of the spaces calculated have been within shopping malls such as the Royals. Local businesses need short term spaces for their customers including care agencies, accountants, lawyers etc. if clients do not find somewhere to park nearby, e.g Clarence Rd. area, the business will relocate somewhere else. This could have an effect upon employment considered elsewhere in the plan and under threat. Disabled people need nearby spaces. So do people with shopping and mums with children. Multi storey car parks are not good for those with walking difficulty or indeed women on their own for safety in darkness.</p> <p>Going to park in a multi storey or driving around for a space just to have lunch in a cafe in The High Street opposite Marks and Spencers does not make sense for boosting the town economy at all.</p> <p>The car park next to SAVS building is vital for users of that building and the meetings and workshops there. It also gives access to the Royals complex without having to drive around to the Royals car park where there is often long waiting to get in especially on Saturdays and when it is raining. Likewise the Clarence Road car park is vital for the residents and business people around there and of course the Baptist Church and Salvation Army.</p>	<p>The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p> <p>Policy seeks to better manage demand on the road network and balance this with the needs of other modes, particularly where this would give greater reliability to road users and priority to pedestrians, cyclists, public transport users and other vulnerable road users.</p> <p>The SCAAP seeks to promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre and access to the seafront by encouraging improvements to the quality of access to parking so that it is convenient, well-signposted, safe and secure. It is considered that reference to a range of parking types, including for disabled people, should be made within Policy DS5.</p>

Question 17; Policy DS5	Belfairs Garden Residents Association (Barbara Armitage) [511]	<b>2345</b>	<b>Comment</b>	The travel centre and management of access to buses is hopeless. Real consultation on the location of the travel centre and safe places for people to wait and queue for buses is overdue. Good and accessible public toilets should be incorporated here and elsewhere in the central area.	Policy PA7 identifies the potential to relocate the bus station to provide for improved facilities. The detailed design of a scheme will be considered at planning application stage and will be subject to consultation. No changes proposed.
Question 17 DS5	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2366</b>	<b>Comment</b>	Would LGO's please stop using the term 'public transport' We only have private companies operating trains and buses.	'Public transport' is a term generally used to refer to transport services provided directly to the public. No change proposed.
Question 17 DS5  323	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2367</b>	<b>Support</b>	Yes, but we need 24/7 concessionary fares for old aged pensioners	Concessionary fares are a matter outside of planning influence. No changes proposed.
Question 17; Policy DS5	Procuresure Consulting (Mr Barrie Evans) [513]	<b>2375</b>	<b>Comment</b>	Southend-On-Sea needs more innovative transport solutions. Other cities such as Manchester etc have reverted back to the tram system and park and ride schemes. There is no connectivity from rail other than car or walking. To resurrect a tram system would not only provide an efficient form of transport but enhance the sea side feel that Southend is missing. Parking is also an issue and any new development should provide a self-sufficient parking solution and stop commercial and retail parking in residential areas	The Plan seeks to enhance and improve public transport within the central area. Innovative transport solutions have been investigated as part of the Local Transport Plan. All development schemes are assessed against adopted car parking standards. No changes proposed.
Question 17; Policy DS5	Procuresure Consulting (Mr Barrie Evans) [513]	<b>2377</b>	<b>Comment</b>	Southend-On-Sea central area is poorly lit and pedestrian routes for commuters from Southend Central station are seen as unsafe. Most commuters will travel in the hours of darkness whether it be morning or night and to encourage walking around the central areas better lighting is required.	Policy DS5 seeks to ensure the provision of appropriate street lighting. Reference will be included for improved lighting in Policy PA1.

Question 17; Policy DS5	Procurement Consulting (Mr Barrie Evans) [513]	2379	Comment	The central area is highly residential and traffic speed is too high; especially in the residential areas of Clifftown. Traffic calming options should be installed to reduce traffic speed in these areas.	Policy DS5 seeks to improve traffic management within the central area. However, it is recognised that the Policy makes no reference to the potential to improve the road safety and environment of the pockets of predominantly residential areas within the central area. It is therefore proposed that the following criteria is added to Policy DS5, <b><u>'Improve road safety and the quality of the environment by introducing traffic calming and related measures within predominantly residential areas as appropriate.'</u></b> Policy PA6.5.b seeks a reduction in general vehicle circulation in residential street.
Question 17; Policy DS5	Procurement Consulting (Mr Barrie Evans) [513]	2388	Comment	1 parking space per flat is totally unrealistic in this age	The Council's parking standards are set out in the Development Management Document and these have been found sound by a planning inspector and subsequently adopted.
Question 17; Policy DS5  324	National Federation for the Blind (Mrs Jill Allen-King) [516]	2433	Comment	All pedestrian areas should be kept free of obstacles, and no cycling should be allowed in these areas. All walking areas should be well lit, and where there are seats they should be so positioned that they do not cause a hazard.	Policy seeks to give priority to pedestrians, cyclists, public transport and other vulnerable users. It also seeks to maintain street lighting. Reference will be included to Policy DS5 to ensure that public realm improvements consider the needs of more vulnerable users as follows: <b><u>'In order to promote and reinforce local distinctiveness, ensure all public realm improvement works, including those outlined in the relevant Policy Areas, should seek to provide a coordinated palette of materials, facilitate a reduction in street clutter, consider the needs of all users including vulnerable and disabled users, the provision of additional seating where appropriate to provide resting places, and have regard to guidance within the Design and Townscape Guide and Streetscape Manual.'</u></b>
Question 17; Policy DS5	National Federation for the Blind (Mrs Jill Allen-King) [516]	2434	Comment	Parking should be provided for disabled drivers close to shops. There is no mention of parking for disabled people in the document.	Site occupiers with reference to national parking guidance and legislation are responsible for providing an adequate number of spaces for people with disabilities The SCAAP seeks to promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre and access to the seafront by encouraging improvements to the quality of access to parking so that it is convenient, well-signposted, safe and secure. It is considered that reference to a range of parking types, including for disabled people, should be made within Policy DS5.

Question 17; Policy DS5	National Federation for the Blind (Mrs Jill Allen-King) [516]	2435	Comment	There should be bus routes to cover all parts of the town these should be reliable, frequent, accessible and available 7 days a week and at Bank holidays. Currently there are no bus routes from Chalkwell to the Kursaal.	Policy DS5, as part of a sustainable approach to transport, seeks to improve provisions for public transport users and for bus priority measures. Specific bus routes are considered as part of on-going partnership working with bus operators. No changes are proposed.
<b>Infrastructure Provision</b>					
Question 18	Essex Chambers of Commerce (Mr John Dallaway) [452]	1947	Support	Agree with the proposed approach to providing infrastructure in Southend Central Area	Noted.
Question 18	Anglian Water (Sue Ball) [37]	2015	Comment	Treatment capacity at Southend Water Recycling Centre is available to serve the proposed level of growth in the plan.(Infrastructure Provision 4.12 paragraph 139)	Noted
Question 18 325	Anglian Water (Sue Ball) [37]	2016	Comment	<b>There may be a</b> need for upgrades to the foul sewerage network to accommodate the used water flows from the proposed development. (Infrastructure Provision 4.12 paragraph 140) This will be assessed for each site when we are approached via our pre planning service and a solution identified. Details can be found at: <a href="http://www.anglianwater.co.uk/developers/pre-planning-service-.aspx">http://www.anglianwater.co.uk/developers/pre-planning-service-.aspx</a> . Developers should be encouraged to submit a pre planning enquiry at the earliest opportunity.	Noted
Question 18	Anglian Water (Sue Ball) [37]	2027	Comment	For Surface water disposal we would expect a SuDS solution to be utilised where at all viable and under no circumstances will surface water be permitted to discharge into the foul sewerage system. (Infrastructure Provision 4.12 paragraph 140)	Noted. It is proposed to add to Policy DS4 point 2 the following: <b><u>'...Under no circumstances will surface water be permitted to discharge into a separate foul sewer or sewerage system. Surface runoff that cannot be discharged into the ground, a surface water body or a surface water sewer or local highway drain, must be discharged to a public, combined sewer system.'</u></b>

Question 18	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2095	Support	<p>The STOCKVALE GROUP understand the concept of a much greater residential intensification of the SCAAP area and would wholeheartedly support the Councils aspirations for an additional 4000+ homes however, this must be in the context of insuring there is suitable amenity and infrastructure.</p> <p>The intensification together with a greater mix of uses in the Town Centre and Central Seafront create a much more buoyant and sustainable economy and the STOCKVALE GROUP welcome the Councils proposals as part of the SCAAP planning document.</p>	<p>Noted. It is proposed to amend paragraph 139 as follows: <b><u>'Water companies are subject to a statutory duty to 'effectually drain' their area. This requires them to invest in infrastructure suitable to meet the demands of projected population growth. Southend Waste Water Treatment Works has adequate capacity to accommodate the Core Strategy growth targets to 2021 and beyond. However, developers will need to consider the effect of their development on the capacity of the local waste water network. Proposals will need to demonstrate that they will not overload this.'</u></b></p> <p>It is also proposed to insert a new paragraph under 139:</p> <p><b><u>'There is statutory provision for developers to fund additional sewerage infrastructure required to accommodate flows from a proposed development. Adequate sewerage infrastructure should be in place to serve the area before development progresses. Developers should seek pre-planning advice from Anglian Water at the earliest opportunity to ensure appropriate provision is made. Further details and useful guidance can be found on Anglian Water's website.'</u></b></p>
Question 18 226	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2105	Comment	<p>Before the SCAAP document is enshrined, the STOCKVALE GROUP would ask that the Council carry out thorough research, analysis and investigation into the capabilities of the existing infrastructure and the capabilities of utility suppliers to meet the aspirational growth. This is essential and will need some degree of consideration in terms of new sub stations around the SCAAP area. This directly links to townscape and Public Realm improvements as these sub stations represent an opportunity to not create a negative space in the overall townscape. Many of the Members of the STOCKVALE GROUP are continuously seeking to improve their offer and find that the limitation of the existing utilities coming into the SCAAP area prohibit their future plans and proposals. This has not been at all addressed in the SCAAP document.</p>	<p>Infrastructure provision is addressed in the Plan. Such provisions have been subject to consultation with utility companies as part of Plan preparation process. No changes are proposed.</p> <p>Further consultation with the National Grid will reveal whether further capacity is required to support the additional development in the central area. There was no objection from the National Grid to the housing and job targets in the Core Strategy.</p>




Question 18	Southend Bid (Mr S Kearney) [496]	<b>2198</b>	<b>Support</b>	The BID understand the concept of a much greater residential intensification of the SCAAP area and would wholeheartedly support the Councils aspirations for an additional 4000+ homes however, this must be in the context of insuring there is suitable amenity and infrastructure. The intensification together with a greater mix of uses in the Town Centre and Central Seafront create a much more buoyant and sustainable economy and the BID welcome the Councils proposals as part of the SCAAP planning document.	Noted.
Question 18	Southend Bid (Mr S Kearney) [496]	<b>2208</b>	<b>Comment</b>	Before the SCAAP document is enshrined, the BID would ask that the Council carry out thorough research, analysis and investigation into the capabilities of the existing infrastructure and the capabilities of utility suppliers to meet the aspirational growth. This is essential and will need some degree of consideration in terms of new sub stations around the SCAAP area. This directly links to townscape and Public Realm improvements as these sub stations represent an opportunity to not create a negative space in the overall townscape. Many of the Members of the BID are continuously seeking to improve their offer and find that the limitation of the existing utilities coming into the SCAAP area prohibit their future plans and proposals. This has not been at all addressed in the SCAAP document.	Infrastructure provision, particularly flood risk management, which has been a major issue in the central seafront area, is addressed in the Plan. Such provisions have been subject to consultation with utility companies as part of the Plan preparation process. No changes are proposed.
Question 18	Southend Bid (Mr S Kearney) [496]	<b>2249</b>	<b>Comment</b>	Energy and Utilities 32% top priority.	Noted.
Question 18	Mr Alan Grubb [59]	<b>2294</b>	<b>Comment</b>	The nearest Health Centre is located in North Road Westcliff, will this health centre be able to handle the additional demand which would be created by the new developments, The old Ekco site, Roots Hall site, the old college site next to the Civic Centre, Heath House and Carby House.	The Plan recognises the potential need for additional community facilities, particularly in the Queensway, Victoria and Sutton Gateway policy areas (Policies PA4, PA8, PA9). No changes are proposed.
Question 18	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2368</b>	<b>Support</b>	Yes, any new school may decide to convert to an Academy	Noted.

Question 18	Procuresure Consulting (Mr Barrie Evans) [513]	2392	Comment	Southend-On-Sea Council should ensure that all new developments both business and residential have Fibre optic ultra high speed broadband infrastructure as standard. This will attract future business and technologies to the city. The Council should be the city to make Southend On Sea the first 100% fibre High speed broadband city in uk. This upgrade of communication across the city along with a wi-fi infrastructure as seen in cities across Romania (yes Romania) would make Southend extremely attractive to global business with high speed Broadband communications being a pinch point for companies across the UK both large and small.	The adopted Core Strategy (CP1) sets out provision for improving broadband infrastructure throughout the Borough.
Question 18	National Federation for the Blind (Mrs Jill Allen-King) [516]	2436	Comment	There is no mention of Public toilets or day centres for disabled people.	These are referred to in the Plan under the generic term 'community infrastructure'. No changes to Plan are proposed.

### Part C: Policy Areas and Opportunity Sites

#### Dwelling Capacity

Question 19	The Co-operative Group (Mr A Thompson) [473]	1971	Object	The Co-operative Group would wish to see the inclusion of land at 53-57 Sutton Road Southend within the SCAAP as an additional Opportunity Site.		The Council is considering inclusion of this site as an opportunity site. This will be assessed against information supplied by the owners of the site to demonstrate that development is deliverable by 2021.
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#### High Street Policy Area – Policy PA1; Opportunity Sites 1 and 2

Question 20 PA1	Anna Hyndnan Lahna [456]	1931	Comment	With regards to the invitation to comment on new plans for Southend High Street, I would like to propose that we introduce trees in an avenue style right down the centre of the pedestrianised area.	Policy PA1 seeks to provide for improved landscaping and 'urban greening' and tree planting in the High Street. No changes proposed.
Question 20 PA1	Anna Hyndnan Lahna [456]	1932	Comment	I think we need to bring the area more glamour. I think we need to curb the amount of pound and temporary shops. We need to encourage individual businesses along with higher class chains, Brown Brasseries for example.	Policy PA1 seeks to encourage development that would contribute to the vitality and viability of the town centre. No changes proposed.

Question 20 PA1	Anna Hyndnan Lahna [456]	1933	Comment	Southend has a reputation for being for being downtrodden and cheap but it needn't be, we could follow the lead of Brighton for example and encourage boutique style shops and bring up the standards.	Policy PA1 seeks to encourage development that would contribute to the vitality and viability of the town centre. No changes proposed.
Question 20 PA1	Essex Chambers of Commerce (Mr John Dallaway) [452]	1948	Support	Agree with the proposed approach to managing development within the High Street Policy Area	Noted.
Question 20 PA1	London Southend Airport (Ms Jo Marchetti) [471]	1967	Support	LSA would like to see improvements made to the top of the high street to entice passengers arriving from the airport via. Southend Victoria Station into the High Street area before making their way to the seafront.  Better signage is needed to encourage visitors to many of the bars and restaurants located in the side streets. Better signage should be considered from the Queensway area to the High Street via. Odeon/New Look alleyway.	Noted. The Plan makes provision for improved signage and way marking throughout the central area, however, it is not directly referred to in Policy PA1 (High Street) where quality signage is important. <b><i>It is therefore proposed that the following words be added to Policy PA1 3 d: <u>'through improved signage and public art provision'</u>.</i></b>
Question 20; PA1.3.c	Burges Estate Residents Association [176]	1997	Support	Southend has been the focus of working class seaside attractions for at least 80 years and continues to be so. It is the mainstay of many seafront businesses. Those day trippers often take advantage of the sea front and the town centre facilities so improving the connectivity between the two is crucial.	Noted.

<p>Question 20; PA1</p> <p style="text-align: center;">330</p>	<p>Anglian Water (Sue Ball) [37]</p>	<p><b>2017</b></p>	<p><b>Comment</b></p>	<p>We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.</p>	<p>The Council understands that water companies are subject to a statutory duty to ‘effectually drain’ their area. This requires them to invest in infrastructure suitable to meet the demands of projected population growth. There is also statutory provision for developers to fund additional sewerage infrastructure required to fund additional sewerage from a proposed development. In relation to this Ofwat provides information for developers where a development would require a new water main or sewer. It is considered, therefore, that there is an obligation on water companies to ensure that sewerage infrastructure is provided to a level to meet housing target in an adopted plan, unless it is a circumstance where a development would be required to provide additional capacity.</p> <p>Specifically, for foul drainage, Section 42 of the Flood and Water Management Act requires developers who want to connect to a public sewer to enter into a binding agreement for the adoption of new connecting sewers by the undertaker (under section 104 of the Water Industry Act 1991). The agreement must specify that new sewers will be built to a standard published by the Minister, or any other such standard as may be agreed. (Review above)</p> <p>Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.</p> <p>Noted. Additional supporting text is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.</p>
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Question 20	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2058	Comment	The Government has also introduced measures to make it easier to change use into residential however this is probably fairly restrictive in the High Street itself but Southend as the Local Planning Authority should consider the widening of that, certainly into some of the units off the High.	Policy DS1 and related Policy Area provisions actively promotes residential use above commercial premises and within proposed mixed use developments. No changes are proposed.
Question 20 331	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2063	Comment	High Streets Adapting for Change (RICS) introduces the Governments recognition that our High Streets have to offer something new and different that neither the shopping centres nor the internet can match. They need to offer an experience that goes beyond retail and they need to be a destination for the socialising culture, health, well being, creativity and learning. Offices alongside shops, alongside housing, alongside eateries.	Noted. The Plan seeks to achieve this.

Question 20	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2072	Support	Support the Council in their aspiration to (i) improve Competitive Performance (ii) Reduce the cost base (iii) Diversify away from retail uses (iv) grow the local economy and/or population, which is a strong theme in the preferred option principle. Recognise that the High Street should be a social place that makes creative use of public space with a vibrant evening economy.	Noted.
Question 20  332	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2078	Comment	Many of our High Streets have been shaped by their past; however they are now trapped in their current configurations and often in poor shape to face the future. In relation to Southend on Sea, this is certainly the case. The High Street in particular has a linearity with no social space for congregation, interaction and the alternative commercial uses that would reactivate these spaces such as cafes, coffee shops, office space and importantly a high intensification of residential uses both at ground level and above The SCAAP and the Stockvale Group recognise that the High Street in particular requires a restructuring on a significant scale.	The SCAAP recognises the need to enhance and broaden the offer in the High Street and seeks to do this by providing a more flexible approach in the determination of planning applications to encourage a mix of retail, cafe and restaurant uses. The Plan also seeks to enhance and promote new public spaces within the centre. No changes are proposed.

Question 20	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2088	Support	Support a higher intensification of residential uses in the Central Area together with a broader mix of commercial uses ranging from A1 retail, A3 restaurant café, D2 leisure and B1 offices to provide a more diverse multi-use High Street and Central Seafront.	Noted.
Question 20  333	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2097	Comment	Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there have been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront. The STOCKVALE GROUP would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.	Noted. The Plan seeks to achieve these aspects through its policy provisions. No changes are proposed.

Question 20	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2111	Comment	There is a great opportunity to display public art in terms of film projection on to the rear of the Victoria Plaza and the existing Odeon building. This further runs in to the top end of the High Street where there is a greater opportunity to enhance Victoria Circus.	<p>Noted. Additional wording is proposed to emphasise the use of visually active frontages within Policy PA2.2 as follows: <b><u>'Encourage visually active frontages, through public art, green walls, architectural fenestration to buildings on Queensway dual carriage-way'</u></b></p> <p>Include an additional criteria to Policy PA1 to encourage visually active frontage within PA1 to the rear of buildings on Queensway dual carriage way to read as follows: <b><u>'Encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way'</u></b></p>
Question 20  334	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2113	Comment	In relation to the middle of the High Street from Pitmans Close, Whitegate Road through to Tylers Avenue, the STOCKVALE GROUP support the extension of the education and cultural quarter into this area and would further suggest that the SCAAP looks at office use within the High Street itself and some residential uses above these offices. This would stimulate a broader economy and a safer pedestrian environment.	<p>Noted. The SCAAP identifies a number of opportunities for achieving residential/office development within this locality. No changes are proposed.</p>



Question 20	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2116	Comment	There could also be an improvement of pedestrian connection from the Central railway station of Southend on Sea into the Public Realm and a vast and extensive soft landscape scheme introduced around the Tylers Avenue/High Street connection to the railway station.	Noted. It is proposed that PA7 is updated as follows: ' <u>facilitate better pedestrian access to the High Street and Southend Central railway station</u> ' And the following amendment is proposed to PA6.3.b: 'streetscape and landscape design improvements, including urban greening and tree planting, to create well lit walking circuits through Clifftown from a newly created public plaza at Southend Central <b>Railway</b> Station/ Central House, to Cliff Gardens and Pier Hill, <u>facilitating better pedestrian access to the High Street</u> '
Question 20 335	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2119	Support	The STOCKVALE GROUP support the Public Realm improvements and further connectivity down through and into the Seafront. The STOCKVALE GROUP recognise that Pier Hill has had a huge success in this regard.	Noted.
Question 20	Southend Bid (Mr S Kearney) [496]	2160	Comment	The Government has also introduced measures to make it easier to change use into residential however this is probably fairly restrictive in the High Street itself but Southend as the Local Planning Authority should consider the widening of that, certainly into some of the units off the High Street.	Policy PA1 and related Policy Area provisions promotes residential use above commercial premises and within proposed mixed use developments where appropriate. No changes are proposed.

Question 20	Southend Bid (Mr S Kearney) [496]	<b>2165</b>	<b>Comment</b>	High Streets Adapting for Change (RICS) introduces the Governments recognition that our High Streets have to offer something new and different that neither the shopping centres nor the internet can match. They need to offer an experience that goes beyond retail and they need to be a destination for the socialising culture, health, well being, creativity and learning. Offices alongside shops, alongside housing, alongside eateries.	Noted. The Plan seeks to achieve this.
Question 20	Southend Bid (Mr S Kearney) [496]	<b>2174</b>	<b>Support</b>	Support the Council in their aspiration to (i) improve Competitive Performance (ii) Reduce the cost base (iii) Diversify away from retail uses (iv) grow the local economy and/or population, which is a strong theme in the preferred option principle. Recognises that the High Street should be a social place that makes creative use of public space with a vibrant evening economy	Noted.
Question 20	Southend Bid (Mr S Kearney) [496]	<b>2180</b>	<b>Comment</b>	Many of our High Streets have been shaped by their past, however they are now trapped in their current configurations and often in poor shape to face the future. In relation to Southend on Sea, this is certainly the case. The High Street in particular has a linearity with no social space for congregation, interaction and the alternative commercial uses that would reactivate these spaces such as cafes, coffee shops, office space and importantly a high intensification of residential uses both at ground level and above. The SCAAP and the Stockvale Group recognise that the High Street in particular requires a restructuring on a significant scale.	The SCAAP recognises the need to enhance and broaden the offer in the High Street and seeks to do this by encouraging a mix of retail, cafe and restaurant uses. The Plan also seeks to enhance and promote new public spaces within the centre. No changes are proposed.
Question 20	Southend Bid (Mr S Kearney) [496]	<b>2191</b>	<b>Support</b>	Support a higher intensification of residential uses in the Central Area together with a broader mix of commercial uses ranging from A1 retail, A3 restaurant café, D2 leisure and B1 offices to provide a more diverse multi-use High Street and Central Seafront.	Noted.

Question 20	Southend Bid (Mr S Kearney) [496]	2200	<b>Comment</b>	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there has been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The BID would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions.
Question 20 337	Southend Bid (Mr S Kearney) [496]	2214	<b>Comment</b>	<p>There is a great opportunity to display public art in terms of film projection on to the rear of the Victoria Plaza and the existing Odeon building. This further runs in to the top end of the High Street where there is a greater opportunity to enhance Victoria Circus.</p>	<p>Noted. Additional wording is proposed to emphasise the use of visually active frontages within Policy PA2.2 as follows: <b><u>'Encourage visually active frontages, through public art, green walls, fenestration to buildings on Queensway dual carriage way'</u></b></p> <p>It is proposed to include an additional criteria to Policy PA1 to encourage visually active frontages to the rear of buildings on Queensway dual-carriage way to read as follows: <b><u>'Encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way'</u></b></p>
Question 20	Southend Bid (Mr S Kearney) [496]	2216	<b>Comment</b>	<p>In relation to the middle of the High Street from Pitmans Close, Whitegate Road through to Tylers Avenue, the BID support the extension of the education and cultural quarter into this area and would further suggest that the SCAAP looks at office use within the High Street itself and some residential uses above these offices. This would stimulate a broader economy and a safer pedestrian environment.</p>	Noted. The SCAAP identifies a number of opportunities for achieving residential/office development within this locality. No changes are proposed.

Question 20	Southend Bid (Mr S Kearney) [496]	2219	Comment	There could also be an improvement of pedestrian connection from the Central railway station of Southend on Sea into the Public Realm and a vast and extensive soft landscape scheme introduced around the Tylers Avenue/High Street connection to the railway station.	<p>Noted. It is proposed that PA7 is updated as follows: <b><u>'facilitate better pedestrian access to the High Street and Southend Central railway station'</u></b></p> <p>And the following amendment is proposed to PA6.3.b: 'streetscape and landscape design improvements, including urban greening and tree planting, to create well lit walking circuits through Clifftown from a newly created public plaza at Southend Central <b>Railway</b> Station/ Central House, to Cliff Gardens and Pier Hill, <b>facilitating better pedestrian access to the High Street'</b></p> <p>And the following amendment to PA6.3.b: 'streetscape and landscape design improvements, including urban greening and tree planting, to create well lit walking circuits through Clifftown from a newly created public plaza at Southend Central <b>Railway</b> Station/ Central House, to Cliff Gardens and Pier Hill, <b>facilitating better pedestrian access to the High Street'</b></p>
Question 20	Southend Bid (Mr S Kearney) [496]	2222	Support	The BID support the Public Realm improvements and further connectivity down through and into the Seafront. The BID recognise that Pier Hill has had a huge success in this regard.	Noted.
Question 20	Southend Bid (Mr S Kearney) [496]	2251	Comment	84% of respondents scored the High Street as a top 10 priority	Noted.
Question 20; PA1	Mr Alan Grubb [59]	2298	Comment	Walking up the High Street I cannot help but notice the area where Elmer Approach joins the High Street there does not appear to be any signs directing the pedestrian to the new library (Forum).	The provision of signage in the town centre is reviewed as appropriate. The Plan makes provision for improved signage and way marking throughout the central area, however , it is not directly referred to in Policy PA1 (High Street) where quality signage is important. <b><i>It is therefore proposed that the following words be added to Policy PA1 3 d 'through improved signage and public art provision'.</i></b>

Question 20; PA1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2307	Comment	Policy PA1 seeks to enhance the High Street experience through a number of improvements such as improving, enhancing and creating new public spaces, improved landscaping and interlinking access roads. Our client requests that specific mention is made to the Council's aspiration to open up the southern façade of The Royals Shopping Centre through the provision of a new restaurant(s) and outdoor public space etc to create a link between the High Street and the Seafront area. This, together with improved signage and access would help to link the seafront with the town centre and High Street benefitting the town centre as a whole.	The Plan seeks to achieve this as set out in Policies PA 1 3c and Policy CS1 10b. It is proposed that an additional criteria is inserted into Policy PA1.2 outlining the Council's support for proposals that create active frontage on the southern façade of The Royals Shopping Centre as follows: ' <u>...the following, will be supported in principle... The provision of active frontage on the southern façade of The Royals Shopping Centre</u> '
Question 20; PA1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2308	Object	Opportunity sites 1 and 2 (Whitegate Road and Pitmans Close) have been identified as being suitable for mixed use office/residential, commercial uses, with the timescale for delivery being post 2021. The delivery timescales within which the development should be delivered should be brought forward with the aim of delivering it pre-2021 on the basis that it will bring further investment to the town centre sooner.	There is insufficient evidence that these sites will be delivered by 2021, the end of the SCAAP's plan period. They will however, be considered during preparation of the Southend Local Plan. No changes are proposed.
Question 20, PA1	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	2369	Comment	Yes, more public toilets and any "steps" must be complimented by ramps for disabled people	Noted. This would be considered during the detailed design stage of any scheme.

Question 20; PA1	Historic England (Dr Natalie Gates) [514]	2406	Comment	<p>Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply.</p> <p>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/</a>. <b><i>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</i></b></p>	<p>This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>‘It should be noted that listed buildings, buildings in Conservation Areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.’</i></b></p> <p>It is proposed to add the words ‘as appropriate’ in paragraph 4 of Policy PA6 so that it reads, <b><i>‘Promote energy efficiency as appropriate, including.....’</i></b></p> <p>This would necessitate similar changes to all other policy areas.</p>
Question 20 40	National Federation for the Blind (Mrs Jill Allen-King) [516]	2438	Comment	The High street should be kept at one level.	The High Street is predominantly at one level and elevators/lifts are provided at the multi-level Victoria Shopping Centre. A public lift was also constructed as part of wider regeneration proposals at the southern end of the High Street to improve accessibility between the differing levels of the High Street and the seafront.
<b>London Road Policy Area – Policy PA2</b>					
Question 21, PA2	Essex Chambers of Commerce (Mr John Dallaway) [452]	1949	Support	Agree with the proposed approach to managing development within the London Road Policy Area	Noted.

Question 21; PA2	Burges Estate Residents Association [176]	2003	Comment	I have no problem with London Road policy area except in one regard, the proposal for an active frontage along the dual carriageway. It is inconceivable that one would wish to encourage footfall along a busy dual carriageway. Far better to improve and emphasise London road as the focus for pedestrian traffic with the enhanced pedestrianisation. By all means improve the appearance but to create active frontages is wrong.	Noted, OS15 Sainsburys & Adjacent Building Site will not be in the final version of the SCAAP as it is unlikely to be implemented by 2021. It is proposed that Policy PA2 will be amended to encourage visually active frontage on Queensway dual-carriage way as follows: <b><i>‘Encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way’</i></b>
Question 21; PA2	Cllr Nevin [489]	2012	Comment	London Road public toilets please so that traders don’t bear the brunt of urinating doorways, a bench with public art/sculpture near to roundabout or top of Princes Street and pedestrianize as much as possible.	In setting out broad development principles for London Road, Policy PA3 seeks the provision of public art. However, the provision of toilets will not be addressed in the SCAAP, this would be considered during the detailed design stage of future development proposals.
Question 21; PA2  341	Anglian Water (Sue Ball) [37]	2018	Comment	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.
Question 21	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2060	Support	Recognise-the importance of the Town Centre in viability and vitality terms and the need to enhance the existing markets where appropriate and introduce and create new markets.	Noted.

Question 21	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2064	Comment	High Streets Adapting for Change (RICS) introduces the Governments recognition that our High Streets have to offer something new and different that neither the shopping centres nor the internet can match. They need to offer an experience that goes beyond retail and they need to be a destination for the socialising culture, health, well being, creativity and learning. Offices alongside shops, alongside housing, alongside eateries.	Noted. The Plan seeks to achieve this.
Question 21  342	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2079	Comment	Both the Council and Stockvale Group recognise the need to diversify the uses within the Central Area and indeed cite the success of London Road eateries. However, this needs to have some further consideration in relation to pedestrianising the stub-end of London Road, introducing a series of stalls that would allow for street food to further define this zone as a place that people come to enjoy, to eat, to meet and to use the cinema, which would include a reconfiguration and animation around Victoria Circus bleeding across into the northern end of the High Street.	The SCAAP seeks to achieve this in Policy PA2. No changes are proposed.



Question 21	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2098	Comment	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there have been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The STOCKVALE GROUP would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions. No changes are proposed.
Question 21 343	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2109	Comment	The STOCKVALE GROUP would further make recommendation that there is an opportunity lost on the Victoria Gateway Public Realm Improvements and the large public space should be activated with a small commercial use and extensive landscaping and planting.	The Victoria Avenue/ Queensway junction has benefitted from significant public realm and access improvements as part as the implemented Victoria Gateway Scheme. However, it is considered that policy should still seek further improvements to the public realm and accessibility. Therefore the following amendments in Policy PA2.7.g. are proposed: <i><b><u>'seek provision of public art and integrated signage that combine with more traditional signage to signal entry to the Town Centre from Victoria Gateway and facilitate clear way-finding to improve legibility and pedestrian access, together with further improvements to the public realm and accessibility.'</u></b></i>

Question 21	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2112	Comment	There is a great opportunity to display public art in terms of film projection on to the rear of the Victoria Plaza and the existing Odeon building. This further runs in to the top end of the High Street where there is a greater opportunity to enhance Victoria Circus.	<p>Additional wording proposed to emphasise the use of visually active frontage within Policy PA2.2 as follows: <b><u>‘Encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way’</u></b></p> <p>Include an additional criteria to Policy PA1 to encourage visually active frontage within PA1 to the rear of buildings on Queensway dual-carriage way to read as follows: <b><u>‘Encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way’</u></b></p>
Question 21  344	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2122	Comment	In relation to the existing Sainsburys site and redevelopment of the whole block OS15. This is a site that could take a significantly higher building to cater for a larger number of residential units, complementing the Victoria Gateway proposals to re-use the redundant office space to residential. Fantastic views are offered here and the larger number of residential units would sustain the A3 restaurant and café uses around the top end of the High Street and stub end of London Road. This all needs to be considered in relation to adequate onsite parking provision, greater connectivity to public transport and a greatly enhanced Public Realm. The Stockvale Group would call for the Council to present design codes to ensure the design quality of development meets the Councils high aspirations.	<p>Noted. Opportunity Site 15 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP’s plan period.</p> <p>Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021. Nevertheless the development principles as set out in Policy PA2, in combination with other adopted local policy and guidance, will provide the necessary framework to guide development proposals in this area.</p>

Question 21	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2123</b>	<b>Comment</b>	The stub end of London Road should be landscaped and enhanced to include mature tree planting as a boulevard into Victoria Circus. The A3 restaurant and café uses at the top end of the High Street have been a huge success and through the SCAAP document this could be further encouraged with the inclusion of some small stalls to encourage street food and pop up food outlets centred around a large kiosk or amphitheatre at Victoria Circus. These small pavilions could then spread to the northern quadrant of the High Street. This would further stimulate the eastern end of London Road and the top end of the High Street as a destination for eateries and later entertainment to extend the evening economy.	Noted. These aspects are incorporated into Polices PA1 and PA2.
Question 21	Southend Bid (Mr S Kearney) [496]	<b>2162</b>	<b>Support</b>	Recognise the importance of the Town Centre in viability and vitality terms and the need to enhance the existing markets where appropriate and introduce and create new markets	Noted.
Question 21 345	Southend Bid (Mr S Kearney) [496]	<b>2166</b>	<b>Comment</b>	High Streets Adapting for Change (RICS) introduces the Governments recognition that our High Streets have to offer something new and different that neither the shopping centres nor the internet can match. They need to offer an experience that goes beyond retail and they need to be a destination for the socialising culture, health, well being, creativity and learning. Offices alongside shops, alongside housing, alongside eateries.	Noted. The Plan seeks to achieve this.
Question 21	Southend Bid (Mr S Kearney) [496]	<b>2181</b>	<b>Comment</b>	Both the Council and the BID recognise the need to diversify the uses within the Central Area and indeed cite the success of London Road eateries. However, this needs to have some further consideration in relation to pedestrianising the stub-end of London Road, introducing a series of stalls that would allow for street food to further define this zone as a place that people come to enjoy, to eat, to meet and to use the cinema, which would include a reconfiguration and animation around Victoria Circus bleeding across into the northern end of the High Street.	The SCAAP seeks to achieve this in Policy PA2. No changes are proposed.

Question 21	Southend Bid (Mr S Kearney) [496]	2201	Comment	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there has been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The BID would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions.
Question 21	Southend Bid (Mr S Kearney) [496]	2211	Support	<p>Wholeheartedly support the improvement on Victoria Avenue as a gateway in to the Town. The BID recognises that much of this work is already underway with the on-going redevelopment of Heath and Carby House.</p>	Noted. The Victoria Avenue/ Queensway junction has benefitted from significant public realm and access improvements as part as the implemented Victoria Gateway Scheme. However, it is considered that policy should still seek further improvements to the public realm and accessibility.
Question 21 346	Southend Bid (Mr S Kearney) [496]	2212	Comment	<p>The STOCKVALE GROUP would further make recommendation that there is an opportunity lost on the Victoria Gateway Public Realm Improvements and the large public space should be activated with a small commercial use and extensive landscaping and planting.</p>	<p>The Victoria Avenue/ Queensway junction has benefitted from significant public realm and access improvements as part as the implemented Victoria Gateway Scheme. However, it is considered that policy should still seek further improvements to the public realm and accessibility. Therefore the following amendments are proposed to Policy PA2.7.g.: <b><i>'seek provision of public art and integrated signage that combine with more traditional signage to signal entry to the Town Centre from <u>Victoria Gateway</u> and facilitate clear way-finding to improve legibility and pedestrian access, together with further improvements to the public realm and accessibility.'</i></b></p>

Question 21	Southend Bid (Mr S Kearney) [496]	2215	Comment	There is a great opportunity to display public art in terms of film projection on to the rear of the Victoria Plaza and the existing Odeon building. This further runs in to the top end of the High Street where there is a greater opportunity to enhance Victoria Circus.	<p>Additional wording proposed to emphasis the use of visually active frontage within Policy PA2.2 as follows: <b><u>‘Encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way’</u></b></p> <p>Include an additional criteria to Policy PA1 to encourage visually active frontage within PA1 to the rear of buildings on Queensway dual-carriage way to read as follows: <b><u>‘Encourage visually active frontages, through public art, green walls, and architectural fenestration to buildings on Queensway dual carriage way’</u></b></p>
Question 21	Southend Bid (Mr S Kearney) [496]	2225	Comment	In relation to the existing Sainsburys site and redevelopment of the whole block OS15. This is a site that could take a significantly higher building to cater for a larger number of residential units, complementing the Victoria Gateway proposals to re-use the redundant office space to residential. Fantastic views are offered here and the larger number of residential units would sustain the A3 restaurant and café uses around the top end of the High Street and stub end of London Road. This all needs to be considered in relation to adequate onsite parking provision, greater connectivity to public transport and a greatly enhanced Public Realm. The BID would call for the Council to present design codes to ensure the design quality of development meets the Councils high aspirations.	<p>Noted. Opportunity Site 15 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP’s plan period.</p> <p>Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021. Nevertheless the development principles as set out in Policy PA2, in combination with other adopted local policy and guidance, will provide the necessary framework to guide development proposals in this area.</p>
Question 21	Southend Bid (Mr S Kearney) [496]	2226	Comment	The stub end of London Road mature tree planting as a boulevard into Victoria Circus. The A3 restaurant and café uses at the top end of the High Street have been a huge success and through the SCAAP document this could be further encouraged with the inclusion of some small stalls to encourage street food and pop up food outlets centred around a large kiosk or amphitheatre at Victoria Circus. These small pavilions could then spread to the northern quadrant of the High Street. This would further stimulate the eastern end of London Road and the top end of the High Street as a destination for eateries and later entertainment to extend the evening economy.	Noted. These aspects are incorporated into Polices PA1 and PA2
Question 21	Southend Bid (Mr S Kearney) [496]	2253	Comment	34% scored the London Road as a top 10 priority.	Noted.

<p>Question 21, PA2</p> <p style="text-align: center;">348</p>	<p>Mr Alan Grubb [59]</p>	<p><b>2295</b></p>	<p><b>Comment</b></p>	<p>In connection with the shared space located outside Southend Victoria Rail Station, I did raise this issue at the time of the redevelopment, saying that in my opinion in order to reduce the possibility of accidents to pedestrians crossing to and from the rail station, Victorian style metal railings could be erected on the feeder road outside the station, the railings should extend from the main entrance of the station, extending down to the area outside the British Transport Police car park and corresponding railings on the opposite side with a gap at the bus stops and a gap in each of the railing outside the side entrance to the rail station with a pedestrian controlled facility. Although some trees have been planted at Victoria Circus, they are not mature enough to attract the wild life (Birds) More trees do need to be planted but the trees do need to be semi matured and be able to support wild life.</p> <p>My comments refer to the road part of the shared access which takes buses from Victoria Avenue into the bus stops outside the side entrance of the rail station this part of the road is also being used by drivers of vehicles who are using Victoria Avenue to access the ring road without using the traffic light at Victoria Circus.</p> <p>There does need to be signs before the start of this section of the road restricting the drivers other than bus or taxi drivers from using this area of road together with A N P R cameras. The problem is further compounded by unauthorised vehicles using the road in the opposite direction. The problem is further compounded by some vehicle owners/ delivery drivers parking their vehicles on the pavement before the side entrance to the Rail station thereby blocking the visibility of the pedestrian who is wishing to cross the road from the station.</p>	<p>The workings of the 'shared space' outside Victoria Railway Station will be kept under review as part of the on-going traffic monitoring of the area. No changes are proposed.</p>
<p>Question 21; PA2.7a</p>	<p>Belfairs Garden Residents Association (Barbara Armitage) [511]</p>	<p><b>2346</b></p>	<p><b>Object</b></p>	<p>Policy PA2.7a - Pedestrianisation of that section of road will make life extremely difficult for anyone with mobility problems and prevent access to cafes shops and the Odeon as detailed elsewhere.</p>	<p>Any pedestrianisation scheme will take into account the needs of vulnerable road users. These issues will be further considered during the detailed design and implementation stage of the scheme. No changes proposed.</p>

Question 21; PA2.7b	Belfairs Garden Residents Association (Barbara Armitage) [511]	<b>2347</b>	<b>Object</b>	Policy PA2.7b - Relocating the taxis will be discriminatory and taxis to the West of College Way will really leave anyone with a walking difficulty stranded. This proposal could sound attractive but has not been properly thought through. There are not enough disabled parking spaces along there now and removing them really hits the Equality legislation.	Any pedestrianisation scheme will take into account the needs of vulnerable road users and taxi provision. These issues will be further considered during the detailed design and implementation stage of the scheme. No changes proposed.
Question 21; PA2.7g	Belfairs Garden Residents Association (Barbara Armitage) [511]	<b>2348</b>	<b>Comment</b>	Policy PA2.7g - Signage is good, public art maybe - but what is essential for people using the station is a crossing across that shared space.	The workings of the 'shared space' outside Victoria Railway Station will be kept under review as part of the on-going traffic monitoring of the area. No changes are proposed.
Question 21; PA2  349	Historic England (Dr Natalie Gates) [514]	<b>2407</b>	<b>Comment</b>	Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply. Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/</a> . <b><i>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</i></b>	This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>'Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.'</i></b> It is proposed to add the words 'as appropriate' in paragraph 4 of Policy PA6 so that it reads, <b><i>'Promote energy efficiency as appropriate, including.....'</i></b> This would necessitate similar changes to all other policy areas.

Question 21	National Federation for the Blind (Mrs Jill Allen-King) [516]	2437	Comment	Victoria Gateway should be made safe by installing a pedestrian crossing across the shared space outside the Victoria Railway station.	Junction improvements are proposed at a number of key junctions in the town. The Victoria Gateway scheme provided for significant pedestrian improvements at the Victoria Avenue/Queensway junction. Its function will be kept under review as part of wider traffic management monitoring.
Question 21, PA2	National Federation for the Blind (Mrs Jill Allen-King) [516]	2439	Object	London Road should not be pedestrianised. The taxi rank should not be moved further away from its current position. It is already quite a distance for elderly and disabled people to walk from the shops to the taxi rank. People visiting the cinema also need the taxi nearby. At night it could cause dangerous situations if people have to walk further to the taxi rank. I would even suggest a bus route to that area, certainly not a pedestrian area. There are many banks in that area where many elderly people need to go, and for safety reasons need taxis close by for their transport requirements. Also more disabled parking to be in that area. If you pedestrianise it you take away access for disabled people to get to their Banks. If you put tables and chairs in a pedestrian area in London Road this will be a nightmare for blind and partially sighted people to walk in this area.	Policy PA2 seeks to pedestrianise London Road in the interests of improving the environment and townscape of this part of the retail area. The provision of taxi facilities enhanced pedestrian facilities and facilities for vulnerable road users will all be considered at the detailed design stage of any scheme. No changes proposed.
Question 21, PA2 250	National Federation for the Blind (Mrs Jill Allen-King) [516]	2440	Object	Throughout the document there are references to pedestrian and cycling routes. Cycling routes should be built on road space and not on pavements. At no time should pedestrians have to share with cyclists. It is too dangerous and will prevent many people who are blind and partially sighted from walking out safely. Cycling should not be allowed in the high street or any other pedestrian area.	Pedestrian and cycle routes will be provided and designed in a safe manner in accordance with best practice and guidance, having regard to the locality, road safety needs and the needs of other road users.
Question 21, PA2	National Federation for the Blind (Mrs Jill Allen-King) [516]	2441	Comment	The direct Link from Victoria railway station and the high street can only happen if a safe pedestrian crossing is installed across the shared space outside the station.	Junction improvements are proposed at a number of key junctions in the town. The Victoria Gateway scheme provided for significant pedestrian improvements at the Victoria Avenue/Queensway junction. Its function will be kept under review as part of wider traffic management monitoring.



Question 22, PA3	Essex Chambers of Commerce (Mr John Dallaway) [452]	1950	Support	Agree with the proposed approach to managing development within the Elmer Square Policy Area	Noted.
Question 22, PA3	Cllr Nevin [489]	2011	Comment	Elmer Square green area ideas to tidy up and prudential building needs redeveloping, units facing the forum, hide the traders rubbish bins with trees, bushes please, we have residents living facing onto this. Picnic area on green space and children's swings or water feature	Noted, detailed design elements will be considered at the implementation stage of Elmer Square Phase 2.
Question 22, PA3	Anglian Water (Sue Ball) [37]	2019	Comment	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.
Question 22	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2114	Comment	In relation to the middle of the High Street from Pitmans Close, Whitegate Road through to Tylers Avenue, the STOCKVALE GROUP support the extension of the education and cultural quarter into this area and would further suggest that the SCAAP looks at office use within the High Street itself and some residential uses above these offices. This would stimulate a broader economy and a safer pedestrian environment.	Noted. The SCAAP identifies a number of opportunities for achieving residential/office development within this locality. No changes are proposed.

Question 22	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2124</b>	<b>Support</b>	The STOCKVALE GROUP wholly supports proposals for Elmer Square and repeats the comments that have been made in relation to the High Street. There is a greater opportunity for this segment of the High Street to have a mixture of uses as well as retail. This includes office space that directly correlates to the education hub and again a strong residential use above this segment of the High Street. The STOCKVALE GROUP recognise that the Local Authority, University and South East Essex College has already delivered significant achievements in realising the aspirations so far.	Noted. These aspects are included within the Plan (Policies DS1, PA1, PA2 and PA3).
Question 22	Southend Bid (Mr S Kearney) [496]	<b>2217</b>	<b>Comment</b>	In relation to the middle of the High Street from Pitmans Close, Whitegate Road through to Tylers Avenue, the BID support the extension of the education and cultural quarter into this area and would further suggest that the SCAAP looks at office use within the High Street itself and some residential uses above these offices. This would stimulate a broader economy and a safer pedestrian environment.	Noted. The SCAAP identifies a number of opportunities for achieving residential/office development within this locality. No changes are proposed.
Question 22	Southend Bid (Mr S Kearney) [496]	<b>2227</b>	<b>Support</b>	The BID wholly supports proposals for Elmer Square and repeats the comments that have been made in relation to the High Street. There is a greater opportunity for this segment of the High Street to have a mixture of uses as well as retail. This includes office space that directly correlates to the education hub and again a strong residential use above this segment of the High Street. The BID recognise that the Local Authority, University and South East Essex College has already delivered significant achievements in realising the aspirations so far.	Noted. These aspects are included within the Plan (Policies DS1, PA1, PA2 and PA3).

Question 22, PA3                353	Historic England (Dr Natalie Gates) [514]	2408	Comment	<p>Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply.</p> <p>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/</a>. <b><i>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</i></b></p>	<p>This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.</i></b></p> <p>It is proposed to add the words ‘as appropriate’ in paragraph 4 of Policy PA6 so that it reads, <b><i>Promote energy efficiency as appropriate, including.....</i></b></p> <p>This would necessitate similar changes to all other policy areas.</p>
Question 22, PA3	National Federation for the Blind (Mrs Jill Allen-King) [516]	2442	Comment	<p>The Forum is not accessible for many elderly people like it was when it was in Victoria avenue. It is not on a bus route and it is a long walk for many people to reach it from a bus stop. There should be taxi ranks and parking for disabled people next to the forum.</p>	<p>The Forum has been established at the heart of the town centre adjacent to the railway station. The provision of taxi ranks and improved connectivity for pedestrians will be considered as part of further phases of the scheme.</p>
Question 22, PA3	National Federation for the Blind (Mrs Jill Allen-King) [516]	2443	Comment	<p>There is mention of mixed pedestrian and cycling routes that should not be allowed on the grounds of safety as already mentioned above.</p>	<p>Pedestrian and cycle routes will be provided and designed in a safe manner in accordance with best practice and guidance, having regard to the locality, road safety needs and the needs of other road users.</p>

Question 23, PA4	Essex Chambers of Commerce (Mr John Dallaway) [452]	1951	Support	Agree with the proposed approach to managing development within the Queensway Policy Area	Noted.
Question 23, PA4	Anglian Water (Sue Bull) [37]	2020	Comment	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.
Question 23  354	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2125	Support	Support a vastly regenerated and improved area. The STOCKVALE GROUP recognises the value in creating innovative housing typologies and a high quality built environment.	Noted.

Question 23	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2127	Comment	There are a couple of issues that the STOCKVALE GROUP want to ensure are adequately addressed through the SCAAP. The first of those is the potential of having residents parking zones, this could have a negative effect on the existing Town Centre on and off street parking and consume spaces that are vital for visitors.	Noted. Such aspects will be kept under review as part of the on-going transport monitoring of the area.
Question 23 355	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2128	Comment	The STOCKVALE GROUP would wish to ensure that the Queensway policy area provides connectivity into the Seaway Car Park and the opportunity to see Seaway as a Gateway to the Seafront and the first visual connection to the Sea is not lost.	Noted. These provisions are identified in Policy CS1.
Question 23	Southend Bid (Mr S Kearney) [496]	2228	Support	Support a vastly regenerated and improved area. The BID recognises the value in creating innovative housing typologies and a high quality built environment.	Noted.
Question 23	Southend Bid (Mr S Kearney) [496]	2230	Comment	There are a couple of issues that the BID want to ensure are adequately addressed through the SCAAP. The first of those is the potential of having residents parking zones, this could have a negative effect on the existing Town Centre on and off street parking and consume spaces that are vital for visitors.	Noted. Such aspects will be kept under review as part of the on-going transport monitoring of the area.

Question 23	Southend Bid (Mr S Kearney) [496]	2231	Comment	<b>STOCKVALE GROUP would wish to ensure that the Queensway policy area provides</b> connectivity into the Seaway Car Park and the opportunity to see Seaway as a Gateway to the Seafront and the first visual connection to the Sea <b>is not lost.</b>	Noted. These provisions are identified in Policy CS1.
Question 23	Southend Bid (Mr S Kearney) [496]	2254	Comment	Queensway was scored by 24% response as a top 10 priority	Noted.
Question 23, PA4	Mr Alan Grubb [59]	2299	Comment	The pedestrian accessibility at the roundabout at Porters Grange does need to be improved, one such improvement would be, to close the stairs which take the pedestrian across the roundabout and to have pedestrian controlled crossings in order the pedestrian can cross the roads in safety.	Junction improvements to improve safety, particularly pedestrian and cyclists, at Queensway/Sutton Road are included in Policy PA4. No changes are proposed.
Question 23, PA4	Mr Paul Bethell [499]	2317	Comment	OS4 – what is urban grain?	Further explanatory included in Para. 165 to define urban grain as follows ' <i>...to re-establish urban grain (i.e. the physical form of former and surrounding street patterns and blocks).</i> '
Question 23, PA4  356	Mr Paul Bethell [499]	2318	Comment	OS4 - When I see the word "regeneration" applied in these circumstances, I think that means the council intends to demolish a lot of buildings close to me and build something which gives people better living conditions. Good for them. I presume, however, that this will be rather noisy and dirty and disruptive and inconvenience anyone living in a house just over the road for however many years it takes. So what are your plans for dealing with that? I suppose what I really want to know is: are the tower blocks going to be demolished and replaced with some affordable low-level social housing? And are there any plans to match it on Coleman Street?	The hours of construction will be controlled though conditions on any planning application permission. Affordable housing levels will be determined in line with local planning policy. The Better Queensway project will outline the detailed plans for the area, which will be assessed as part of a planning application.
Question 23, PA4	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	2370	Comment	Yes, whether to demolish or refurbish the tower blocks of flats needs to be carefully considered. Keep them for another 30 years, if possible	Noted.

Question 23, PA4  357	Historic England (Dr Natalie Gates) [514]	2409	Comment	<p>Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply.</p> <p>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/</a>. <b><i>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</i></b></p>	<p>This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.</i></b></p> <p>It is proposed to add the words ‘as appropriate’ in paragraph 4 of Policy PA6 so that it reads, <b><i>Promote energy efficiency as appropriate, including.....</i></b></p> <p>This would necessitate similar changes to all other policy areas.</p>
<b>Warrior Square Policy Area – Policy PA5, Opportunity Site 5</b>					
Question 24, PA5	Essex Chambers of Commerce (Mr John Dallaway) [452]	1952	Support	Agree with the proposed approach to managing development within the Warrior Square Policy Area and Opportunity Site	Noted.
Question 24, PA5	Burgess Estate Residents Association [176]	2004	Comment	Warrior Square would be enhanced by residential development on the south side to enclose and complete the square. There must be strong support for the stated principle of maintaining the quality of the square since it is the absence of any reasonable level of maintenance that led to the “improvement scheme”. There is no sign that maintenance levels have improved.	Noted, the SCAAP is not the appropriate document to set out the maintenance procedures of public spaces.
Question 24, PA5	Cllr Nevin [489]	2009	Comment	Warrior Square protecting green space in warrior square where swimming pool was, a five aside football pitch/ or multi use area, jogging track round outside. Happy with houses both sides of Whitegate, with trees please, or water feature.	Noted. This site is unlikely to be deliverable in the SCAAP timeframe and therefore will not be included in the final version of the document.

Question 24, PA5	Anglian Water (Sue Bull) [37]	2021	Comment	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.
Question 24  358	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2130	Comment	The retention of the green space is paramount and greater legibility needs to be brought through to the High Street. Clearly the previous swimming pool site is a development opportunity and the STOCKVALE GROUP would seek that that this is of the highest quality providing some activity around the ground floor to support the small pocket park of Warrior Square. The STOCKVALE GROUP recognise that this site could deliver a significant number of residential units.	Noted. Opportunity Site 5 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021. Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021. Nevertheless the development principles as set out in Policy PA5, in combination with other adopted local policy and guidance, will provide the necessary framework to guide development proposals in this location.
Question 24	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2131	Comment	The STOCKVALE GROUP would want to see that the existing public car park provision is retained and any residential and commercial development yield aims to meet the requirements of the Councils development management plan in regard to parking provision.	The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.



Question 24	Southend Bid (Mr S Kearney) [496]	2233	Comment	The retention of the green space is paramount and greater legibility needs to be brought through to the High Street. Clearly the previous swimming pool site is a development opportunity and the BID would seek that that this is of the highest quality providing some activity around the ground floor to support the small pocket park of Warrior Square. The BID recognise that this site could deliver a significant number of residential units.	Noted. Opportunity Site 5 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.  Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021. Nevertheless the development principles as set out in Policy PA5, in combination with other adopted local policy and guidance, will provide the necessary framework to guide development proposals in this location
Question 24	Southend Bid (Mr S Kearney) [496]	2234	Comment	The BID would want to see that the existing public car park provision is retained and any residential and commercial development yield aims to meet the requirements of the Councils development management plan in regard to parking provision.	The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
Question 24	Southend Bid (Mr S Kearney) [496]	2255	Comment	Warrior Square was scored by 18% as a top 10 priority.	Noted.
Question 24, PA5	Historic England (Dr Natalie Gates) [514]	2410	Comment	Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply. Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-ptl/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-ptl/</a> . <b><i>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</i></b>	This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>'Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.'</i></b> It is proposed to add the words 'as appropriate' in paragraph 4 of Policy PA6 so that it reads, <b><i>'Promote energy efficiency as appropriate, including.....'</i></b> This would necessitate similar changes to all other policy areas.

Question 24, PA5	National Federation for the Blind (Mrs Jill Allen-King) [516]	2444	Comment	Pedestrian and cycle routes should be kept separate. No cycling should be allowed on the footway or footpath.	Pedestrian and cycle routes will be provided and designed in a safe manner in accordance with best practice and guidance, having regard to the locality, road safety needs and the needs of other road users.
Question 24, PA5	National Federation for the Blind (Mrs Jill Allen-King) [516]	2445	Comment	All bus routes should be kept in this area with shelters and seating provided.	The Plan seeks to improve public transport provision in the Central Area.
Question 24, PA5  360	National Federation for the Blind (Mrs Jill Allen-King) [516]	2446	Comment	This is a suitable area for a day centre for disabled people. Since the Queensway building was closed there has been nowhere for disabled people to go.	The site is considered to be most suitable for a mixed use residential led development, which could include an element of community uses. Opportunity Site 5 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period. Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021.  Community infrastructure provision is promoted on the nearby Queensway site as part of the provisions of Policy PA4. No change proposed.
Question 24, PA5	National Federation for the Blind (Mrs Jill Allen-King) [516]	2447	Comment	There is no longer a swimming pool in the centre of the Town as the Warrior Square pool was closed. A new facility should be provided in this central area.	Policy PA5 seeks to regenerate this area with a mixed use development that respects the character and setting of the adjacent Conservation Area. The former swimming pool site is identified as having the potential to provide additional open space to mirror that of Warrior Square Gardens. A new improved swimming pool facility has been established at Garon Park outside the Plan area. No changes proposed.
<b>Clifftown Policy Area – Policy PA6</b>					
Question 25, PA6	Essex Chambers of Commerce (Mr John Dallaway) [452]	1953	Support	Agree with the proposed approach to managing development within the Clifftown Policy Area	Noted.

Question 25 PA6	Cllr Nevin [489]	<b>2013</b>	<b>Support</b>	Cliff town great to see development of Empire Theatre and Alexandra Street.	Noted, specific site allocations for these areas will not be included in the final version of the SCAAP as there remains insufficient evidence that they will be delivered by 2021. However, this does not preclude development coming forward and this will be guided by the policy area development principles.
Question 25 PA6	Anglian Water (Sue Bull) [37]	<b>2022</b>	<b>Comment</b>	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.
Question 25  361	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2061</b>	<b>Support</b>	Recognise the importance of the Town Centre in viability and vitality terms and the need to enhance the existing markets where appropriate and introduce and create new markets.	Noted.

Question 25	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2099	Comment	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there have been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The STOCKVALE GROUP would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions. No changes are proposed.
Question 25  362	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2117	Comment	<p>There could also be an improvement of pedestrian connection from the Central railway station of Southend on Sea into the Public Realm and a vast and extensive soft landscape scheme introduced around the Tylers Avenue/High Street connection to the railway station.</p>	<p>Noted. Further provision is made that seeks to improve the connectivity and public realm between Policy Area PA7 and the High Street and the Central Station, with an additional criteria to PA7 as follows: <b><u>'facilitates better pedestrian access to the High Street and Southend Central railway station'</u></b></p> <p>And the following amendment to PA6.3.b:  'streetscape and landscape design improvements, including urban greening and tree planting, to create well lit walking circuits through Clifftown from a newly created public plaza at Southend Central <b>Railway</b> Station/ Central House, to Cliff Gardens and Pier Hill, <b>facilitating better pedestrian access to the High Street'</b></p>

Question 25	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2132	Support	Wholly support the aspirations for the Clifftown area and recognise the value of the Clifftown conservation area. There are two development sites namely Clarence Street and Alexandra Street car parks which have been identified for redevelopment. The STOCKVALE GROUP generally support the redevelopment of these areas providing they respond to the fine grain character of the Conservation area and the scale of Alexandra Street. The STOCKVALE GROUP recognise there is an opportunity to further enhance the boutique offer of this part of Southend by means of high quality architecture and high quality retail together with A3 uses and residential uses at upper levels.	Noted. Opportunity Sites 16 and 17 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021. Comments in relation to the sites will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021. Nevertheless the development principles as set out in Policy PA6, in combination with other adopted local policy and guidance, will provide the necessary framework to guide development proposals in this location
Question 25 363	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2133	Comment	In relation to car parking, the STOCKVALE GROUP would seek that the existing public car parking spaces are either allocated as part of the museum provision or are included elsewhere within the south west corner of the SCAAP area. There is an opportunity with the Empire Theatre as a large basement already exists. A public car park could form part of a wholesale mixed use redevelopment of the Empire theatre.	OS9: New Southend Museum includes provision for public parking.
Question 25	Southend Bid (Mr S Kearney) [496]	2163	Support	Recognise the importance of the Town Centre in viability and vitality terms and the need to enhance the existing markets where appropriate and introduce and create new markets	Noted.

Question 25	Southend Bid (Mr S Kearney) [496]	2202	Comment	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there has been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The BID would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions.
Question 25  364	Southend Bid (Mr S Kearney) [496]	2220	Comment	<p>There could also be an improvement of pedestrian connection from the Central railway station of Southend on Sea into the Public Realm and a vast and extensive soft landscape scheme introduced around the Tylers Avenue/High Street connection to the railway station.</p>	<p>Noted. Further provision is made that seeks to improve the connectivity and public realm between Policy Area PA7 and the High Street and the Central Station, with an additional criteria to PA7 as follows: <b><u>'improve pedestrian accessibility and public realm enhancement that facilitates better access to the High Street and Southend Central railway station'</u></b></p> <p>And the following amendment to PA6.3.b: 'streetscape and landscape design improvements, including urban greening and tree planting, to create well lit walking circuits through Clifftown from a newly created public plaza at Southend Central <b>Railway</b> Station/ Central House, to Cliff Gardens and Pier Hill, <b>facilitating better pedestrian access to the High Street'</b></p>
Question 25	Southend Bid (Mr S Kearney) [496]	2235	Support	<p>Wholly support the aspirations for the Clifftown area and recognise the value of the Clifftown conservation area. There are two development sites namely Clarence Street and Alexandra Street car parks which have been identified for redevelopment. The BID generally support the redevelopment of these areas providing they respond to the fine grain character of the Conservation area and the scale of Alexandra Street. The BID recognise there is an opportunity to further enhance the boutique offer of this part of Southend by means of high quality architecture and high quality retail together with A3 uses and residential uses at upper levels.</p>	<p>Noted. Opportunity Sites 16 and 17 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.</p> <p>Comments in relation to the sites will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021. Nevertheless the development principles as set out in Policy PA6, in combination with other adopted local policy and guidance, will provide the necessary framework to guide development proposals in this location</p>

Question 25	Southend Bid (Mr S Kearney) [496]	2236	Comment	In relation to car parking, the BID would seek that the existing public car parking spaces are either allocated as part of the museum provision or are included elsewhere within the south west corner of the SCAAP area. There is an opportunity with the Empire Theatre as a large basement already exists. A public car park could form part of a wholesale mixed use redevelopment of the Empire theatre.	Noted. OS9: New Southend Museum will include public parking provision.
Question 25	Southend Bid (Mr S Kearney) [496]	2256	Comment	Cliff town was scored by 18% as a top 10 priority	Noted.
Question 25; PA6  365	Belfairs Garden Residents Association (Barbara Armitage) [511]	2349	Object	Against redevelopment of Alexandra Street and Clarence Road Car Parks. Both needed for local business and for access to Royals, shops in the High Street and cafes and restaurants for short term use. People will be deterred from using the facilities if they have to go some way to park. Families, older people, those helping older/disabled people all want to set down nearby and not be banished to a multi storey.	Opportunity Sites 16 and 17 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period. Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021.  The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
Question 25, PA6	Procurement Consulting (Mr Barrie Evans) [513]	2378	Comment	The Cliff town conservation area is poorly lit and pedestrian routes for commuters from Southend Central station are seen as unsafe, where the traditional lighting is cosmetic and does not assist in the safety and security of pedestrians.	Policy DS5 seeks to ensure the provision of appropriate street lighting. Reference will be included for improved lighting in PA6.
Question 25, PA6	Procurement Consulting (Mr Barrie Evans) [513]	2380	Comment	Southend-On-Sea central area parking currently relies on parking in residential streets. This is especially true in the Cliff town area where the theatre and London commuters, rely on on-street parking in the residential areas surrounding. Cliff town Parade is particularly bad and has become dangerous for locals due to speed and congestion made by over parking. This causes stress and major issues for local residents. Car parks on the fringes of the city centre should be built to host and rectify these issues. Any sea front investment, regeneration or build should have a self-sustaining carpark which does not impede the local residents. A Tram system should be investigated further which would solve the train to car issue and reduce road congestion in the area; in turn reducing carbon emissions.	The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.

Question 25, PA6	Procuresure Consulting (Mr Barrie Evans) [513]	2393	Comment	<p>All regeneration of the Clifftown conservation area should be in keeping with the surrounding residential areas in Clifftown. The whole area should come under planning consent within the Clifftown Article 4 planning policy.</p> <p>There should not be a concentration of Bars and restaurants in the area and if these are brought to the area then no such planning should be given to Wetherspoons or budget chain pubs which cause drinking issues and encourage daytime drinking lowering the tone of the area and degrading surrounding properties. Concentrating bars in one area has proven to be bad for the area and its surroundings. union street in plymouth proves this. sports pubs and night clubs should not be allowed in the clifftown conservation area. only quality high end pubs, restaurants and cafes should be allowed to reflect the residential area of clifftown, thus drawing in financially solvent residents who will naturally have the capital to improve the area themselves and in turn create employment.</p>	Policy PA6 seeks to protect and enhance the character, heritage and amenities of the Clifftown Conservation Area. The provisions of Article 4 Directions are kept under review as appropriate. No changes proposed.
Question 25, PA6  366	Procuresure Consulting (Mr Barrie Evans) [513]	2395	Comment	<p>Clifftown conservation area should have strict parking control as seen in the residential areas of Thorpe Bay. More double yellow lines should be introduced on all roads in Clifftown especially Clifftown Parade where parking congestion makes the road dangerous due to speeding vehicles and the narrowing of the road by parked cars. The theatre should provide parking as should the Council facilitate parking for commuters elsewhere. All official driveways should be white lined by the council to stop illegal parking and allow residents access to their own driveways. Over parking in Clifftown is a major issue, especially in Clifftown Parade. Over parking ruins what is supposed to be a conservation area, the vehicle fumes is also bad for the buildings and occupants themselves. The summer time parking restrictions do not go far enough and they should be year round. why should we the residents have to adhere to the planning rules in article 4 when all the extra money we spend on keeping our properties in aesthetic order is then ruined by hundreds of cars jam packed in along the streets we live in which ruin the look of the area anyway?! Clifftown Parade should have no on street parking at all.</p>	<p>Traffic management will be kept under review as part of the provisions of Policy DS5 and the Local Transport Plan. No changes proposed.</p> <p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
Question 25, PA6	Historic England (Dr Natalie Gates) [514]	2404	Comment	<p>Recommend that paragraph 2 is extended to cover proposals that are outside of a conservation area (particularly those that are adjacent to a conservation area) but offer an opportunity for enhancement of setting.</p>	This is covered by Policy DM5 of the Development Management DPD and Policy DS3 of the SCAAP.



Question 25, PA6  367	Historic England (Dr Natalie Gates) [514]	2405	Comment	Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply. Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/</a> . <b>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</b>	This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.</i></b> It is proposed to add the words ‘as appropriate’ in paragraph 4 of Policy PA6 so that it reads, <b><i>Promote energy efficiency as appropriate, including.....’</i></b> This would necessitate similar changes to all other policy areas.
Question 25, PA6	National Federation for the Blind (Mrs Jill Allen-King) [516]	2448	Comment	It is not clear about the plan for outside the Central railway Station. Taxi ranks need to be kept and bus routes need to improve in this area to encourage more people to travel to the station and High street by bus and not use their cars.	Policy PA6a seeks to improve the forecourt, public realm and space in front of Central Railway Station. The provision for taxis, bus stops, street furniture etc. will be taken forward at the design stage. No changes proposed.
Question 25, PA6	National Federation for the Blind (Mrs Jill Allen-King) [516]	2449	Comment	What is a public square? I do not agree with outside dining unless tables and chairs are properly guarded by a metre high barrier to prevent blind and partially sighted people walking in to them.	The Plan seeks to provide an improvement to soft landscaping and open space provision within the Clifftown policy area. Access arrangements to shops are considered as part of the design stage of planning applications to ensure accessibility for all users. No changes to policy are proposed.
Question 25, PA6	National Federation for the Blind (Mrs Jill Allen-King) [516]	2450	Object	Again pedestrian and cycle routes are suggested these must be kept separate. Cyclists should be on road space and not pedestrian areas.	Pedestrian and cycle routes will be provided and designed in a safe manner in accordance with best practice and guidance, having regard to the locality, road safety needs and the needs of other road users.

**Tylers Policy Area – Policy PA7, Opportunity Site 6 (OS6)**

Question 26, PA7	Essex Chambers of Commerce (Mr John Dallaway) [452]	1954	Support	Agree with the proposed approach to managing development within the Tylers Policy Area and Opportunity Site	Noted.
Question 26, PA7, OS6  368	Mr Harry Chandler [219]	1987	Comment	<p>OS6 - For many years I have been disappointed by the lack of a comprehensive bus station in Southend. The present arrangements in Chichester Road are unsatisfactory for both residents of Southend and surrounding area and visitors to Southend. Many people especially elderly residents, mothers with babies and small children and the disabled have to stand in cold and wet weather without adequate shelter and heating. Visitors to Southend to whom I have spoken have been appalled by current arrangements. The glazed structure south of Heygate Avenue is shunned by most bus passengers as it does not appear to be fit for purpose. To be constructive a bus station along the lines of the one in Preston, Lancashire shown below, seems the obvious solution.</p> <p>Having used this bus station for many years, is a joy to use compared with the arrangements in our town, Southend. I understand that the bus station in Preston, opened in 1969, is to be refurbished. On a smaller scale, the bus stations in Harrogate and Bath, both residential and tourist towns, also work well for passengers.</p> <p>The current location of our bus station does not seem to be ideal. The large car park adjacent to the bus station seems to work. It would seem sensible to use this large car par to build a structure similar in purpose to the one in Preston and to provide car parking and a first class bus station for the people of Southend and district and visitors as part of the Better Southend.</p>	<p>Policy PA 7 identifies the potential to relocate the existing Travel Centre (bus station) to the adjacent Tylers Avenue car park as part of a comprehensive redevelopment scheme. However, it is accepted that OS6 does not clearly state why such relocation would be appropriate. It is therefore proposed that the following wording be added to the end of point 5ii of OS6:</p> <p><b><i>'...to provide for enhanced passenger transport facilities and improved pedestrian connectivity to the town centre.'</i></b></p>
Question 26, PA7	Burges Estate Residents Association [176]	2005	Comment	Tylers policy area is a big challenge to get right and maybe the fact that the travel centre is coming down reflects that difficulty. Either way explaining to the public how so much public money was wasted is necessary as well as explaining why it will not happen again.	Noted, OS6 simply sets out the opportunity for relocation of the travel centre.

Question 26, PA7	Cllr Nevin [489]	2010	Comment	Tylers Home zoning approach design features with trees for Quebec Ave to York Rd & Heygate Ave & cul-de-sac where possible, redirecting traffic flows down York Road, to design out difficult areas.	Noted. The final design of any Home Zone scheme for these areas will be taken forward in conjunction with transport schemes. They will be able to explore the opportunities of redirection of traffic flows.
Question 26, PA7	Anglian Water (Sue Bull) [37]	2023	Comment	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.
Question 26  369	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2100	Comment	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there have been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The STOCKVALE GROUP would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions. No changes are proposed.

Question 26	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2115	Comment	The STOCKVALE GROUP would want to ensure that OS6 Tylers Avenue includes a replacement car park for the existing public spaces plus the parking requirements for a future development. In relation to Public Realm improvements, there is an opportunity to create a southern square as part of the Tylers Avenue proposals and link this through to the pedestrianised High Street.	<p>Noted. Adjustments to the boundary of the OS6: Tylers opportunity site will be made and will accommodate such proposals.</p> <p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
Question 26  370	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2118	Comment	There could also be an improvement of pedestrian connection from the Central railway station of Southend on Sea into the Public Realm and a vast and extensive soft landscape scheme introduced around the Tylers Avenue/High Street connection to the railway station.	<p>Noted. Further provision is made that seeks to improve the connectivity and public realm between Policy Area PA7 and the High Street and the Central Station, with an additional criteria to PA7 as follows: <b><u>'improve pedestrian accessibility and public realm enhancement that facilitates better access to the High Street and Southend Central railway station'</u></b></p> <p>And the following amendment to PA6.3.b: 'streetscape and landscape design improvements, including urban greening and tree planting, to create well lit walking circuits through Clifftown from a newly created public plaza at Southend Central <b>Railway</b> Station/ Central House, to Cliff Gardens and Pier Hill, <b>facilitating better pedestrian access to the High Street'</b></p>

Question 26	Southend Bid (Mr S Kearney) [496]	2203	Comment	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there has been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The BID would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions.
Question 26 371	Southend Bid (Mr S Kearney) [496]	2218	Comment	<p>The BID would want to ensure that OS6 Tylers Avenue includes a replacement car park for the existing public spaces plus the parking requirements for a future development. In relation to Public Realm improvements, there is an opportunity to create a southern square as part of the Tylers Avenue proposals and link this through to the pedestrianised High Street.</p>	<p>Noted. Adjustments to the boundary of the OS6: Tylers Opportunity Site will be made and will accommodate such proposals.</p> <p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
Question 26	Southend Bid (Mr S Kearney) [496]	2221	Comment	<p>There could also be an improvement of pedestrian connection from the Central railway station of Southend on Sea into the Public Realm and a vast and extensive soft landscape scheme introduced around the Tylers Avenue/High Street connection to the railway station.</p>	<p>Noted. Further provision is made that seeks to improve the connectivity and public realm between Policy Area PA7 and the High Street and the Central Station, with an additional criteria to PA7 as follows: <b><u>'improve pedestrian accessibility and public realm enhancement that facilitates better access to the High Street and Southend Central railway station'</u></b></p> <p>And the following amendment to PA6.3.b:  'streetscape and landscape design improvements, including urban greening and tree planting, to create well lit walking circuits through Clifftown from a newly created public plaza at Southend Central <b>Railway</b> Station/ Central House, to Cliff Gardens and Pier Hill, <b><u>facilitating better pedestrian access to the High Street'</u></b></p>

Question 26	Southend Bid (Mr S Kearney) [496]	2257	Comment	Tylers was scored by 22% of respondents as a top 10 priority	Noted.
Question 26, PA7	Mr Alan Grubb [59]	2297	Comment	I understand that there is thought of transferring the Travel Centre to a larger site, one site might be the Tyler's Avenue car park. If so I would hope that the Council engages with the residents and the bus user in order to create a travel centre fit for purpose together with flats above the travel centre.	Policy PA7 provides for the possible relocation of the Travel Centre to Tylers Avenue car park. Further consultation will be carried out at the planning application stage, if this were considered to be a viable and feasible option. No changes are proposed.
Question 26, PA7	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2309	Support	Policy PA7 seeks to ensure stronger integration within the Central Seafront Policy Area including improved walking and cycling linkages via St John's Church and Seaway Car Park and Marine Parade Opportunity Site (OS8) and via Pier Hill. Enhancing linkages will help to increase footfall, linked trips and in turn, help to bolster the vitality and viability of the town centre and on this basis, Valad (Europe) Ltd support this policy.	Noted.
Question 26, PA7 372	Historic England (Dr Natalie Gates) [514]	2411	Comment	<p>Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply.</p> <p>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-ptl/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-ptl/</a>. <b><i>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</i></b></p>	<p>This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>'Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.'</i></b></p> <p>It is proposed to add the words 'as appropriate' in paragraph 4 of Policy PA6 so that it reads, <b><i>'Promote energy efficiency as appropriate, including.....'</i></b></p> <p>This would necessitate similar changes to all other policy areas.</p>

Question 26, PA7	National Federation for the Blind (Mrs Jill Allen-King) [516]	2451	Comment	A new travel centre should be covered completely. It would be better located next to Victoria railway station on the old B&Q site, if not next to the Central railway station. In most towns this happens. Buses would not then hold up the traffic near to the Royals.	A central location for the bus station is considered the most appropriate to serve the needs of the town centre and central seafront area.
Question 26, PA7	National Federation for the Blind (Mrs Jill Allen-King) [516]	2452	Comment	What is the public square you refer to? We certainly do not want a shared space like at Victoria Gateway and City Beach. Why are railings to be removed at crossing points? This will cause danger for all pedestrians including children.	Policy identifies the potential for a new public space, as part of an overall development, in the locality of the current travel centre; should this be relocated to the Tylers Avenue car park site.
Question 26, PA7	National Federation for the Blind (Mrs Jill Allen-King) [516]	2453	Comment	Cycle routes must be on road space and not on footpaths or footways. Pedestrians must have uncluttered walk ways with safe pedestrian crossings at all junctions, with audible signals and tactile markings.	Pedestrian and cycle routes will be provided and designed in a safe manner in accordance with best practice and guidance, having regard to the locality, road safety needs and the needs of other road users.
<b>Central Seafront Policy Area – Policy CS1, Opportunity Sites 7, 8, 9 and 10</b>					
Question 27, CS1 and OS8	Mr Kenton Theobald [1930]	1930	Comment	OS8 - new cinema not needed already one up top of high street, small low rent curio/artisan shops needed instead to compliment new square at OS8 (make a Southend lanes like in Brighton), new seaway car park to recognise blue badges and give them free parking	Policy CS1 identifies the potential of the seaway car park site to provide for a mixed use development comprising leisure, cultural and tourism facilities which are considered to be appropriate in this location. The Policy allows for design and layout solutions and seeks to take advantage of the sites elevation with views of the estuary (OS8). The SCAAP is a planning policy document and does not directly cover parking charges. No changes are proposed.
Question 27, CS1	Anna Hyndnan Lahna [456]	1934	Comment	Let's hope that when the jetty is up and running, the nasty slot machine seafront will be brought upmarket with nice restaurants and shops.	Noted.
Question 27, CS1	Essex Chambers of Commerce (Mr John Dallaway) [452]	1955	Support	Agree with the proposed approach to managing development within the Central Seafront Policy Area and sites OS7, OS8, OS9 AND CS1.1 on the proviso that there is adequate car parking provision to support the growth in footfall.	Noted. The Council has commissioned an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.

Question 27, CS1	London Southend Airport (Ms Jo Marchetti) [471]	1968	Support	LSA supports the development of new hotels, tourist attractions and would strongly encourage the development of a dedicated conference and exhibition centre. LSA has the opportunity to host and attract aviation conferences which would bring interest for the town from other countries. The centre and supporting infrastructure would need to be able to host 500+ delegates and should be positioned in the best place possible to highlight Southend's key tourist sites.	Noted. The Plan seeks to develop the new museum which provides potential provision for new conference facilities (OS 9).
Question 27, CS1, OS8  374	Ms Lise Hodgson [467]	1982	Object	<p>OS8 - Placing a cinema that close to the beach is a waste of valuable land. A cinema will not attract more people to the town. I do not know anyone who has ever decided to go on holiday somewhere because there is a cinema. If the Council wants another cinema in the town an area further inland would be far more appropriate.</p> <p>Once you are inside a cinema you are not going to spend a lot of money in the area. In the SCAAP the Council says they want to create an area where people want to live, but who would want to live in a place where they have to look out on a cinema instead of beautiful sea views. The Council's plans are completely devaluing our homes and destroying our enjoyment of them.</p> <p>If the Council really wanted to do the best for this area, (which I am beginning to doubt) instead of destroying it as at present, a series of restaurants and cafes with green areas in between would be more in keeping with a seaside town, perhaps with a large underground car park. That would attract people and get them to spend money in the area. Once you are inside a cinema you are not going to spend a lot of money in the area.</p>	Policy CS1 identifies the potential of the seaway car park site to provide for a mixed use development comprising leisure, cultural and tourism facilities which are considered to be appropriate in this location. The possible inclusion of a cinema is considered to be compatible with providing a mix of leisure uses to enhance the offer on this key site. The Policy also sets out design and layout principles to guide development and allow for 'urban greening', creation of new public and private green space, and seek to take advantage of the sites elevation with views of the estuary (OS8). No changes are proposed.
Question 27, CS1, OS8	Ms Lise Hodgson [467]	1983	Object	OS8 - Regarding the hotel, is there a need? Since I moved to Southend in 2006 the Royal Hotel in the High Street has been empty and is now being developed as a restaurant. If there was a need for another hotel in the area, surely someone smart would have snapped up this gem long ago.	A hotel development is considered appropriate in this location. Southend has the potential for further hotel development to promote 'longer stay' holidays (see Southend Hotel Futures Report 2010). No changes are proposed.



Question 27, CS1, OS8	Ms Lise Hodgson [467]	1984	Object	OS8 - Traffic. Have anybody from the Council ever been in this area during a summer weekend or even weekends leading up to Christmas? The area around the roundabout and Chancellor Road get completely gridlocked at least once a day and the few extra parking spaces in the developer's plan will barely fill the shortfall, let alone accommodate more traffic.	Policy CS1 provides for junction improvements at Queensway/Seaway Car Park/Chancellor Road. All major development proposals will be accompanied by a transport assessment and will have to take account of adopted parking standards. The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
Question 27, CS1	Ms Lise Hodgson [467]	1985	Object	Regarding the plans for the seafront, I am worried about all the high rise buildings being planned. If the Council are trying to create Benidorm on Sea, don't forget, we don't have the climate to make up for the dreadful buildings.  The Council do not want to make the most of the natural attractions of this place, but please, please, please do not destroy it completely.	The Plan, alongside Policy DM4 of the Development Management Document, seeks to provide for appropriately sited taller and larger buildings having regard to the amenity of the area (Policy CS1). It also seeks to enhance and protect the natural attractions of the area (Policies CS2 and 3). No changes are proposed.
Question 27, CS1.10.a 375	Burges Estate Residents Association [176]	1998	Support	Southend has been the focus of working class seaside attractions for at least 80 years and continues to be so. It is the mainstay of many seafront businesses. Those day trippers often take advantage of the sea front and the town centre facilities so improving the connectivity between the two is crucial.	Noted.
Question 27, CS1	Burges Estate Residents Association [176]	2006	Comment	Central seafront policy area principles contain reference to the "use of high quality coordinated materials, durable and easy to maintain". May I suggest that such a requirement be applied to all policy areas where appropriate. There is no reason why the seafront should be singled out for exceptional treatment.	Noted, reference to the ' <i>use of high quality coordinated materials, durable and easy to maintain</i> ' will be removed from CS1, as this is covered by Streetscape Manual Supplementary Planning Guidance.
Question 27, CS1	Cllr Nevin [489]	2014	Support	Seaway Like cycle paths, would be happy to have more green area here, as natural viewing point towards seafront and Spanish steps	Noted.
Question 27, CS1	Anglian Water (Sue Bull) [37]	2024	Comment	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.

Question 27, CS1, OS8	Mr Michael Davies [493]	<b>2034</b>	<b>Support</b>	OS8 - As a long-time resident of the Southend area (since 1959), I have the following comments to make about the proposed development. This area obviously needs development, as it has become progressively more and more run down over the past few years. The council is right to develop the area, and understandably, local business people are excited about the prospect. The SCAAP plan is bold and ambitious.	<b>Noted.</b>
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<p>Question 27, CS1, OS8</p> <p style="text-align: center;">377</p>	<p>Mr Michael Davies [493]</p>	<p>2035</p>	<p><b>Comment</b></p>	<p>In para 192, it mentions a proposed large development area known as Marine Plaza", and that "The site offers potential for taller and larger buildings" However, I feel that this proposed development requires carefully consideration as regards its impact on the local area. Has the Council considered the following aspects?</p> <p>The Kursaal is a historic landmark in a historic seaside town. A tall, multi-storey buildingg right next to it will completely overshadow it, and in my view, look totally out of place. I believe that any building plan should take the current building style into account. In my view, the proposed development may well not do that. If this proposal goes ahead, it will probably not be sympathetic with the existing architecture, and character of the area.</p> <p>I realise that the developers need to make a reasonable profit from their endeavours, and building upwards is always a good way of achieving that end. However, the people of Southend will be the ones that have to live with the result, not the developers, who may not live in the area, and therefore it may not be too much of a consideration for them. A local example of a development that does not fit into the local scene can be found not far away, along the Cliff tops near the Cliffs Pavilion in Westcliff. There are two high rise buildings along there. One is Westward Ho, which has 10/11 storeys (depending on whether you count the ground floor). A little further along is Tower Court, rising 16 storeys into the sky. What a couple of eyesores they are! In my view they should never have been given planning permission. These two buildings look totally and completely out of place. But, now, of course, it's too late. They will remain there, in all their 'glory', and outlive us all. Once mistakes like that have been made, that's it. End of story. <i>There's</i> no going back. Demolition, (although desirable!), is not a realistic option now.</p> <p>In my view, they are on a par with what's now being proposed for the Seaway area. Two nearby cliff top buildings, Stratton House (7 storeys?) and Heathfield House (5 storeys?), are about the same height as the historic Westcliff Hotel (5 Storeys), and the former Overcliff Hotel (long since demolished), and so, don't look too out of place. In my view, a similar approach should be taken with the proposed Seaway development.</p> <p>Another example is- in the 1960s, a long string of tall, square office blocks were thrown up along Victoria Avenue. They now look like shabby eyesores. They served their purpose at the time, of course, but I hope that Southend Council will consider these examples, and bear the future in mind.</p>	<p>It should be noted that Marine Plaza now has planning permission (July 2015) for a residential led mixed-use development and will be allocated within the SCAAP.</p> <p>The Grade II listed Kursaal is recognised as a Landmark Building (Policy DS3) within the SCAAP and any new development proposals within the area will be expected to demonstrate that it is compatible with and/ or enhances key views of the building (Policy DS2). Furthermore, Policy DM5 of the Development Management Document provides detailed policy regarding the historic environment, recognising the significance of heritage assets.</p>
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Question 27, CS1, OS8	Mr Michael Davies [493]	2038	Comment	If the whole of the Seaway car park is built on, where will people who now park there to shop in Southend High Street and the Royals, and use the seafront facilities, park? The Royals car park and the ones at the back of Marks and Spencer already get filled up. If Seaway car park disappears, or is severely reduced in size, those two car parks will have even more strain put upon them, much to the frustration of local shoppers and visitors to the town, some of whom may well decide it's just not worth the bother, and head out to Basildon, or other seaside resorts. I note that local traders are also now expressing concerns about parking, as reported on the front page of the Yellow Advertiser of Friday 29 January 2016.	Noted. OS 8 makes provision for car parking in any development scheme.  The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
Question 27  378	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2083	Support	Supports the enhancement and quality of the Green and Open spaces along the Central Seafront and the protection of Southend's unique heritage including the nationally important Grade II Listed Southend Pier.	Noted.
Question 27	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2469	Support	Support enhancement of the Pier as national icon and a significant regeneration and enhancement of this key tourist attraction, which at present underperforms both in terms of its attraction and in terms of its visitor experience.	Noted

Question 27	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2089	Support	Support a higher intensification of residential uses in the Central Area together with a broader mix of commercial uses ranging from A1 retail, A3 restaurant café, D2 leisure and B1 offices to provide a more diverse multi-use High Street and Central Seafront.	Noted.
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<p>Question 27</p> <p style="text-align: center;">380</p>	<p>Stockvale Group representing Sands &amp; Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]</p>	<p><b>2093</b></p>	<p><b>Comment</b></p>	<p>Some members of the STOCKVALE GROUP and representatives of the Seafront businesses believe that one way in which the congestion into the Town could be improved is for an additional 3840 parking spaces to be made accessible and available within close proximity to the Seafront and core High Street Area. This is due to the day visitor attraction industry, particularly family attractions such as the Seafront receiving the vast majority of its income in a few weeks of the year. These generally coincide with the school holidays. During this peak period a visitor attraction business needs to be able to accommodate every visitor that wants to visit as these peak days effectively subsidise the operation for the rest of the year.</p> <p>If the access to the main attractions is limited on peak days by the availability of car parking spaces, this could and does have serious impact on the viability of the Seafront businesses. The main parking areas are generally at capacity on peak holiday periods. Any loss of capacity as a result of the SCAAP proposals would result in a cap of visitors during these peak periods. This limits the amount of investment within the Seafront to the current status quo.</p> <p>Transport and access is not just limited to the Seafront and does have a huge impact on the High Street, combined with parking tariffs, access and egress, and poor legibility around the Town Centre. Whilst the changes outlined in the SCAAP from a space and use perspective will do an awful lot to reinvigorate and regenerate the High Street, this must be inclusive of a renewed and fresh approach to parking provision within the SCAAP Area.</p>	<p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
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Question 27	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2101</b>	<b>Comment</b>	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there have been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The STOCKVALE GROUP would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions. No changes are proposed.
Question 27  381	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2103</b>	<b>Support</b>	<p>The STOCKVALE GROUP note the townscape improvements and guidance on design quality and Heritage preservation and enhancement are inextricably linked to improvements to Public Realm and pedestrian connectivity. The STOCKVALE GROUP like the majority of the Town support the continued regeneration and reinvention of the Towns greatest icon Southend's Pleasure Pier. As there are a number of opportunity sites outlined in the SCAAP document, the STOCKVALE GROUP would suggest that the Council (through the SCAAP document) develop design codes and development briefs to ensure that the townscape improvements and quality of design of future developments meet the aspirational high standard to create a coherent and consistent Central Area. This needs to reflect on the Towns Heritage and look towards the future to create Southend as unique place and destination for leisure, shopping, living and working.</p>	Noted. The Plan seeks to achieve this and will be updated to identify a number of proposal sites that could be subject to a masterplanning approach.

<p>Question 27, OS8</p>	<p>Stockvale Group representing Sands &amp; Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]</p>	<p><b>2106</b></p>	<p><b>Comment</b></p>	<p>There is concern that proposals for the Seaway Car Park (OS8) are missing the opportunity to see this as a key gateway site for both the Town and Seafront and an opportunity to provide a greatly enhanced Public Car Park provision as part of the overall site redevelopment.</p>	<p>OS8 recognises that this is a key gateway site and opportunities exist to improve connectivity with the central seafront area. Provisions are included within OS8 to achieve this.</p> <p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
<p>Question 27, OS9</p> <p>382</p>	<p>Stockvale Group representing Sands &amp; Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]</p>	<p><b>2107</b></p>	<p><b>Support</b></p>	<p>In relation to OS9 the STOCKVALE GROUP wholly support the Museum but would seek the inclusion of a public car park which would appear to be feasible as the construction method for creating the Museum would involve extensive ground work, which could utilise the lower levels for a covered car park.</p>	<p>OS9 makes provision for public car parking (Policy CS1). No changes are proposed.</p>



Question 27	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2120	Support	The STOCKVALE GROUP support the Public Realm improvements and further connectivity down through and into the Seafront. The STOCKVALE GROUP recognise that Pier Hill has had a huge success in this regard.	Noted.
Question 27, OS8  383	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2121	Comment	There is a greater opportunity to look at the Seaway site as a Gateway both connecting the High Street around St Johns through Lucy Road and down onto the Seafront. This is a fantastic opportunity that could yield both greatly improved Public Realm, High Street offer and experience and a significant number of residential units.	Noted. The Plan seeks to achieve this (Policy CS1).

Question 27	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2129	Comment	The STOCKVALE GROUP would wish to ensure that the Queensway policy area provides connectivity into the Seaway Car Park and the opportunity to see Seaway as a Gateway to the Seafront and the first visual connection to the Sea is not lost.	Noted. These provisions are identified in Policy CS1.
Question 27 384	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2135	Comment	Due to the topography of the Seaway Car Park there is an opportunity to cut into the site and create a formal entrance that can create a visual gateway as part of the access route. There is an opportunity to accommodate somewhere in the region of 1500 parking spaces arranged over 2-3-4 floors. Traffic movements would then come in directly at the northern edge of the site and filter through into the layered car park. To the south side Lucy Road could then be completely pedestrianised and a punch through to the seafront creating a large piazza activated by new A1, A3 uses to compliment both the Central Seafront and the links into the High Street. The pedestrian link would then improve the environs around St John's church. To drive some additional value it is perfectly legitimate to consider a number of floors of residential uses above the car park and retail/A3 commercial offer.	Noted, Some of these aspects are included in Policy CS1.  The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
Question 27	Southend Bid (Mr S Kearney) [496]	2185	Support	Supports the enhancement and quality of the Green and Open spaces along the Central Seafront and the protection of Southend's unique heritage including the nationally important Grade II Listed Southend Pier.	Noted.
Question 27	Southend Bid (Mr S Kearney) [496]	2186	Support	Support enhancement of the Pier as national icon and a significant regeneration and enhancement of this key tourist attraction, which at present underperforms both in terms of its attraction and in terms of its visitor experience.	Noted.

Question 27	Southend Bid (Mr S Kearney) [496]	2192	Support	Support a higher intensification of residential uses in the Central Area together with a broader mix of commercial uses ranging from A1 retail, A3 restaurant café, D2 leisure and B1 offices to provide a more diverse multi-use High Street and Central Seafront.	Noted.
385 Question 27	Southend Bid (Mr S Kearney) [496]	2196	Comment	<p>Some members of the BID and representatives of the Seafront businesses believe that one way in which the congestion into the Town could be improved is for an additional 3840 parking spaces to be made accessible and available within close proximity to the Seafront and core High Street Area. This is due to the day visitor attraction industry, particularly family attractions such as the Seafront receiving the vast majority of its income in a few weeks of the year. These generally coincide with the school holidays. During this peak period a visitor attraction business needs to be able to accommodate every visitor that wants to visit as these peak days effectively subsidise the operation for the rest of the year.</p> <p>If the access to the main attractions is limited on peak days by the availability of car parking spaces, this could and does have serious impact on the viability of the Seafront businesses. The main parking areas are generally at capacity on peak holiday periods. Any loss of capacity as a result of the SCAAP proposals would result in a cap of visitors during these peak periods. This the amount of investment within the Seafront to the current status quo. Transport and access is not just limited to the Seafront and does have a huge impact on the High Street, combined with parking tariffs, access and egress, and poor legibility around the Town Centre. Whilst the changes outlined in the SCAAP from a space and use perspective will do an awful lot to reinvigorate and regenerate the High Street, this must be inclusive of a renewed and fresh approach to parking provision within the SCAAP Area.</p>	The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.

Question 27	Southend Bid (Mr S Kearney) [496]	2204	Comment	<p>Various improvements have been made to the Central Seafront including improved connectivity through Pier Hill and the City Beach. Whilst there has been some improvements to the landscape of the High Street, this needs a much greater consideration and linking to new development. There needs to be greater inclusion of soft landscaping and public spaces and improved connections from the High Street through to the Seafront. This is particularly the case with Seaway Development. This development site represents a great opportunity to create a gateway development and pedestrian links and improved Public Realm linked to the Seafront.</p> <p>The BID would wish to see a breaking down of the linearity of the High Street and the creation of a number of destination and unique quarters. This resonates particularly through with the Tylers Avenue, London Road and Alexandra and Clarence Street opportunity sites.</p>	Noted. The Plan seeks to achieve these aspects through its policy provisions.
Question 27  386	Southend Bid (Mr S Kearney) [496]	2206	Support	<p>The BID townscape improvements and guidance on design quality and Heritage preservation and enhancement are inextricably linked to improvements to Public Realm and pedestrian connectivity. The BID like the majority of the Town support the continued regeneration and reinvention of the Towns greatest icon Southend's Pleasure Pier. As there are a number of opportunity sites outlined in the SCAAP document, the BID would suggest that the Council (through the SCAAP document) develop design codes and development briefs to ensure that the townscape improvements and quality of design of future developments meet the aspirational high standard to create a coherent and consistent Central Area. This need to reflect on the Towns Heritage and look towards the future to create Southend as unique place and destination for leisure, shopping, living and working.</p>	Noted. The Plan seeks to achieve this and will be updated to identify a number of proposal sites that could be subject to a masterplanning approach.
Question 27	Southend Bid (Mr S Kearney) [496]	2209	Comment	<p>There is concern that proposals for the Seaway Car Park (OS8) are missing the opportunity to see this as a key gateway site for both the Town and Seafront and an opportunity to provide a greatly enhanced Public Car Park provision as part of the overall site redevelopment.</p>	<p>OS8 recognises that this is a key gateway site and opportunities exist to improve connectivity with the central seafront area. Provisions are included within OS8 to achieve this.</p> <p>The Council has commissioned an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>

Question 27	Southend Bid (Mr S Kearney) [496]	2210	Support	In relation to OS9 the STOCKVALE GROUP wholly support the Museum but would seek the inclusion of a public car park which would appear to be feasible as the construction method for creating the Museum would involve extensive ground work, which could utilise the lower levels for a covered car park.	OS9 makes provision for public car parking. No changes are proposed.
Question 27	Southend Bid (Mr S Kearney) [496]	2223	Support	The BID support the Public Realm improvements and further connectivity down through and into the Seafront. The BID recognise that Pier Hill has had a huge success in this regard.	Noted
Question 27	Southend Bid (Mr S Kearney) [496]	2224	Comment	There is a greater opportunity to look at the Seaway site as a Gateway both connecting the High Street around St Johns through Lucy Road and down onto the Seafront. This is a fantastic opportunity that could yield both greatly improved Public Realm, High Street offer and experience and a significant number of residential units.	Noted. The Plan seeks to achieve this (Policy CS1).
Question 27	Southend Bid (Mr S Kearney) [496]	2232	Comment	<b>STOCKVALE GROUP would wish to ensure that the Queensway policy area provides</b> connectivity into the Seaway Car Park and the opportunity to see Seaway as a Gateway to the Seafront and the first visual connection to the Sea <b>is not lost.</b>	Noted. These provisions are identified in Policy CS1.
Question 27	Southend Bid (Mr S Kearney) [496]	2252	Comment	68% of recipients scored the Seafront as a top 10 priority.	Noted.
Question 27	Southend Bid (Mr S Kearney) [496]	2260	Comment	Due to the topography of the Seaway Car Park there is an opportunity to cut into the site and create a formal entrance that can create a visual gateway as part of the access route. There is an opportunity to accommodate somewhere in the region of 1500 parking spaces arranged over 2-3-4 floors. Traffic movements would then come in directly at the northern edge of the site and filter through into the layered car park. To the south side Lucy Road could then be completely pedestrianised and a punch through to the seafront creating a large piazza activated by new A1, A3 uses to compliment both the Central Seafront and the links into the High Street. The pedestrian link would then improve the environs around St John's church. To drive some additional value it is perfectly legitimate to consider a number of floors of residential uses above the car park and retail/A3 commercial offer.	Noted. These aspects are included in Policy CS1.

Question 27, CS1	Mr Rod Levin [497]	2281	Comment	Put 'The Golden Mile' under a high-level cover to provide for inclement weather	Policy CS1 seeks to achieve a whole range of environmental and related improvements to the 'Golden Mile'. It will be an issue of practicability and viability when or whether development proposals come forward. No changes are proposed.
Question 27, CS1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2310	Comment	Valad (Europe) Ltd are concerned about the proposal to produce a development brief in relation to Seaway Car Park and Marine Parade. There is sufficient opportunity to provide an appropriate level of detail in Policy CS1 and avoid the potential delay and uncertainty that may arise if a development brief is now progressed. The submission of an application for its redevelopment should not be delayed a result of a failure to produce a development brief	As Policy CS1 sets out a number of design and layout solutions, and any major development of OS8 will be the subject of detailed consultation, <b>reference to a development brief is to be removed.</b>
Question 27, CS1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2311	Support	Valad (Europe) support the proposed allocation of Opportunity Site 8 on the basis that it proposes a mixed use development that will help to bolster the town centre economy. The indicative phasing for the redevelopment of Opportunity Area 8 is supported but the Council must actively resist developments that would undermine this policy and what it seeks to achieve for the town centre.	Noted.
Question 27, CS1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2312	Comment	Further wording should be provided which states that the redevelopment of the Central Seafront Policy Area will be key to the success of the Southend Central Area Action Plan and that in turn, it will help to improve the vitality and viability of the town centre and sustain existing facilities in the town centre such as The Royals by increasing footfall and linked trips within the town centre.	There is considered to be merit in bringing greater attention to the role of the central seafront area. It is therefore proposed that paragraph 184 (page 111) be amended to read as follows: <b><u>'The Central Seafront Policy Area, as defined on the Policies Map, is a thriving leisure and tourism area. Although there has always been a physical separation of the Central Seafront Policy Area and Town Centre, if access was more straightforward and more pronounced there may be a better exchange of visitors between the Central Seafront and Town Centre and their functions.'</u></b>

Question 27, CS1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2313	Comment	The Council should consider whether the inclusion of retail at Opportunity Site 8 would bring further benefits to the town centre. The success of this development and the subsequent beneficial spin off effects will largely be down to how well the site links in with the town centre.	OS8 is a key development site in the central seafront area. The uses identified in the Plan are considered to be the most appropriate given its location adjacent to tourism and leisure facilities. No changes are proposed.
Question 27, CS1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2314	Comment	The provision of appropriate signage to increase and enhance connectivity between the High Street and the seafront should also be included in the design criteria for both the Policy Area and Opportunity Site 8.	It is recognised that quality signage is essential in the central area. <b>It is therefore proposed that the words 'improve and' are added to Policy CS1 10e so that it would read: 'remove unnecessary street furniture and <u>improve and rationalise</u> signage in accordance with.....'</b> In OS8 add: <b>'h. the provision of appropriate <u>seating, signage and way finding.</u>'</b>
Question 27, CS1	Indigo Planning on behalf of Royals Shopping Centre (Helen McManus) [498]	2315	Comment	There is a real concern that if the revised proposals (yet to be submitted to the Council) for Fossetts Farm are approved, then town centre developments such as that at Seaway Car Park and further town centre investment generally will not go ahead to the detriment of the town centre.	The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study). Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy and require a further retail impact assessment. No changes are proposed.
Question 27, CS1.13.2	Carter Jonas on behalf of Turnstone Southend Ltd (Mr Matt Hare) [503]	2325	Support	Policy CS1.13.2 - Broadly support the proposed policy approach for OS8, but we do suggest some minor changes to the policy wording to Section 13ii in Policy CS1 to ensure the delivery of the development. The proposed development would make more efficient use of previously developed land within the town centre	Noted

Question 27, CS1.13.2	Carter Jonas on behalf of Turnstone Southend Ltd (Mr Matt Hare) [503]	2326	Comment	<p>Policy CS1.13.2 - Delete the requirement for a development brief to be prepared for the site. A planning application is currently being prepared for the Seaway Car Park site. Section 13ii of Policy CS1 already provides sufficient detail to guide the proposed development on the site. The land required to deliver the proposed development at the Seaway Car Park site is controlled by a single developer. A development brief would add unnecessary delay and cost to the proposed development.</p> <p>Suggested Change: <del>This should be taken forward through the preparation of a development brief.</del> Design and layout solutions should allow for:</p>	Reference to the development brief will be removed as it is considered that there is sufficient detailed contained in the OS8 of CS1 and further detailed will be provided at the design stage as part of the planning proposal and be subject to consultation.
Question 27, CS1.13.2	Carter Jonas on behalf of Turnstone Southend Ltd (Mr Matt Hare) [503]	2327	Comment	<p>Policy CS1.13.2 It should be clearly stated that residential development should be located on the Marine Parade site. Planning permission has already been granted for residential development on the Marine Parade site; referred to as Marine Plaza. Residential uses are not proposed within the current scheme on the Seaway Car Park site. It is not clear whether residential uses would be compatible with the proposed leisure uses, and further investigation would be required if residential uses were proposed.</p> <p>Suggested Change:</p> <p>We request the following changes to Section 13ii of Policy CS1:</p> <p>ii Opportunity Site 8: Seaway Car Park and Marine Parade, the Council will pursue with private sector partners, landowners and developers a high quality, mixed use development including the provision of leisure, cultural and tourism attractions including: restaurants, cinema, gallery, hotel, residential development, public and private open spaces, and car parking. The Marine Parade site would provide most of the residential development for the opportunity site.</p>	<p>The final version of the SCAAP will separate OS8 into Seaway Car Park and Marine Parade, with the latter benefitting from an approved planning permission.</p> <p>It is considered that Seaways may be able to provide some residential development and reference to this is considered appropriate to apply flexibility to the policy. The policy wording has been changed to allow for this to be explored. Policy OS8.13.2 will read: <u>'...including the provision of leisure, cultural and tourism attractions including: restaurants, cinema, gallery, hotel, public and private open spaces, and vehicle and cycle parking. The potential for residential development may also be explored. Design and layout solutions should allow for:'</u></p>
Question 27, CS1.13.2	Carter Jonas on behalf of Turnstone Southend Ltd (Mr Matt Hare) [503]	2328	Comment	<p>Policy CS1.13.2 There is a requirement for allocated sites to be deliverable and viable, and as such all parts of a policy should meet those requirements. The delivery of a new link from the Seaway Car Park site to Marine Parade is uncertain, and this should be expressed in the policy.</p> <p>Suggested Change:</p> <p>c. explore opportunities for a new link to Marine Parade from the Seaway site designed around 'Spanish Steps' subject to deliverability and viability;</p>	This part of policy seeks to identify possible innovative design solutions to improving connectivity across this key site between the town centre and seafront and seeks to 'explore opportunities.' Including the words 'subject to deliverability and viability' is considered inappropriate. These matters would be considered as part of the planning application process. No changes proposed.



Question 27, CS1.13.2	Carter Jonas on behalf of Turnstone Southend Ltd (Mr Matt Hare) [503]	2329	Comment	<p>Policy CS1.13.2 It is not clear at this stage where the coach drop-off point would be relocated to, and it could be on or off site or a combination of both, and as such this uncertainty should be expressed in the policy.</p> <p>Suggested Change e. relocation of the coach-drop off point, either on or off-site or a combination of both, following the development of the Seaway site.</p>	<p>The wording of Policy CS1 will be amended to state that relocation of the coach drop off point should be provided on the Seaways site. Coach parking bays may be provided either on or off-site or a combination of both, as long as offsite provision is well connected to the Seaways site and would not significantly adversely impact the local transport network. Policy OS8 13.2 will be amended as follows: <b><u>'relocation of a coach-drop off point within the site. The relocation of coach parking bays may be provided either on or off-site or a combination of both, provided offsite provision is well connected to the Seaways site and would not significantly adversely impact the local transport network;'</u></b></p>
Question 27, CS1.10g  391	Belfairs Garden Residents Association (Barbara Armitage) [511]	2350	Object	<p>Policy CS1.10g - Against proposal 10g to further develop City Beach. Comments have been made about flooding, accidents, risks to pedestrians in non-stopping traffic and no marked and lighted official crossing with blister paving put down where there is no crossing risking the lives of blind people.</p> <p>City Beach - Kerbs are not only a safety zone for pedestrians but help to direct rain water to drains which should be adequate.</p> <p>The seafront shared space is dangerous, has no formal crossings and various accidents have occurred. A proper crossing should be in place and not further extension of any shared space.</p>	<p>The extension of the City Beach scheme is considered to be essential to regenerating the central seafront area and to improving the leisure and tourism offer and environment. Issues such as flooding, road safety and provision for vulnerable road users will need to form an integral part of the design stage of the scheme. No changes proposed.</p> <p><b>Reference to flood mitigation measures will be included in Policy CS1.</b></p> <p><b>Reference to managing the road network safely will be incorporated into Policy DS5.a</b></p>
Question 27, CS1.12.ii.c	Belfairs Garden Residents Association (Barbara Armitage) [511]	2351	Object	<p>Policy CS1.12.ii.c - The 'Spanish Steps ' are a thoroughly dangerous idea for all users and will have to go through property(ies). This should not be pursued.</p>	<p>This is a key gateway site in the town. This part of policy seeks to identify possible innovative design solutions to improving connectivity across this key site between the town centre and seafront and seeks to 'explore opportunities'. It is essential that the design and layout of the site is of the highest quality that enhances the area and takes full advantage of its location and setting. The needs of vulnerable road users will be taken into account at this design stage. No changes are proposed.</p>
Question 27, CS1.12.iii	Belfairs Garden Residents Association (Barbara Armitage) [511]	2352	Object	<p>Policy CS1.12.iii - The Museum is in the wrong place not in tune with the leisure area. If just to shore up the cliffs it should be a leisure building. We do not have the like of the Mary Rose in the town and the thought that people coming down for the day to the beach, pier or lagoon will spend time in a museum is not considered to be sensible.</p>	<p>The museum is one of a number of cultural and leisure uses proposed to be incorporated within the new building. No changes proposed. It is considered that a museum is complimentary to other leisure uses and will provide a valued destination.</p>

Question 27, CS1 & OS8	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2371</b>	<b>Comment</b>	(191) Seaways car park and Marine Parade. We do not believe another cinema is sustainable in Southend	Policy CS1 identifies the potential of the seaway car park site to provide for a mixed use development comprising leisure, cultural and tourism facilities which are considered to be appropriate in this location. The possible inclusion of a cinema is considered to be compatible with providing a mix of leisure uses to enhance the offer on this key site. The Policy also proposes design and layout solutions, for 'urban greening' and seeks to take advantage of the sites elevation with views of the estuary (OS8). No changes are proposed.
Question 27, CS1 & OS9	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2372</b>	<b>Comment</b>	(193) We believe that the Saxon King find should be housed in Prittlewell. It is not appropriate to have this displayed on our seaside tourist area.	The museum is one of a number of cultural and leisure uses proposed to be incorporated within the new building on the seafront, which is considered to be the best location for such a scheme where visitor numbers are at their greatest. No changes proposed.
Question 27, CS1  392	Southend and District Pensioners Campaign (Mr Robert Howes) [476]	<b>2373</b>	<b>Comment</b>	Yes agree with the proposed approach to managing development within the central seafront policy area apart from the above	Noted.

<p>Question 27, CS1 &amp; OS9</p> <p>393</p>	<p>Procuresure Consulting (Mr Barrie Evans) [513]</p>	<p><b>2383</b></p>	<p><b>Support</b></p>	<p>OS9 - Southend-On-Sea museum position on the seafront below Clifftown parade is a good idea in principle but the residents of Clifftown will not allow access to be gained from Clifftown parade and all access to development whether it be by bus coach or car should be via the sea front road and not Clifftown parade, which is a residential road. The Museum should be reduced in height from its current plan to ensure it steps down from the cliff and not in line with the cliff. This drop down would stop the extension that juts out impeding on the Clifftown area, local views and the Victorian design of the area. Any brickwork used in the build should be of high quality and fit with the old red brick of the backdrop houses on the clifftop. The band stand and shelters should be incorporated in to the design of the museum to gently mix new with old and celebrate the heritage as the museum should not only exhibit artefacts and images but the local architecture of the seaside town too. Should the conservation area be extended this would assist in making the whole seafront and town in to a living museum but with a modern function. Remember shabby Chic is the new modern!! If the museum is built then all parking for the museum should be provided by the museum in underground parking (including coaches) and access should be from the seafront and no access at all from Clifftown Parade. This is a residential area and should be protected as such.</p>	<p>The detailed design and layout of any new development at this location will be considered and consulted upon during the planning application process. Policy CS1.13.3 outlines that the design of new development will need to retain the 'open feel' of the area. Policy DM1 of the Development Management Document and the Design and Townscape Guide provides additional design related guidance. In addition the conservation area will be a material consideration. It is recognised that the policy can be further enhanced by outlining that vehicular access of a new development in this location should be via Western Esplanade. Therefore, the following wording is proposed to be added to the policy 13.iii <b><i>'Vehicular access should ensure that the primary road network, i.e. via Western Esplanade, is used to access the development and any new parking facilities.'</i></b></p>
<p>Question 27, CS1 &amp; OS7</p>	<p>Procuresure Consulting (Mr Barrie Evans) [513]</p>	<p><b>2390</b></p>	<p><b>Comment</b></p>	<p>OS7 - The Council should actively seek investment for the pier and include such things as a proper boat marina for Southend, 24 hour access to the pier, ferry access to the pier, quality restaurants and cafes. Why not move the museum to the end of the pier, have yacht club facilities and a purpose built sheltered marina at the end of the pier?</p>	<p>Policy CS1 seeks to provide for a mix of cultural and leisure uses. Any development would need to have regard to the environmental designation on the foreshore. No changes proposed.</p>

Question 27, CS1	Historic England (Dr Natalie Gates) [514]	2412	Comment	<p>Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply.</p> <p>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-pt/</a>. <b><i>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</i></b></p>	<p>This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.</i></b></p> <p>It is proposed to add the words ‘as appropriate’ in paragraph 4 of Policy PA6 so that it reads, <b><i>Promote energy efficiency as appropriate, including.....</i></b></p> <p>This would necessitate similar changes to all other policy areas.</p>
Question 27, CS1	Historic England (Dr Natalie Gates) [514]	2415	Comment	<p><b><i>In the aims of the Central Seafront Policy Area</i></b> we would suggest a small word order change as it currently reads as if the Pier is not an iconic landmark, but will be rejuvenated into one, instead of being the iconic landmark that it is which you are proposing will be rejuvenated.</p>	<p>Agreed. It is therefore proposed to amend the first paragraph of the Aims of the Central Seafront Policy Area so that it reads, <b><i>The Central Seafront will be a thriving and vibrant leisure, cultural and tourism area centred on the iconic Grade II listed Pier which will be rejuvenated as a key local landmark and attraction.</i></b></p>
Question 27, CS1, Para 184	Historic England (Dr Natalie Gates) [514]	2416	Comment	<p>Recommend that paragraph 184 in the supporting text includes conservation areas and listed buildings as specific ‘environmental designations’ as this links through to paragraph1 of Policy CS1 which talks about the impact of proposals on ‘environmental designations’.</p>	<p>Environmental designations cover SSSI, SPA and Ramsar sites. The term is not meant to cover heritage assets, which are covered by Policy CS1.4. However, it is proposed to include reference to conservation areas and listed buildings in paragraph 185 as follows: <b><i>There is a need to strike a balance between the protection and conservation of natural and built assets, including Conservation Areas and listed buildings, with the needs of residents and visitors</i></b></p>
Question 27, CS1.8	Historic England (Dr Natalie Gates) [514]	2417	Support	<p>We welcome paragraph 4 of Policy CS1</p>	<p>Noted.</p>

Question 27, CS1	Environment Agency (Miss Lizzie Griffiths) [334]	2423	Support	We are very supportive of this policy. Point 5, in particular, is very positive, as it recognises the opportunity that new development provides for integrating tidal defences into the public realm.	Noted.
Question 27, CS1	Environment Agency (Miss Lizzie Griffiths) [334]	2424	Comment	Point 7 could potentially be strengthened by allowing development south of the sea wall only by exception and where the proposed land use is deemed to be 'water compatible' as defined in the Planning Practice Guidance.	Noted, therefore the following amendment is proposed: <b><i>'Not normally permit development south of the seawall. Any proposed use will also have to be water compatible as defined in the Planning Practice Guidance.'</i></b>
Question 27, CS1	National Federation for the Blind (Mrs Jill Allen-King) [516]	2454	Comment	Up to 1970 we had a direct bus route from Southend Victoria Railway station down the High street, down Pier Hill to and along the sea front. Unfortunately engineers of the day ignored our access committee and went along with their plans of pedestrianising the high street and cutting off the sea front from the high street restricting hundreds of people getting to the shops by bus.	Policy, as part of the sustainable approach to transport, seeks to improve the provision for public transport users and provides for bus priority measures. Specific bus routes are considered as part of on-going partnership working with bus operators.
Question 27, CS1 395	National Federation for the Blind (Mrs Jill Allen-King) [516]	2455	Comment	OS8 - Spanish steps will certainly stop many disabled people from getting to the sea front. Spanish steps are not accessible and should not be used.	The provision of 'spanish steps' is part of an innovative design approach to the site. The needs of vulnerable users to access and cross the site will also be taken into account at the design stage of any scheme.
Question 27, CS1	National Federation for the Blind (Mrs Jill Allen-King) [516]	2456	Comment	The City Beach scheme was built without proper consultation and did not take into account the needs of disabled people. The Courtesy crossings are not legal crossings and cannot be used safely by blind people. The whole area should be reinstated to a proper road with kerbs and proper pedestrian crossings, with audible signals and tactile markings. The City Beach scheme should not be extended.	Further phases of the City Beach scheme will consider the needs of all users and be subject to public consultation.
Question 27, CS1	National Federation for the Blind (Mrs Jill Allen-King) [516]	2457	Comment	A bus service should be established from the Kursaal to Chalkwell along the sea front.	Policy, as part of the sustainable approach to transport, seeks to improve the provision for public transport users and provides for bus priority measures. Specific bus routes are considered as part of on-going partnership working with bus operators.

Question 27, CS1	National Federation for the Blind (Mrs Jill Allen-King) [516]	2458	Comment	OS9 - The proposed new Museum will not be able to be visited by non car drivers if you do not have a bus service, which is discrimination.	Policy, as part of the sustainable approach to transport, seeks to improve the provision for public transport users and provides for bus priority measures. Specific bus routes are considered as part of on-going partnership working with bus operators.
Question 27, CS1	National Federation for the Blind (Mrs Jill Allen-King) [516]	2459	Comment	Currently there is no cycle route at City Beach and cycles ride illegally on the foot path. A cycle route should be built the whole length of the sea front on the road and not on the footway. The cyclists should stop at pedestrian crossings.	This area has been established as a shared walking and cycling route. Cycle provision forms part of the shared space in the central seafront area.
Question 27, CS1	National Federation for the Blind (Mrs Jill Allen-King) [516]	2460	Comment	There should be more public toilets, none are listed.	Noted. Toilets and related facilities will be considered at the design stage of any redevelopment scheme and through on-going review of current provision.

<p>Question 27, CS1</p> <p>397</p>	<p>National Federation for the Blind (Mrs Jill Allen-King) [516]</p>	<p>2461</p>	<p><b>Comment</b></p>	<p>In the central seafront policy area there should also be parking for disabled people at frequent intervals.</p>	<p>Site occupiers with reference to national parking guidance and legislation are responsible for providing an adequate number of spaces for people with disabilities.</p> <p>The SCAAP seeks to promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre and access to the seafront by encouraging improvements to the quality of access to parking so that it is convenient, well-signposted, safe and secure.</p> <p>It is considered that reference to a range of parking types, including for disabled people, should be made within Policy DS5.</p>
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<p style="text-align: center;">398</p> <p>Question 28, Policy CS2</p>	<p>Natural England (Mr Gordon Wyatt) [264]</p>	<p><b>2032</b></p>	<p><b>Comment</b></p>	<p><i>Wording of Policy not considered accurate and it is suggested that Policy CS2.1, is amended to read as follows:</i></p> <p><i>“1. Ensure that all development proposals within the Central Seafront Area are accompanied by a Habitats Regulations Assessment and associated documentation to ensure there will be no adverse effect on the European and International foreshore designations (SPA and Ramsar) either alone or in combination with other plans or projects;”</i></p> <p>The Habitats Directive requires competent authorities to decide whether or not a plan or project can proceed having undertaken the following "appropriate assessment requirements" to:</p> <ol style="list-style-type: none"> <li>1. Determine whether a plan or project may have a significant effect on a European site, either alone or in combination;</li> <li>2. If required (ie when there is a likely significant effect), undertake an appropriate assessment of the plan or project;</li> <li>3. Decide whether there may be an adverse effect on the integrity of the European site in light of the appropriate assessment.</li> </ol> <p>This whole process is generally referred to as Habitats Regulations Assessment (HRA).</p> <p>The responsibility for carrying out a HRA rests squarely upon the decision-making competent authority; except insofar as it may be appropriate for the competent authority to adopt the reasoning or conclusions of another competent authority as to whether a plan or project is likely to have a significant effect on a European site, or will adversely affect the integrity of a European site. The Regulations transposing the Habitats Directive also provide that a competent authority is not required to assess any implications of a plan or project that would be more appropriately assessed by another competent authority. Planning applications are often accompanied by a document which is described as being a HRA; however such a document produced by or on behalf of an applicant does not have any legal weight and is therefore sometimes referred to as a 'shadow HRA'. As the competent authority, it remains Southend-on-Sea Borough Council's responsibility to produce the definitive HRA; either by adopting an applicant's 'shadow HRA', or by carrying out its own HRA. Where a competent authority chooses to carry out its own HRA, it will normally require the applicant to provide the necessary background information to support the assessment process.</p>	<p>Noted. It is proposed to amend the wording of Policy CS2 point 1 to read: <b><i>‘Ensure that all development proposals within the Central Seafront Area are accompanied by a Habitats Regulations Assessment and associated documentation to ensure there will be no adverse effect on the European and International foreshore designations (SPA and Ramsar) either alone or in combination with other plans or projects.’</i></b></p>
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Question 28, CS2.3	Belfairs Garden Residents Association (Barbara Armitage) [511]	2353	Comment	Policy CS2.3 - Under what circumstances could there be development and what is the 'public interest' that could make this possible.	Policy CS2.3 provides for exceptional circumstances where exceptions may be made if there are no alternative solutions or the reasons for the development clearly outweigh the nature conservation value of the site and is in the public interest. Potential flood defence improvements may be an example of where this might apply. No changes proposed.
Question 28, CS2.6	Belfairs Garden Residents Association (Barbara Armitage) [511]	2354	Comment	Policy CS2.6 - Would want to know more about 6 and what kind of high quality visitor facility is envisaged	Such a facility would be subject to a planning application and wider publicity where more detailed information will be available. No changes proposed.
Question 28 399	Environment Agency (Miss Lizzie Griffiths) [334]	2425	Support	We are supportive of this policy which seeks to relieve pressures on the seafront area. We support the idea of drawing people to the waterfront, especially where it may help to raise awareness of the ecosystems and their importance, provided the sensitive areas themselves are protected.	Noted.
<b>The Waterfront</b>					
Question 29, CS3	Essex Chambers of Commerce (Mr John Dallaway) [452]	1956	Support	Agree with the proposed approach to managing the Waterfront	Noted.
Question 29, CS3	Mr Rod Levin [497]	2287	Comment	Provide public Slipways over beach to promote use of small (sail) boats.	Public slipways are provided along the foreshore. The Plan (Policy CS3) seeks to improve such facilities as appropriate. No changes are proposed.
Question 29, CS3	Belfairs Garden Residents Association (Barbara Armitage) [511]	2355	Comment	Similar to above in that it appears to open the possibility of development which could be manipulated. Transparency will be required in both nature conservation and the waterfront.	The policy wording is considered to achieve an appropriate balance between protecting the waterfront whilst seeking to provide improved leisure facilities. No changes proposed.

Question 29, CS3	Procurement Consulting (Mr Barrie Evans) [513]	2391	Comment	People come to the sea side to sit on the beach so create more sand beaches. Yes there are small stretches that have been rejuvenated, however a lot more of the water front and shoreline need to be more accessible and enlarged, with the spits rebuilt and sand infilled to create beaches the length of the seafront; yes there are natural sites of interest and beach expansion can still happen if managed correctly. Reclaimed beaches across the world are the main success of any area. Example being the new beaches in Gibraltar which were introduced with sheltered sea walls has been the main success of those areas. The beach rather than the seafront road is the most important part of Southend when it comes to attracting tourism and local well-being and fitness.	Policy CS3 promotes the beach and foreshore for appropriate cultural, leisure and tourism activities. Specific proposals for the maintenance of the beach and foreshore is implemented through other Council services. No changes proposed.
Question 29, CS3	Environment Agency (Miss Lizzie Griffiths) [334]	2426	Support	We support this policy which seeks to integrate tourism activities and recreation with the public realm and biodiversity features. We are particularly supportive of Point 2, which seems to prevent any impacts on biodiversity or flood risk.	Noted.
<b>Victoria Gateway Neighbourhood Policy Area – Policy PA8, Opportunity Site 11, 12 and 13</b>					
Question 30, PA8 400	Essex Chambers of Commerce (Mr John Dallaway) [452]	1957	Support	Agree with the proposed approach to managing development within the Victoria Avenue Gateway Neighbourhood Policy Area	Noted.
Question 30, PA8	London Southend Airport (Ms Jo Marchetti) [471]	1969	Support	We support the regeneration of this area as it is the entrance point to Central Southend from the airport by road. The area currently is not appealing and is run down. The airport is keen to push inbound passengers into the town before heading to London and this area needs to be more attractive in appearance in order for us to market Southend's tourism sites.	Noted.
Question 30	Rev Phyllis Owen [456]	1976	Comment	Whilst I agree these are good objectives, the reality is that people want to use cars. I am very concerned that the number of additional dwellings proposed in my area (OS11 and OS12 and Roots Hall) will lead to even more cars looking for places to park, increasing the problems that already exist in these area. I have raised this point when previous plans for Victoria Avenue have been raised and have never received an adequate or indeed any response. I feel very strongly that existing residents will be greatly disadvantaged.	Development Proposals that come forward in the SCAAP area will have regard to the Councils parking standards set out in the Development Management Document. These have been found sound by a planning inspector and subsequently adopted.

Question 30, PA8	Burges Estates Residents Association [176]	2007	Comment	In the same way the Victoria Gateway policy principles contains reference to "promoting energy efficiency". Why is this policy area singled out? Why is that not one of a wide range of common policies applicable across the board?	All policy areas include reference to promoting energy efficiency.
Question 30, PA8	Burges Estates Residents Association [176]	2008	Comment	I have already mentioned that the church should be included in the area and that makes sense in the context of policy DP8: 7g.	St Marys Church is referenced in Policy DS2 – Key views and Policy DS3 – Landmarks and Landmark Buildings, and therefore development or infrastructure proposals that are likely to impact on the church will be required to have regard to the policy criteria contained within these policies. No change proposed.
Question 30, PA8	Anglian Water (Sue Bull) [37]	2025	Comment	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.
Question 30, PA8	Capita Property Infrastructure On behalf of Genesis Housing [465]	2029	Comment	The SCAAP preferred approach is supported. However, Capita P&I and Genesis consider that the OS11 site does not extend far enough, and that the adjacent Genesis site at Baxter Avenue should be incorporated within the OS11 site boundary. There are several reasons as to why, these are all explored in the supporting document. These considerations are: <ul style="list-style-type: none"> <li>• The overall shortfall in housing supply and how the development of the site can help deliver the target;</li> <li>• The policy compliance of the proposal;</li> <li>• The removal of low quality housing;</li> <li>• Given the area of the site, a coherent regeneration masterplan approach should be adopted in accordance with OS11;</li> <li>• The site is well positioned on an access vista and therefore well located for a housing led regeneration initiative.</li> </ul> The site is available, achievable and deliverable. The redevelopment of the site would allow for the residential density of the site to be optimised, whilst also providing a quality mixed use development with active frontages.	The Council is considering inclusion of this site as an opportunity site. This will be assessed against information supplied by the owners of the site to demonstrate that development is deliverable by 2021.

Question 30	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2108</b>	<b>Support</b>	Wholeheartedly support the improvement on Victoria Avenue as a gateway in to the Town. The STOCKVALE GROUP recognises that much of this work is already underway with the on-going redevelopment of Heath and Carby House.	Noted. The Victoria Avenue/ Queensway junction has benefitted from significant public realm and access improvements as part as the implemented Victoria Gateway Scheme. However, it is considered that policy should still seek further improvements to the public realm and accessibility.
Question 30 402	Southend Bid (Mr S Kearney) [496]	<b>2147</b>	<b>Object</b>	The proposed redevelopment of Roots Hall and Roots Hall Stadium are predicated on the Football Club relocating to Fossetts Farm with a significant volume of retail use. Whilst the redevelopment of these sites is supported the retail use and volume at Fossetts Farm would see the end of the High Street as a retail offer. The BID most strongly opposes the Fossetts Farm proposals and any movement of retail away from the SCAAP area and Town Centre.	The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact was taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study).  Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy and require a further retail impact assessment. No changes are proposed.
Question 30	Southend Bid (Mr S Kearney) [496]	<b>2259</b>	<b>Comment</b>	Victoria Neighbourhood was scored by 22% of respondents as a top 10 priority.	Noted.
Question 30	Southend Bid (Mr S Kearney) [496]	<b>2262</b>	<b>Comment</b>	Whilst the BID do not object or have any particular concern regarding the moving of the Southend Football Club the move is predicted on the suggested development requirement to combine a significant number of retail outlets. This is being presented as a financial necessity to allow the Club to move to new premises, however, if this is supported many if not all the High Street chains are likely to follow.	Noted.

<p>Question 30, OS13</p> <p>403</p>	<p>Southend Bid (Mr S Kearney) [496]</p>	<p><b>2270</b></p>	<p><b>Object</b></p>	<p>Another major issue for the BID is the opportunity site (OS13) re-development of Roots Hall, and Roots Hall Stadium. There is extreme concern that the redevelopment of these sites is predicated on Southend Football Club moving out to Fossetts Farm and the supposed enabling development to allow this to happen, which consists of a large quantum of A1 retail units. As highlighted earlier in this representation the retail proposals at Fossetts Farm would be fatal to Southend's retail offer in the High Street and the aspirations for a vast improvement to the retail provision in the SCAAP area.</p>	<p>Noted. Opportunity Site 13 and will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.</p> <p>Comments in relation to the sites will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021. Nevertheless the development principles as set out in Policy PA8, in combination with other adopted local policy and guidance, will provide the necessary framework to guide development if a proposal was to come forward.</p> <p>The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study). Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy and require a sequential test and further retail impact assessment. No changes are proposed.</p>
<p>Question 30 PA8, OS13</p>	<p>Mr Alan Grubb [59]</p>	<p><b>2290</b></p>	<p><b>Comment</b></p>	<p>OS13 - As the parking situation in the ladder roads which connect Fairfax Drive with West Road/Westborough Road is chronic, if planning permission is granted for additional housing on the Roots Hall site, the parking provision on the site does need if possible to be increased by 15% above the normal requirements.</p>	<p>Any planning application on the Roots Hall site would be determined in accordance with adopted car parking standards as set out in the Development Management Document. No changes are proposed.</p>
<p>Question 30 PA8, OS13</p>	<p>Mr Alan Grubb [59]</p>	<p><b>2291</b></p>	<p><b>Comment</b></p>	<p>OS13 - The site used by Prospects Collage located next to Roots Hall was to become part of the Sainsbury development. There is a strong possibility that if Sainsbury do not buy the site, the site will be used for additional housing development. Last year the company Lidi wanted to buy the Prospect site to build a Lidi store. As the site also includes a car park, in my view this development would have been ideally suited for this location, providing employment and services to the local community which would also include the new housing development on the Roots Hall Site. Southend Council should re-engage with Lidi in order to reach a successful conclusion.</p>	<p>OS 13 provides for the development of a mixed use scheme which may include a retail outlet. No changes are proposed.</p> <p>Opportunity Site 13 and will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.</p> <p>Comments in relation to the sites will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021.</p>

Question 30, PA8, OS11	Mr Alan Grubb [59]	2292	Comment	OS11 – <i>With reference to the redevelopment of the</i> old college site, located next to the Civic Centre, new homes <i>proposed in</i> Victoria Avenue old offices (Heath House and Carby House), parking of vehicles will be a problem unless the developers of large developments are forced to provide parking 15% in excess of the developments requirements. There is also a need for more Social Housing, Houses not Flats. I understand that additional housing in the High Street and possible Elmer Approach is also being contemplated.	Any planning application on these sites will be determined in accordance with adopted car parking standards. The provision of social housing will be sought in accordance with planning policy as appropriate. No changes are proposed.
Question 30 PA8	Mr Alan Grubb [59]	2293	Comment	While I accept the need for additional housing, there does need to be provision for additional school places and a location identified for a new primary school. To this effect I did suggest that serious consideration should be given to the building of a primary school on the old goods yard site at Prittlewell Rail Station. If a school is built on this site, the school would serve the population (children) who would be living in the new housing estates mentioned above and below and in close proximity to all of the proposed developments. Any new school must be built at the same time as the new developments are converted or built.	In terms of education provision the Plan considers that the planned population growth in the central area will be accommodated through the expansion of existing schools. However, it is recognised that in the longer term there may be a need for additional schools and this will be kept under review – see also Southend Infrastructure Delivery Plan. No changes are proposed.
Question 30, PA8.2 404	Belfairs Garden Residents Association (Barbara Armitage) [511]	2356	Comment	Policy PA8.2 - Concerns that <i>use of</i> backland for 'lanes' <i>type development</i> might occupy car park areas needed for the redevelopment of the substantial buildings in this area into residential and business units <i>forcing</i> cars on to the nearby streets causing problems.	Car parking provision will be taken into account in the detailed design and planning application stage of any scheme. No changes proposed.
Question 30, PA8.8g	Belfairs Garden Residents Association (Barbara Armitage) [511]	2357	Comment	Policy PA8.8g - This is a sensitive junction with an historic building and St, Mary's churchyard. While it will be the Council's intention to take care with any design and appointment of contractors some reassurance perhaps in the press would save questions from local people.	Noted.



Question 30, PA8	National Federation for the Blind (Mrs Jill Allen-King) [516]	2464	Comment	There is no mention of public toilets or parking for disabled people in the <b>Victoria Gateway area</b> , why not?	These issues would be considered as part of the detailed design of any planning application.
<b>Sutton Gateway Neighbourhood Policy Area – Policy PA9, Sites PA9.1, PA9.2, PA9.3 and Opportunity Site 14</b>					
Question 31, PA9	Essex Chambers of Commerce (Mr John Dallaway) [452]	1958	Support	Agree with the proposed approach to managing development within the Sutton Gateway Neighbourhood Policy Area	Noted.
Question 31, PA9	The Co-Operative Group (Mr A Thompson) [473]	1972	Object	The Co-operative Group would wish to see the inclusion of land at 53-57 Sutton Road Southend within the SCAAP as an additional Opportunity Site.	The Council is considering inclusion of this site as an opportunity site. This will be assessed against information supplied by the owners of the site to demonstrate that development is deliverable by 2021.
Question 31, PA9 906	Anglian Water (Sue Bull) [37]	2026	Comment	We would recommend within each Policy Area a requirement is stipulated that the necessary sewerage infrastructure is in place to serve the area before development progresses.	Refer to Rep. 2017.  Reference to this has been included within section 4.12 Infrastructure Provision. As it is a statutory undertaking it is not considered necessary to repeat it in planning policy.  Noted. Additional supporting is proposed to be included in 4.12 Infrastructure Provision, referencing the need for developers to make provision for the foul sewerage network.





Question 31	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	<b>2134</b>	<b>Support</b>	Support the Councils aspiration but have no further comments to make in relation to the Sutton Neighbourhood. The STOCKVALE GROUP also support the improvements to connect the Sutton Neighbourhood into the Town Centre, improve connections to key public transport nodes and retail and employment areas.	Noted.
Question 31	Southend Bid (Mr S Kearney) [496]	<b>2237</b>	<b>Support</b>	Support the Councils aspiration but have <b>no</b> further comments to make in relation to the Sutton Neighbourhood. The BID also support the improvements to connect the Sutton Neighbourhood into the Town Centre, improve connections to key public transport nodes and retail and employment areas.	Noted.
417 Question 31	Southend Bid (Mr S Kearney) [496]	<b>2258</b>	<b>Comment</b>	Sutton Neighbourhood was scored by 22% of respondents as a top 10 priority.	Noted.

Question 31, PA9	Historic England (Dr Natalie Gates) [514]	2414	Comment	<p>Paragraph 4 of Policy PA6 relates to energy efficiency and relates to existing development. We note that versions of this paragraph appear under other policy areas and opportunity sites, so the following comments also apply.</p> <p>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Regulations where compliance would unacceptably alter their character and appearance. Special considerations under Part L are also given to locally listed buildings, buildings of architectural and historic interest within registered parks and gardens and the curtilages of scheduled monuments, and buildings of traditional construction with permeable fabric that both absorbs and readily allows the evaporation of moisture. Any policy encouraging energy efficiency should note that the application will be different in relation to these classes of buildings. Further information can be found in the Historic England advice Energy Efficiency and Historic Buildings –Application of Part L of the Building Regulations to historically and traditionally constructed buildings <a href="https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-ptl/">https://historicengland.org.uk/images-books/publications/energy-efficiency-historic-buildings-ptl/</a>. <b>This similarly applies to Policy PA1, PA2, PA3, PA4, PA5, PA7, CS1, PA8 and PA9.</b></p>	<p>This point is noted. It is therefore proposed to reference this in paragraph 85 as follows: <b><i>Listed buildings, buildings in conservation areas and scheduled monuments are exempted from the need to comply with energy efficiency requirements of the Building Regulations where compliance would unacceptably alter their character and appearance.</i></b></p> <p>It is proposed to add the words ‘as appropriate’ in paragraph 4 of Policy PA6 so that it reads, <b><i>Promote energy efficiency as appropriate, including.....</i></b></p> <p>This would necessitate similar changes to all other policy areas.</p>
Question 31 808	National Federation for the Blind (Mrs Jill Allen-King) [516]	2465	Comment	<p><b>In the Sutton Gateway neighbourhood this</b> whole area is not very clear about open spaces and I would like to know what this means.</p>	<p>This refers to the Opportunity Site 14 at Sutton Road and the potential to incorporate open space within the development scheme.</p>
<b>Part D: Implementation and Monitoring Framework</b>					
<b>Development Phasing</b>					
Question 32	Essex Chambers of Commerce (Mr John Dallaway) [452]	1959	Support	<p>Agree with the indicative phasing of development within the SCAAP area</p>	<p>Noted.</p>

Question 32	The Co-Operative Group (Mr A Thompson) [473]	1973	<b>Object</b>	The Co-operative Group would wish to see the inclusion of land at 53-57 Sutton Road Southend within the SCAAP as an additional Opportunity Site.	The Council is considering inclusion of this site as an opportunity site. This will be assessed against information supplied by the owners of the site to demonstrate that development is deliverable by 2021.
Question 32	Capitia Property Infrastructure On behalf of Genesis Housing [465]	2031	<b>Comment</b>	<p>The SCAAP preferred approach is supported. However, Capita P&amp;I and Genesis consider that the OS11 site does not extend far enough, and that the adjacent Genesis site at Baxter Avenue should be incorporated within the OS11 site boundary. There are several reasons as to why, these are all explored in the supporting document. These considerations are:</p> <ul style="list-style-type: none"> <li>• The overall shortfall in housing supply and how the development of the site can help deliver the target;</li> <li>• The policy compliance of the proposal;</li> <li>• The removal of low quality housing;</li> <li>• Given the area of the site, a coherent regeneration masterplan approach should be adopted in accordance with OS11;</li> <li>• The site is well positioned on an access vista and therefore well located for a housing led regeneration initiative.</li> </ul> <p>The site is available, achievable and deliverable. The redevelopment of the site would allow for the residential density of the site to be optimised, whilst also providing a quality mixed use development with active frontages.</p>	The Council is considering inclusion of this site as an opportunity site. This will be assessed against information supplied by the owners of the site to demonstrate that development is deliverable by 2021.

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#### Indicative Figures for SCAAP Potential New Developments

Question 33	Essex Chambers of Commerce (Mr John Dallaway) [452]	1960	<b>Support</b>	Believe it to be useful to include indicative figures for potential new development to enable private sector partners to fully appreciate the level of commitment required which will help to facilitate partnership working and delivery	Noted.
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#### Implementation – Projects and Tasks

Question 34	Essex Chambers of Commerce (Mr John Dallaway) [452]	1961	Support	Agree. Please see response to question 33 <i>'Believe it to be useful to include indicative figures for potential new development to enable private sector partners to fully appreciate the level of commitment required which will help to facilitate partnership working and delivery'.</i>	Noted.
<b>Implementation - Approach</b>					
Question 35	Essex Chambers of Commerce (Mr John Dallaway) [452]	1962	Support	Agree with overall approach for the Implementation Plan	Noted.
Question 35	Belfairs Garden Residents Association (Barbara Armitage) [511]	2358	Comment	Regular updates on progress of projects would be valuable. The Council needs to be in control of its own plan and not be unduly influenced by the objectives of partners.	Progress is regularly provided in Annual Monitoring Reports.
<b>SCAAP Monitoring Framework</b>					
Question 36	Essex Chambers of Commerce (Mr John Dallaway) [452]	1963	Support	Agree with Monitoring Framework	Noted.
<b>General and Further Comments</b>					
Question 37	Essex Chambers of Commerce (Mr John Dallaway) [452]	1964	Support	No further comments	Noted.

Question 37	London Southend Airport (Ms Jo Marchetti) [471]	1970	Support	Overall LSA strongly supports the redevelopment of the areas outlined in the SCAAP documents and is pleased to see that improvements are planned for the area which will in turn make it easier to attract inbound visitors.	Noted.
Question 37	Milton Conservation Society (Mr Andy Atkinson) [488]	1981	Comment	Town centre planning in Southend (as in many other towns it has to be acknowledged) has been little short of disastrous since the war with only odd examples of good buildings. The last people to build to a consistently high quality were the Edwardians, saying very little for modern town planning. If we can adopt the right, aggregated approach with genuinely good quality architecture we might start again to build a long term high quality, human town centre where the best retailers want to participate, mixed uses can succeed and the town might earn the thriving city status it so wants.	The SCAAP seeks to promote design excellence and good quality development proposals and public realm improvements to reinforce a distinctive sense of place. The importance of high quality, innovative design is also set out within the Development Management Document Policy DM1 and further guidance contained within the Design and Townscape Guide SPD. No changes are proposed.
Question 37 411	Ms Lise Hodgson [467]	1986	Comment	Finally I would ask the Council to be more open with your plans. When I bought my flat my solicitor did the usual searches and got told there were no plans for the area. A few months later the first plans for Seaway were published. I do not believe these were drawn up in such a short time. I know the Council does not have a legal obligation to reveal plans, but surely you have a MORAL obligation so that people can make the right decision where to live. Had I known about the Council's plans for this area I would have saved myself the heartache I'm going through now and not bought the flat.	The SCAAP has been subject to extensive public consultation since 2007. The development potential of Seaway Car Park has been recognised for a number of years and was identified in earlier iterations of the Plan and other plans (adopted Borough Local Plan, March 1994). No changes are proposed.
Question 37 Part A, Strategic Planning Context	Burges Estates Residents Association [176]	1989	Comment	Reference is made to work being jointly undertaken to establish jobs and housing need. Jointly with whom? And is <i>there a</i> need to review the core strategy in the light of that further work. What timescales are we talking about? To what extent will anyone be able place any reliance on the SCAAP knowing it is so very tentative?	The SCAAP seeks to deliver the remaining growth targets for the town centre and central area set out in the Core Strategy by 2021. Paragraph 7 of the SCAAP explains the preparation of a new Southend Local Plan. It will replace the existing Core Strategy and include a review of the SCAAP. Evidence on housing and economic need is being prepared by the south Essex authorities.
Question 37 Context G	Burges Estate Residents Association [176]	1991	Comment	On page 7 there is reference to successful recent public realm and access improvement schemes. Some examples would be useful because I cannot think of them.	Such schemes include City Beach and Victoria Gateway. It is not considered necessary to reference these in the context and issues section.

Question 37	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2066	Comment	Changes in consumer behaviour, the growth in car ownership and its impact on accessibility of in and out of Town Centre shopping are reasonably well understood. This is particularly likely to be an issue should the Council grant the Fossetts Farm application to subsequently increase a retail offer in an out of town centre location.	Noted.
Question 37  412	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2068	Comment	The growth of out of town shopping centre has been widely blamed for Town Centre decline and planning policies have attempted to restrict this growth, which the Stockvale Group wish to see reflected in the Council's determination of planning applications at Fossetts Farm. (The outcome of Planning Applications for extensive retail at Fossetts Farm will determine the value of the Council progressing the SCAAP process. If Fossetts Farm retail development is approved the Stockvale Group feel the SCAAPs aspirations will be undeliverable.	The Fossetts Farm proposals are located outside the SCAAP boundary. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact was taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study). Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy provisions and require a further retail impact assessment. No changes are proposed.

Question 37	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2168	Comment	Changes in consumer behaviour, the growth in car ownership and its impact on accessibility of in and out of Town Centre shopping are reasonably well understood. This is particularly likely to be an issue should the Council grant the Fossetts Farm application to subsequently increase a retail offer in an out of town centre location.	Noted.
Question 37 413	Stockvale Group representing Sands & Southend Radio, Three Shells, Pavilion, Adventure Island, Adventure Sealife (Mr S Kearney) [483]	2170	Comment	The growth of out of town shopping centre has been widely blamed for Town Centre decline and planning policies have attempted to restrict this growth, which the Stockvale Group wish to see reflected in the Council's determination of planning applications at Fossetts Farm. (The outcome of Planning Applications for extensive retail at Fossetts Farm will determine the value of the Council progressing the SCAAP process. If Fossetts Farm retail development is approved the Stockvale Group feel the SCAAPs aspirations will be undeliverable).	<p>The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact was taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study).</p> <p>Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy and require a further retail impact assessment. No changes are proposed.</p>

Question 37	Southend Bid (Mr S Kearney) [496]	<b>2264</b>	<b>Object</b>	<p>In relation to the Fossetts Farm development. Proposals to have a large quantum of A1 retail provision would have a major impact on the Town Centre which is highly likely to lead to a further decline of an already struggling retail offer within the High Street and surrounding environs. Furthermore, the highway connection and infrastructure would not support the level of traffic journeys that the proposals at Roots Hall are likely to generate.</p> <p>The BID would ask that the Council ensure that in accordance with advice in the National Planning Policy Framework (NPPF) a sequential test is undertaken and would like to be informed of the conclusions in relation to the impact on the vitality and viability of the Town Centre. The BID are rightly concerned that the Fossetts Farm proposals will have negative impact on the future of the High Street and the existing retail economy of the SCAAP area.</p>	<p>The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study).</p> <p>Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy and require a sequential test and further retail impact assessment. No changes are proposed.</p>
Question 37	Mr Rod Levin [497]	<b>2272</b>	<b>Comment</b>	Ensure that all local parks have sufficient toilet facilities	Toilet provision is administered and maintained through associated Council services and will not be detailed within the SCAAP. No changes are proposed.
Question 37	Mr Rod Levin [497]	<b>2273</b>	<b>Comment</b>	In particular to insist that the Tea-shop / Café in Southchurch park is regularly cleaned, re-painted and that an appropriate menu is available.	This is outside the plan area.
Question 37	Mr Rod Levin [497]	<b>2274</b>	<b>Comment</b>	Maintain the Free Bus passes for elderly residents	This is not a planning matter.
Question 37	Mr Rod Levin [497]	<b>2275</b>	<b>Comment</b>	Ensure there are Police available to respond to incidents 24/7	This is not a planning matter.
Question 37	Mr Rod Levin [497]	<b>2470</b>	<b>Comment</b>	Crack down on crime	The Plan in association with other local planning policy seeks to achieve quality design in new developments to design out crime, to maintain and upgrade CCTV provision where appropriate. No changes are proposed.
Question 37	Mr Rod Levin [497]	<b>2277</b>	<b>Comment</b>	Educate people Out of Spitting in public	This is not a planning matter.
Question 37	Mr Rod Levin [497]	<b>2278</b>	<b>Comment</b>	Action heavily on people for not picking-up their dog's Faeces	This is not a planning matter.
Question 37	Mr Rod Levin [497]	<b>2282</b>	<b>Comment</b>	Ensure local Tax-breaks, of Real Value, to attract New / Start-up businesses	This is not a planning matter.
Question 37	Mr Rod Levin [497]	<b>2283</b>	<b>Comment</b>	Provide for local residents to travel to Pier head Free or Half-price	Pricing of leisure and tourism facilities is not a planning issue.
Question 37	Mr Rod Levin [497]	<b>2288</b>	<b>Comment</b>	Widely advertise Air routes from Southend Airport	Outside the Plan area and not a planning matter.



Question 37	Mr Rod Levin [497]	2289	Comment	Review plans for Old Leigh. It has the potential for a First Rate Marina and Pleasure-land with residential and Hotel accommodation. Develop as 'Oldie-World'.	This is outside the plan area.
Question 37	Mr Paul Bethell [499]	2316	Comment	It is far too technical for most people not involved in planning to understand	A non-technical summary document was published along with the Preferred Approach version. A similar document will be published with the Proposed Submission Plan.
Question 37	Belfairs Garden Residents Association (Barbara Armitage) [511]	2359	Comment	The demographics have not been considered. There are statistics to show that the proportion of older people in Southend will increase. With age these people will become less able or mobile and therefore it is unlikely that the emphasis on walking or cycling will be viable. The plan is based on more active folk to the detriment of those who are living longer and still expect a reasonable quality of life.	It is considered that the Plan adequately addresses the needs of all road users. No changes proposed.
Question 37 415	Procurement Consulting (Mr Barrie Evans) [513]	2374	Comment	This process has not been made user friendly at all and it even appears to have been made deliberately complex so that the general public get lost in legal jargon and policy grammar. You will not gain a real sense of what Southend people want or need through a complex series of download PDFs and this form!	Public consultation has been carried out throughout the Plan making process and every effort has been made to make the documents as 'user friendly' as possible. A non-technical summary document was published along with the Preferred Approach version. A similar document will be published with the Proposed Submission Plan.
Question 37	Procurement Consulting (Mr Barrie Evans) [513]	2384	Comment	Southend-On-Sea Council need to draw large companies out of London and encourage large build office space and technology parks on the outskirts of the city.	Noted. The SCAAP only covers the central area of the town.
Question 37	Procurement Consulting (Mr Barrie Evans) [513]	2394	Comment	Like it or not the founding reasons of Southend's original success was rich London families who wanted to live by the sea. Their money was its reason for success and the city needs that cash injection again. Attracting London professionals to the area would not force out social or affordable housing but actually pay for it, create jobs and maintain a balance in society which Southend Centre currently lacks. This would also break down that Leigh On Sea and Southend social divide which is ridiculous as Southend itself could be far nicer than crammed in Leigh On Sea with its lack of sea front and crammed streets.	Noted.
Question 37	Historic England (Dr Natalie Gates) [514]	2396	Support	Pleased to see that heritage has been integrated into the plan. We do not have any over-riding concerns in relation to the plan.	Noted.

Question 37	National Federation for the Blind (Mrs Jill Allen-King) [516]	2466	<b>Comment</b>	<p>When considering any of this plan you must take in to account your responsibilities under the Equality Act.</p> <p>With the numbers of elderly and disabled people that will live in Southend during the next ten years I do not think you have taken enough account of it. No listed Sheltered housing, no day centre facilities for disabled people, no parking for disabled people, no facilities for guide dog owners or other assistance dogs, no public toilets.</p> <p>The maps referred to were not explained on the cd so I could not comment on them.</p> <p>There were many references to your Website which I and many other blind people do not have access to.</p>	The Plan seeks to provide facilities for all users that are safe and accessible. The SCAAP will be accompanied by an Equalities Impact Assessment.
Question 37	National Federation for the Blind (Mrs Jill Allen-King) [516]	2467	<b>Comment</b>	<p>Millions of pounds were wasted on the Travel Centre, Victoria Gateway and City beach. Before wasting millions of pounds will the Council please consult with local residents and listen to what we say and not ignore us like you did in 1970 and in 2006 and 2008.</p> <p>When consulting Stakeholders, please include all residents living in the Southend district and not only a few listed in your appendix.</p>	Appropriate public consultation in line with statutory requirements is carried out at all stages of plan preparation.
Question 37	Amec Foster Wheeler on behalf of National Grid [519]	2468	<b>Comment</b>	We have no comment to make	Noted.



## Appendix 4: Southend Central Area Action Plan Preferred Approach (2015) – Detailed Summary of Workshop Comments Held on 20<sup>th</sup> and 21<sup>st</sup> January 2016

During the workshop sessions ‘discussion stations’ were made available, which included material to depict and summarise each proposed Policy Area of the Southend Central Area Plan (SCAAP). Participants were able to comment on each Policy Area and were asked to do so under the following themes: ‘Support/ like’; ‘What is missing’; ‘What can be improved’; ‘Other issues’. The matters raised during the workshops, together with response, are provided below.

Note: that any reference made in this document to changes to specific text or sections of the Southend central Area Action Plan will be in relation to the December 2015 version i.e. the Preferred Approach

Support/ Like
What is Missing
What can be Improved?
Other Issues

PA1 - High Street Policy Area		Response
	<b>PUBLIC</b>	
418 H1	Improve quality of shops	Noted; although the Council cannot control the ‘quality’ or type of shop within the Town Centre, the SCAAP seeks to support retail in the High Street Policy Area. Further, Policy PA1 seeks to enhance the public realm of the High Street, thereby adding to the appeal of the area, which may attract additional retail providers. In addition the Policy seeks to conserve and restore historical shopfronts.  Also, in respect of shop frontages, the SCAAP in Policy DS1: Maintaining a Prosperous Retail Centre seeks to ensure that all new frontages will be of a high standard of design that is compatible with the architectural style and character of the building and surrounding area.
H2	Victoria circus public events space	Noted. Policy PA1: High Street Policy Area Development Principles seeks to transform and enhance the existing public space at Victoria Circus, enabling use for public events.
H3	High street lighting very good	Noted.
H4	Vibrancy	Noted.
H5	Public events space very attractive to young people – helps attract more footfall &	Noted. See response to H2.

	possible expenditure	
H6	Piazza idea sounds good	Noted. See response to H2.
	<b>COUNCILLOR</b>	
H7	Broadening the High Street to create vibrant segments	The SCAAP seeks to support the broadening of the High Street through the provision of quality pedestrian links and the allocation of sites for development that may help create 'retail circuits'.
H8	Connections between seafront & town centre	Noted. The desire to improve linkages between the seafront and the High Street are highlighted in the Central Seafront Area, the High Street, Tylers and Clifftown Development Principles.
H9	Pedestrianisation of High Street and London Road	Noted. Policy PA2: London Road encourages this on a new pedestrianised section which also includes provision for a street market.
	<b>PUBLIC</b>	
H10	Must include provision of new toilet block, which needs to be central	There are a number of public conveniences within the Town Centre and located close to the High Street. It is not considered necessary for the SCAAP to deal with such a detailed issue, which will be addressed by other teams within the Council or during the design stage of a planning proposal.
H11	Under-deeping – skate board park needed in town centre	The Deeping provides service access to the Victoria's shopping centre.
H12	Create a large open multi-purpose space linking High Street to Forum Piazza (remove buildings to give large central space)	There is no accompanying evidence that such a measure would be deliverable or viable during the SCAAP's plan period and therefore no reference is made.
H13	There is no point in building on car parks if shopping areas are to be viable. Out of town shopping will be more attractive	The SCAAP seeks to improve the quality of access to parking so that it is convenient, well-signposted, safe and secure. Further the SCAAP will seek to maintain capacity at a level that supports the vitality and viability of the town centre and enables the delivery of relevant opportunity sites. The approach to car park management will be informed by an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
H14	More trees and landscaping	All Policy Area's include provision to enhance urban greening through landscaping or tree planting.
H15	Café culture on High Street	Policy DS1: Maintaining a Prosperous Retail Centre sets out an approach for managing town centre frontages. In recognising the changing role of shopping patterns and the positive contribution of non-A1 retail units, particularly 'cafes', the SCAAP sets a lower threshold for A1 retail use within Town Centre Primary Shopping Frontage, thereby allowing the potential provision of more cafes along

		the High Street.
H16	Using empty shop fronts for community projects, such as Slack Space project in Colchester	<p>Policy DS1 seeks to encourage the landowner/landlord to display local art within the windows of empty shops to create visual interest from the public realm.</p> <p>Slack Space is not an element of the Development Plan in Colchester. Rather it is a project that is not covered by planning policy.</p>
	<b>BUSINESS</b>	
H17	Provision of public toilets at OS2	<p>Opportunity Site 2 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.</p> <p>Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021.</p>
H18	Make it clear we support residential on upper floors above shops	Policy PA1: High Street Policy Area Development Principles provides support for a net increase in dwellings above commercial development. Furthermore Policy DS1: Maintaining a Prosperous Retail Centre sets out support for a range of uses, including residential, above town centre shopping frontages.
H19 420	Signage at the top and bottom of high street for directions to seafront with distances	Noted. Additional provision will be made in Policy PA1: High Street Policy Area Development Principles to link the town centre and the central seafront through improved signage and public art. The aim of improving linkages between the seafront and the High Street are highlighted in the Central Seafront Area, the High Street, Tylers and Clifftown Development Principles Policies.
	<b>COUNCILLOR</b>	
H20	Central glass roof with panels to provide cover and seating for restaurants	There is no accompanying evidence that such a measure would be deliverable or viable during the SCAAP's plan period and therefore no reference is made.
H21	Allow traffic down the high street	Allowing traffic down the High Street would adversely impact the setting and public realm and hinder pedestrian flows between shops and services. No change proposed.
	<b>PUBLIC</b>	
H22	Victoria Circus has limited potential as a public event space as it's not flat	Noted, although it is considered that the existing public space at Victoria Circus could be enhanced and a suitable area provided for a range of public events despite the gradient in floor level and this is acknowledged in the Policy for the High Street PA1: High Street Policy Area Development Principles.

H23	Consider motor cycle parking in the town centre as much has been lost at The Forum and if Alexandra street might go	<p>The SCAAP seeks to promote a positive approach to public car parking provision that provides levels that support the vitality of the town centre and access to the seafront by encouraging improvements to the quality of access to parking so that it is convenient, well-signposted, safe and secure.</p> <p>It is considered that reference to a range of parking types, including for motorcycle's, should be made within Policy DS5.</p> <p>The approach to car park management will be informed by an independent car parking study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
H24	Like the idea of extra public space away from the high street, e.g. pier entrance	Noted.
H25	If the top end of the High Street is developed for 'eating out' ensure there is a close drop off for taxis for the elderly and disabled	Noted. The SCAAP seeks to provide provision for the relocation of taxi facilities close to the top end of the High Street, west of College Way on London Road, its specific location and facilities to be determined in consultation with taxi providers.
H26	Abandon out of town developments – Garon park and Fossetts Farm are wrong	These areas are outside of the SCAAP boundary and are not covered by its policy. However, Policy DS1: Maintaining a Prosperous Retail Centre does refer to the Southend adopted Core Strategy, which establishes the town centre as the first preference for retail and town centre development within the Borough. Within Policy DS1, further reference will also be made to the National Planning Policy Framework, which reinforces the town centre first approach for locating retail and town centre uses.
H27	Street furniture – current dazzles in the sunlight and the pavement is a trip hazard	Noted. The Council has now adopted a Streetscape Manual Supplementary Planning Document that provides guidance to ensure a coordinated, high quality, user friendly streetscape is sustainably achieved within the Borough, including any improvements to the High Street such as new street furniture and paving.
H28	Create some nice features in the High Street, as was there previously – wooded seating and surrounding flower beds	Noted. The SCAAP seeks to maintain and improve the High Street as public space for pedestrians, addressing the principles of the Southend Streetscape Manual and by providing quality landscapes including urban greening and tree planting. The Streetscape Manual also includes a palette of agreed materials.
H29	There needs to be additional parking provision	The approach to car park management will be informed by an independent car parking study which will investigate and present findings on the capacity of the

		car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
H30	Improve access/ view of High Street from Vic. Circus – remove part of New Look for fantastic views down the high street	Noted, provision included within Policy PA2 that promotes improved pedestrian access and legibility from Victoria Gateway to the High Street. If circumstances were to arise the Council would always explore with developers/owners ways to improve the aesthetics and functioning of the High Street and Central Area.
H31	Narrow alley way from Victoria Gateway to the High Street is horrible	Noted, provision included within Policy PA2 that promotes improved pedestrian access and legibility from Victoria Gateway to the High Street. If circumstances were to arise the Council would always explore with developers/owners ways to improve the aesthetics and functioning of the High Street and Central Area.
H32	Need to get a better visual aspect of the Forum from the High Street – even if it means redevelopment of some current buildings	The Forum is identified as a Landmark Building in Policy DS3: Landmarks and Landmark Buildings and, therefore, policy seeks to enhance the setting and views of the building from new development and via public realm improvements.  Further reference to landmark buildings and Policy DS3: Landmarks and Landmark Buildings will be made to Policy PA3.
H33	Turn the former subterranean toilets, opposite old Mothercare) into a coffee shop – like “The Attendant” in London	The subterranean toilets in the High Street were shut and covered over many years ago. It has not been considered environmentally appropriate or economically viable to try and re-open such facilities at the present time. This does not preclude the Council considering something in the future if it was considered to meet the criteria above and someone made an approach.
422		
H34	More independent shops	Although the SCAAP can manage Use Classes (under the Use Class Order) from a planning perspective, it cannot control the type of premises that come under the same umbrella use class. For instance, Policy DS1: Maintaining a Prosperous Retail Centre seeks to control the proportion of A1 retail within designated Town Centre Primary Frontages, however, the SCAAP cannot be specific on the type of A1 use, including whether a shop is ‘independent’ or not.
H35	Tramway in the High Street	There is inadequate evidence that a tramway would be deliverable or viable during the SCAAP’s plan period and therefore no specific policy reference is made in the document. However, Policy DS5 seeks to improve public transport. Innovative schemes such as tram provision may be considered as part of wider traffic management proposals having regard to economic feasibility.
H36	Allow taxi down the High Street in the evening would make it feel safer and less desolate	This is not considered a viable proposal given the facilities that would need to be located in the pedestrianised High Street and that taxis would need to drive on this surface. Taxi’s are able to drop off and pick up close to the High Street in the



		adjoining side roads, and can also gain access to the vehicular service area in the southern end of the High Street.
H37	More trees and greenery	All Policy Area's include provision to enhance urban greening through landscaping or tree planting.
H38	Need to improve the appearance of some of the buildings, e.g. BHS	<p>The Council has limited control in improving the appearance of private buildings. However, the SCAAP makes extensive policy reference for improving the public realm in the town centre and central area. If a building was considered for redevelopment in the future, there may be opportunity to address the exterior appearance as part of a planning application.</p> <p>Further, Policy DM1 – Design Quality of the Development Management Document, outlines that the Council will support good quality, innovative design that contributes positively to the creation of successful places, and development proposals should add to the overall quality of an area.</p>
H39	Vic Circus - Public spaces need to be provided at varying levels and must be accessible	Noted. Policy PA1: High Street Policy Area Development Principles seeks to transfer and enhance the existing public space at Victoria Circus, enabling use for public events.
H40	Need to maintain side roads to high street for disabled parking & access to buses and provide public toilets – don't want High Street at varying levels	The side roads already provide facilities for disabled parking. Plans to pedestrianise some of the stub end roads will take into account the provision of all users, including vulnerable users and disabled parking needs, at the design stage. The town centre also benefits from good transport links and further enhancement of these are proposed in the document. However, it is proposed that reference to the provision of disabled parking be included within Policy DS5.
	<b>BUSINESS</b>	
H41	Public events space needs levelling and flexible usage and street furniture needs careful consideration	<p>Noted, it is considered that the existing public space at Victoria Circus could be enhanced and provides a suitable area for a range of public events despite the gradient in floor level. The Council will have regard to the adopted Streetscape Manual Supplementary Planning Document in terms of potential future street furniture provision.</p> <p>Policy PA1: High Street Policy Area Development Principles seeks to transform and enhance the existing public space at Victoria Circus, enabling use for public events. Reference to flexibility of the design and layout of the public event space at Victoria Circus will be made in Policy PA1.</p>
H42	With regards to Southend Airport – develop sites that would encourage visitors to	Noted. The SCAAP includes an objective to encourage new development,

	the town centre	including visitor accommodation that enhances the leisure and tourism offer in the Plan area, having particular regard to the assets offered by the Central Seafront Area.
H43	Improve quality of shop near Vic. Station to encourage visitors into the High Street	Policy DS1: Maintaining a Prosperous Retail Centre seeks to improve the retail offer throughout the Southend Central Area.
H44	Shield the service area - looks terrible from Forum	Policy PA3: Elmer Square Policy Area Development Principles OS 3 identifies the detrimental visual impact of the service area, servicing High Street uses, has on the Forum development scheme. It seeks to promote environmental improvements as part of any development scheme for the site.
H45	Needs consultation and buy in from local business	Extensive public consultation has been carried out throughout the Plan making process, including with local businesses
H46	Central square needed in High Street, but not near the railway	There is no accompanying evidence that such a measure would be deliverable or viable during the SCAAP's plan period and therefore no reference is made. However, opportunities for enhancing the existing public space around the railway bridge will be made.
H47	Business concerned that large events space at Vic. Circus will go	Policy PA1: High Street Policy Area Development Principles seeks to transform and enhance the existing public space at Victoria Circus.
	<b>COUNCILLOR</b>	
424 H48	Cycle path down centre of High Street	Noted. This may be considered as part of the cycling strategy for improving accessibility in and around the town centre. The needs of cyclist would need to be considered and balanced with those of pedestrians, to ensure there wouldn't be any safety concerns that would need to be addressed.
H49	Need to address the linear nature of high street	It is considered that the proposed development at Queensway (OS4) and Tylers Avenue (OS6), together with widespread public realm improvements and pedestrianisation at London Road, Queensway and the High Street Stub-end roads encourage more lateral footfall across the High Street and help establish viable retail circuits.
H50	Create a central structure to maximise footfall at shop fronts, not walking down the centre	There is inadequate evidence that a central shelter running down the High Street would be deliverable or viable during the SCAAP's plan period and, therefore, no policy reference is made in the document
H51	New public space proposed at railway – would be better positioned at other junction (wider)	There is no accompanying evidence that such a measure would be deliverable or viable during the SCAAP's plan period and therefore no reference is made. However, opportunities for enhancing the existing public space around the railway bridge will be made.
H52	OS2 Pitmans Close – public toilets result in anti-social behaviour. Need to create a	Noted, however there is inadequate evidence that OS2 Pitman's Close will be

	more public friendly positive use here	delivered during the SCAAP's plan period and, therefore, OS2 will not be included in the final version of the SCAAP.  Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the sites that are to be delivered after 2021.
H53	Improved lighting to create interest and shows	Noted. Reference to improved lighting has been incorporated into Policy PA1.
H54	Potential for a café culture in the evening	Policy DS1: Maintaining a Prosperous Retail Centre sets out an approach for managing town centre frontages. In recognising the changing role of shopping patterns and the positive contribution of non-A1 retail units, particularly 'cafes', the SCAAP sets a lower threshold for A1 retail use within Town Centre Primary Shopping Frontage, thereby allowing the potential provision of more cafes along the High Street. Further policy support for A3 cafes will be included in Policy DS1.  The opening times of businesses within the town centre will be determined through the Licensing regime of premises.
H55	Improve public experience by removing metal benches, burn in summer, freeze in winter	Any future installation of street furniture along the High Street will have regard to the adopted Streetscape Manual Supplementary Planning Document.
H56	Improve lighting	Noted, reference to improved lighting has been incorporated into Policy PA1.
H57	Livework units down side of High Street	Noted, it is proposed that policy reference that supports the provision of live-work units above existing or new commercial development will be included in Policy PA1.
	<b>PUBLIC</b>	
H58	If businesses are to trade with customers, then they need to come and go in cars. There must be car parking provision – not build on them all	The SCAAP seeks to promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre and access to the seafront by encouraging improvements to the quality of access to parking so that it is convenient, well-signposted, safe and secure.  The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
H59	Could park and ride be considered	Park and Ride schemes have been considered a number of times in recent years but have not been considered feasible given the limited land available and linear peninsula geography of the town. Even so, the provision of Park and Ride would

		only be feasible outside the SCAAP boundaries. Such options will be kept under review as part of the Local Transport Plan and development of the Southend Local Plan.
	<b>COUNCILLOR</b>	
H60	Shops close too early	The SCAAP is not the appropriate document to set out opening times of businesses in the Town Centre. This is determined through the licensing regime of premises.
H61	No to café culture	Noted, however it is considered that cafes are a town centre use and can add to the vitality of a centre and enhance the experience of visitors.
H62	Too much cycling	The SCAAP seeks to provide for a range of sustainable transport improvements and options as an alternative to the car, which includes cycling.

PA2 - London Road Policy Area		Council Response
	<b>PUBLIC</b>	
L1	Tree Planting	Noted. Delivering tree planting and urban greening is included in the Policy Areas.
L2	Public Art	Noted. The provision of public art is included in the Policy Areas.
L3	Public art & signage consideration	Noted. See above (L2) and improved signage is included in Policy.
	<b>BUSINESS</b>	
L4	Market Space	Noted. Policy PA2: London Road Policy Area Development Principles includes provision for a street market.
L5	Street dining/ café space	Noted.
L6	Needs revamping as intended	Noted.
L7	Potential to lift aspirations of the area – quality market street food	Noted. Policy PA2: London Road Policy Area Development Principles includes provision for a street market.
L8	<b>COUNCILLOR</b>	
L9	Support Pedestrianisation	Noted.
L10	Pedestrianisation	Noted.
L11	Pedestrianisation	Noted.
L12	Open Market	Noted.
L13	Open Market	Noted.
L14	Victoria Gateway space good as mutli purpose, including skateboarding etc.	Noted.
L15	Victoria Gateway junction improvements	Noted.
	<b>PUBLIC</b>	
L16	Issue around safety of bus lane at Vic Gateway	Victoria Gateway shared surface will be monitored by the appropriate Council department to ensure that it is functioning safely. However how it operates is outside the scope of this Plan.
L17	<b>BUSINESS</b>	
L18	Electric and water provision for market	Noted, this level of detail will be considered during the implementation stage rather than be set out in the SCAAP document itself.
	<b>COUNCILLOR</b>	
L19	Continue with LED lighting	Noted.
L20	Better signage to toilets at Victoria Shopping Centre	It is considered that this level of detail is not required in the SCAAP. Further, the toilets provided in the Victoria Shopping Centre are not managed by the Council.
	<b>PUBLIC</b>	

L21	If pedestrianized then taxi drop off should be provided in Queens Road	The preferred option for the relocation of the taxi rank as identified in the SCAAP and depicted on the Policies Map is west of College Way on the London Road, however, the final location will be determined in consultation with taxi providers.
L22	Mixed-mode route to Elmer should not give cycle preference over pedestrians – each should have separate designated area	The intention is that the mixed mode route would prioritise pedestrians and cyclists in a way that is safe and in accordance with best practice and guidance. The specific detail of the scheme will be considered during the implementation stage to ensure that the needs of both users are addressed fully.
L23	Traffic signal phasing to be looked at, particularly Victoria Gateway	Traffic light phasing and timings are reviewed as part of wider traffic management proposals. This issue cannot be addressed by this Plan, and is a matter for other Council functions.
L24	If pedestrianised then need to have dedicated pedestrian walkways not shared with cyclists	Noted, If it is a mixed mode route that accommodates both pedestrians and cyclists it will be implemented in accordance with best practice and guidance. The specific detail of any scheme would be considered during the implementation stage to ensure that the needs of both users are addressed fully.
L25	Rethink of Victoria Gateway required to speed up traffic flow, e.g. two lanes west and filter lane north/ east	Victoria Gateway shared surface will be monitored by the appropriate Group at the Council to ensure that it is functioning safely. However how it operates is outside the scope of this Plan.
L26	Make statement outside of Vic Station. A big fountain would look good and more impressive than the small existing statue	The SCAAP in Policy PA2 includes provision for public art at this location.
L27	'Soften' this area – Cherry Blossom or small Silver Birch trees	Policy PA2 includes provision for tree planting and landscaping at this location
L28	Potential re-evaluation of Victoria junction with lanes to Vic Avenue and towards Sainsburys	This may be a matter for the Local Transport Plan to consider when it is reviewed.
L29	Improve Vic Gateway to improve traffic movements	This may be a matter for the Local Transport Plan to consider when it is reviewed.
L30	Cars + taxis leaving Vic Station should be made aware of buses coming down Vic Ave needed space to turn into bus stops outside station	This issue cannot be addressed by this Plan, and is a matter for other Council functions.
L31	Vic. Gateway share space a disaster. Unsafe, should be a crossing not having to dodge traffic including buses.	Victoria Gateway shared surface will be monitored by the appropriate Group at the Council to ensure that it is functioning safely. However how it operates is outside the scope of this Plan.
L32	Review and tweak road layout at Vic. Gateway to speed up traffic.	This may be a matter for the Local Transport Plan to consider when it is reviewed.
L33	London Road pedestrianisation should link to Queens Road and the Forum, create a gap mid-way along London Road	Noted, this proposal is unlikely to be deliverable within the SCAAP plan period, by 2021, and therefore cannot be included in the Plan. Proposals that are likely to be delivered after 2021 will be considered during the preparation of the Southend Local Plan.
L34	Taxi rank is needed for elderly and disabled to access shops and cinema. It needs to be retained here not moved further away.	The preferred option for the relocation of the taxi rank as identified in the SCAAP and on the Policies Map is west of College Way on the London Road, however, the location

		will be determined in consultation with taxi providers. This would allow for the pedestrianisation of the London Road which will improve pedestrian circulation and access in the area for all.
	<b>BUSINESS</b>	
L35	Keep the road network of the Deeping	The SCAAP does not include any proposals for the Deeping under Victoria Shopping Centre.
L36	<b>COUNCILLOR</b>	
L37	Any cycle route should be colour coded not lipped	Noted. Detailed consideration of cycle routes will be addressed at implementation stage and will take account of best practice and guidance.
L38	Bench for elderly people near the roundabout	Noted. However the SCAAP does not deal with the specific siting of street furniture.
L39	Public Toilets	Noted. However the SCAAP does not deal with the siting of toilets.
	<b>PUBLIC</b>	
L40	No loss of taxi rank	The SCAAP and its Policies Map sets out provision for the pedestrianisation of the London Road and the possible relocation the existing taxi rank west of College Way. The exact location of the taxi rank will be considered during the implementation stage and subject to consultation
L41 429	Open up walkway from station at New Look	Noted, this proposal is unlikely to be deliverable within the SCAAP plan period, by 2021, and therefore cannot be included in the Plan. Proposals that are likely to be delivered after 2021 will be considered during the preparation of the Southend Local Plan.
L42	Need regular and reliable bus service, including evenings, weekends and bank holidays	The SCAAP sets out measures to improve the bus service in the town centre, including within the Transport, Access and Public Realm Strategy set out in Appendix 5. However, determination a detailed bus service will be considered outside of the SCAAP in consultation with bus operators.
L43	Clearer marking of road boundaries is needed outside Vic station – clearer left and right road markings	Victoria Gateway shared surface will be monitored by the appropriate Group at the Council to ensure that it is functioning appropriately. However, operational matters including road markings are outside the scope of this Plan.
	<b>COUNCILLOR</b>	
L44	Via BID money more trees can be planted at London Road, Queensway and other residential areas including Milton	The SCAAP includes provision for tree planting and landscaping at these locations.

PA3 - Elmer Square Policy Area		Council Response
	<b>PUBLIC</b>	
E1	Love the area that has been created where the Forum is – this could really be a hub with new bars/ cafes with outside seating & patio heaters	Noted.
E2	A good space a good library	Noted.
E3	College looks good – surrounding area should compliment	Noted, the SCAAP includes provision to improve the public realm and the visual appearance of buildings in this location as well as development of the Elmer Square Phase 2.
E4	Great Library	Noted.
E5	Great Signposting	Noted.
OS6	The Forum & Elmer Square has proved to be successful enterprise attracting young people – really nice and modern	Noted.
E7	Support OS3 opening up the High Street	Noted.
E8	Support OS3: Elmer Phase 2	Noted.
	<b>PUBLIC</b>	
E9	Improve the surrounding landscape	Reference will be included in Policy PA3 in respect to improved landscaping.
E10	Improve signage links with High Street	Specific reference to enhanced signage to the High Street will be incorporated within Policy PA3.
E11	Open Forum piazza to High Street by removal of buildings, creating large focal point to High Street and multi-use space	Noted, however this proposal is unlikely to be deliverable within the SCAAP deliverability period, by 2021, and therefore will not be included in the Plan. Proposals that may delivered after 2021 will be considered during the preparation of the Southend Local Plan.
E12	Should High Street buildings east of Forum Plaza be included in this Policy Area? They are relevant to the public space and link with Policy Area 1	Noted, Policy PA3 includes provision to enhance the visual appearance to the rear of buildings on the High Street that front onto the public space. However, it is considered that these buildings have a stronger relationship with the High Street and



		are therefore included within the High Street Policy Area.
E13	Maintain area outside of the Forum as open	PA3 includes Opportunity Site 3: Elmer Square Phase 2, which supports development proposals for education and supporting uses. PA3 also includes reference to 'public space' and 'public realm enhancements' in this location. Further reference to retaining outside high quality public space public space will be considered.
E14	Seating on the green space	The SCAAP does not set out the specific siting of seating or other street furniture. The specific layout and proposals for OS3 will be considered during the implementation stage and be subject to consultation.
	<b>COUNCILLOR</b>	
E15	Seating is not necessarily in the best locations	Noted, the SCAAP does not set out the specific siting of seating or other street furniture. The specific layout and proposals for OS3 will be considered during the implementation stage and be subject to consultation.
E16	Improve this green area for kids, perhaps play area	Noted, the SCAAP includes more general criteria for development in this location, including new educational and support facilities and public realm improvements. It is not considered necessary for the SCAAP to define the exact nature of these improvements. The specific layout and proposals for OS3 will be considered during the implementation stage and be subject to consultation.
431		
	<b>PUBLIC</b>	
E17	More student accommodation in high rise building bounding Elmer Square and the High Street – rather than private flats	Noted, Policy PA3 seeks to ensure new student accommodation has a positive impact on the surrounding area. Policy PA3 does not contain any specific proposals for further residential development in this area. All planning applications will be considered on their merits and assessed against planning policy, including relevant policies contained with the SCAAP
E18	Route linking up to college	Noted.
E19	Bring the prudential building back into use – e.g. residential or commercial business use. This will help tackle anti-social behaviour	The Prudential Buildings has had a prior approval granted for change of use from office to residential. The scheme is for 72 flats. Under prior approval the Council only has control over certain aspects of the development and planning policy that would be applied under a planning application is not possible.
E20	Why include residential homes (West of college) & terrace houses (West of Forum) in this Policy Area?	It is considered necessary to include these residential properties within the plan to ensure any impact on their amenity from future development proposals identified for this area is taken into account.
	<b>BUSINESS</b>	
E21	This area if OK other than Prudential and other big buildings block access to	Noted. The Prudential Buildings has had a prior approval granted for change of use

	High Street	from office to residential. The scheme is for 72 flats. Under prior approval the Council only has control over certain aspects of the development and planning policy that would be applied under a planning application is not possible.
	<b>COUNCILLOR</b>	
E22	Improve ground floor activity, especially Prudential building	The SCAAP seeks to designate the frontages along a section of Elmer Approach and Queens Road as Secondary Shopping Frontage, and thereby maintain active frontages and ensure new frontages are of a high standard of design.  The Prudential Buildings has had a prior approval granted for change of use from office to residential. The scheme is for 72 flats. Under prior approval the Council only has control over certain aspects of the development and planning policy that would be applied under a planning application is not possible.
E23	Additional green space to support new developments	Policy PA3 seeks to pursue urban greening projects, including the creation of green space within new development.
	<b>PUBLIC</b>	
E24	Need public transport service to it	The SCAAP sets out measures to improve the bus service in the town centre, including within the Transport, Access and Public Realm Strategy set out in Appendix 5. However, determination a detailed bus service is outside of the scope of the SCAAP. The Council would discuss potential for amended or new routes with bus operators.
E25	Any building needs to be fully accessible	Noted.
	<b>COUNCILLOR</b>	
E26	Dog amenity area is good	The public realm within Elmer Policy Area is accessible for all.
E27	Needs to be more published especially for outdoor events (i.e. big screen)	Noted. This is not a matter for the SCAAP but may be considered by other functions at the Council.

PA4 - Queensway Policy Area		Council Response
	<b>PUBLIC</b>	
Q1	Improving views across the Borough	Noted.
Q2	Wholesale regeneration of tower blocks – start again	Noted.
Q3	More greenery,, tree planting – encourage wildlife	Noted.
Q4	Provision of new open space with CCTV designed to limit anti-social behaviour	Noted, it is not proposed to include reference to CCTV within the SCAAP. This may be considered separately during the implementation stage and subsequent management

		of the properties/ public areas by other Council functions.
Q5	New housing should provide a mix of types	Noted, the mixed of housing types will be assessed against adopted policy, including the Core Strategy and Development Management Document. Policy PA4 seeks to ensure that redevelopment does not result in a net loss of affordable housing in the area.
Q6	Need accessibility of dual carriageway by foot and bike	Noted.
Q7	Public art	Noted.
Q8	Tree planting	Policy PA4 promotes urban greening in the area, which may include tree planting.
	<b>BUSINESS</b>	
Q9	Strong supporter of better pedestrian access here	Noted.
Q10	OS4 Queensway – strongly support plans – do it ASAP	Noted.
	<b>COUNCILLOR</b>	
Q11	Muli-functional open space for all	Noted.
Q12	Support open space provision, including children’s play area with play equipment and provision for ball games	Noted. Policy PA4 promotes public realm improvements, including the provision of new public open space. The specific type of open space and public facilities provided will be considered during implementation and will be subject to consultation.
433 Q13	Good for comprehensive re-development – move away from existing high rise	Noted, however it is considered that a range of building heights would be suitable in this location. The specific detail of the scheme, including building height, will be considered during implementation and will be subject to consultation
Q14	Public realm	Noted.
Q15	Access	Noted.
Q16	Green lung	Noted.
Q17	Create central park with good access to communities and Warrior Square Policy Area	Noted, Policy PA4 seeks to create an urban park and improve links to a number of policy areas, including Warrior Square.
	<b>PUBLIC</b>	
Q18	Queensway/ Sutton roundabout needs to be redeveloped to allow ‘street level’ crossing – underpasses are not elderly/ disabled friendly and so need level crossing.	Policy PA4 seeks to create an improved crossing at Queensway/ Sutton Road junction. The precise detail and layout of this will be determined during the implementation phase of the Better Queensway project, which will be subject to public consultation
Q19	Must keep under-road at Queensway	Policy PA4 seeks to improve access and permeability at various junctions along the Queensway dual carriageway. The precise layout will be determined during the implementation phase of the Better Queensway project, which will be subject to public consultation
Q20	Make more use of historic buildings – mark prominence	Policy PA4 includes provision to enhance the setting of Porters and All Saints Church. Policy DS3: Landmarks and Landmark Buildings seeks to conserve this type of building and structure.

		Policy DM5 of the Development Management Document seeks to conserve and enhance heritage assets, including listed and locally listed buildings.
Q21	No mention of community facilities, e.g. doctors, dentists, rehabilitation services	Policy PA4 supports the provision of community infrastructure, which may include facilities such as community centres, doctor and dental surgeries, and children's nurseries.
	<b>COUNCILLOR</b>	
Q22	Is there enough green space?	Policy PA4 includes provision for urban greening and an urban park. In addition, there is provision for the creation of green space in other areas within the SCAAP which will help provide a holistic approach to green space provision in the central area.
Q23	Provision for community hall/ buildings (mixed-use)	Policy PA4 supports the provision of community infrastructure, which may include facilities such as community centres and clubs.
	<b>PUBLIC</b>	
Q24	Continue to maintain Queensway dual carriage way to keep traffic flows	Noted. Policy PA4 promotes a number of access and public realm improvements. The precise layout will be considered during implementation of Better Queensway and associated projects, which will be subject to public consultation.
Q25	Safe pedestrian access, not by shared surface	Noted. The SCAAP seeks to enhance access for pedestrians and cyclists, including a 'mixed mode – shared priority' route. Any scheme will be designed and implemented in line with current best practice and appropriate guidance.
Q26	Consideration of railings to stop cars & vehicles stopping along pavement	Noted, however this level of detailed is not considered to be required in the SCAAP, but may be considered by other functions at the Council and Better Queensway.
Q27	Quality finish required and value for money	Noted.
Q28	Pedestrian crossings should be at surface level (Porters roundabout)	Noted. The SCAAP sets out guiding principles that should be addressed during development of the area. The precise layout will be considered during implementation of Better Queensway and associated projects, which will be subject to public consultation.
Q29	If cars are allowed to turn South into Chichester Road from Queensway they should not be allowed to turn right into Victoria Shopping Centre car park.	This is not a matter for the SCAAP but may be considered by other functions at the Council. Detailed traffic movements will be considered as part of wider traffic management proposals.
Q30	G.P. facilities must be maintained or improved	Noted. Policy PA4 supports the provision of community infrastructure, which may include facilities such as doctor surgeries.
Q31	Ensure road underpass remains. Filing it in would be disastrous for Thorpe Bay	Noted. Policy PA4 promotes a number of access and public realm improvements. The precise layout will be considered during implementation of Better Queensway and associated projects, which will be subject to public consultation.
Q32	Accessible services should include day centre for disabled people	Noted. Policy PA4 supports the provision of social and community infrastructure.

	<b>BUSINESS</b>	
Q33	Do we really need an underpass here?	Noted. Policy PA4 promotes a number of access and public realm improvements. The precise layout will be considered during implementation of Better Queensway and associated projects, which will be subject to public consultation.
Q34	The underpass needs filling in to compliment the church and porters	Noted. The precise road layout will be considered during implementation of Better Queensway and associated projects, which will be subject to public consultation. Policy PA4 seeks to enhance the setting of Porters and All Saints Church.
	<b>COUNCILLOR</b>	
Q35	Top of Queensway underpass should be opened up and decked (no shelters). For open air games – skateboarding/ basketball	Noted. Policy PA4 promotes a number of access and public realm improvements. The precise layout will be considered during implementation of Better Queensway and associated projects, which will be subject to public consultation.
	<b>PUBLIC</b>	
Q36	Better design of buildings will raise moral for local residents and promote cleanliness overall	Policy DM1 – Design Quality of the Development Management Document, supports good quality, innovative design in new development, which adds to the overall quality of an area. Repetition of this policy is not required within the SCAAP. Nevertheless, included in the Queensway Policy Area is the aim for development to be an exemplar of successful design-led estate regeneration.
Q37	Cleaning and lighting at underpass is infrequent	The SCAAP is not the appropriate document to set out lighting and cleansing arrangements for the underpass, but it may be considered by other functions at the Council.
Q38	Clearer signage for direction to Victoria station	Noted, it is proposed that reference will be included to improve legibility to aid way finding to Victoria Station from the Queensway policy area.
Q39	All pedestrian crossings should have audible signals and have tactile surfaces.	This issue and level of detail is not a matter for the SCAAP. Detailed design of road crossings will be considered at the design stage, having regard to the needs of all road users and will be implemented by other Council functions.

<b>PA5 - Warrior Square Policy Area</b>		<b>Council Response</b>
	<b>PUBLIC</b>	
W1	Good provision of green space	Noted.
W2	The continuation of the idea that this should be an area that coincides with conservation and preserving green landscapes	Noted.

W3	Great existing green footprint – add to this by offering something to attract people in the evening	Noted.
	<b>BUSINESS</b>	
W4	OS5 Warrior Square - Support the plan for redevelopment – do it ASAP	Noted.
	<b>PUBLIC</b>	
W5	Seating on the green space	Policy PA5: Warrior Square Policy Area Development Principles provisions seek to improve public open space and related facilities. The SCAAP does not set out the specific siting of seating or other street furniture. The specific layout of proposals will be considered during the implementation stage of schemes and be subject to consultation.
W6	Healthcare as Queensway is already over-burdened	Policy PA4: Queensway Policy Area Development Principles seeks to provide for additional/enhanced community facilities.
W7	Attractive areas for young people and the community – the green space for the park as a focal point with activities, e.g. book sales	The Policy Area provisions seek to improve public open space and related facilities. It will be for the community in conjunction with the Council to explore ways that the space can be flexibly used, if appropriate.
	<b>PUBLIC</b>	
W8	Prefer segregated cycle/ walking routes for safety	Noted, detailed consideration of cycle/ walking routes will be considered at implementation stage and will take account of best practice and guidance.
W9	Better link between Warrior Sq. and High Street	Policy PA5: Warrior Square Policy Area Development Principles 5b seeks to achieve this.
W10	Ensure any new development has adequate parking - either basement or high rise	All development proposals are subject to adopted car parking standards as set out in the Development Management Document.
W11	Entrance from Queensway into Whitegate Road, and Warrior Sq., should be allowed	Policy PA5: Warrior Square Policy Area Development Principles seeks to provide for a package of measures to improve connectivity for pedestrians and cyclists. Changes to the junction design or function would need to be considered by other Council functions. It is not for the SCAAP to determine.
W12	Improve character and promote use – at the moment only drunks use the area and is therefore not family friendly	Policy PA5: Warrior Square Policy Area Development Principles seeks to maintain the environmental and design quality of Warrior Square Gardens and promote future public realm improvements that respect and engage with the Gardens.
W13	Have a square in Warrior Square, rather than additional housing	Opportunity Site 5 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.  Comments in relation to the site will be considered during preparation of the

		Southend Local Plan, which will consider the delivery of sites post 2021.
W14	Move the market to Warrior Sq or have special summer events	The SCAAP seeks to move the street market to London Road/High Street which is considered a more appropriate location as part of the future pedestrianisation scheme proposed for the area.
W15	Shared routes/ facilities – needs respect between road users parking and issues with the blind and partially sighted and deaf.	Noted, detailed consideration of cycle/walking routes will be considered at implementation stage and will take account of best practice and guidance as well as the needs of specific users.
	<b>BUSINESS</b>	
W16	Queensway divides the town from the East – need better pedestrian access across it	Policy PA4: Queensway Policy Area Development Principles and related provisions of the Plan seek to improve pedestrian connectivity between east and west and Queensway Dual Carriageway.
W17	More diverse use of retail to encourage people to the green areas	Policy DS1: A Prosperous Retail Centre seeks to provide for a diverse range of retail uses.
W18	Car parking needs replacing, not necessarily on this site	Adequate car parking provision in the town centre is essential to the vitality and viability of the town centre.
	<b>COUNCILLOR</b>	
437 W19	Need a better use of temporary green space – it was a former swimming pool	Policy PA5: Warrior Square Policy Area Development Principles seeks to achieve this. However, Opportunity Site 5 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.  Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021
W20	Improve profile of the conservation area	Policy PA5: Warrior Square Policy Area Development Principles seeks to achieve this.
W21	Improve natural surveillance to reduce potential for anti-social behaviour	Policy PA5: Warrior Square Policy Area Development Principles seeks to achieve this by promoting residential development facing the square.
W22	Protect green space for sport usage – 5 aside football pitch or running track round the edge or picnic area, or a maze or a fountain	The use of the potential provision of additional green open space will be considered at the design stage.
W23	Lighting scheme to promote a safer environment, but important not to negatively impact residents	Lighting has already been improved in the square as part of the implementation of a previous regeneration scheme for the site. Policy DS5: Transport, Access and Public Realm seeks to maintain lighting provision in the Central Area.
W24	OS5 Warrior Square – can support higher density	Policy PA5: Warrior Square Policy Area Development Principles promotes development that is compatible with and respects the character and amenities of the adjacent Conservation Area. Opportunity Site 5 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before

		2021, the end of the SCAAP's plan period.
		Comments in relation to the site will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021.
	<b>PUBLIC</b>	
W25	Buildings should all be accessible for disabled people	Provision of facilities for vulnerable users would be considered at the design stage of schemes. Provisions in the Development Management Document assist with this in respect of Space Standards and the new Government National Space Standards and associated documentation.
W26	Needs area for guide dogs to do their toilet	Provision of facilities for vulnerable users would be considered at the design stage of schemes.



PA6 - Clifftown Policy Area		Council Response
	<b>PUBLIC</b>	
C1	Support	Noted.
C2	Good	Noted.
C3	Good	Noted.
C4	Positive approach to development with access to Eastern Esplanade	Noted.
C5	Signage to development areas	Noted. It is proposed to add reference to improved signage to Policy PA6.
C6	Good to protect views – rethink Esplanade pub redevelopment – to high and not in keeping with conservation area.	Noted. Esplanade public house has planning permission to demolish existing building, and erect a 5 storey building comprising 23 self-contained flats with ground floor restaurant and basement parking, layout amenity area, refuse and cycle storage and landscaping, form new vehicular access onto Western Esplanade.
	<b>BUSINESS</b>	
C7	OS16 & OS17 – agree with redevelopment of car parks as described, but need provision for replacement parking	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	<b>COUNCILLOR</b>	
488	Redevelop Empire Theatre	Policy PA6: Clifftown Policy Area Development Principles seek to regenerate the site of the Empire Theatre.
	<b>PUBLIC</b>	
C9	Motorcycle parking – where is it going to be re-provided?	It is considered that reference to a range of parking types, including for motorcycle's, should be made within Policy DS5: Transport, Access and Public Realm and it is proposed that the SCAAP will be updated to reflect this.
C10	Make more of the cinema	Policy PA6: Clifftown Policy Area Development Principles seeks to regenerate the site of the Empire Theatre.
C11	Potential area of cliff slip, which needs to be addressed	Policy CS1: Central Seafront Policy Area Development Principles Opportunity Site 9 seeks to achieve this with the initial work carried out on the development of a new museum. Policy DM14 of the Development Management Document sets out policy regarding land instability.
C12	Concerned that plans for a public square will take away bus stops and parking for disabled people near shops	Plans to regenerate the forecourt of the Central Railway Station will take into account the provision of bus stops to provide an interchange, and the needs of vulnerable road users at the design stage.

	<b>COUNCILLOR</b>	
C13	Make sure a multi storey car park is provided on Tylers, before other car parks are developed	Policy PA7: Tylers Policy Area Development Principles Opportunity Site 6 seeks to address the need for replacement car parking provision.
C14	Improve derelict Royal Terrace properties	Policy PA6: Clifftown Policy Area Development Principles seeks to improve and enhance the townscape of the Conservation Area.
C15	More trees and landscaping near car parks	Policy DS5: Transport, Access and Public Realm 2d. seeks to achieve this.
	<b>PUBLIC</b>	
C16	Area of anti-social behaviour – need better public space	Policy PA6: Clifftown Policy Area Development Principles seeks to improve the public open spaces within the area.
C17	Bring back the bandstand & tea room on the green	Such a proposal may be considered investigated as part of future proposals for the area.
C18	Suggest we should protect front doors + consider listing front elevation of Clifftown Area, with respect to boiler flues etc	The conservation of buildings is subject to strict controls under planning laws and supplemented by the Policy for the specific Policy Area and other planning policy documents.
C19	Reinstate some of the street furniture around the bowling green & surrounding roads	Policy PA6: Clifftown Policy Area Development Principles seeks to provide for public realm improvements, including street furniture, in the area.
C20	Encourage bakers, butchers, florists in Alexandra St.	Policy DS1: Maintaining a Prosperous Retail Centre seeks to provide for a mix of retail units in the centre.
C21	Empire Theatre, potential for it to become developed like Clements Arcade in Leigh-on-Sea	Policy PA6: Clifftown Policy Area Development Principles seeks to regenerate the site of the Empire Theatre with uses that contribute to the Policy Area's aim.
C22	Southend Central forecourt – needs to have a taxi rank on both sides and facilities going in both directions	Taxi rank facilities are regularly reviewed as part of wider traffic management measures. Locations for proposed and existing taxi ranks are shown on the Policies Map. The SCAAP also highlights the need for appropriate lighting around taxi ranks and parking for taxis.
C23	Empire theatre development should be accessible & have a bus route	Policy PA6: Clifftown Policy Area Development Principles seeks to regenerate the site of the Empire Theatre with uses that contribute to the Policy Area's aim.
C24	Alexandra and Clarence Car Parks – if they go other public spaces should be provided not just for residents	<p>Noted. Policy PA5: Clifftown Policy Area Development Principles seeks to achieve this. However, Opportunity Site 16 &amp; 17 will not be included in the final version of the SCAAP as there is insufficient evidence that the sites will come forward before 2021, the end of the SCAAP's plan period.</p> <p>Comments in relation to the sites will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021.</p> <p>The approach to car park management will be informed by an independent car</p>

		parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	<b>COUNCILLOR</b>	
C25	Improve healthy opportunities such as, walking circuits, new public square	Policy PA6: Clifftown Policy Area Development Principles seeks to achieve this.
C26	OS16 & OS17 – should be in the plan pre-2021	Opportunity Sites 16 & 17 will not be included in the final version of the SCAAP as there is insufficient evidence that the sites will come forward before 2021, the end of the SCAAP's plan period.  Comments in relation to the sites will be considered during preparation of the Southend Local Plan, which will consider the delivery of sites post 2021.
C27	Phase car park release to see how they come forward and if they are a success	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	<b>PUBLIC</b>	
C28 44	More motorcycle bays	It is considered that reference to a range of parking types, including for motorcycle's, should be made within Policy DS5: Transport, Access and Public Realm.
C29	Royal Mews onto Alexandra Street should be right turn only	Detailed road improvements will be considered as part of wider traffic management proposals and are not a specific matter for the SCAAP to address.
C30	No coach parking or dropping off on Clifton Parade for purposes of the proposed museum	Detailed road improvements will be considered as part of the detailed design stage of the new museum and wider traffic management proposals. However Policy CS1.13.3 outlines that the design of new development will need to retain the 'open feel' of the area. Policy DM1 of the Development Management Document and the Design and Townscape Guide provides additional design related guidance. In addition, the conservation area designation will be a material consideration.  It is recognised that the policy can be further enhanced by outlining that vehicular access of a new development in this location should be via Western Esplanade.

PA7 - Tylers Policy Area		Council Response
	<b>PUBLIC</b>	
T1	More trees	Policy PA7: Tylers Policy Area Development Principles seeks to achieve this.
T2	Support relocation of bus station, with commercial and car parking	Noted.
T3	Create a central bus station for all buses & for coaches to encourage tourism	Noted.
T4	Support the enhancement of bus station, wider stops & routes in the SCAAP area	Noted.
T5	Support the relocation of travel centre, with commercial, cafes & residential above – independent shops	Noted.
T6	Older Peoples Assembly welcome the relocation of the travel centre, but please ensure it is user friendly and a safe environment	Noted.
T7	Support relocation of Bus Station & shops fronting York road	Noted.
T8	Support development of OS6	Noted.
T9	Welcome consideration of better access for pedestrians from town centre to seafront via Chancellor Road	Noted.
T10	Support public realm improvements of Tylers Policy Area	Noted.
	<b>BUSINESS</b>	
T11	Tylers is well located and used & needs stacking up to re-provide for other lost car parks	Noted. Tylers Avenue Opportunity Site includes reference for addressing replacement parking. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
T12	OS6 Tylers- agree with plans – do it ASAP	Noted.
T13	Support the redevelopment to create a circuit with Town Centre	Noted.
T14	The bus station works well for me	Noted.
	<b>COUNCILLOR</b>	
T15	Support relocation of bus station + decked parking, look to also provide retail if possible	Noted. Tylers Avenue Opportunity Site includes reference for addressing replacement parking and the provision of retail at ground floor. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.

T16	OS6 – support development if there are clear benefits to the local community	Noted.
	<b>PUBLIC</b>	
T17	Increase in housing will require additional doctors, dentists and school places	The SCAAP recognises this and makes appropriate provision for community facilities as part of development, where required.
T18	Regarding the proposals for a new cinema on Seaways and a new travel centre – there is no provision for a safe route between the two for pedestrians	Policy PA7: Tylers Policy Area Development Principles 4d seeks to achieve this.
T19	More trees, parks and landscaping	Policy PA7: Tylers Policy Area Development Principles 4e seeks to achieve this.
	<b>BUSINESS</b>	
T20	OS6 Tylers – needs to provide replacement number of parking and whatever the development yields	Noted. Tylers Avenue Opportunity Site includes reference for addressing replacement parking. Development proposals will have regard to the parking standards set out in the Development Management Document. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	<b>PUBLIC</b>	
T21	Shared space would not be welcome by the elderly nor would we want to see further introduction of cycle + pedestrians sharing space	Noted. Detailed consideration of mixed-mode pedestrian and cycle routes will be considered at implementation stage and will take account of best practice and guidance.
T22	Do not like using existing travel centre, and new one will need to be welcoming and focussed around a square	Noted.
T23	Provide more trees and landscaping	Policy PA7: Tylers Policy Area Development Principles 4e seeks to achieve this.
T24	Seating uncomfortable and metal does not work	The provision of seating would be considered at the design stage, having regard to the Southend Streetscape Manual SPD.
T25	Improve bus station with green area, trees, landscaping and sitting areas with cycle parking	Policy PA7: Tylers Policy Area Development Principles seeks to achieve this.
T26	Turn the area where the existing travel centre is into green space, with trees to encourage footfall between OS6 & High Street	Policy PA7: Tylers Policy Area Development Principles includes reference to the potential relocation of the travel centre, and green space provision and tree planting would be considered as part of any development on the former site. However, the detailed layout of a scheme will be considered and consulted upon at the design stage.
T27	Replace existing bus station as it does not work well. Reprovide on OS6 with multi-storey parking behind	Policy PA7: Tylers Policy Area Development Principles include reference to the potential relocation of the travel centre and Tylers Avenue Opportunity Site includes reference for addressing replacement parking.  The approach to car park management will be informed by an independent car

		parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
T28	Separate cyclists and pedestrians	Noted. Detailed consideration of mixed-mode pedestrian and cycle routes will be considered at implementation stage and will take account of best practice and guidance to ensure the needs of all users are met.
T29	Improve bus access to all routes	Policy DS5: Transport, Access and Public Realm seeks to improve traffic accessibility including appropriate provision for public transport. Bus routes are considered as part of partnership working with bus operators.
T30	Travel centre is in a good location as it is close to shops, but should be redesigned so it is all undercover	Policy PA7: Tylers Policy Area Development Principles Opportunity Site 6 makes provision for the potential relocation of the bus station in the interests of providing improved facilities.
	<b>BUSINESS</b>	
T31	OS6 Tylers - if redeveloped where will the existing parking be re-provided?	<p>Policy PA7: Tylers Policy Area Development Principles Opportunity Site 6 outlines that development of the area should address replacement car parking provision, identifying how any displaced parking needs are to be met on the site or in this part of the town centre.</p> <p>The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
444		
T32	Need to be clear on what we want to arrive at Queensway / York Road junction	Detailed access arrangements will be considered at the design and implementation stage in association with the Council's Local Transport Plan and wider transport management services.
	<b>COUNCILLOR</b>	
T33	Tree planting and home zoning for Baltic/ Quebec/ Heygate/ Portland & York Rd or resident parking	Policy PA7: Tylers Policy Area Development Principles 4c. seeks to achieve this.
T34	Proper travel centre facilities relocated to OS6 Tylers. Bus pick up points on Chancellor Rd are to short and not fit for purpose.	Noted. Policy PA7: Tylers Policy Area Development Principles include reference to the potential relocation of the travel centre. Bus pick-up points would be considered at the design stage of any relocation proposals.
	<b>BUSINESS</b>	
T35	Single access onto Queensway and low level housing with better access needed	Noted. Detailed traffic measures will be considered as part of wider traffic management proposals.

CS1 - Central Seafront Policy Area		Council Response
	<b>PUBLIC</b>	
CS1	City Beach looks great – extend it further	Policy CS1: Central Seafront Policy Area Development Principles seeks to achieve this.
CS2	City Beach phase 2 east of Marine Parade	Policy CS1: Central Seafront Policy Area Development Principles seeks to achieve this.
CS3	Redevelopment of cliff face	Policy CS1: Central Seafront Policy Area Development Principles seeks to achieve this in Opportunity Site 9 New Southend Museum.
CS4	OS8 Seaway Cinema	Noted.
CS5	OS8 Seaways – Regeneration is supported	Noted.
CS6	Delivery of a Lido	Noted.
CS7	Any regeneration is welcome	Noted.
CS8	Water fountains	Noted.
CS9	Cliff lift	Noted.
CS10	City Beach lighting columns	Noted.
CS11	Marine Plaza will uplift end of seafront – deliver ASAP	Noted.
CS12	Spanish steps creating better links	Noted.
445	<b>BUSINESS</b>	
CS13	OS8 Seaways could be benefit of adjacent area – e.g. High Street/ Royals	Noted.
CS14	OS8 Seaways can provide more restaurant development	Noted.
CS15	OS8 Seaways – support opening up the site and punching through to the seafront and creating views of the seafront	Noted.
CS16	Better connect Seafront and High Street	Noted.
CS17	Pier good for tourists and should be looked after and well maintained	Noted.
CS18	Need to provide more quality hotels with conference centres – linked to Southend Airport	The Plan seeks to achieve this.
CS19	Lights and statues be incorporated in public spaces	The Plan seeks to achieve this.
	<b>COUNCILLOR</b>	
CS20	Museum car park	Noted
	<b>PUBLIC</b>	
CS21	Parking infrastructure not addressed. This area needs to be looked at to drive tourism and business. If people cannot park they will not come	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.

CS22	Marine Parade from Kursaal going west is devoid of transport links	Noted. The provision of future bus routes will be reviewed in partnership with bus operators. Policy DS5: Transport, Access and Public Realm provides for a range of transport and access improvements.
CS23	Consideration for the residents	Noted.
CS24	Residents permit parking	Residents permit parking schemes are reviewed as part of wider traffic management measures.
CS25	Residents parking in summer months	Residents parking schemes are reviewed as part of wider traffic management measures.
	<b>BUSINESS</b>	
CS26	OS8 Seaways should be the prime parking in the central area – Proposed development would need 1500 parking spaces linked to DM15	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS27	OS8 Seaways – parking should be free after 6pm	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS28	OS8 Seaways – only 11 coach spaces being provided, 30 spaces are required	Noted. Reference to Seaways coach drop off and parking provision included within CS1: Central Seafront Policy Area Development Principles. The precise quantum of coach spaces will be decided at the design and implementation stage of the development.
440 CS29	OS8 Seaways – Toilets being removed, which everyone needs after 2 hour journey	Noted. Toilets and related facilities will be considered at the design stage of any redevelopment scheme.
CS30	OS8 Seaways – at least 1000+ parking spaces needed to give the new development a fighting chance with existing business	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS31	OS9 Museum – not a good idea, why not create something similar to a bandstand here	Such a proposal could possibly be investigated as part of the proposals to provide for a new museum (OS9).
CS32	More parking on City Beach	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS33	Car parking is not sufficient. Should be at least 3 times current spaces.	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in



		the submission version of the SCAAP.
CS34	Car parking spaces – want to see council data from their car parks to confirm they are not at capacity in peak times	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS35	Shelter for disabled and elderly to sit (sun shelters)	Noted. Such facilities will be considered at the design stage of any redevelopment scheme.
CS36	Something needs to be put at the end of the pier to give tourists a good experience	Policy CS1: Central Seafront Policy Area Development Principles Opportunity Site 7 seeks to achieve this.
	<b>COUNCILLOR</b>	
CS37	No taxi space/ bay on seafront	Transport, Access and Public Realm Strategy (Appendix 5), seeks to improve the provision of taxis at key locations throughout the Central Area. The Policies Map identifies a new taxi rank on Eastern Esplanade. Further reference to the provision of taxis is to be included to Policy DS5: Transport, Access and Public Realm and CS1: Central Seafront Policy Area Development Principles.
	<b>PUBLIC</b>	
CS38 447	OS8 Seaways – can more parking be created through layout + extra floors of multi-storey	Noted. The detailed layout of the scheme will be considered and consulted upon during the design and implementation stage. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS39	OS8 Seaways – against development unless better access roads are provided + measures against congestion	Policy seeks to achieve this, including policies CS1: Central Seafront Policy Area Development Principles and DS5: Transport, Access and Public Realm.
CS39	OS8 Seaways – any development should incorporate houses/ low rise fronting Herbert Grove	Opportunity Site 8 seeks to encourage residential development as part of a mixed use scheme. Any design matters would be addressed at a detailed planning application stage.
CS40	OS8 Seaways – include multi-storey + retail and leisure	Opportunity Site 8 seeks to provide for leisure, cultural and tourism facilities. Further reference will be made to the re-provision of parking.  The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS41	OS8 Seaways – Cinema not needed	A cinema is considered to be an appropriate use in this locality. It will be for a developer to determine whether it is a viable enterprise.

CS42	OS7 Pier –should be free and more uses should be created, including rides, restaurants, pubs, boat trips, make better use of cultural centre	Opportunity Site 7 seeks to achieve a mix of cultural and leisure uses on the Pier.
CS43	OS7 Pier – more at the end needed	Opportunity Site 7 seeks to achieve a mix of cultural and leisure uses on the Pier.
CS44	OS7 Pier – ample opportunity to make more fun/ interesting place to visit. Amenities needed along its length and at the end	Opportunity Site 7 seeks to achieve a mix of cultural and leisure uses on the Pier.
CS45	OS7 Pier to long and more attractions required	Opportunity Site 7 seeks to achieve a mix of cultural and leisure uses on the Pier.
CS46	OS7 Pier – should be free to walk on and have more facilities	Charging regimes are reviewed as part of the wider tourism strategy for the Borough. It is not for the SCAAP to address this matter.
CS47	The vision for the Policy Area seems vague	The aims are considered to be clear in their objectives and appropriate for this prime leisure and tourism area.
CS48	The Marine Parade to Chancellor Road walk way doesn't offer a good link into the town centre – need to accommodate this and public transport	Policy PA7 seeks to achieve this.
CS49	Can Marine Parade be traffic free – pedestrians only	Marine Parade is a key traffic route and provides 'shared space' for users.
CS50	Drainage & flood protection	Policy DS4: Flood Risk Management and Sustainable Drainage provides appropriate policy on this issue.
CS51	Shared space a disaster – pedestrianise	Marine Parade is a key traffic route and provides 'shared space' for users.
CS52	Install another fountain	This will be considered as part of wider regeneration proposals.
CS53	More green space – this does not seem to have been considered	This is considered throughout the Plans provisions.
CS54	Use the Kursaal as an exhibition centre for the saxon king/ London wreck	Opportunity Site 9: the new Southend Museum seeks to provide for such a facility.
CS55	Setup a tram system along the seafront	Policy DS5: Transport, Access and Public Realm seeks to improve public transport. Innovative schemes such as tram provision would be considered as part of wider traffic management proposals having regard to economic feasibility.
CS56	Join up the seafront	Noted.
CS57	Refuse collection and general upkeep, including road surfaces and pathways, street lighting	This is not a direct matter for the SCAAP. Refuse collection is considered as part of the Borough Council's service provision whilst road servicing funding forms part of the Local Transport Plan provisions.
CS58	The pier lift is usually broken or at least one of them	This is not a direct matter for the SCAAP. Maintenance issues are considered as part of the Borough Council's service provision.
CS59	Flood risk was in the 2010 plan for the seafront and yet shared space allowed water to run into businesses. Therefore, no more shared space and improve drains.	Policy DS4: Flood Risk Management and Sustainable Drainage provides appropriate policy.
CS60	Summer bus routes along the seafront – regular service	Policy DS5: Transport, Access and Public Realm seeks to improve traffic accessibility including appropriate provision for public transport. Bus routes are considered as part of partnership working with bus operators.
CS61	Drainage systems need improving	Policy DS4: Flood Risk Management and Sustainable Drainage provides appropriate

		policy.
	<b>BUSINESS</b>	
CS62	OS8 Seaways – Any development should provide as much parking as there already	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS63	OS9 Museum – get on a provide the additional parking	Noted. Opportunity Site 9 the New Southend Museum makes provision for public car parking.
CS64	How will the car parking study and survey capture the additional capacity in the road?	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS65	Need more car parking, residential development and A3 restaurants	The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP. The Plan seeks to achieve more residential development together with A3 uses.
CS66	Significant improvements to the transport infrastructure	Policy DS5: Transport, Access and Public Realm in combination with other Council initiatives seeks to achieve this.
CS67	Transport network cannot cope	Policy DS5: Transport, Access and Public Realm seeks to achieve appropriate improvements to the transport network.
CS68	Replace dedicated cycle lane with parking	Cycle lanes are an integral part of the transport strategy for improving sustainable transport links in line with national planning policy.
CS69	Make more accident proof with the cycle lane	This is not a direct matter for the SCAAP to address. Safety issues are considered as part of wider on-going traffic management proposals.
CS70	Replace existing parking with chevron parking along the Esplanade	This is not a direct matter for the SCAAP to address. Parking provision design is reviewed periodically as part of wider traffic management proposals.
CS71	Get on and develop	Noted.
CS72	Full bus service along the seafront – Thorpe Bay to Chalkwell	Policy DS5: Transport, Access and Public Realm seeks to improve traffic accessibility including appropriate provision for public transport.
CS73	Traffic flow and congestion signage	Policy DS5: Transport, Access and Public Realm provides for such issues.
CS74	Pedestrianisation of Pier Hill, creating an open area with designer lighting	Policy CS1: Central Seafront Policy Area Development Principles seeks to regenerate the seafront areas including the provision of Phase 2 of the City Beach scheme.
CS75	Need to improve access to sea front from the bus station and between High Street and Seafront.	Policies PA7 and CS1: Central Seafront Policy Area Development Principles seek to achieve this.

<b>COUNCILLOR</b>		
CS76	Positive about new development but it must provide parking provision for residents and commercial need	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS77	City Beach Phase 2 – replacement of the car parking within the same area	Noted. Detailed consideration of the scheme will be considered and consulted upon during the design stage. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS78	Creative uplighting and more trees	Policy CS1: Central Seafront Policy Area Development Principles seeks to achieve this.
CS79	Ensure coach parking provision is maintained in the Central Seafront area	Noted. Additional wording is proposed to Policy CS1: Central Seafront Policy Area Development Principles OS8 to take into account coach parking.
CS80	OS7 Pier – creative lighting scheme for the pier	Policy CS1: Central Seafront Policy Area Development Principles Opportunity Site 7 seeks to achieve appropriate improvements to the Pier, including the provision of creative lighting. Development Principles for the central seafront area also includes provision for creative lighting.
<b>PUBLIC</b>		
CS81	Cost of parking during the day is expensive. Basildon and Lakeside is free	Car parking charges are reviewed as part of wider traffic management proposals. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS82	Bigger and more bins outside cafes and Wimpy.	Refuse collection is considered as part of the Borough Council's service provision and is not a direct matter for the SCAAP to address.
CS83	With new buildings taking car parking where will they go? Approx 6 million trippers over the summer period.	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
CS84	Individual rubbish bins for each house will mean less bags on the streets	Refuse collection is considered as part of the Borough Council's service provision and is not a direct matter for the SCAAP to address.
CS85	Street furniture – replace 'Black Balls' with posts at Hartington Road	Specific elements of street furniture would be considered at the design stage of any scheme in accordance with the Council' adopted Streetscape Manual SPD3.
CS86	City Beach shared space needs to be re-designated with proper pavements	This is not a direct matter for the SCAAP to address. Safety issues are considered as

	and audible crossings – current crossings are not legal	part of wider on-going traffic management and maintenance proposals.
CS87	Bus service from OS8, via Kursaal, to Chalkwell required	Policy DS5: Transport, Access and Public Realm seeks to improve traffic accessibility including appropriate provision for public transport. Bus routes are considered as part of partnership working with bus operators.
CS88	OS9 – needs a proper bus service and segregated cycle/ pedestrian routes	Such issues will be considered at the detailed design stage.
	<b>BUSINESS</b>	
CS89	Coach & car parking for conference facilities	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.  The museum development provides for conference facilities with additional parking.

<b>PA8 - Victoria Gateway Neighbourhood Policy Area</b>		<b>Council Response</b>
	<b>PUBLIC</b>	Noted.
V1	Support aims of the conservation area preserving important buildings	Noted.
V2	Support secondary frontage on West Street – should not be turned into housing	Noted.
V3	OS11- Support residential development on existing empty office blocks, but do not build on car parks on Baxter Avenue	Noted.
V4	OS11 – Support open space at Victoria Avenue. Also need CCTV to limit anti-social behaviour	Noted.
V5	Development good standards of design for buildings	Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles seeks to achieve this in combination with other adopted planning policy, notably Policy DM1 – Design Quality of the Development Management Document.
V6	Making the area more aesthetically pleasing	Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles seeks to achieve this.
V7	Enticing businesses	Business promotion is pursued under wider economic development proposals.
V8	<b>BUSINESS</b>	
V9	Re-development of Vic Avenue office blocks	Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles seeks to achieve this.

	<b>PUBLIC</b>	
V10	Churchill Gardens feels unsafe to walk through – improvements may uplift area and make it feel safer	Improvements to existing public parks are considered as part of wider parks serving and maintenance provision.
V11	Artist workspaces – to attract arty people to the area	Reference to cultural faculties, which could include artist workspace is included in the policy.
V12	Need healthcare, dentists, schools and jobs for residents	Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles seeks to achieve this within the Policy Area or as part of the wider SCAAP regeneration.
V13	Protect Edwardian homes and other buildings of merit	The Plan seeks to protect and enhance the areas Conservation Areas, listed and locally listed buildings and other heritage assets.
V14	Need care of the street scene	Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles seeks to achieve this together with the Council's Streetscape Manual SPD3.
V15	More trees, everywhere and landscaping	Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles seeks to achieve this.
V16	Very few public toilets are accessible	Noted. Toilets and related facilities will be considered at the design stage of any redevelopment scheme.
	<b>PUBLIC</b>	
V17	Demolish concrete bridge between Vic station and Vic Shopping Centre	This provides access to and as such is considered to be an integral part of the Victorias shopping centre.
V18	Lack of open and green space, improve landscaping	Policy PA8 seeks to achieve improved 'urban greening' in the area.
V19	OS13 – keep football club at Roots Hall and improve overall offer	The Football Club have long-term proposals to relocate to Fossetts Farm. The Core Strategy supports the relocation of the Football Club. However, Opportunity Site 13 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.
V20	OS13 – current location is a sustainable location for football stadium	The Football Club have long-term proposals to relocate to Fossetts Farm. The Core Strategy supports the relocation of the Football Club. However, Opportunity Site 13 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.
V21	OS13 – redevelop football stadium where it is	The Football Club have long-term proposals to relocate to Fossetts Farm. The Core Strategy supports the relocation of the Football Club. However, Opportunity Site 13 will not be included in the final version of the SCAAP as there is insufficient evidence that the site will come forward before 2021, the end of the SCAAP's plan period.
V22	OS11 – if residential where will the cars be located and stored	Car parking provision will be considered at the design stage in accordance with adopted car parking standards in the Development Management Document.
V23	Need to ensure good quality design of buildings that is distinctive to Vic.	Policy PA8 seeks to achieve this.

	Avenue	
V24	Churchill gardens are sub-standard – need action now.	Improvements to existing public parks are considered as part of wider parks servicing and maintenance provision.
V25	Need to create a more visible link to the High Street – currently there is no view of it from Vic. Avenue.	It is proposed that further wording is incorporated into Policy PA2: London Road Policy Area Development Principles related to improving legibility and pedestrian access, alongside improvements to the public realm.
V26	Beecroft Art Gallery could be made higher profile. A wonderful asset to the town but poor advertising and publicity.	Such provision is considered as part of the Council's wider cultural strategy.
V27	Museum could be made higher profile and could be a strong cultural asset to the town	Noted.
V28	Ensure good access to development so that it doesn't affect Vic. Avenue – and ensure adequate parking.	Noted. Car parking and access provision will be considered at the design stage in accordance with the Development Management Document and Core Strategy.
V29	Improve pedestrian access into the High Street from Victoria area.	It is proposed that further wording is incorporated into Policy PA2: London Road Policy Area Development Principles related to improving legibility and pedestrian access, alongside improvements to the public realm.
V30	Pedestrians and cyclists should be separated, not shared facilities	Noted. Detailed consideration of mixed-mode pedestrian and cycle routes will be undertaken at implementation stage and will take account of best practice and guidance.
453 V31	Don't want tables and chairs in public spaces, unless there is a 3ft barrier around them	Noted. This is covered by licensing policy and the associated department at the Council.
V32	Offices need to be accessible	Noted.
	<b>BUSINESS</b>	
V33	Connectivity to the Town Centre	It is proposed that further wording is incorporated into Policy PA2: London Road Policy Area Development Principles related to improving legibility and pedestrian access, alongside improvements to the public realm.
V34	Urgent that PA8.1 Heath and Carby and PA8.8 Victoria House are redeveloped	Noted.
V35	Walking access from rail station for people with luggage	The Victoria Gateway junction improvements have greatly enhanced and improved pedestrian linkages to the town centre.
V36	Public space needs to be more active and animated, including Vic. Circus	Policy PA1: High Street Policy Area Development Principles and PA2: London Road Policy Area Development Principles seeks to achieve this.
V37	Better signage to High Street and Seafront	It is proposed that further wording is incorporated into Policy PA2: London Road Policy Area Development Principles related to improving legibility and pedestrian access, alongside improvements to the public realm.
	<b>COUNCILLOR</b>	
V38	Potential for use in this area	Noted.

V39	Zoning of car parking in the town centre	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
V40	Potential for a viaduct at this junction	Noted.
	<b>PUBLIC</b>	
V41	When museum is relocated to cliffs, what will happen to the existing one?	The building will be preserved and its setting enhanced as part of Policy in the SCAAP. This matter for Council's wider cultural strategy provisions rather than directly related to the SCAAP.
V42	Bus service should be accessible and reliable and available evenings and weekends	Bus services and routes are reviewed as part of partnership working with the bus operators.
V43	<b>BUSINESS</b>	
V44	Free parking after 6pm	Car parking charges/regime are reviewed as part of the Council's wider traffic management proposals.  The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
V45	Road signage for airport from Southend Central	Policy DS5: Transport, Access and Public Realm seeks to improve road signage.
V46	Traffic flows and way finding and issue with parking costs	Policy DS5: Transport, Access and Public Realm seeks to improve traffic management /road signage. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	<b>COUNCILLOR</b>	
V47	Timings on traffic lights can be problematic	Traffic timings on traffic lights are reviewed as part of wider traffic management proposals.

PA9 - Sutton Gateway Neighbourhood Policy Area		Council Response
	<b>COUNCILLOR</b>	
S1	Agree with brownfield sites being developed first	Noted.



S2	OS12 – support more housing – flats and apartments to create density	Noted.
	<b>PUBLIC</b>	
S3	Total lack of planning for floods – no more shared space	Policy DS4: Flood Risk Management and Sustainable Drainage provides for appropriate measures for flood risk management and sustainable drainage.
S4	Need to ensure cycle land is appropriate for the road size – dedicated doesn't work everywhere	Noted.
S5	Keep the area as commercial not residential	Policy PA9: Sutton Road Policy Area Development Principles seeks to maintain and promote Grainger Road and Short Street as employment growth areas. It is considered that there are planning merits of allowing a number of existing employment uses along Sutton Road to be redeveloped for additional housing, as evidenced by the Employment Land Review supporting document.
S6	Separate pedestrian and cycle routes and better paths	Noted, detailed consideration of pedestrian and cycle routes will be considered at implementation stage and will take account of best practice and guidance.
S7	Public toilets needed	Noted. Toilets and related facilities will be considered at the design stage of any redevelopment scheme.
	<b>COUNCILLOR</b>	
S8 455	OS14 – consider height of new residential, too high may affect existing residents	The height of buildings will be considered at the design stage of any development scheme. Policy DM4 of the Development Management Document sets out the Council's approach for managing tall and large buildings.
S9	Open up access to short street	Policy PA9: Sutton Road Policy Area Development Principles seeks to achieve an enhanced pedestrian/cycle route along Short Street.
S10	Improve aspects of the public realm. Ensure connectivity with other areas with good highway and pedestrian links.	Policy PA9: Sutton Road Policy Area Development Principles and other related aspects of the Plan seek to achieve this.
S11	Open space for all ages, including ball games etc	Noted.
S12	Include children's play area	Recreation provision within open spaces and parks is considered at the design stage of new proposals and reviewed as appropriate part of the Council's wider recreation provision.
S13	Please consider existing residents – houses not flats	The Plan seeks to achieve a variety of residential development and tenures appropriate to its location and setting to meet housing needs in lined with local policy. Policy DM7 of the Development Management Document sets out the Council's approach regarding dwelling mix, size and type.

Overall or other Issues	Council Response
All Sites – Support regeneration of sites – caveats maintain parking, provide	Noted. The approach to car park management will be informed by an independent car

	parking for development, increases legibility, enhances public realm, provides more critical mass	parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	Residents Parking in Milton Place	This is not a direct matter for the SCAAP to address. Residents Parking Schemes are reviewed as part of wider traffic management proposals.
	More trees everywhere	The Plan seeks to provide for improved landscaping, tree planting and 'urban greening' as appropriate.
	Vision statement should include opportunity sites (6,3,4,11,8 etc.) Policy Area black lines are unhelpful in this sense (Overall – Map 2)	The vision is an overarching aim and it is not appropriate to refer to specific opportunity sites.
	Where are the toilets?	Noted. This is not a direct matter for the SCAAP to address .Toilets and related facilities will be considered at the design stage of any redevelopment scheme.
	Concerned about the context – need to explain clearly how all the proposals inter-relate and their impact on traffic movements and car parking	It is considered that the Plan and the Policies Map clearly explains its context and how its policy provisions interrelate.
	Support the provision of more social housing. People are being priced out of the area	Noted. The adopted Core Strategy provides provision for affordable housing.
	Essential to get the document in place to make BIDs for government funding for transport and infrastructure improvements	Noted.
456	Ensuring long term use of retail spaces to be creative. If empty then fill them with smaller units in a market style, like Stratford has in the old shopping centre	Noted.
	A large retail anchor is needed with lots of quality small units	Policy DS1: A Prosperous Retail Centre seeks to improve the retail offer in the town centre.
	Please consider safer parking schemes	Safety is an integral consideration in the design of any road scheme.
	Council needs to be pro-active to stop vehicles parking on the pavement	The Council actively pursues traffic enforcement.
	Elmer Sq. project funds in Council budget 2016/17 – what impact does this have	The Implementation Section Tables will be updated including details of any allocated funding. Elmer Square phase 2 is outlined in PA3: Elmer Square Policy Area Development Principles.
	Improve legibility in the town centre, advertise where shops are (for pedestrians and vehicle users)	Noted. The Plan and wider tourism publicity seeks to achieve this.
	Further taxi ranks to help the elderly and disabled and more blue badge spaces	Noted. Taxi rank and disabled parking provision is considered as part of wider traffic management proposals for the town and location of proposed and existing are shown on the Policies Map.
	Insufficient road infrastructure coming into the town is killing business and events	The Council is actively seeking to improve road accessibility into the town through its Local Transport Plan provisions and partnership working/bidding for appropriate funding

		for infrastructure improvements.
	Any undercroft parking must be safe and useable	Noted.
	Car parking desperately needs to be considered in more detailed and a balance achieved	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	All policy areas should maintain green, open and public space and create new ones	The Plan actively seeks to achieve this.
	Cost of car parking is too high – differentiated parking costs need to be reasonable	Car parking charges are considered as part of wider traffic management proposals. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	Maintaining & improving the East-West transport & access through the SCAAP area	The Plan seeks to achieve this.
457	Parking spaces in the height of season is a major problem	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	Create a forum or lists of contacts for professionals with similar interests to come together	The Council actively promotes a Business Partnership and other partnership working.
	Complaints from customers staff attitude at the pier museum	Noted.
	Spending money on the library car park won't assist town centre & central seafront	Noted. Car parks repairs and management are considered as part of wider traffic management proposals.
	Issue of cliff slip at former yacht club	Noted. Policy DM14 of the Development Management Document sets out the approach for managing development close to land instability.
	Create links between vacant parking and transport in town	The Plan actively seeks to improve connectivity. The VSM system outlined in the SCAAP will assist this and part of the Transport, Access and Public Realm Strategy.
	Throughout the document the importance of public toilets, accessibility and outdoor seating should be made	Toilets and related facilities will be considered at the design stage of any redevelopment scheme.
	Bus stops need to be located closer to shops	Bus stops and routes are considered in partnership with the bus operators.
	Encourage more industrial space in the Borough for industry and pharmaceutical science-based firms	The Plan seeks to improve employment provision within the Central Area. New employment space is also being created at the new airport business park.
	Improved signage for heavy vehicles	The Plan seeks to achieve this in Policy DS5: Transport, Access and Public Realm

	More public art everywhere! "Leake Street" in London Waterloo	The Plan seeks to achieve this.
	Do not signpost new stadium and in particular retail/cinema/town centre uses – being built outside the SCAAP area i.e. Fossetts Farm would result in further decline of Prittlewell and town centre	Noted. Any development proposed as Fossetts Farm that included retail development would need to provide an impact assessment if over the qualifying threshold.
	How does this differ from the "Renaissance project"??	The Masterplan for the Central Area has been incorporated into the development of the SCAAP. The SCAAP will be a Council planning policy document which will include proposals for development as well as guide any prospective planning applications.
	Is the plan for Victoria Avenue to knock down the existing buildings? Or Renovate	A mixed use residential led scheme is proposed in Policy PA8: Victoria Gateway Neighbourhood Policy Area Development Principles. It plans for comprehensive redevelopment but there may be some retention of existing buildings owing to the new Government Prior Approval process.
	Improvement to the bus station to allow easier access	Policy PA7: Tylers Policy Area Development Principles seeks to achieve this.
	No mention of people anywhere – effect of vision on all ages should be stated	The Plan is designed to be fully inclusive of all persons.
	Street lighting – improve above standard regulations – make street feel safe and encourage people walking	Policy DS5: Transport, Access and Public Realm seeks to maintain an appropriate level of street lighting.
	Public transport needs serious improvements. Currently very poor and bus companies need to work together /co-ordinate.	Policy DS5: Transport, Access and Public Realm seeks to provide for improved sustainable transport provision. The Council works in partnership with bus operators with the objective of improving facilities.
458	Loss of parking facilities could cause problems. Please consider distance, disabled management of all car parks, public transport and coach drop off zones	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	Stronger links to transport improvements around the Borough	The Plan seeks to improve connectivity.
	Maintain and enhance Southchurch Road and Woodgrange Drive	Policies PA4: Queensway Policy Area Development Principles and Policy CS1: Central Seafront Policy Area Development Principles seek to achieve this.
	Advertising transport links to the Hospital Clearly, Shuttle bus service from town centre to hospital	The Hospital is outside the SCAAP area. The SCAAP seeks to improve and enhance public transport and signage.
	Will the roads from the east of the borough be affected by the proposals including the construction phase?	Proposals will not affect east/west links by road.
	All these potential developments of the High Street and environs would be negatively affected by the threatened environmentally destructive development of Fossetts Farm which as Basildon Council leader said with reference to the effects of out of town "retail parks" have had on his area "suck the life out of the town centre". Build up the High Street , seafront etc. Government leave the Fossetts Farm Green Belt alone!	The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study). Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy and require a retail impact assessment.

	Graffiti at first and second floor on high street and on the new bridge – can BID do something about it?	The Council actively pursues a programme of removing graffiti. It is something that the BID may be able to address.
	Site 10 Woodgrange Estate will require full input on BREEAM and Secure by Design	Noted.
	Summer holiday park and ride outside town	Park and Ride schemes have been considered a number of times in recent years but have not been considered feasible given the limited land available and linear peninsula geography of the town. Even so, the provision of Park and Ride would only be feasible outside the SCAAP boundaries. Such options will be kept under review as part of the Local Transport Plan and development of the Southend Local Plan.
459	Provision of motorcycle parking with shelter and secure	<p>The SCAAP seeks to promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre and access to the seafront by encouraging improvements to the quality of access to parking so that it is convenient, well-signposted, safe and secure.</p> <p>It is considered that reference to a range of parking types, including for motorcycle's, should be made within Policy DS5.</p> <p>The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.</p>
	All areas – car parking is a key issue, need to provide enough parking for new development and public spaces	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	How will the proposed Fossetts Farm Retail Development affect this, Is there room for both?	The Fossetts Farm proposals are located outside the SCAAP boundaries. Planning permission for retail development at Fossetts Farm has been previously granted and its potential impact taken into consideration in the preparation of the Core Strategy and SCAAP (see also Southend Retail Study). Any new proposal at Fossetts Farm will require planning permission, be subject to planning policy and require a retail impact assessment.
	Parking development on seafront needed	Noted. The approach to car park management will be informed by an independent car parking Study which will investigate and present findings on the capacity of the car parks that service the town centre and central seafront area. This will be included in the submission version of the SCAAP.
	Better connected roads around the town, too much one way or disconnected	Policy DS5: seeks to improve traffic management in the Central Area.

	from one another	
	What is the scope for additional new development outside of the main town/urban area?	This is an issue for the new Local Plan.
	Making spaces available for community projects/group (such as empty shops/buildings)	<p>A number of policy areas seek to promote the provision of social and community infrastructure. Policy DS1 seeks to encourage the landowner/landlord to display local art within the windows of empty shops to create visual interest from the public realm.</p> <p>Reference to marketing will be included for vacant units. In respect to Policy DS1 vacant units could include units occupied for temporary or 'flexible' uses, permitted through a temporary planning permission or under permitted development rights.</p>



## **Appendix 5: Summary of the Sustainability Appraisal for the Southend Central Area Action Plan Preferred Approach (December 2015)**

7.1 The following paragraphs provide a summary of the Sustainability Appraisal comments made to each issue raised in the Southend Central Area Action Plan Preferred Approach (December 2015).

**TO BE INSERTED – Please Refer to Sustainability Appraisal**





# Southend on Sea Central Area Action Plan Revised Proposed Submission Version

**Sustainability Appraisal**

On behalf of **Southend on Sea Borough Council**



Project Ref: 28511/3002 | Rev: AB | Date: September 2016



## Document Control Sheet



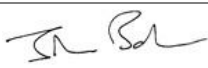
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Revision	Date	Description	Prepared	Reviewed	Approved
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AB	02/09	Updated to take into account change to SCAAP resulting from draft SA	01/09	02/09	02/09

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# 1 Introduction

## 1.1 Background

- 1.1.1 Peter Brett Associates LLP (PBA) has been commissioned by Southend-on-Sea Borough Council (SBC) to undertake the Sustainability Appraisal (SA) of the Southend Central Area Action Plan (SCAAP).
- 1.1.2 This report documents the process of SA that has been completed to date of the iterations of the SCAAP.

## 1.2 The Southend Central Area Action Plan

- 1.2.1 The purpose of the SCAAP is to set a detailed and comprehensive planning policy framework for Southend's Central Area to guide the delivery of development to 2021. The SCAAP sits in the context of the other planning documents that make up the Local Development Framework (LDF) for the Borough. These include the Core Strategy, which sets the policies that set out the strategic approach to development in the Borough and covers the general spatial approach to development and the high tier policies to manage the delivery of development. The Development Management Document (DMD) also sets Borough-wide policies to be used in determining development delivery throughout the plan area, including in the Central Area. The other AAP prepared in Southend is the London Southend Airport Joint Area Action Plan that covers the airport and its environs and is located in the north of the Borough and extends into Rochford district.
- 1.2.2 The SCAAP, with its current boundary, has been through several preparation stages to reach the current 'Preferred Approach' version. These stages have entailed:
- SCAAP Issues and Options (March 2010): this was the first version of the AAP that covered Southend Central including parts of the seafront, prior to this stage separate town centre and seafront AAPs were proposed.
  - SCAAP Proposed Submission (October 2011): this took forward the discussion of Issues and Options in the earlier version to present a complete set of policies to for the Central area;
  - SCAAP Preferred Approach (October 2015): this version again set out a preferred policy approach for consultation, which includes some options for consultees to consider. At that stage it was necessary to take a step back in preparation to ensure the SCAAP was in keeping with national planning policy as published in the National Planning Policy Framework (2012), as well as reflected the Southend DMD (2014) that had been adopted in the intervening period; and
  - SCAAP Revised Proposed Submission Version (October 2016): this is the current version of the SCAAP addressed through this SA Report. This version of the SCAAP included revisions to the Preferred Approach as a result on comments made at consultation, as well as new sites for allocation that have come forward since 2015.
- 1.2.3 The SA of the SCAAP has been an ongoing process through each consultation stage of the SCAAP, with an SA report prepared at each consultation stage.

## 1.3 The Sustainability Appraisal Process

- 1.3.1 The purpose of the SA is to review the content of the emerging plan and articulate to the plan makers during preparation of a plan, and to a wider audience at consultation the likely effect of the plan on achieving sustainable development. The aim is to help mitigate any adverse effects as far as possible as well as securing benefits. The SA method is in line with good practice on SA and the European Community Directive on Strategic Environmental Assessment (SEA).
- 1.3.2 In undertaking the SA it is essential that assessment remains focused on those issues that the SCAAP could influence and be influenced by. More detailed information on the SA process can be found in the SA Core Strategy, which should be read alongside with this SA report.
- 1.3.3 As well as helping make a better plan there is a legislative requirement for SA. The statutory requirement for SA is from the Planning and Compulsory Purchase Act 2004. With planning policy set in the National Planning Policy Framework (NPPF) 2012 paragraph 165 which states: *“A sustainability appraisal which meets the requirements of the European Directive on strategic environmental assessment should be an integral part of the plan preparation process, and should consider all the likely significant effects on the environment, economic and social factors”*. The European Strategic Environmental Assessment (SEA) Directive (2001/42/EC) that is referred to transposed into national law in the Environmental Assessment of Plans and Programmes Regulations 2004 (SEA Regulations). The SEA Directive sets the framework for what any SA report should contain as the requirement from Planning and Compulsory Purchase Act 2004 is not specific on this. Therefore, this SA Report follows the requirements of the SEA Directive and Regulations.
- 1.3.4 In line with the SEA Regulations a report must be prepared (the ‘SA Report’) and published for consultation alongside a draft of the plan. This report must identify the likely significant effects of the plan, as well as ‘reasonable alternatives’.
- 1.3.5 The SA should consider the following questions:
- Could these effects be of special significance?
  - Are there ways of reducing or mitigating adverse effects?
  - Can any beneficial effects be further enhanced by positive planning?

## 1.4 The Planning Context

- 1.4.1 The SCAAP has been developed as part of suite of documents that together form the planning policies for Southend, as part of the LDF. All relevant component documents have undergone SA these include:
- the Core Strategy Development Plan Document;
  - the DMD;
  - the London Southend Airport and Environs Joint Area Action Plan;
  - Design and Townscape Guide Supplementary Planning Document;
  - the Planning Obligations Supplementary Plan Document. and

- the Streetscape Manual Supplementary Planning Document
- 1.4.2 These other plans particularly the Core Strategy and DMD set a framework of the SCAAP and reduce the extent to which alternatives and options can be considered now. The SCAAP should not seek to cover matters covered by these other parts of the LDF and should be in conformity with it. For this reason the SA of the SCAAP should not reappraise any issues covered elsewhere in the LDF and have therefore already been addressed in other SA Report. Most relevant to the SCAAP are the Core Strategy, which sets the framework for development throughout the Borough and the DMD that sets policies to manage delivery of development. The SA of both of these plans can be found on the Southend LDF website and should be read for further detail on the overall sustainability implications of the LDF, beyond the role of the SCAAP.
- 1.4.3 In addition, it is important to recognise that the Community Infrastructure Levy would be payable on new development, including residential and commercial development. The requirement in Southend was brought into force in July 2015. The money collected will be spent on new infrastructure within the Borough (i.e. roads, flood defences, schools, parks) to support growth.

## 1.5 The SA Report

- 1.5.1 SA reporting consists of several main sections, these are:
- To define sustainable development in the context of development in Southend, through investigation of background material on sustainability and identifying the sustainability issues and context for central Southend and, where appropriate, the Borough as a whole (Section 2);
  - How sustainability considerations have helped guide the preparation of the SCAAP to date, including the SA of alternatives to the preferred approach based on the understood context and how mitigation has been incorporated to help reduce effects (Section 3);
  - What the sustainability effects of the SCAAP are at this stage, for instance, what the implications would be for sustainable development if the policies and proposals of this version of the SCAAP were implemented as proposed (Section 4).
- 1.5.2 There is also a need to consider 'what next' and this may include additional stages of SCAAP preparation as well as proposals for monitoring the SA once the plan is complete (Section 6). Following this stage of the SCAAP it will move to submission alongside comments made to the Revised Proposed Submission Version plan. If necessary, there may be amendments made to the SCAAP before or during the examination period. If this is the case these will be screened to determine if they are likely to have any effects that need to be addressed through an SA. For each published stage the SA Report will be updated to reflect changes and identify sustainability effects and recommendations. A final SA Report will be available as part of the adopted SCAAP and to include the monitoring proposals, as well as an accompanying 'adoption statement' as required by the SEA Regulations.



## 2 Baseline and Context

### 2.1 Introduction

- 2.1.1 This section of the SA Report provides a review of the character and context of the SCAAP to inform the SA. The process of gathering data and information about the area has been ongoing through each stage of the SA, updated at each stage to ensure information is relevant.
- 2.1.2 The initial full scoping process, including the preparation of a Scoping Report for consultation with statutory consultees, was undertaken for the SA of the Core Strategy and subsequently for the Core Strategy Issues and Options reporting. As the Core Strategy is the overarching strategic part of the LDF this scoping stage also covered the SA of the SCAAP, which is a daughter document to the Core Strategy. For more information refer to the Core Strategy SA on the Southend's LDF website.
- 2.1.3 **Appendix A** contains a review of the full plans and programmes and baseline information and this section summarises the findings.
- 2.1.4 The baseline information and objectives of other plans and strategies are then combined into a set of sustainability objectives that define sustainable development for the SCAAP and which are used as the basis for the SA.

### 2.2 Summary of sustainability issues

- 2.2.1 From the baseline information and review of plans and strategies key issues have been identified that the SCAAP may have a role in helping to address and that the SA will need to take into account in defining sustainable development for the Central Area. These are:

#### The environment

- development should help in the continued enhancement of the built environment in the Central Area, with new buildings of high quality and developed to sound urban design principles and reduce inequalities in the quality of the built environment;
- there is a lack of public open greenspace in the town centre, wherever possible opportunities need to be taken to improve urban greening and introduce new public open space;
- the area is under quite high risk of flooding, although direct tidal inundation is largely mitigated for through sea flood defences. However, surface water flooding in times of high rainfall may be more of a risk to the Central Area, and effects of climate change will increase this and therefore new development will need to take this into account, including through the provision of sustainable drainage;
- nature conservation and biodiversity assets within the built up area are limited, and every attempt should be made to conserve and enhance existing assets, and create new ones, as well as the protection and enhancement of wildlife corridors
- habitats of international significance are located within the Borough, although outside the built development boundary. These must be protected not only from direct disturbance

from development but also change that would threaten their integrity, such as increased pollution or changes in water availability;

- there are increasing traffic levels in the Borough and Central Area, with consequences for air quality, and new development must help to limit any increase in this, by endeavouring to suggest a change to travel patterns (number, length and mode), through the spatial strategy. The number of people who walk to work or use public transport is higher than the national average, although over a fifth drive (Census 2011);
- the Central Area is well connected in terms of public transport with three railway stations within the area and the bus station. However, there is a need to improve the pedestrian and cycling environment, especially where dual carriageways and the railway lines bisect the area;
- studies have identified limits to the availability and accessibility of open space of different types and standard, especially in central Southend-on-Sea; and
- the quality of the built environment is important, not only with the effect of new building in 'mending the fabric', but also in affecting existing areas of identifiable character. Parts of central Southend are characterised by a current low quality in the built environment, although the underlying quality of the natural and built environment is high in many areas with many listed and landmark buildings and several Conservation Areas.

## Communities

- the Central Area has a role to play in creating a high quality residential environment that encourages people to take healthy lifestyle choices, such as more Active Travel. To achieve this there is a need to improve the public realm to encourage more people to walk and make cycling safer. This includes reducing severance caused by main roads running through the area and links over the railway;
- approximately 22,000 people live in the Central Area, 13% of the Borough's population; there is an identified need for affordable housing in the Borough and in central Southend. Most of the housing in the Central Area is flats or maisonettes (and one-bedroom properties are more prevalent). There may therefore be a need for larger units including family homes. There is a demand for affordable housing, including the need to provide new homes to replace others lost, this will include development as part of the 'Better Queensway' project;
- the Central Area is well served by public transport and in particular railway services, there is a need to improve some bus services including reducing journey times on the A127 to encourage more people to travel by bus and link the new major employment hub at the airport;
- there are longstanding inequalities in the Central Area with pockets within the Kursaal, Victoria and Milton wards all experiencing high levels of deprivation, which are some of the highest in in England compared to other areas of the same or neighbouring wards with low levels of deprivation (e.g. Chalkwell ward contains some of the least deprived areas in England);
- there is the potential to improve the cultural offer in Southend, including through providing more services for the community such as a new public square for events;
- the Central Area can accommodate more residential development, helping reduce pressure on other parts of the Borough, including potential threats to open space and agricultural land on the periphery; and

- there is a growing student population, both resident and visiting, in the Central Area.

## The economy

- The Seafront is a principal asset in the Central Area, attracting visitors to the town and also being well used by the local community. Routes along the seafront also provide a sustainable transport route for walking and cycling;
- the Central Area is a focus of employment for the Borough (33% of total employment), while this role needs to be maintained it is important to ensure high quality jobs in a range of employment sectors to avoid the need for residents to commute to London for work;
- There are a mix of employment types in the Central Area, with the financial sector, real estate more prevalent than for the Borough as a whole, there is also a growing sector of cultural and creative industries;
- car parking needs to continue to be managed in the Central Area to support the retail and visitor economy but help encourage more people to travel to work by sustainable modes and ensure land is used efficiently;
- there are 6 million visitors to the Borough a year, focused in the Central Area, the majority are day visitors who spend around £330 million / year. Tourists and visitors to the Central Area need to be encouraged to stay longer and spend more through the creation of new visitor attractions and support for hotels;
- the Central Area is the focus for tourism and creative industries in the Borough, as well as the location of much of the office stock, some of which is dilapidated and in need of renewal. The public transport connectivity of the Central Area make it ideally suited to high employee density development, such as office space;
- the Gross Value Added of Southend is lower than the regional average and there is a need to encourage more jobs that retain the skilled and professional workforce in the town;
- the town centre is the main retail area of the Borough, although retail vacancy is above national levels in some areas. Much of the Central Area retail is dominated by national retailers rather than independents with the main retail core along the High Street between the Royals and Victoria Shopping Centres;
- there is a great disparity in the types of jobs held by residents of the Central Area, the greatest proportion of are in 'professional' occupations and the second highest proportion are in 'elementary' occupations; and
- the Central Area has become the focus for higher and further education with development by Southend Essex College and University of Essex bringing landmark buildings into the area and an increasing student population.

## 2.3 Sustainability Framework

- 2.3.1 In order to be able to test the emerging policies of the SCAAP a set of sustainability objectives as part of a 'sustainability framework' has been prepared, **table 2.1**.
- 2.3.2 This framework is made up of a number of sustainability objectives that have been derived from the characterisation and context of the Borough. These reflect the principal elements of sustainable development over which the SCAAP could have some influence. The framework below is based upon that in the Core Strategy Sustainability Appraisal Framework. Changes

have been made to bring it up-to-date, based on the updated policy context, the baseline data as reported in **Appendix A** and Section 2.2.

2.3.3 The framework shows headline sustainability issues and how these could be expected to change to demonstrate more sustainable development. The objectives for each headline relate to the plan area. To help monitor the objectives a suggested set of indicators is included in **Appendix G**.

2.3.4 Further details on the derivation of the objectives of the sustainability framework are shown in the Core Strategy SA report, including the Scoping stage report.

Table 2.1: Sustainability Appraisal Framework for the SA of Southend-on-Sea LDF

Concern	Explanation and desirable direction of change	Objectives
<b>Social progress which recognises the needs of everyone</b>		
Accessibility	<ul style="list-style-type: none"> <li>▪ enable all to have similar and sufficient levels of access to services, facilities and opportunities</li> </ul>	<ul style="list-style-type: none"> <li>▪ maintain Southend Central Area as the centre for all services, as the most accessible location</li> <li>▪ improve accessibility to the town centre</li> <li>▪ improvement in public transport accessibility along the entire length of the seafront</li> </ul>
Housing	<ul style="list-style-type: none"> <li>▪ to provide the opportunity for people to meet their housing need</li> </ul>	<ul style="list-style-type: none"> <li>▪ ensure a sufficient number of dwellings</li> <li>▪ encourage a suitable mix of dwellings, including tenure and size</li> </ul>
Education & Skills	<ul style="list-style-type: none"> <li>▪ to assist people in gaining the skills to fulfil their potential and increase their contribution to the community</li> </ul>	<ul style="list-style-type: none"> <li>▪ improve accessibility to employment and education facilities</li> <li>▪ support continued development of the University campus in the town centre</li> </ul>
Health, safety and security	<ul style="list-style-type: none"> <li>▪ to improve overall levels of health, reduce the disparities between different groups and different areas, and reduce crime and the fear of crime</li> </ul>	<ul style="list-style-type: none"> <li>▪ improvements to reduce fear of crime in the town centre, especially at night</li> <li>▪ improve pedestrian routes through the town centre and seafront to help design out crime</li> </ul>
Community	<ul style="list-style-type: none"> <li>▪ to value and nurture a sense of belonging in a cohesive community, whilst respecting diversity</li> </ul>	<ul style="list-style-type: none"> <li>▪ improve the viability and distinctive character of Southend town centre</li> <li>▪ provide public art and improvements to the design of seafront tourist buildings, such as beach huts and kiosks to provide a recognisable unified approach for Southend</li> <li>▪ provide new community open spaces in the town centre and seafront</li> </ul>

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Concern	Explanation and desirable direction of change	Objectives
<b>Effective protection of the environment</b>		
Biodiversity	<ul style="list-style-type: none"> <li>to maintain and enhance the diversity and abundance of species, and safeguard these areas of significant nature conservation value</li> </ul>	<ul style="list-style-type: none"> <li>protect undeveloped parts of the coastline</li> <li>protect key habitats directly or indirectly from developments which may harm them</li> <li>ensure new development brings enhancements to the built environment where appropriate</li> <li>ensure 'appropriate assessment' of all development is carried out where appropriate</li> </ul>
Landscape character	<ul style="list-style-type: none"> <li>to maintain and enhance the quality and character and cultural significance of the landscape, including the setting and character of the settlement</li> </ul>	<ul style="list-style-type: none"> <li>protect undeveloped parts of the coastline</li> <li>retain notable features and areas of open space along the coast line</li> <li>protect views of the estuary</li> </ul>
Built environment	<ul style="list-style-type: none"> <li>to maintain and enhance the quality, safety and distinctiveness of the built environment and the cultural heritage</li> </ul>	<ul style="list-style-type: none"> <li>enhance and protect land mark and listed buildings on the sea front</li> <li>enhance and protect listed buildings and those of interest in the town centre</li> <li>improve urban design quality through policy</li> <li>protect existing and create new open and green space</li> </ul>
<b>Prudent use of natural resources</b>		
Air	<ul style="list-style-type: none"> <li>to reduce all forms of air pollution in the interests of local air quality and the integrity of the atmosphere</li> </ul>	<ul style="list-style-type: none"> <li>reduce traffic congestion in the town centre</li> <li>encourage freight modal shift and encourage a reduction in emissions of new buildings</li> </ul>
Water	<ul style="list-style-type: none"> <li>to maintain and improve the quantity and quality of ground, sea and river waters, and minimise the risk of flooding</li> </ul>	<ul style="list-style-type: none"> <li>ensure no increased risk of coastal flooding</li> <li>acknowledge the risk to water quality from on-shore developments</li> </ul>
Land	<ul style="list-style-type: none"> <li>to use land efficiently, retaining undeveloped land and bringing contaminated land back into use</li> </ul>	<ul style="list-style-type: none"> <li>protect undeveloped coastline in the Borough</li> </ul>

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Concern	Explanation and desirable direction of change	Objectives
		<ul style="list-style-type: none"> <li>encourage development on previously developed land</li> <li>encourage high density residential development and mixed use development in the town centre</li> </ul>
Soil	<ul style="list-style-type: none"> <li>to maintain the resource of productive soil</li> </ul>	<ul style="list-style-type: none"> <li>protect productive soil where applicable (little overall impact likely)</li> </ul>
Minerals and other raw materials	<ul style="list-style-type: none"> <li>to maintain the stock of minerals and other raw materials</li> </ul>	<ul style="list-style-type: none"> <li>minimise use of aggregates for new development (relevance to sea defences)</li> </ul>
Energy sources	<ul style="list-style-type: none"> <li>to increase the opportunities for energy generation from renewable energy sources, maintain the stock of non-renewable energy sources and make the best use of the materials, energy and effort embodied in the product of previous activity</li> </ul>	<ul style="list-style-type: none"> <li>encourage efficient use of energy use of more energy from low carbon sources</li> <li>encourage decentralised energy supply, including through renewable energy or CHP.</li> </ul>
<b>Maintenance of high and stable levels of economic growth and employment</b>		
Local economy	<ul style="list-style-type: none"> <li>to achieve a clear connection between effort and benefit, by making the most of local strengths, seeking community regeneration, and fostering economic activity</li> </ul>	<ul style="list-style-type: none"> <li>improve the viability and vitality of the town centre as economic hub for the Borough</li> <li>improve the viability and vitality of the seafront as a major and flexible tourist destination</li> <li>identify sites for local business start-ups in accessible locations</li> </ul>
Employment	<ul style="list-style-type: none"> <li>to maintain and enhance employment opportunities matched to the size of the local labour force and its various skills, and to reduce the disparities arising from unequal access to jobs</li> </ul>	<ul style="list-style-type: none"> <li>work to create new jobs in a range of sectors within the Borough</li> <li>work to make the coast a major destination for conferences</li> <li>support a diverse range of businesses premises to meet different needs, as well as supporting existing business clusters</li> </ul>
Wealth creation	<ul style="list-style-type: none"> <li>to retain and enhance the factors which are conducive to wealth creation, including personal creativity, infrastructure, accessibility and the local strengths and qualities that are attractive to visitors and investors</li> </ul>	<ul style="list-style-type: none"> <li>contribute to creating attractive environment for business to flourish</li> <li>improve access for all residents to a range of jobs</li> </ul>

## 3 SA of the Plan to Date

### 3.1 Introduction

- 3.1.1 This section of the SA Report sets out the process of SCAAP preparation and SA that has led to the current consultation version of the plan, in the preferred approach. This has included successive stages of preparation of the SCAAP and accompanying sustainability appraisal. The SA stages to date have included the appraisal of the preferred options and their alternatives, as well as iteration of the preferred approach from broad options.
- 3.1.2 This section also sets out the role of the SCAAP as part of the overall Southend LDF, with other documents providing the strategic overview and development management detail that the SCAAP does not need to repeat.
- 3.1.3 To demonstrate the iteration of allocations and proposals in the SCAAP area this section also identifies the reasoning behind the way that Priority and Opportunity Areas have been covered through the emerging SCAAP, to reach this 'submission version' stage.

### 3.2 Plan context

- 3.2.1 As set out in **Section 1** this SA Report of the SCAAP will form part of the LDF for Southend. All adopted component documents of the LDF have been subject to SA during their preparation and therefore the SA of the SCAAP does not reiterate any of the findings of these SA. The SA of the SCAAP also recognises that these documents together make up the LDF. So development proposals in the SCAAP area will need to comply with all other relevant policies of the LDF. For example, matters covered elsewhere include the quantum of development to be delivered, protection of the heritage environment and affordable housing delivery. There is no need for the SCAAP to repeat these unless they are adding area specific detail.

#### The core strategy

- 3.2.2 This SA process follows on from the SA carried out on the Core Strategy, which already identified key implications on sustainability in Southend, and provides a basis for this appraisal. There are no specific Core Strategy policies relating solely to the AAP area although several policies contain relevant criteria. Most specific is Policy KP1: Spatial Strategy which sets out the following provisions for the two areas:

**Policy KP1: Spatial Strategy:** 'Southend Town Centre and Central Area – to regenerate the existing town centre, led by the development of the University campus, and securing a full range of quality sub-regional services and providing 6500 new jobs and 2000 additional homes, and the upgrading of strategic and local passenger transport accessibility, including development of Southend Central and Southend Victoria Stations as strategic transport interchanges and related travel centres.'

- 3.2.3 The SA of the Core Strategy supported this policy in helping to deliver sustainability development. The emphasis given to continued improvement of the town centre as the focus for growth in the Borough and improved accessibility by non-car modes should have a



beneficial effect on delivering sustainable development. This is because the Central Area is the most accessible location for most people in the Borough. It is already being the focus for much of the employment, retail and leisure facilities makes it suitable for growth. Continued development here will support public transport accessibility for all, and walking and cycling improvements, with the overall aim of achieving a modal shift from car use to more sustainable travel, in addition to equitable access for all.

- 3.2.4 The central location of the University campus also helps improve physical access to higher education and the other education and skills training the university may offer. Providing a mix of uses in a relatively compact area is also welcomed as part of achieving sustainable development, with the continued improvement to the vitality of the town centre at all times of day.
- 3.2.5 There were some concerns in the SA of the Core Strategy over development in areas at risk of flood, and that continued maintenance or development of new flood defences may adversely impact on the biodiversity value of the foreshore.

### The Development Management Development Plan Document

- 3.2.6 The DMD was adopted in 2015 and subject to SA through the stages of its preparation, culminating in a final SA Report in 2014. The purpose of the document was to set out 'development management' policies that would be used in determining the suitability of planning applications for permission. The DMD covers a variety of topics that would help deliver more sustainable development for the area and consisted of 15 policies. The summary of the final SA Report findings, where that the DMD "*provides an additional level of detail to Core Strategy and national planning policies that should help deliver more sustainable development in Southend.*" The SA goes on to say:

*"There is much in the policies of the DPD that should help in delivering sustainable development. This includes addressing climate change through development, securing better quality design, retaining employment land and protecting the borough's natural and built assets."* (section 9).

- 3.2.7 Several of the policies aim to protect and enhance the built character of the Borough. For instance **Policy DM1** reference the standards and guidance that should be applied to the design of new development and **Policy DM5** sets the principles of protection of built heritage. In addition, **Policy DM6** covers the Seafront and the specific measures to manage this, as one of the Borough's greatest assets. The design of tall and large buildings is covered in **Policy DM4**, with the potential for ensuring these make a positive contribution to the townscape. Appendix D contains more detail on the coverage of DMD and Core Strategy polices relevant to the SCAAP.

### 3.3 The Stages of SCAAP preparation and SA

- 3.3.1 This is the fourth stage of consultation on the SCAAP in its current form, with the SA providing iterative feedback between each stage of preparation in the SCAAP.

#### Issues and Options – Town Centre and Seafront Area Action Plans

- 3.3.2 The initial stage of pre-preparation starting on the SCAAP was Issues and Options consultation in 2007 for separate '**Town Centre**' and '**Seafront**' Area Action Plans (AAP).
- 3.3.3 To a similar timetable, in 2007, Renaissance Southend Limited published the Central Area Masterplan, a non-statutory guidance document on renewal of the area. The principles of this masterplan were endorsed by the Council in 2008 as corporate policy. The masterplan linked the central seafront to the town centre and consultation responses on the Issues and Options plans identified the need for an amended site boundary for the Central Area. Therefore, plan makers decided to pursue an AAP for the Central Area. The 'Southend Central' AAP option was pursued combining the town centre and central seafront. This approach allows for a better coordination in the policies and proposals of the area, as many of the issues that the town centre will need to address link through to the seafront east of the pier, which was previously not included in the 'Town Centre' AAP area. This approach is likely to be beneficial in securing sustainable development as it allows for better integration of strategies to deliver economic and other benefits. The remainder of the seafront, outside of the Central Area, is covered through the Core Strategy specific DMD policies.

#### Issues and Options 2010

- 3.3.4 This was the first consultation stage on the SCAAP area as covered by the current consultation SCAAP.
- 3.3.5 The SA, at this stage, provided an opportunity to appraise the emerging options and approach to development of the area. The consideration of alternatives and identifying the relative sustainability impacts of these approaches is important for the SA and an SEA requirement. At this early stage the alternatives, or options, presented were very broad with decisions still to be made about the type and number of policies to be included, as well as on specific sites for development. Therefore, the approach taken to appraisal it was to provide an overview of the potential sustainability impacts, beneficial and adverse, of the SCAAP as proposed through the Issues and Options. The SA Report made recommendations of where the SCAAP policies and proposals have the potential to mitigate adverse effects, and also achieve further beneficial effects.
- 3.3.6 SA recommendations of the Issues and Options SCAAP are included in **Appendix F**.

#### Proposed Submission 2011

- 3.3.7 SCAAP Proposed Submission (October 2011) took forward the discussion of Issues and Options in the earlier version to present a complete set of policies and proposals for the Central area. The SA at this stage include an SA of all of the proposed policies, both sites specific and development management to determine the likely sustainability impact of the SCAAP and make recommendations for updates that could improve the beneficial impacts.

- 3.3.8 This version of the SCAAP was very detailed with almost 50 policies in addition site specific proposals. The length of the SCAAP was partially due to no current Development Management policies being in place at that stage as the document had not been adopted yet. This mean for the SCAAP area that development management decisions were reliant on saved policies in the Southend Borough Local Plan,; some of which may have become outdated, with sustainable development issues and a strategic vision for the area less well covered in policy.
- 3.3.9 The recommendations on mitigating adverse impacts and securing beneficial effects were taken into account in moving forward with the Preferred Approach SCAAP in 2015.

### Preferred Approach 2015

- 3.3.10 This stage of the SCAAP preparation shared many similarities with the Revised Proposed Submission Version. For instance, setting out the same suite of policies and containing a very similar set of sites for allocation. However, there were distinct differences, including some changes to the proposed allocations. To provide a more complete picture of the areas of growth and change within the SCAAAP area it included strategic sites as potential allocations within each of the Policy Areas. In addition, sites were included for comment that were likely to be deliverable only post-2021 – the end of the plan period.
- 3.3.11 When moving from Proposed Submission 2011 to the Preferred Approach the structure and length of the SCAAP changed, with the plan becoming more focused than previous versions appraised. This focus removed repetition within the plan and avoided the repetition of higher tiers of policy including from the NPPF, Core Strategy and DMD. This reflected comments in the SA of the Proposed Submission 2011 on the 'usability of the AAP' and the need to avoid internal repetition within the SCAAP where several issues are covered by a number of policies.
- 3.3.12 The Preferred Approach SCAAP also contained policy options for the retail policy, see Section 3.5 of this SA Report. As this is an issue that SBC were keen to receive feedback on from consultees.
- 3.3.13 SA recommendations and findings on the draft Preferred Approach were fed back to the plan-making team for the preparation of the final version for consultation. The appraisal of policies reflected the iteration of the SCAAP and how the initial SA findings had been taken into account.
- 3.3.14 No responses were received specifically relating to the SA Report.

## 3.4 Sustainability Appraisal of Strategic Options

- 3.4.1 Considering alternatives is an obligation of the Sustainability Appraisal (SA), and particularly the Strategic Environmental Assessment (SEA) process. That requires, '*reasonable alternatives taking into account the objectives and the geographical scope of the plan or programme*' (paragraph 12(2)), *are appraised and documented in the SA report*'.
- 3.4.2 For the SCAAP there have been few explicit options put forward for consideration. Most options have been considered as part of the iterative process of plan making and the gradual evolution of alternatives for use of sites through the evolution of the SCAAP.

3.4.3 The Issues and Options SCAAP presented three strategic options for managing the development of central Southend, each with an increasing level of intervention and change. Following evaluation by the plan making team and input from the SA, the preferred approach was selected from the three presented.

3.4.4 The three spatial options were:

- Option 1: Strengthening the status quo;
- Option 2: Reinforcing the urban circuits; and
- Option 3: City by the Sea (the preferred option).

3.4.5 The Issues and Options SCAAP only provided a limited amount of information on what each option would mean in practice, and as a result there would be uncertainties in implementation. Therefore, the SA was kept brief with an identification of the main sustainability issues that might arise from pursuing each option and their relative sustainability effect. It should be noted that the preferred approach was Option 3, and it is this that the Issues and Options SCAAP and each subsequent stage of the plan is aiming to pursue.

#### **Option 1: Strengthening the status quo**

This would see the High Street remain the focus for all retail development in the centre, although some improvements will also take place in nearby areas. This is commensurate with a 'do-nothing' approach as it is what would occur without the SCAAP, relying on Core Strategy policy only.

- This option would focus on the High Street and this focus of resources may help in better securing improvements in this area. However, this option does not take advantages of wider environmental improvements that could help raise the overall image of Southend to potential investors (local and national), with wider benefits for the town. The option therefore may be too limited to successfully attract new inward investment.
- This option would also miss opportunities for more mixed use regeneration of the town centre, including additional education and cultural facilities and new housing.
- The overall scale of development may reduce impacts on natural resource use and on the natural environment.
- Lack of enhancement to public transport and improved movement routes is unlikely to encourage people to choose more sustainable modes. This option is unlikely to help promote Southend as a retail destination, with people choosing to make longer trips elsewhere for their shopping needs. However, fewer attractions in the town centre may reduce overall traffic volumes and avoid additional congestion.
- This option is unlikely to have any great benefit for the economic sustainability of the town, nor will help in meeting social sustainability objectives.

#### **Option 2: Enhance urban circuits**

This option would widen the central Southend regeneration and improvement to a larger area than Option 1. Chichester Road, London Road, Clifftown and Farringdon neighbourhoods would all be the focus for specific regeneration proposals. This option also includes Seaway car park as a new linking feature at the south east end of the High Street, although not a destination in itself.

- This option would help create a High Street of a quality to attract national retailers, and improving links to adjacent streets could help increase the attractiveness of the town centre for shopping and support retail businesses of the existing main route.
- This option does not include a residential element therefore not meeting objectives of delivering new homes or supporting a more mixed use and vibrant town centre.
- This option allows for growth of the education role of the town centre, supporting skills training, higher education and the related benefits this can bring to the town centre
- This option may risk not being bold enough in seeking regeneration potential, therefore missing opportunities to create a high quality town centre, which is successful in improving the image of Southend as a place to live, visit or do business.

### Option 3: City by the Sea

This is the most far reaching option seeking the greatest amount of regeneration of the town centre, surrounding area and mix of uses. It is taken forward as the preferred option for the SCAAP.

- This option would see the largest amount of redevelopment and therefore require the greatest use of natural resources. However, if new buildings use water and energy more efficiently than older ones it could have net benefits in the medium or longer term.
- This option is for comprehensive redevelopment of the town centre over a wide area, although with specific uses for different zones. It has the potential to have a positive benefit for Southend as a whole, improving its image as a place to live, visit and do business.
- This option may be the most risky to implement, particularly in a recession. However, having a joined up approach for the Central Area that sets out all development potential is likely to be beneficial in the long-term in ensuring a coherent redevelopment of the area. Implementation and funding schemes will need to be fully developed to ensure full delivery.
- Given the large amount of change this option may bring about, there is a need to ensure it is delivered in conjunction with other plans and strategies for the area to avoid adverse impacts. This could include travel and parking management, South Essex Rapid Transit (SERT) improvements and tourism strategies. This option does recognise the need for transport and movement improvements in the town centre, especially making walking a more attractive option.
- This option is for a large scale redevelopment of areas of the town centre, for a range of uses including employment. There is a risk that this will have an impact on regeneration and growth options for other parts of the Borough. Therefore, the plan makers should ensure the objective for growth in central Southend does not adversely impact on the objectives for growth in other locations.

3.4.6 The third option was one investigated in further detail through the rest of the SA, as the preferred approach.

3.4.7 SBC justify their choice of the 'City by the Sea' option as:

*“Based on a thorough understanding of opportunities and constraints and consultation feedback to date, the preferred overarching rationale for the Southend Central Area Plan is the creation of a ‘City by the Sea’ – a change in the function and transformation in the quality of the town centre and seafront and renewal to the remaining parts of the Town Centre.” (5.17)*

### 3.5 Retail Options

- 3.5.1 The Preferred Approach SCAAP included clear options for the proportion of the Primary Shopping Frontage that should be retained for retail. The options were:
- Option A: 70% protection of A1 use on the primary frontage;
  - Option B: Protection of retail use so that there are never more than two consecutive non-A1 uses and never below 50% of the total (other uses only A2 or A3); or
  - Option C: Protection of retail use so that there are never more than two consecutive non-A1 uses (other uses only A2 to A5).
- 3.5.2 The SA found that although Southend does experience a vacancy rate in retail units above the national average; this could be due to existing policies that restrict the use of retail units in this area to a retail use, meaning other A Uses, such as restaurants, financial services etc. are not permitted under usual circumstances. Therefore, allowing additional non-A1 uses could reduce this vacancy under Options B and C. However, these options could also undermine the Primary Retail Centre. Other uses such as restaurants, cafés, bars, banks, estate agents etc. can change the character and footfall of an area. There is also the risk that diluting the retail offer in these parts of centre may have a knock on effect in undermining the retail role, causing possible further decline in its function.
- 3.5.3 The preferred approach taken forward into the Revised Proposed Submission Version SCAAP is to further reduce the area of Primary Shopping Frontage to a more robust defensible area, as referred to as part of the recommendations of the SA. Within this space the decision has been made to pursue a mid-point between 'Option A' and 'Option B', with 60% retention of A1 'retail' uses. This was the identified as most likely to protect a sustainable town centre as it should help prevent the character of these areas and the 'core' retail area from dilution, by allowing protection of its primary use, but allowing some diversification. The aim is for this approach to allow higher quality type of commercial uses in these area (including restaurants) rather than simply relying on retail.

### 3.6 Opportunity Sites and Proposals

- 3.6.1 The role of the SCAAP is not to identify all of the sites in the Central Area that could be the location for new development during the plan period. Instead it identifies key areas where there is an identified need of regeneration and renewal and where there is reasonable prospect of this development coming forward during the plan period. Through the stages of plan making the way that these areas have been identified has changed, as has the specific delivery management criteria for each area.
- 3.6.2 From the start of the SCAAP preparation the plan area has been divided into a number of 'Policy Areas'. Although the way that these policies areas is referred to has changed from the earlier versions of the SCAAP to the Preferred Approach, as they are now referred to as Policy Areas and previously as Quarters and Gateway neighbourhoods, that was perhaps misleading in their function. Each Policy Area/Quarter or Gateway Neighbourhood is covered by a policy which was appraised as part of the SA Report 2011.
- 3.6.3 Within each of these policy areas there are specific sites that are the focus of regeneration and renewal, referred to as 'Proposal Sites' or 'Potential Development Sites' in the Proposed

Submission version 2011. All of these sites were covered by specific policies in the SA Report of the Proposed Submission 2011. There has been some iteration of these sites in the emerging SCAAP. These changes have been driven by the consultation on the report, revised evidence base, changing Council strategy and the SA.

- 3.6.4 **Appendix B** shows the iteration of the proposed sites from the Proposed Submission 2011 versions to the Preferred Approach 2015 version to the current Revised Proposed Submission version 2016. The Appendix shows how the previous SCAAP sites differed from the current version, the findings of the sustainability appraisal at that stage and the justification for the amendments.
- 3.6.5 The SCAAP has not included any alternatives to the Opportunity Sites and all those that are feasible for development and can contribute to the enhancement of the Central Area have been identified and therefore assessed in the SA, during its preparation. Should additional sites be promoted for inclusion these will be considered for allocation in the SCAAP during its preparation in the same way. More detail can be found on the appraisal of all proposed sites in the SA Report of the Preferred Approach 2015, available on the Southend SCAAP website. It should be noted that the allocation of these sites does not preclude other sites in the Central Area receiving planning permission should they comply with the design principles for the area in which they fall and other LDF policies.
- 3.6.6 No alternative sites have been excluded from allocation all major housing sites where there is evidence to demonstrate that they are capable of being delivered by 2021 (the end of the plan period) are allocated. This does not include any sites that are already committed for development (i.e. have an extant planning permission). More details of the iteration of sites included in the SCAAP are shown in **Appendix B** and the appraisal of allocated sites as part of **Appendix D**. Any sites identified as potentially suitable in the Preferred Approach SCAAP, but not included in the current SCAAP as they cannot be delivered by 2021, will be considered for allocation as part of the new Southend Local Plan that will be prepared.
- 3.6.7 The reorganisation of some of the Policy Areas (previously known as Quarters and Gateway Neighbourhoods) has made them into more coherent parcels of land with a shared aim. For example, the Queensway Policy Area is now more contiguous with the Queensway regeneration area and the High Street now includes both of the shopping centres emphasises the role of the retail core of the town. This should help in delivering more sustainable outcomes by enabling policy to clearly iterate the aims for each area.

### 3.7 Sustainability Appraisal of the Vision and Objectives

- 3.7.1 The SCAAP sets a vision and objectives for the area. These have been assessed at each stage of the emerging SCAAP, with little change between the Issues and Options 2010 and Proposed Submission 2011. The findings of the two stages of SA can be found in the relevant SA Reports on the SBC SCAAP website.
- 3.7.2 The objectives have undergone some amendment over the various iteration of the SCAAP to the current version. In their iteration the objectives have been amended to take into account the outputs of the SA, for instance from the Proposed Submission 2011 to the Preferred Approach additional material was included on the leisure and tourism role of the Central Area.

### **3.8 Sustainability Appraisal of the draft Revised Proposed Submission SCAAP**

- 3.8.1 PBA prepared a draft of this SA report on the SCAAP for the plan preparation team. This allowed for recommendations and mitigation proposed by the SA to be taken into account in preparing the final Revised Proposed Submission Version SCAAP for consultation. Details of these changes can be found in **Appendix D**, where the policy appraisal matrices show changes between draft and final versions of the Revised Proposed Submission Version SCAAP, which take into account the SA recommendations on the draft of this version of the SCAAP.



## 4 The SA of the Preferred Approach SCAAP

### 4.1 Introduction

4.1.1 This section of the SA Report contains the appraisal of the SCAAP in as it appears in the Revised Proposed Submission Version. The intention is to identify the likely effects of the plan on sustainable development, identify how impacts have been mitigated and include recommendations to consider when moving forward with the SCAAP to the Submission version.

### 4.2 Sustainability appraisal of the objectives

4.2.1 The SCAAP sets out the ambition, aim and objectives for the plan. These are:

#### *Proposed Vision*

- Our **vision** for Southend Central Area, which includes the Town Centre and Central Seafront Area, is for it to be a 'City by the Sea'. As a prosperous and thriving regional centre and resort, it will be an area that is vibrant, safe and hospitable, rich in heritage commerce, learning and culture and an attractive, diverse place where people want to live, work and visit both for daytrips and overnight and longer stays.
- Our **aim** is to transform the perception and image of Southend through sustainable economic growth, high quality development and social provision, and for it to be independently recognised as a popular location for businesses, residents, students and visitors.

#### *The SCAAP objectives are:*

- To improve and transform the economic vitality, viability and diversity of Southend Central Area by encouraging the establishment of a wider range of homes, businesses and shops whilst providing new opportunities for learning, recreation, leisure and tourism.
- To promote design excellence and good quality development proposals and public realm improvements to reinforce a distinctive sense of place, complement new and existing development, and contribute towards the Council's aspirations to establish Southend as a Low Carbon City.
- To increase the number and diversity of people living within Southend Central Area and its Gateway Neighbourhoods by building more homes, and ensure that living in the area becomes appealing to more families with children, supported by social and community infrastructure that contribute to reducing inequalities in health and wellbeing and support all ages to lead independent lives and live healthy lifestyles.
- To encourage the establishment and expansion of businesses in Southend Central Area by identifying, promoting or actively bringing forward suitable sites for development to meet modern user and investor requirements.
- To promote and enhance the tourism, cultural and leisure offer within the Central Area, including visitor accommodation, having regard to the assets offered by the area, in order to attract greater visitor numbers, promote more overnight stays.
- To promote the Central Area as a thriving learning quarter that provides state of the art facilities and well-designed student accommodation.

- To improve accessibility to the area, ensuring streets, public and green spaces are connected, well-designed and safe, utilising a coordinated palette of materials and furniture that enhance the quality of the streetscape and improve opportunities for walking and cycling, and access to more sustainable modes of transport, such as rail and bus.
- To promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre in the locations where it is needed, and provide good access to the seafront by encouraging improvements to the quality of access to and from parking that are convenient, well signposted, safe and secure.
- To address climate change matters and appropriately manage and mitigate flood risk and to encourage the provision of Sustainable Drainage Systems and urban greening measures in order to reduce surface water run-off.
- To enhance the quality of, and access to, Southend Central Area's natural environment and open spaces, and to improve connectivity between the Town Centre and Central Seafront Area in order to relieve pressure on the Site of Special Scientific Interest (SSSI), Ramsar site, Special Protection Areas (SPA) and other environmental designations, to protect and enhance local biodiversity and nature conservation, and to encourage opportunity for linked trips.
- To celebrate and enhance the setting of Southend's unique heritage assets, such as the Grade II listed Pier, to ensure these assets are appropriately conserved and enhanced and continue to form an integral part of how Southend Central Area is experienced by those who live, work and visit the area.

4.2.2 The vision and the aim have remained largely consistent since the previous SA of the SCAAP Proposed Submission. It sets an aspiration that should be beneficial in achieving many sustainability benefits, with particular benefits for sustainable communities and economy. This will be through protecting what is best about Southend and achieve actual improvements to the area as well as enhancing the image of the area. However, could contain more detail on the need to protect and enhance the natural environment of the area, not only relating to the internationally important nature conservation sites but also the greening of the built urban environment.

4.2.3 The SA of the current set of objectives, which have undergone some minor not significant clarifications and reordering since the Preferred Approach SCAAP, is included in **Appendix C**. The SA of the objectives concludes that, if implemented, meeting the objectives should have largely positive impacts on the achieving sustainable development. There is some ambiguity assessing the effect of some SCAAP objectives on sustainable development as much will depend on implementation, so although there may be the potential for beneficial impacts it is not possible to assess these with any great certainty.

4.2.4 The only potential conflict relates to an ambiguity around the objective for car parking as this is not clear if it is rise or drop from current levels. To achieve more sustainable development it will always be preferable to enhance access by sustainable travel modes and gradually reduce reliance on visitors and workers travelling by car to the Central Area. This will also have a long-term benefit of reducing land required for car parking, and allowing it to be released for alternative uses, such as housing. Sustainability appraisal recognises that maintaining a certain level of car parking essential to allow those with mobility impairment to travel to the Central Area and can help town centre shops and businesses compete with out of centre and out of town alternatives.

- 4.2.5 There are also several sustainability issues that are not covered by any SCAAP objective, or with limited reference. The 'soil' sustainability objective is not addressed specifically, but the SA does recognise that one of the principal purpose of the SCAAP is to allow the regeneration of the Central Area and in doing so may reduce the quantity of land that is required for housing and other uses in other parts of the borough – thereby helping to protect soil resources. There will be the need to ensure new residents (and existing water bodies) are protected from harm related to contaminated land, although this is covered by policy in the DMD. The 'waste/minerals' and 'energy' sustainability objectives are also not covered, although both of these 'resource use' topics may be too specific to be covered by a SCAAP objective and be addressed through policy in the DMD and Core Strategy.
- 4.2.6 Implementation of the aims and objectives for the SCAAP will inevitably create some tensions. The key areas where this might arise are:
- **Shift in transport mode:** The SCAAP supports an intensification of central Southend with much new development to take place to 2021 and beyond. There is the risk that this will lead to increase demand for car travel to access these new services, despite these being in a location that could support the most sustainable travel options. Therefore, it needs to be shown how traffic, transport and accessibility proposals will need to successfully secure a mode shift away from car use.
  - **Relationship with wider Southend:** There is the risk development in central Southend may have adverse impacts on the objectives for other parts of the Borough. However, this location is the most sustainable as it has good access by a variety of types of transport. Therefore, the centre is a good location for high trip generating uses, such as offices, shops and educational establishments. Consideration could be given to the impacts on economic objectives for the airport environs and other proposed business locations – including those in neighbouring authorities;
  - **Mix of development:** As the type of development in the Central Area becomes more mixed, in particular through new housing, there will be more of a competition for space. For instance, new residential development demands new community space, schools as well as public open space and recreation sites.

### 4.3 The SA of Policies and Proposals

- 4.3.1 The regeneration of central Southend is proposed through delivery of a number of redevelopment, renewal and enhancement schemes. These are made up of specific development sites, as well as proposals for improving the unique qualities of specific 'Policy Areas'. SCAAP also includes a number of development strategy policies to help guide the way development is delivered the Central Area as a whole.
- 4.3.2 The appraisal is based around eight sustainable development themes based on the SCAAP sustainability objectives. These themes relate to the issues identified during the identification of baseline information and other plans and programmes, as in **section 3**. The themes have been 'scoped' in as being those that are most pertinent to sustainable development in the Central Area.

4.3.3 The themes covered in sections 4.4 to 4.11 are:

- Travel and movement;
- Residential development and communities;
- The built and heritage environment;
- Education and culture;
- Employment development and retail;
- Leisure, recreation and open space;
- Sustainable construction and flooding;
- The natural environment.

4.3.4 The SA Report identifies the relationship of the SCAAP development strategy and site specific policies on delivering the sustainable development themes. The aim is to review the coverage of issues in the SCAAP taking into account that some matters are covered in other parts of the LDF.

4.3.5 The full appraisal of policies is shown in **Appendix D** and this should be read for the detailed comments and recommendations on the generic policies and in particular the site specific policies.

## 4.4 Transport and Movement

### Implications for sustainable development

#### *Non-car access*

- 4.4.1 One of the principle aims of the SCAAP is to achieve a mode shift to non-car travel. This shift is essential to ensure the new development proposed by the SCAAP does not result in additional car use in the centre, as well as helping to reducing current car use.
- 4.4.2 Improvements to non-car travel in the Central Area could help meet sustainability objectives relating to social sustainability, such as accessibility and health, and environmental sustainability including air quality and use of resources.
- 4.4.3 The central Southend area is a transport hub, containing Southend Victoria Station, Central Station, Prittlewell Station and the bus station 'Travel Centre'. This connects the centre to the wider Borough, as well as other south Essex towns and London. The centre is also in easy walking distance from a large number of Southend's residents, including those residential neighbourhoods included as part of the Central Area.
- 4.4.4 Non-car travel access to the Central Area could be improved, with particular emphasis on providing better quality walking routes. Currently there are areas where permeability of the streets is poor and/or routes are unattractive or car dominated this discourages walking even for local residents. In these areas the natural flow of routes round the centre is disrupted or where poor physical built quality and cars taking priority means walking is not always a safe or attractive option.

- 4.4.5 Roads also create barriers to people travelling by foot and bicycle, and can even have adverse impacts on the character of neighbourhoods by cutting them off from the Central Area. Queensway is the greatest barrier; it forms an obstacle to access from residential neighbourhoods to the east, the 'Sutton' Gateway Neighbourhood to the north as well as Victoria Avenue and Victoria Station.
- 4.4.6 Achieving a modal shift away from car use will also be essential to prevent the adverse impacts that may result from more office development, shops and services being available in central Southend. Helping reduce these car trips is a role of the SCAAP (including the through implementation of the 'Transport, Access and Public Realm Strategy' in Appendix 5. The SCAAP also works alongside other plans and strategies and policies of the LDF to deliver this aim. In particular, policy DM15 Sustainable Transport Management of the DMD and supporting car parking standards Appendix.
- 4.4.7 To deliver the 'City by the Sea' objective for central Southend there needs to be improvements to travel and transport. This will be in tandem with the Local Transport Plan and Phase 2 of the City Beach. The transport and travel policies of the SCAAP include criteria that relate to these and other plans and strategies. The SCAAP brings together diverse threads of transport planning helping to integrate the strategy for improvements in the town centre. Although some schemes will need to be delivered in conjunction with partners, such as bus operators, the SCAAP sets a clear vision for the area and may assist with securing additional funding, in addition to the Local Growth Fund.
- 4.4.8 The proposed mix of uses in the town centre could also help reduce the overall number of trips. This can include more linked trips, with one trip to the Central Area to meet leisure, retail and service needs, as well as more people living in the centre and therefore not needing to drive or own a car.

#### ***Car Parking***

- 4.4.9 There are also policies to help deliver changes to the parking in the town centre. The short term aim to largely maintain the level of parking in the area, with a particular emphasis on retaining the net number of spaces south of the railway line.
- 4.4.10 A car parking study has shown that in the Central Area the demand for available spaces is below 85% and only reaching a peak of 87% a few times a year. . This indicates, through further improvements to the management of car parks to allow drivers to be directed to where there are free spaces there is could be the potential to reduce overall parking levels in the Central Area.
- 4.4.11 There are benefits of reducing car parking for sustainable development objectives. This includes the potential to release car parking land for alternative uses e.g. houses, shops and open space. Release of land could help to bring social, economic and environmental benefits to Central Southend. Also, reducing car parking spaces for the Central Area has a role in helping to encourage visitors to make more sustainable travel choices. Increasing public transport use, walking and cycling can all help reduce carbon emissions and local air pollution, as well as reducing the adverse impact on the public realm from traffic. In achieving sustainable development, the beneficial effects of town centre car parking are also recognised as the ease of parking helps retain the attractiveness of central Southend to shoppers and visitors. It is important to maintain a level of parking to ensure people do not abandon the

town centre in favour of out-of-town shopping, which can be detrimental to community character and economic performance of the town centre and have other environmental effects, especially where people are travelling further.

- 4.4.12 Development in the Central Area will need to be considered in the context of the wider area, ensuring schemes for central Southend do not adversely impact on other parts of the Borough e.g. more on-street car parking on residential streets around the centre or more traffic on residential streets.
- 4.4.13 The car parking strategy notes that north of the railway line car parks have lower levels of utilisation and rarely reach high levels of capacity, whereas south of the railway space should there is a great demand and the SCAAP is seeking to protect these spaces. To achieve some economic benefits for the town centre consideration could be given to managing car parking to direct more visitors to park north of the railway line. They would then walk through the town centre to get to the seafront. For instance, through variable pricing or signage favouring these locations.
- 4.4.14 Also, by better managing car parking it should be possible to provide sufficient spaces to meet demand on a smaller land area. Measures to manage car parking could include additional Variable Message Systems (VMS), better static signage as well as pricing policy, as set out in Appendix 5 of the SCAAP on the 'Transport, Access and Public Realm Strategy'.
- 4.4.15 To achieve more sustainable development a long-term strategy of reducing car parking in the Central Area should be implemented, at the same time as measures are implemented that make using alternatives an attractive and viable option for more people. This could be supplemented by temporary car parking at peak seasons.

### Policy coverage

- 4.4.16 Travel and movement is one of the main themes of the SCAAP and many of the policies and site proposals include criteria to help encourage more sustainable travel choices, especially walking and cycling. The measures included in policies to achieve this cover two principal themes. These are:
- Making the town centre a more pleasant place for pedestrians through public realm improvements;
  - Improving access to the town centre for everyone by all modes of transport.
- 4.4.17 Policy DS5 'Transport, Access and the Public Realm is the principal policy of the SCAAP covering all matters relating to access, sustainable travel as well as roads for the SCAAP, with principles applicable to all development in the Central Area. As well as measures to improve the public realm through urban greening and pedestrianisation, but also by helping traffic circulate better around the Central Area reducing congestion and adverse air quality and amenity effects of cars in the Central Area.
- 4.4.18 Specific improvement schemes identified in the SCAAP will help encourage walking and cycling. These include:
- Identification of routes and improved linkages for cycling and walking improvements. Examples include PA1 High Street identifies the need to connecting neighbourhoods to

the north and east of Queensway to the High Street to the seafront; connecting Queensway to the High Street (PA4 and PA9); better east west links including to Clifftown (PA6); connection to the seafront to in CS1: OS8 Marine Parade;

- The Policies Map shows the locations of new/improved routes around the Central Area for cyclists and pedestrians as 'priority links' are specifically referred to for some Policy Areas, such as around Elmer Square (PA3) and London Road (PA2) and links from Queensway Road and Warrior Square (PA5) and to Chichester Road (PA7, Tylers) as well as in the Central Seafront (CS1);
- Improve gateway crossings of Queensway dual carriageway for pedestrians and cyclists (PA2 London Road, PA4 Queensway, PA5 Warrior Square and PA7 Tylers) and Chichester Road (PA5) – and covered in DS5 Transport;
- Public realm improvements to make the built environment more attractive for those on foot, including continuation of the City Beach Scheme and Elmer Square (PA3: PA3.1), urban greening at Queensway (PA4), Chichester Road and Seaway Car Park and new planting (PA2 London Road, PA3 Elmer Square, PA4 Queensway; and PA8: PS8.1 Victoria Avenue);
- Public realm improvements through public art in all Policy Areas policies and a stated 'Aim' for policy areas London Road, Tylers and the Central Seafront. Specific reference to the use of art in signage is identified as part of the transport policies in DS5, as well as for specific areas such as the Central Seafront (CS1), London Road (PA2) and Elmer Square (PA3). High Street policy area (PA1) includes the use of lighting to improve the area around the railway bridge.
- Improvements to pedestrian access around the stations, such as the relationship of Central station to Clifftown (PA6) and Elmer Square (PA3);
- Maintaining and improving active frontage at ground level, with all policy areas including some requirement to maintaining an element of active frontage, for instance through residential over ground floor commercial development, such as PA7 Tylers with active frontages on Chichester Road
- Pedestrianisation of London Road at other High Street 'stub' roads (PA1 High Street);
- Improved traffic management to reduce cars circulating and improve roads for non-car users such as PA6 Clifftown and reducing cars circulating to car parks; CS1 Central Seafront identifies the need to manage all traffic in this area better including parking.
- There is also a policy to introduce 'Home Zones' in the Tylers residential area (PA7);
- PA8 Victoria Gateway includes a new priority public transport route to link Southend Central Area with London Southend Airport and its environs.

4.4.19 In addition, Appendix 5 of the SCAAP covers 'Transport, Access and Public Realm Strategy'. This presents the SBC objectives for action to secure a 'step change' in transport in Southend Central Area to a more modern integrated transport system. Many of the measures are also picked up in relevant policies of the SCAAP and LDF, but it also identifies other measures and strategies that will be implemented to secure improvements, such as *Ideas in Motion* Travel Planning for educational facilities and businesses and the *Cycle Southend* scheme.

## Sustainability effects and recommendations

- 4.4.20 The SCAAP has the potential to deliver significant benefits from achieving a modal shift to more sustainable travel, including walking and cycling that can also have benefits for the community and economy of Southend.
- 4.4.21 Where new car parks are planned, or where there is potential change existing car parks, consideration should be given in policy to improvements to their quality and contribution to the public realm. This could include the scale and design of multi-storey parks, use of lighting and green walls.
- 4.4.22 The policies of the SCAAP show a clear intention to make the town centre a better place with improvement to the pedestrian environment to encourage more people to walk. Improved links to the Central Area will also help reduce car use in the town centre, with benefits for the natural and residential environment.
- 4.4.23 Other SA recommendations, for inclusion in the SCAAP or other implementation strategies that could help delivery of improvements include:
- Some additional detail as part of policy on the design of new pedestrian/cycle linkages across the Central Area for example signage, width, segregation (or not) or uses, and road markings; and
  - A single Queensway enhancement policy, plan or briefing for the improvements for Queensway and the Urban Park to show a clear picture of the intentions for the area. This is likely to be part of any 'Better Queensway' project.

## 4.5 Residential development and communities

### Implications for sustainable development

#### *Housing delivery*

- 4.5.1 Southend Central Area is identified as needing to accommodate at least 2,474 new homes between 2001 and 2021 in the Core Strategy; this is around 30% of the total requirement for the Borough. Monitoring for the borough shows that from 2001 to 2016 1,087 homes have been built in the area. Also, there were a further 1,040 dwellings with planning permission at 1<sup>st</sup> April 2016.
- 4.5.2 As it is unlikely all sites with permission will be delivered there remains a need to identify additional sites for housing in the Central Area. There is also the need for the re-provision of any homes that have or may be demolished as part of town centre regeneration. This demonstrates the significant role central Southend has to play in achieving target housing figures to meet identified needs in an accessible location,
- 4.5.3 Housing is proposed to be distributed throughout the Central Area, including to areas that currently do not have a large resident population, such as around the High Street. In many central locations residential on upper floors is supported, maintaining active frontage at lower floors. Some areas, such as the Victoria Gateway would see large scale renewal with the potential to create new sustainable communities, containing homes, offices, open space and community uses.



- 4.5.4 The Revised Proposed Submission Version of the SCAAP only includes as allocations where there is clear evidence of deliverability in the plan period (at least in part) by 2021. For instance, this includes sites that are being actively promoted and sites, such as Seaways, with a recent planning permission. Sites have been removed from inclusion where there they have not been actively promoted. However, the SCAAP recognises that these sites may come forward and they will be identified as part by the emerging new Local Plan.
- 4.5.5 Therefore, this provides a level of certainty on the development that will come forward in the locations identified. However, it should be noted that the Opportunity Sites ('PA' and 'CS' sites) do not represent the full extent of changes anticipated in the Central Area. For instance, previously not identified sites may come forward through planning applications, which will be determined according to policy.
- 4.5.6 In total the SCAAP includes a net increase in dwellings of 1,732 in the Central Area Opportunity Sites, of which 1,040 are committed as they have planning permission or other prior approval (approximately 60%). At 1<sup>st</sup> April 2016 there are a further 434 committed sites expected for delivery by 2021 outside of the Opportunity Sites. This provides a total of 2,166 new homes in the plan period in the Central Area.

#### ***Affordable housing***

- 4.5.7 There is also the potential for improvements to areas of social housing identified in the SCAAP. PA4 Queensway the site PA4.1 'Better Queensway' this is an area of renewal to the north of the High Street where there has been recent housing clearance and is the location for the new provision of social housing. The policy includes a requirement to ensure there is no 'net loss' of affordable housing. In addition in the Victoria Avenue (PA8) policy area there is an Opportunity Site (PA8.2 Baxter Avenue) that is identified for mixed tenure housing including sheltered and affordable units. Promoting and protecting social and other affordable housing is compatible with achieving sustainability objectives related to housing and community, as ensuring all parts of the community have access to a home is an essential part of social sustainability.
- 4.5.8 Part of the long-term vision for CS1: Central Seafront there is also the potential for the regeneration of Woodgrange Drive (Kursaal) Estate, this site was previously included as a potential opportunity site but as it could only be delivered post-2021 it is not included in the Revised Proposed Submission Version SCAAP.
- 4.5.9 The SCAAP does not contain any generic policies to manage the delivery of new residential development as these are already covered by policies in the Core Strategy and DMD; **Appendix D** contains some detail of this. The housing mix in new development in the Central Area will have to be compatible with identified need from the Strategic Housing Market Assessment, as well as in line with DMD policies on housing type and tenure. These policies will help ensure the right mix of new homes in terms of type and affordability as well as residential standards that will need to be met. The SCAAP does contain some specific requirements for housing tenures, such as sheltered and affordable housing at Baxter Avenue (PA8.2) and Queensway (PA4.1) for social led housing. However, there may be a role for the SCAAP to specify as part of site allocations the type or tenure of housing that will be favoured on different sites, which is not currently part of policy. For instance, where sites may be more

suited for larger family homes or other areas with good access to community services and schools and smaller 1 or 2 bedroom properties.

### ***Community facilities***

- 4.5.10 The SCAAP does identify some locations where there may be a need to provide new community facilities. Examples of the type of facility is included in the policy and it will be important that implementation of the policy takes this into account. For example, at PA4 Queensway community uses have been lost through demolition and therefore there will be a need for new provision in these areas, policy wording could be stronger to 'require' rather than 'promote'.
- 4.5.11 As set out in The SA acknowledges that there been recent delivery of new community services including a Health Centre in North Road (Victoria Gateway Neighbourhood) and a new public library at the Forum. However, there is a need for centrally located community facilities that are accessible to the deprived communities of Milton ward. Work is underway on a new strategy for a 'Better Queensway' and this should help to identify the type of community development needed in this area as part of its regeneration. The Southend Infrastructure Delivery Plan also identifies the need for new classroom space, which could include a new school, in the Central Area (likely to be in the Sutton or Victoria Gateway Neighbourhoods). This will need to be kept under review in the SCAAP plan period and beyond.

### ***Student accommodation***

- 4.5.12 Student accommodation is also promoted throughout the Central Area, with the focus of new higher and further educational facilities at Elmer Square (PA4). Student accommodation needs to be of a high quality design, covered by policy DM1 of the DMD. Due to the short tenancy of these types of property they can occasionally result in a deterioration of the local environment quality, for instance from litter, refuse storage, and outdoor space maintenance. Therefore, including policy criteria that would require management plans as part of any planning permission could help address these issues.
- 4.5.13 The University of Essex private student landlord accreditation scheme may also help manage some potential effects, although it would be useful if this was a requirement for planning of all new build student accommodation.

### ***Communities***

- 4.5.14 The SCAAP presents the potential for the character of the Gateway Neighbourhoods and other central residential areas to be enhanced. Queensway road creates a physical and perceived barrier between areas and the town centre and seafront. The SCAAP policies include many references to the need to improve the links across the dual carriageway to help people move more freely between these residential areas and the town centre. There are sustainability advantages of physically connecting and removing perceived barriers between the two areas, helping people feel more part of the town and able to access services and facilities without driving and reduce social isolation.
- 4.5.15 There will be general benefits of less traffic in the Central Area for residential amenity. Reduced on-street parking, traffic and congestion will help improve the quality of the urban

environment and reduce health impacts of car exhaust. Specific benefits may include the creation of new 'Home Zones' in the Tylers Avenue area, in association with car park changes.

### Policy coverage

4.5.16 Provision of new residential development is integrated throughout the SCAAP.

4.5.17 Policies that will help deliver residential development include:

- **Promoting use of upper storeys for residential development:** DS1 A prosperous Retail Centre; PA2 London Road; PA4 Queensway; PA7 Tylers as part of PA7.1 Tylers Avenue; PA8 Victoria Gateway Neighbourhood as part of PA8.1 Victoria Avenue Office Area.
- **Areas of more general residential led development:** There are several Opportunity Sites in the Central Area with the potential to deliver significant residential growth in the plan period. These are: PA4.1 'Better Queensway', PA8.1 Victoria Avenue, CS1.3 Marine Plaza, PA8.2 Baxter Avenue, PA9.1 Sutton Road, PA7.1 Tyler – and the smaller site at PA9.2 Guildford Road.
- **Other:** PA3 Elmer Square is identified as suitable for student housing and PA8.2 specifically to include specialist housing.

4.5.18 Policy Areas are identified where new community/education infrastructure should be provided. For example, PA8 Victoria Gateway and PA9 Sutton Gateway policies identify the potential for a new school in these areas and PA5 Warrior Square the potential for new community infrastructure such as doctors or dentist is included in policy. However, no specific sites are identified in either of these opportunity areas.

### Sustainability effects and recommendations

4.5.19 The policies of the SCAAP go some way towards helping development in the Central Area make a suitable contribution to meeting Southend's housing needs.

4.5.20 The SCAAP has the potential to deliver environmental benefits from supporting additional housing in the Central Area, supporting its viability and vitality and contributing to reducing car travel.

4.5.21 Continued monitoring of the housing delivery through the AMR, as proposed, will help keep track of housing delivery in the area. This should also include monitoring of the mix of housing type and tenure being delivered. Where possible records should be obtained showing loss of registered affordable housing through right-to-buy schemes. Residential development coming forward through permitted development rights should also be monitored, as the underused office stock in the borough generate a significant proportion of new housing the area.

4.5.22 Community services are an essential part of ensuring sustainable communities in the Central Area. Implementation of policy must ensure that consideration is given to the need for new community facilities to meet the need of a growing population in the Central Area. For instance, there may be a particular demand as a result of the loss of community spaces and services at Queensway House, as well as the potential loss of other community spaces such as at Short Street.

- 4.5.23 Recognition of the 'Better Queensway' project in the SCAAP should aid its delivery. To allow development is delivered in a coordinated way that secures benefits for the wider community as anticipated the policy could include a commitment to preparing a masterplan or development brief for the area or, subject to programme, more detail included in the policy prior to adoption of the SCAAP.
- 4.5.24 For Elmer Square the policy (PA4) identifies the importance of managing student accommodation in this location, although there is a need to ensure that all new build student accommodation is managed in a similar way.
- 4.5.25 The SCAAP clearly sets out the anticipated housing yield in each Policy Area. Tables for each Policy Area as well as a summary table show how housing will come forward on a mixture of new allocations, sites with expired permissions and sites with existing permission. This inclusion provides useful clarity on the role of each area and appreciation of the likely growth planned for and that already ready for delivery.

## 4.6 The built and heritage environment

### Implications for sustainable development

#### *Built environment*

- 4.6.1 Alongside transport improvements one of the main themes of the SCAAP is to bring improvements to the built environment of central Southend. This ranges from small change to the built environment, such as new public art or lighting to the regeneration of whole areas through rebuilding.
- 4.6.2 Improvements to the built environment will help to enhance the image of the area, which will have a variety of sustainability benefits for the town. This is not only in terms of meeting objectives relating to protection and enhancement of the built environment, but also vibrant communities and economic prosperity.
- 4.6.3 For instance, the quality of the environment is an essential part of the role of the town for tourism. An improved quality built environment can only enhance this, drawing in a broader range of visitors and more people for longer stays. This will be helped not only from new leisure and cultural facilities and improvements to hotels in the town, but also the perception of the town as a place to visit.
- 4.6.4 There are specific proposed improvements that should aid tourism, including seafront enhancements continuation of the 'City Beach' scheme and improvements at the Seaway Car Park (CS1.2).
- 4.6.5 Other built environment improvements will be beneficial to different aspects of the economy such as redeveloping redundant office space along Victoria Avenue, commercial uses at Sutton Gateway and other dilapidated spaces.
- 4.6.6 The Central Area is also essential in giving the whole Borough of Southend a unique character that can be an important part of selling the town as a place for investment. Poor quality new development and underused spaces can detract from this character, reducing the image of the town for inward investments.

- 4.6.7 Economic benefits are not the only effects, peoples' pride in the place where they live can also be improved through a high quality environment. A place that people feel proud to live in can help foster community identity and cohesion, with social sustainability benefits. Improving the appearance of the town and fostering its image as a 'City by the Sea'. The urban layout, design of streets and provision of good quality outdoor space for informal recreation also helps in creating a healthy place to live. Public realm enhancements that can help to encourage more active travel with more trips made by bicycle or on foot.
- 4.6.8 Cars can have an impact on the built environment, including through heavy traffic, congestion and through the impact of large surface car parks and on-street parking. Management and rationalisation of the existing car parks can help enhance the built environment. Policies could be included on helping to ensure the good design of all new car parks, for instance tree planting, street furniture, innovative lighting or green walls on multi-storey car parks.

#### ***Landmark Buildings and Key Views***

- 4.6.9 The SCAAP includes the possibility of new taller buildings being developed as part of the centre regeneration. There are potential positive benefits of creating new distinctive urban features. Any new large or tall buildings will need to be carefully designed to complement the town centre, as set out in SCAAP policy and DMD policy (DM4), taking into account microclimate effects (particularly environmental wind).
- 4.6.10 The SCAAP also identifies the importance of protecting and enhancing landmark buildings and views as these contribute to the character of Southend to the benefit of local communities and visitors. These landmarks can provide a sense of place and civic pride and also help people travelling around the centre to orientate themselves, particularly important for visitors. Policy sets out how these buildings should be respected and enhanced by new development, as well as the potential locations to create new landmarks.

#### ***Historic Environment***

- 4.6.11 Many parts of the urban area have an underlying high quality historic and heritage environment, although in many locations this has become degraded over time and needs attention to be an asset again for the town. Areas where regeneration can help enhance areas of underlying historic quality are at Warrior Square, Clifftown Conservation Area, Kurssal, Eastern Esplanade and Prittlewell Conservation Area in the Victoria Gateway. The SCAAP will help repair some of the loss of integrity in these areas through carefully planned new urban development.
- 4.6.12 The SCAAP does not contain policies specifically on the protection of historic environment of the Central Area as heritage is fully covered in DMD Policy DM5 Southend-on-Sea's Historic Environment and through national protection policies. Site specific protection measures related in particular to archaeology are included in relevant Policy Area policies. Policy DS3 also contains the requirement to respect the setting of heritage buildings in any new landmark buildings. Policy DS2 also aims to protect 'key views' the majority of which are to views of specific heritage features. In addition, frontages of townscape value are also to be preserved from harm, with restoration and design of new development needing to complement these.
- 4.6.13 Reducing car dominance in some areas by improving the quality of roads and streets through planting, reduced on street car parking and a reduction in traffic speeds could all help improve

the character of the built environment in areas of heritage importance, as set out in policies for each Policy Area. This will include areas east and west of the High Street, such as St John's and Clifftown, as well as on the seafront.

### Policy coverage

4.6.14 Improvements to the built environment are part of most of the policies of the SCAAP. This ranges from specific criteria for the development of proposals sites to general requirements for the enhancing the quality of the wider built environment.

4.6.15 Policies on general improvements to the built environment include:

- **Landmark Buildings and Key Views:** This is covered by the area wide policies DS2 Key Views and DS3 Landmarks and Landmark Buildings, with elements of DS3 repeated within each Opportunity Area policy in Section C. In addition, some PA policies specifically identify suitable locations for new landmark buildings.
- **Frontages of townscape merit:** Area policies that identify the importance of historic shopfronts or other frontages or merit are: PA1: High Street and PA6 Clifftown;
- **Open space and public civic space:** When well implemented and managed these types of space can contribute to the quality of the built environment, there are several such spaces included in area specific policies. This includes: PA2 London Road includes promotion of pedestrianisation of this area to create a new public space linked to the underused space outside Victoria Shopping Centre; PA1 a new space on the High Street by the railway bridge as well as improved public spaces on the seafront, such as a new piazza at the southern end of the High Street and a PA8 Victoria Gateway.
- **Providing urban greening:** The majority of policy area policies contain requirements for 'urban greening'. Others specifically related to tree planting, such as PA1 High Street, PA2 London Road, PA5 Warrior Square, PA6 Clifftown, PA7 Tylers, CS1 Central Seafront and PA9 Sutton Gateway. Other areas also have more specific proposals such as the Queensway Urban Park (PA4 and PA5) and improvements to greenspace at PA5 Warrior Gardens and urban greening projects as part of the PS7 Victoria Gateway (PA8.1 Victoria Avenue), including green walls, landscape and a linked public greenspace. Several policies also identify the need for open space to contribute to the 'green grid' covering the wider area, such as CS1 and CS2 on the Central Seafront and PA8 and PA9 on Victoria and Sutton neighbourhoods.
- **Archaeology:** There are areas of potential archaeology in the Central Area. These areas are identified in area specific policy to ensure any development in these areas takes appropriate measures to identify potential for remains and take appropriate action. They include at the Central Seafront (CS1) and PA8 Victoria Gateway.
- **Setting of listed buildings:** Queensway (PA4) includes criteria on the importance of protecting locally listed buildings and designated heritage assets, which in the area include the Grade I listed porters and All Saints Church. Protection of heritage assets is also part of Policy PA6: Clifftown including the station building, Policy CS1 Central Seafront, including the pier; and finally PA8 Victoria Gateway and the Grad II listed museum.

4.6.16 Site proposals contain specific criteria for improving the built environment. Some of the larger schemes include:

- Enhancing the quality of the seafront through the ongoing City Beach scheme (CS1), as well as other Central Seafront design principles including use of public art and lighting schemes, urban greening and the design of new buildings;
- Enhancing the High Street including new public space near the railway bridge and new lighting in this area, pedestrianisation and greening of 'stub' roads off the High Street (PA1 High Street);
- Better links from the town centre to the seafront, including multi-level spaces in PA1: High Street;
- Enhancing the Prittlewell Conservation Area including realising the potential of the backland area to the rear of Victoria Avenue as a 'lanes' style development (PA8);
- Renewal of the Queensway area including through a new Urban Park and new buildings at the former Queensway House PA4 and PA4.1 Better Queensway;
- Wholesale redevelopment of the Victoria Office Area (PS8 (PS8.1)) for a mix of uses;
- Improving links from the High Street to the Seafront as part of the Seaway Car Park and Marine Plaza improvements (CS1.2 and CS1.3)
- Housing renewal areas including PA4: Queensway.

4.6.17 In addition to the built and historic environment policy coverage in the LDF, SBC also has adopted Supplementary Planning Documents (SPD) on the securing built environment and public realm improvements. Together all policies and SPD will support and guide the implementation of development in the Central Area. These SPD are *Design and Townscape SPD1*; and *Streetscape Manual SPD3* and these should be consistently included in policy or supporting text as are essential to all new development.

### Sustainability effects and recommendations

4.6.18 The SCAAP has the potential to deliver significant environmental benefits from supporting development that improve the built environment; this can include benefits for the economy and the communities of the town.

4.6.19 The policies of the SCAAP are not detailed on the precise design details for new development. To ensure that development is delivered to the high quality standards SBC or others may need to prepare development briefs, masterplans and/or design codes for specific areas as set out in policy. This will help provide the fine grain guidance that will help developers deliver good quality development.

4.6.20 There are several policies that propose the comprehensive renewal of areas, guided by policy, to deliver coordinated regeneration to areas with a mix of new buildings, homes, offices and community space. However, changes to permitted development rights have resulted in successful approvals for change of use from office use to residential development in parts of the Central Area. This type of permitted development has the potential to undermine the delivery of a coordinated policy led renewal of these areas. Therefore, removing permitted development rights in these areas, for instance through an Article 4 direction, could help secure benefits for areas including Victoria Avenue.

4.6.21 As identified elsewhere in this SA Report there is a need to ensure future car parks (including multi-storey and ground level) contribute to the quality of the built environment. For instance,

through creating visual diversity through use of paving, planting and layout and use of features such as green walls.

## 4.7 Education and culture

### Implications for sustainable development

#### **Education**

- 4.7.1 The delivery of new higher education and learning facilities will be a positive for the town centre. Not only will these help enhance the perception of Southend as a place of learning but also will encourage students into the town centre supporting local services and creating a vibrant area. However, avoiding conflicting land uses will be important and maintaining a balance in the type of facilities and retail offer available. This may include the potential incompatibility of a night-time economy in the town centre and delivery of new residential development.
- 4.7.2 The SCAAP includes less coverage of meeting the educational needs of the resident population of the area, either existing or as a result of growth planned through the SCAAP. The context of the SCAAP identifies the need for school places and the infrastructure section identifies that it is *“considered that the planned population growth in the Central Area will be accommodated via the expansion of existing schools, however it is recognised that there may be a need for additional schools, and this will be kept under review.”* Therefore, although no specific sites identified as being suitable for schools in the SCAAP, the potential for these new schools to be part of the larger policy areas of the Sutton or Victoria Gateway Neighbourhoods is identified in policy. To achieve sustainable growth, it is essential that there are sufficient local education facilities to meet the needs of children in the Central Area, especially as the SCAAP is encouraging the residential growth in the town centre, including new family homes and should be monitored.

#### **Culture**

- 4.7.3 New cultural facilities can also be of great benefit in developing sustainable communities. For new cultural venues to be successful local people and visitors will need to be supportive of what is on offer. Any new facilities will need to work with existing communities and visitor representatives to identify needs.
- 4.7.4 The SCAAP also supports the regeneration of parts of the Clifftown. The aim is to create a new cultural area, using the existing historic character combined with new development to create a tourism location west of the High Street. Further support is given to the importance of the Central Area as a centre for culture is given is included in CS1 Central Seafront, as well as PA8: Victoria Gateway Neighbourhood being identified as a location for new cultural facilities, such as the relocated Beecroft centre, museum and the former Water Board site.
- 4.7.5 The development of a new museum related to the Saxon King finds as part of the Seafront Development at the Western Esplanade could deliver sustainability benefits for the town. The museum has the potential to show the importance of a town at Southend since early times, these links to the past can help people feel pride in the place where they live.



- 4.7.6 These aims combine well with those on employment and new leisure facilities to help support Southend's economy.

### Policy coverage

- 4.7.7 The SCAAP makes specific reference to the importance of this area for education, especially further and higher education. There is also reference to the importance of protecting and retaining cultural facilities.
- The Elmer Square development area is to be the main focus of new higher and further education facilities, addressed PA3 and PA3.1 Elmer Square, although Policy PA2: London Road also identifies the potential in this area;
  - PA8 Victoria Gateway Neighbourhood and PA9 Sutton Gateway Neighbourhood policies identify the area as possibly suitable for new educational facilities;
  - Clifftown (PA6), Central Seafront (CS1) and Victoria Gateway (PA8) are identified as a key area for enhancing Southend's cultural life. There is an aim for this area to be the location of new galleries, cafés, shops and workshops and there is also the new museum (CS1.4);
  - Heritage and archaeology are an important part of the cultural identity of the town. Policies on protecting the heritage assets and the towns landmarks should help secure cultural links of the town to its past, for instance through the continued maintenance of the pier and seafront in a way that respects its history.

### Sustainability effects and recommendations

- 4.7.8 The SCAAP has the potential to deliver benefits relating to education, supporting local communities and the economy.
- 4.7.9 Ensuring that the supply of school places is monitored and new facilities provided as necessary is essential, although there is currently predicted capacity to 2021. Lack of accessible local school facilities or overcrowding of schools can have a detrimental impact on local communities, particularly affecting the more deprived communities of the Central Area.
- 4.7.10 It is important that any new student accommodation that comes forward in the Central Area does not prevent other town centre uses being brought forward. For example, some locations may be preferable for new homes rather than student accommodation. Concentration of student accommodation can also have detrimental impacts on neighbourhoods from a high transient population and planning applications should ensure visually interesting design and long-term management proposals. However, there are benefits of increasing the numbers of students in the Central Area from greater vibrancy and support to local businesses.
- 4.7.11 The policies that support cultural life in Southend should have a positive benefit for sustainable development with benefits for the economy and communities.

## 4.8 Employment development and retail

### Implications for sustainable development

#### *Employment*

- 4.8.1 The Central Area is the most sustainable place to locate new office development in Southend, based on reducing impacts of travel to work, and creating a central hub of employment development.
- 4.8.2 The Central Area is accessible by sustainable travel modes by a high proportion of Southend's residents, as well as residents of nearby towns such as Rochford, Benfleet, Haleigh and Rayleigh. The Central Area contains three railway stations, a bus station and is within easy walking and cycling distance of many residential neighbourhoods. As offices generate a high number of trips each day an accessible location is essential in reducing the environmental impacts from car travel, as well as ensuring equitable access for those are unable to drive to work.
- 4.8.3 The SCAAP puts a strong emphasis on employment growth in the town centre. With an expectation coming from the Core Strategy that this area will be the focus of 6,500 new jobs between 2001 and 2021. This job figure is half of the total anticipated growth for the whole of Southend set out in the Core Strategy and will need to ensure a step change in job provision to address the fall in the number of jobs in the Central Area according to baseline data.
- 4.8.4 Currently, much of the employment office space is on Victoria Avenue, although a large amount is outdated and long-term vacant. The SCAAP is aiming for new office space throughout the Central Area, with development principles for most of the Policy Areas identifying the need for office space.
- 4.8.5 Removing some of the surplus office space on Victoria Avenue (PA8) may help secure delivery of better quality new space here and elsewhere. Economic assessment has identified the poor quality of the supply on Victoria Avenue may be pushing down office rental value in the area, meaning provision of new office space to meet modern demands cannot be made viable. However, overall supply of office floorspace needs to be monitored and managed in the Central Area to ensure change of use is not undermining supply of office space and resulting relocation to less sustainable locations and a sustainable and flexible supply is maintained.
- 4.8.6 Similarly, in the Sutton Gateway the Sutton Road (PA9.1) existing employment uses are allocated for alternative uses. This land has been shown to be surplus to existing employment needs in Southend in the Employment Land Review and Strategic Housing Land Availability Assessment. Therefore, its loss should not adversely impact on employment in the Borough, especially as other central employment areas are maintained. All redevelopment of surplus employment space could bring benefits to the quality of the built environment and ensure the best use of land.
- 4.8.7 Different types of employment growth are also anticipated in other parts of the centre, including in a new cultural and creative quarter in Clifftown and near Southend Central Station. These locations in particular could be tailored to the needs of small local businesses.

- 4.8.8 In some parts of central Southend there may be competing land uses on some sites. Policies for these areas set out the range of uses that the site could accommodate and this allows for some flexibility in how they are developed. The SCAAP has a role in specifying the uses most suitable to any location, rather than solely rely on the market. Controlling land uses is an important part of securing the long-term mix of uses in the town centre, helping creating sustainable and vibrant places. For instance, at PA7.1 Tylers the policy lists multiple potential uses of this Opportunity Site.
- 4.8.9 There are also other potential benefits to the local economy in the SCAAP. This includes the residential development to maintain a workforce in the Central Area and business links with the university.

### ***Tourism***

- 4.8.10 Policies of the SCAAP that encourage and support the tourism role of the Central Area are also essential in supporting the economy of the area. This includes specific policies or Opportunity Sites where the tourism role is to be maintained and enhanced, such as at the Pier, improvements to Marine Parade, the continued City Beach improvements and the new museum to incorporate space for conferences. The more general policies on improving the public realm could also be of benefit to the tourism economy.
- 4.8.11 Maintaining a range of hotels to cater for diverse needs is an important part of delivering sustainable tourism in Southend. Good quality hotels can encourage visitors to stay longer and therefore spend more. The SCAAP identifies that site CS1.2 Seaway Car Park could be suitable for a new hotel and the Central Seafront area in general (CS1), with the control of visitor accommodation set through the DMD (DM12: Visitor Accommodation).

### ***Retail***

- 4.8.12 Supporting retail growth helps maintain one of the essential roles of any town centre. As well as supporting the continued role of the primary shopping area defined as the High Street, anchored by The Victoria shopping centre at the north and The Royals shopping centre at the south. This is the retail focus of the central area and the SCAAP identifies the need to encourage and support retail in this location, but also the potential to deliver additional small retail units to support independent and local retailers in other areas, such as in Clifftown, and Queensway. Smaller, locally owned business can have a positive benefit for the economy by improving the range of retail on offer, or promoting specialist retailers and supporting independent business.
- 4.8.13 Improved pedestrian routes are proposed around the High Street, including at Chichester Road and at London Road. New pedestrian routes, links and 'pedestrian circuits' will help connect the various shopping areas of the Central Area to broaden the retail offer, for example getting more east west trips linking the High Street to Clifftown and linking the Seafront to the High Street.
- 4.8.14 Policy DS1 sets out the policy principles that will be used to maintain the Central Area as the retail core of Southend and to ensure it can compete with other nearby centres, including out of town shopping areas. This includes identifying the length of defined 'primary shopping frontage' in the central area and measures to maintain the A1 (retail) use by controlling the non-retail (especially A3) uses. The aim is for 60% of the primary shopping frontages (ground

floor) to be retained as A1. This policy requirement should help to maintain the retail character of the area, as other uses in these areas can detract from the overall role and character. The loss of character in these areas can have reinforcing effects on shoppers and retailers that could lead to further decline. More detail on the iteration of this option is included as part of **section 3.5**.

- 4.8.15 The current version of the SCAAP has seen a reduction in the overall length of the primary shopping frontage from earlier versions. However, a high level of protection has been assigned. Retail outside these primary areas will still be protected, but to a lesser extent. This approach should help support a sustainable retail economy, while allowing for some change to help retain vibrancy and reduce vacancy in the central area, as a result of the national change in shopping habitats e.g. more online shopping and retail superstores. There remains the risk that both the reduction in area of A1 uses and the percentage to be retained could result in a gradual decline in the retail officer in the town centre and therefore must be monitored.
- 4.8.16 The policy and supporting Appendix of the SCAAP provide details of the type of evidence that must be provided in order to demonstrate that robust marketing of units has taken place, prior to permission for change of use being allowed. These additional details will have benefits in retaining existing uses.
- 4.8.17 The supporting text to the policy also allows aims to improve the character of units that are long-term vacant by the use of local art. This has the potential to have a positive impact on the character. The policy could include support of temporary use for long-term vacant units for other use-classes or pop-up shops selling locally made goods. This will require working with the relevant landlords and leaseholders to allow appropriate tenancies.

### Policy coverage

- 4.8.18 The SCAAP includes many policies that will help create a stronger local economy and reinforce the retail role of the Central Area.
- 4.8.19 New employment uses are promoted in all of the Policy Areas and several Opportunity Sites. Areas that include a specific reference to economic development and space for new commercial use are:
- The principal location for new (and refurbished) office space is in the Victoria Gateway Neighbourhood, with Victoria Avenue Office Area (PA8.1) specifically relating office use in this location. Policies also identify the use of upper floors throughout the Central Areas for offices, including in the High Street, PA2 London Road, PA4 Queensway PA5 Warrior Square, PA6 Clifftown at Central House, PA7.1 Tylers Avenue;
  - Other policies promote the cultural life and tourism offer of the Central Area. Cultural and tourism uses are centred on the Central Seafront (CS1) with specific areas helping to deliver new or enhanced visitor and tourism attractions including: The Pier (CS1.1); the new Southend museum (CS1.4) and new development at Seaway Car Park (CS1.2) as well as the Waterfront (CS3). There is also the ambition to deliver new cultural development Clifftown (PA6), Victoria Gateway (PA8) and Elmer Square (PA3.1).
- 4.8.20 The retail areas of the centre are to be enhanced and the land use class change within primary and secondary retails areas is to be managed, with beneficial economic impacts as

well as benefits for the built environment and accessible services. Policies of the SCAAP that identify the retail role include:

- DS1 sets the requirement for new retail in the centre; with measure to protect A1 retail in primary shopping frontages;
- Area policies that include the importance of retail include PA1 High Street, PA2 London Road, which includes the potential for a street market; PA6 Clifftown with a focus on smaller shop units as a whole but the possible redevelopment of Central House for larger retail units; PA7.1 Tyles Avenue with potential for new ground floor retail.

## Sustainability effects and recommendations

- 4.8.21 The SCAAP should have a beneficial impact on supporting a sustainable economy in the Central Area as well maintaining its retail role.
- 4.8.22 Suitable office provision needs to be maintained in the Central Area, as the most accessible part of the borough. Therefore, there is a need to make sure that existing office and business space is not lost in favour of other uses, such as residential or education use. Policies protecting employment uses are included in the DMD, including details for protection of use in Grainger Road, Short Street and Tickfield industrial areas; protection of employment is also part of Core Strategy Policy CP1. Also, because of change in the type of needed by modern business (for example more flexible space of a higher quality) a reduced office floorspace is likely to be achievable while still maintaining or growing the Central Area economy. However, there will be a need to monitor the net changes in supply and ensure availability of space is maintained.
- 4.8.23 The Central Area is the most sustainable place for high trip generating office uses and therefore retaining this use in the area is essential for sustainable development.
- 4.8.24 Protection of the retail use in the primary shopping areas is important to maintain the retail role of the town centre and High Street. The approach in the SCAAP is to identify and define a focused primary retail area and then set high levels of protection for A1 uses in this area. This approach should help in maintaining the character of this frontage and avoid it becoming diluted and decline further as its role and function becomes diluted. Allowing flexibility beyond these areas should help maintain the vibrancy of the other parts of the Central Area, as retail habitats change. Permitting other retail uses throughout the Central Area can also help improve the character of others areas and provide local services, especially where the residential population of the Central Area is set to increase. However, retail performance should be monitored to ensure that policy strategy is not exacerbating retail decline in the town centre.

## 4.9 Leisure, recreation and open space

### Implications for sustainable development

#### *The Seafront*

- 4.9.1 Central Southend has a continued importance as a leisure destination for the people of Southend as well as visitors from a wide area. This provides an important economic income for the town. This is covered in the SA section on employment and retail (**section 4.8**).

- 4.9.2 The seafront, especially the central seafront (including the pier), and Eastern and Western Esplanades, are some of the major recreation and leisure assets of the town. The SCAAP includes proposals for how these areas can be further improved, such as new public space at the end of the High Street, expansion of the City Beach and redevelopment at the Seaway Car Park.
- 4.9.3 The SCAAP also seeks to improve access to the sea and seafront through improvements at the waterfront, to include new facilities, such as jetties and slipways and good management. These measures should help secure the better recreation access at the seafront, a leisure resource for residents and visitors. As set out in policy, new development on the waterfront and seafront must ensure to protect the nature conservation interests of the area, and the quality of the natural environment that is part of the attraction of the area.

#### ***Open space and Public Space***

- 4.9.4 In addition to meeting the leisure and recreation needs of the wider Southend population and visitors to the area, consideration also needs to be given to the more everyday needs of central Southend residents. New and existing housing in the area means open space is needed for formal and informal recreation and currently the Central Area has a lack of such space. Where residential development is delivered there will need to be access to local public, private or semi-private open space, this should be incorporated into development on larger regeneration sites or for smaller sites enhancement to what is already there. Several sites are put forward where there is the potential for new or improved open space or public space, such as at Warrior Square, and improvements to the existing greenspace along Queensway.
- 4.9.5 The SCAAP does make reference to the need to supply new space for informal recreation to help alleviate visitor pressure on the important foreshore nature conservation sites. This is part of the 'green grid' strategy, as shown in the SCAAP. The SCAAP includes some detail of what these spaces should contain, such as tree planting and landscaping. Additional detail of the green network through the Central Area included on the Policies Map, to show how the various green spaces will be interlinked providing connectivity through the centre and act as an attractive alternative to the walking on the seafront. In particular, maps should cover the anticipated improvements along Queensway.
- 4.9.6 The SCAAP identifies that the public space at the north end of the High Street could be better used and this could be in combination with the proposed pedestrianisation of London Road. This site could also be used as the venue for periodic local produce or farmers markets, and could include some permanent market stalls.

#### **Policy coverage**

- 4.9.7 The SCAAP includes many policies that will help deliver new and protect existing leisure and recreation development in the Central Area. Policies include:
- CS1 policies relate to the improvement of the Central Seafront as an important leisure and recreation asset for local residents and visitors, this includes new facilities at the waterfront;
  - Several area specific policies include principles covering new public space and open space. PA8 Victoria Gateway a new civic space at North Road junction with Chelmsford

Avenue and an enhanced space between the Civic Centre and Law Courts; PA1 High Street including new public space including at Victoria Circus, at the railway bridge and multi-level spaces to connect the seafront to the Town Centre including the public piazza (CS1.3) and at Tylers Avenue (PA7) as well as public space in others areas such as Clifftown (PA6), and as part of PA8.2 Baxter Avenue;

- There are also policies covering new and improved areas of public green spaces such as PA5 Warrior Square; a 'Queensway Urban Park' (PA4) and linked green spaces at PA8.1 Victoria Avenue Office Area.

## Sustainability effects and recommendations

- 4.9.8 The SCAAP should have beneficial impacts on sustainable development from supporting the provision of improved space for leisure and recreation in the Central Area.
- 4.9.9 New seafront and waterfront leisure and recreation development will need to take into account the potential conflict of uses. There will be different demands on the area from areas of quiet enjoyment of the natural environment to places for active water-sports. For everyone's enjoyment different uses will need to be managed to ensure high quality leisure opportunities for all.
- 4.9.10 There is also the possibility that new development for waterfront tourism and leisure conflict with the nature conservation interest of the site. This will need to be managed to ensure no harm comes to internationally designated sites.
- 4.9.11 To help deliver new high quality urban green spaces as proposed through policy landscape masterplans should be established to ensure that it is delivered in a coordinated way, in particular where linked spaces are proposed. These should identify the features of importance in these areas, not only the location.

## 4.10 Sustainable construction and flooding

### Implications for sustainable development

#### *Sustainable Construction and energy*

- 4.10.1 The large amount of redevelopment proposed through the SCAAP will require substantial use of natural resources during construction and in operation. To reduce these impacts it is essential that new development is designed in such a way to reduce overall natural resource demands.
- 4.10.2 Many older buildings in the town centre are unlikely to be energy efficient, therefore upgrading or redevelopment has the potential to deliver energy savings in the long-term. In terms of overall resource use, and protection of heritage, upgrading and refurbishment is likely to be preferable over demolition and new development. However, where this is not possible replacement buildings should be constructed to high standards of sustainability.
- 4.10.3 As well as energy savings through efficiency there is also the possibility in larger development schemes or networks of schemes including on-site energy generation for low carbon sources. This could include community heat and power schemes, that may be suitable as part of larger redevelopment areas, including University buildings, mixed use or new office developments.

- 4.10.4 The SCAAP includes policy criteria for all Policy Areas to support decentralised energy supply and energy efficiency in new development, which may be possible for many development sites. For example, there may be good potential for wind energy on new seafront development, subject to the choice of the right technology that is suitable for its location and generates a viable amount of energy and solar power will be suitable on many development sites in the Central Area.
- 4.10.5 Water resources in the east of England can be limited, especially in times of low rainfall. Therefore, new buildings and refurbishments must incorporate water efficiency measures, including re-use of rainwater or grey water. However, this will be managed by DMD Policy DM2 that covers efficient use of resources.
- 4.10.6 On larger and landmark buildings SBC should take every opportunity to ensure buildings include sustainable design measures, such as renewable energy generation, efficiency and urban greening. For tall and larger buildings this is covered by DMD Policy DM4, although consideration should be given to all projects that will be focal point of Southend.

#### ***Flood risk***

- 4.10.7 Part of sustainable construction is making sure new development does not put existing and future residents at increased risk of flood. Flood risk can occur from new development being located in areas identified as being at risk of being inundated from rivers or the sea. There is also flood risk created by surface water during heavy rainfall. There is an increasing risk from intense storms due to a changing climate and therefore this risk needs to be factored into the design of new development.
- 4.10.8 The SCAAP addresses flood risk and includes a policy to manage this risk in the seafront area where tidal inundation is a real possibility. Several sites have also been identified that are at particular risk of surface water flooding. There are also proposals for urban greening and areas of soft landscaping that can be extremely beneficial in terms of reducing surface water runoff and helping prevent associated flooding.

#### **Policy Coverage**

- 4.10.9 There is coverage of the need to ensure sustainable construction in the policies of the SCAAP, which should help reduce resource use and ensure the more efficient use of energy, as well as in the DMD Policy DM2. Each area specific policy contains criteria on energy, including decentralised energy supply and the retrofit of existing development.
- 4.10.10 The principal flooding policy of the SCAAP is DS4 that sets development principles specific to flood risk. The policy referenced within the Central Seafront sections of the SCAAP (CS1). The policy must ensure it allows for the latest flood risk policy from the Environment Agency to be taken into account, including the most recent government guidance on flood risk allowances for climate change that were published in early 2016.
- 4.10.11 Supporting text also specifically notes the surface water flooding issues in the Victoria Gateway Neighbourhood.
- 4.10.12 Area policies also include requirements for urban greening that have the potential to reduce surface water runoff and therefore flooding relate. This primarily relates to urban greening,



including include tree planting, green and brown roofs and also more comprehensive schemes to deliver biodiversity improvement and new/enhanced parks, such as at Queensway including the creation of the 'Urban Park' and other spaces as referred to in **paragraph 4.9.7**;

## **Sustainability effects and recommendations**

- 4.10.13 The SCAAP has the potential to have some beneficial effects on sustainable development. In relation to sustainable construction and the more efficient use of resources.
- 4.10.14 The large mixed use and landmark sites proposed have real potential to deliver buildings to exemplar sustainability standards, both in construction and use of resources. Building to high standards can have benefits for the resource use of the individual buildings as well as providing an example of standards that can be achieved. This can help guide the delivery of other development in the Borough, helping guide the way for sustainable construction.
- 4.10.15 Flood issues are covered by policy DS4. The wording of this policy may help in reducing flood risk as a result of new development.

## **4.11 The natural environment**

### **Implications for sustainable development**

#### ***Conservation of nature conservation***

- 4.11.1 The SCAAP recognises the importance of protecting the biodiversity assets along the coast and foreshore. Of particular importance is the foreshore area that is the location of internationally important areas designated for their nature conservation value.
- 4.11.2 The natural environment is a major asset to the Borough in terms of the character of the area and value this gives to the visitor economy and local people. The Central Area also contains areas of water, open space, trees and landscaping that all contribute to the natural environment quality.
- 4.11.3 These areas have statutory protection from harm set through legislation. New development in central Southend must not adversely impact on the internationally designated nature conservation sites; evidence needs to be in place to demonstrate this has been addressed. A Habitats Regulations screening assessment will be needed to show how impacts on biodiversity have been taken into account.
- 4.11.4 Policies at the seafront specifically recognise the potential for impacts, requiring new development to take this into account. The policies refer to the need of development in the seafront area to undergo appropriate assessment under the Habitats Directive. The Core Strategy contains the policies to ensure obligations under the Habitats Directive are met elsewhere in the Central Area, as even development some distance from the foreshore could put nature conservation sites at risk. For instance, drainage of new development needs to ensure that storm water runoff does not harm the protected site, or sufficient open space is provided as part of new development to reduce visitor pressure on the foreshore.
- 4.11.5 Policies of the central seafront include policy principles that could require new information and interpretation opportunities on the nature conservation value of the seafront. This is a positive

step in helping visitors and residents understand and appreciate the unique value of the foreshore and may also help protect these assets from unintentional harm.

### ***New green space***

- 4.11.6 The town centre currently has poor provision of green public open space, with the exception of the seafront. Site specific Policy Area principles identify how new green open space can be integrated into development. This includes a new Urban Park at Queensway, linked open space at the Victoria Gateway Neighbourhood and new green space at Warrior Square. Delivering parks such as this can be positive in helping secure more sustainable urban environments.
- 4.11.7 The SCAAP includes proposals to improve the quality of existing areas, through new planting, such as at the High Street, Victoria and Sutton Gateway Neighbourhoods and in many instances the policy principles identify the need for 'soft' over 'hard' landscaping. In helping achieve more sustainable development the SCAAP supports an approach where urban greening is not only be considered for its aesthetic benefits but also the biodiversity value. Examples of the type of new species promoted throughout the Central Area could be provided, for instance wildflowers for bees and butterflies and fruiting species to support birds and bats.
- 4.11.8 Areas of green space within the urban context can have multiple sustainability benefits, not only in relation to wildlife, these include:
- providing open space for rest and relaxation of residents, workers and visitors to the town centre;
  - providing shading and reducing urban heat island effects caused by the sun reflecting off hard surfaces on hot days that make outdoor spaces uncomfortably hot;
  - views of trees and green spaces have been shown to have positive benefits for mental wellbeing;
  - planting can make a valuable contribution to the quality of the built environment, such as tree lined avenues and pocket parks.
- 4.11.9 There are also potential positive impacts of the proposals on the protection of the natural environment elsewhere in the Borough and region. This comes from the intensification of use of land in the Central Area, meaning more development can be accommodated on previously developed land rather than requiring greenfield sites. The redevelopment of surface car parking to more space efficient alternatives is an option as part of a longer term strategy for Southend, especially north of the railway line. In addition, redevelopment of redundant office and business sites in the Victoria and Sutton Gateways allows these accessible sites to be bought back into good use.

### **Policy coverage**

4.11.10 Area policies addressing urban greening include:

- PA4 Queensway, PA5 Warrior Square PA7 Tylers all cover the greening of Queensway including the reference to the 'Urban Park' in PA4 and PA5.
- General urban greening including new tree planting and support for soft landscaping rather than hard landscaping. For policy areas there are now policy criteria to promote new tree planting, use of green walls and roofs. Specific areas of greening include at

PA5 Warrior Square that includes the potential for a new urban greenspace and restriction of hard landscaping and PA8 Victoria Gateway includes criteria to help deliver a new linked greenspaces in PA8.1 Victoria Road Office Area.

- CS1 Central Seafront identifies the importance of maintaining a 'green grid' with the intention linking the urban open spaces and helping to alleviate pressure on the internationally designated foreshore area. CS3 relating to waterfront development also identifies the need ensure the seafront nature conservation designations are protected from harm.
- CS2 relates to the need to protection the nature conservation sites on the seafront, including the requirements for assessment as part of the Habitats Regulations.
- Policies that will help land be used more efficiently and therefore potentially protect existing open space from harm include policies that will help bring redundant or unused sites back into full use. This includes PA3 Elmer Square and PA8 and PA9 the Vitoria and Sutton Gateway Neighbourhoods.

### Sustainability effects and recommendations

4.11.11 Polices of the SCAAP are a likely to be beneficial in protecting the natural environment, particularly areas of high designated quality with likely beneficial effects on sustainable development.

4.11.12 Providing detail on how elements of urban greening will be achieved, for example, the Urban Park at Queensway has the potential to bring the natural environment into the heart of the town. A masterplan for the site, as referred to in policy, may help secure contributions from local developers or aid individual developments identify what is needed from them to deliver part of the new green space. Similarly, such an approach could be taken for the linked green spaces at Victoria Avenue as part of the masterplan for this area, where there is need for an overarching strategy for the area, given the way development is occurring in a more piecemeal way in this location.

4.11.13 The inclusion of details of the 'green grid' strategy in the SCAAP should help in alleviating pressure on the seafront. To help delivery this areas of new linked green infrastructure should be included on the Policies Map and delivery and understanding promoted through new signage and information. New open space is expected to help provide an alternative to using the foreshore for recreation and help reduce visitor pressure in the designated area. Additional detail on what the characteristics of such spaces could be and where they will be provided should be included in the plan or be considered as part of the future Southend Local Plan as the continued growth of Southend and the strategy to attract more visitors to the town could put sensitive bird habitats at increasing risk of harm

4.11.14 There are several references to lighting strategies in the SCAAP. These can help create a more attractive night-time environment and lighting of 'green grid' links could help improve safety. However, for nocturnal wildlife lighting can create barriers to movement. Therefore, lighting schemes need to take potential impacts into account, using suitable wattage, directional lighting, timings and low level lighting to avoid adverse impacts.

## 4.12 Implementing the SCAAP

- 4.12.1 For any of the plans, policies and proposals of the SCAAP to be achieved and sustainable development delivered, they must be implemented. This section of the SA Report considers how effective the SCAAP might be in securing implementation and delivery of sustainable development.
- 4.12.2 This assessment does not consider issues such as viability and availability of sites, as this is matter to be determined by plan makers. However, it will be necessary to have reasonable certainty that any of the projects and proposal of the SCAAP have can be delivered. Without this certainty there is little value in including them in the SCAAP.

### Implementation

- 4.12.3 The SCAAP has an essential role to play in showing how the multiple proposals and regeneration schemes for central Southend will be delivered and how they will be implemented. The SCAAP acts a co-ordination document to join-up the schemes of various delivery partners and to help secure further funding by providing supporting documents for bids, with the intention of creating a better and more sustainable Southend.
- 4.12.4 The SCAAP as a whole, and as reflected in the implementation plan, only includes site allocations where there is the realistic probability that they can be delivered within the plan period. Therefore, only sites that have evidence of being deliverable within the plan period remain the current version of the SCAAP.
- 4.12.5 The proposed 'Implementation and Monitoring' framework of the SCAAP contains several aspects that help demonstrate that the AAP could be delivered. This includes:
- **Identification of the delivery partners for policies:** Delivery partners include public and private bodies, including: developers, property owners, infrastructure providers, transport providers, the university. The Council also are a major stakeholder in the Central Area as they manage the town centre.
  - **Identification of ownership or responsible agencies for proposal sites:** The majority of the sites are in Council ownership, demonstrating that these sites will be available for development as set out in the SCAAP. Using the SCAAP to co-ordinate work with the others should help development to be bought forward in a unified way. The particular complexities of sites in multiple ownerships are identified.
- 4.12.6 The proposed monitoring framework will help demonstrate how effectively the SCAAP is being implemented. The SCAAP monitoring framework will be used in to help monitor the SCAAP and with details included at the next version of the SCAAP and accompanying SA report.

### The usability of the AAP

- 4.12.7 For the AAP to be successfully implemented it needs to be a usable document.
- 4.12.8 As this SA report shows the majority of AAP objectives, policies and proposals are compatible with achieving sustainable development for central Southend and beyond. The SCAAP is clearly set out avoiding too much internal repetition or unnecessary repetition with other policies of the LDF, which also will be applicable where relevant to any development proposed in the SCAAP area.

## 4.13 Mitigation

### Introduction

- 4.13.1 The SEA Directive requires that consideration be given to how any significant impacts identified during the SA process could be mitigated.
- 4.13.2 Mitigation of the potential adverse impacts of the strategy can be achieved in a number of ways. The matrix showing the SA of each policy in **Appendix D** gives examples of how the potential adverse impacts of the policy could be mitigated against through adjustments to policy wording or the addition of potential policy principles.

### Methods of mitigation

- 4.13.3 In addition to adjustment of policy wording there are several other ways mitigation of possible impacts can take place; these are shown in paragraphs 4.13.4 to 4.13.8.
- 4.13.4 **Implementing other planning policies:** Many potential impacts will be mitigated through the use of other policies including those of the LDF and national policy. This has a particular role to play in avoiding the adverse impacts from the quantity of development to be delivered through the strategy. Policies that will help mitigate impacts include those on natural environment protection, community infrastructure provision, design and the historic environment.
- 4.13.5 **Requirements for developers:** Policy sets out measures that developers will have to use to demonstrate they have mitigated the impacts of their development. The SCAAP itself contains some requirements, such as Habitats Regulations assessments and Flood Risk Assessment, with other such requirements set elsewhere in the SCAAP, such as requirements for Travel Plans or meeting sustainable construction standards. There may also be site-by-site planning application requirements these could include ecological assessment and enhancement plans, air quality assessments and ground conditions assessment and remediation plans as required.
- 4.13.6 Up-to-date **Development Briefs** or **Masterplans** for all of the larger development sites, or groups of small sites or for parks. These will help implement a cohesive development strategy for whole areas. This has greater potential to deliver high quality and sustainable development rather than a piecemeal approach. It should cover issues such as:
- design protocols and the layout of development;
  - biodiversity protection or enhancement measures;
  - planting schemes;
  - creation of permeable spaces and links to neighbouring development.
- 4.13.7 The delivery of infrastructure improvements to mitigate some impacts, such as loss of public open space and public transport enhancement, will also be dependent on developer **CIL** and **other contributions** or **obligations**. These will be used to deliver sustainability benefits associated with new development.
- 4.13.8 Implementation of **other strategies and plans** in the plan area, which will include measures such as the transport improvement strategies, delivery of bus priority routes, tourism

strategies, 'City Beach', Local Transport Plan and other regeneration strategies, as well as the plans of neighbouring local authorities.

## 5 Future stages of Sustainability Appraisal

### 5.1 Next stages of the SA

- 5.1.1 The SA of the SCAAP will continue to adoption of the SCAAP. Each consultation stage of the SCAAP will be accompanied by an updated SA Report that document the appraisal process and the decisions that have been made.
- 5.1.2 It is likely that following consultation on the Revised Proposed Submission SCAAP next stage of preparation will be submission of the plan for Examination by the Planning Inspector. Prior to this there may be suggested amendments made to the plan for the Inspector to consider, taking into account representations received on the SCAAP or the SA.
- 5.1.3 At Examination the Inspector will consider the SCAAP, representations and the SA in identifying the need for further modification to the plan. These modifications will be consulted on and it therefore may be necessary to consider if they require update of the SA.
- 5.1.4 All changes to the SCAAP before, during or after the Examination will need to be reviewed to determine if there are likely to be significant effects and if it is necessary for an SA.
- 5.1.5 Once the SCAAP is found sound and modifications included it will formally adopted by the Council. At the time of Adoption a 'Statement' must published that sets out (amongst other things) 'the measures decided concerning monitoring'.

### 5.2 Monitoring

- 5.2.1 There is a requirement for monitoring the sustainability appraisal arising from the SEA Regulations. The intention is to monitor the impact on the SCAAP for significant environmental effects. Monitoring will need to consider positive and negative impacts, triggering a review if necessary.
- 5.2.2 The specific requirements of the SEA Regulations on monitoring are to:

*"Monitor the significant environmental effects of the implementation...with the purpose of identifying unforeseen adverse effects at an early stage" (Regulation 17(1))*

#### Developing the framework

- 5.2.3 The sustainability framework (section 2) provides a good starting point for developing targets and indicators for monitoring. As set out in the SEA Regulations there is no need for the SA monitoring to be in isolation from other monitoring measures put in place for the plan. Therefore, it is recommended that monitoring is integrated into LDF and SCAAP specific monitoring.
- 5.2.4 Monitoring need only begin once the SCAAP has been adopted and implementation begun. Therefore, a monitoring framework for the SA need not be agreed until the final monitoring framework for the SCAAP is in place. **Appendix G** sets out a set of potential monitoring indicators for the SA, based on those used to monitor the Core Strategy and Development

Management DPD. It also includes suggestions of additional elements that could be monitored to fill gap.

5.2.5 For a successful SCAAP monitoring framework the Council must ensure that the indicators they choose for monitoring are manageable, really measure the effects of SCAAP implementation, and are matters over which the SCAAP can have a direct influence. The indicators should also only address matters that are required through policy and not set indicators that exceed policy expectations.

5.2.6 In setting a monitoring framework for the AAP the chosen indicators and targets need to be:

- **specific** – in that it relates to policy objectives, indicators reflect what is set out in policy and strategy, and do not appear to be defining requirements that go beyond, or differ from, policy;
- **attributable** – monitoring the indicator must give results that can be directly related to the LDP policies, and should not be issues that are influenced or are more likely to be influenced by matters outside the control of the LDF;
- **measurable** – it must be the case that data or information can realistically be gathered on the indicators, including whether this is possible given time and resources. Indicators could be linked to data already been gathered by other bodies, besides the planning authority;
- **timescale** – the indicator must be capable of being monitored on a regular basis, usually annually, to be an effective part of a monitoring programme.

5.2.7 The SA already identifies some matters that could be considered as part of monitoring the SCAAP, including:

- Community facilities e.g. primary school class size, GP patient ratio in local health centres, community events held in the Central Area
- Relative health deprivation e.g. obesity levels (adults and children), mortality statistics;
- Projects requiring HRA screening/assessment
- Impacts on locally listed buildings; and
- Air quality at Southend air quality monitoring stations, including days of medium/high NO<sub>2</sub> and PM<sub>2.5</sub> and PM<sub>10</sub>.



## 6 Conclusion and Summary

### 6.1 Introduction

- 6.1.1 The Sustainability Appraisal (SA) has recognised that the Central Southend AAP (SCAAP) has an important role to play in the sustainable development of this area and the wider Borough.
- 6.1.2 The proposed SCAAP vision and objectives provide the foundation for the development of policies and proposals for the Central Area. However, these objectives are stronger for some areas, such as the importance of the conserving and enhancing the natural environment.

### 6.2 SCAAP and SA iteration

- 6.2.1 The SCAAP has been through several iterations prior the current Preferred Approach version, starting with Issues and Options in 2010 and the Proposed Submission version consulted on in 2011. From the 2011 version to the 2015 version of the SCAAP the plan was made much more succinct to reflect that national planning guidance that came into force in 2012 and the Southend DMD that was adopted in 2015. The current version of the SCAAP is much the same format as the previous versions, with policies amended to take into account comments on earlier versions and new evidence base, such as on parking.
- 6.2.2 At the Issues and Options (2011) version of the SCAAP three spatial options were considered for development. All three options were appraised as part of the SA, including the preferred approach that was 'City by the Sea'. The SA found that this option was likely to help delivery the greatest benefits for sustainable development, although this would depend on funding being available.
- 6.2.3 Other alternatives for delivery were considered as an iterative process, with no other options for development explicitly stated and assessed with the exception of various options put forward for the managing the Primary Shopping Frontage.
- 6.2.4 Each stage of the SCAAP was subject to SA before being finalised, allowing the opportunity the appraisal to inform the final version of the SCAAP.

### 6.3 SA of the Preferred Approach SCAAP 2015

- 6.3.1 The SA has identified that the objectives, policies and proposals of the SCAAP have the potential to have beneficial sustainability development effects in the Central Area, as well as wider Southend and beyond. The SCAAP contains much that is very compatible with achieving sustainable development. There are many positive aspects of the plan in relation to delivering sustainable development that include:
- securing more sustainable transport access to town centre, with emphasis on walking and cycling as well as public transport, with the aim of aiding a change of travel mode from car use in the medium to long term and more equitable access for all;
  - delivering improvements to the built environment though designing places and buildings that fit the context of the area and provide for a vibrant town centre;

- supporting the economy of the town through providing new spaces for a diverse range of businesses, including offices, retail and strong emphasis on the growing the tourism economy;
- protecting the assets of the Central Area, including historic and cultural heritage of the built environment and protecting the high quality natural environment and seafront;
- supporting new mixed communities in the Central Area through the provision of new homes and community services, including schools, health centres and open space;
- clear policies on the anticipated residential development in the SCAAP area, showing the status of individual sites and the timeframe for their delivery;
- encouraging urban greening with policy criteria in place to help deliver new areas of planting throughout the Central Area, with the potential to have multiple sustainability benefits for the area; and
- continuing to expand the university and college facilities in the town centre to support a thriving education sector, this will help create a vibrant town, skilled workforce and opportunities for business growth.

6.3.2 The SA of the submission SCAAP reveals some other sustainability issues. The SA makes recommendations on how some aspects of the SCAAP could improve the sustainability performance of the plan.

6.3.3 **Transport and movement:** To successfully achieve a modal shift away from car use, there is a need to ensure the transport, movement and public realm improvement strategies present a proactive and joined up approach to managing traffic in the town centre. Without this the regeneration of central Southend could be adversely affected by increasing congestion, with negative health and environmental impacts.

6.3.4 The SCAAP has the potential to deliver significant benefits from achieving a modal shift to more sustainable travel, including walking and cycling that can also have benefits for the community and economy of Southend.

6.3.5 The policies of the SCAAP show a clear intention to make the town centre a better place with improvement to the pedestrian environment to encourage more people to walk. Improved links to the Central Area will also help reduce car use in the town centre, with benefits for the natural and residential environment. In the long-term a strategy of reducing car parking could be pursued to release land for alternative uses and further encourage sustainable travel. However, a level of parking needs to be maintained to support the visitor economy and compete with out-of-town retail where car parking is usually free and plentiful.

6.3.6 **Residential development and communities:** Central Southend has a significant role to play in delivering new residential development for the Borough. The requirement is set through the Core Strategy for Southend.

6.3.7 The policies of the SCAAP are essential in helping ensure development in the Central Area makes a suitable contribution to meeting Southend's housing needs. Therefore, the SCAAP has the potential to deliver sustainability benefits from supporting additional housing in the Central Area, supporting its viability and vitality and contributing to reducing the dependency on car travel.

- 6.3.8 As the SCAAP evolves some further information could be included to secure delivery of community facilities to meet the needs of a growing Central Area population – although the SCAAP does identify the areas where new facilities should be provided. For instance, there is a particular demand as a result of the loss of community spaces and services at Queensway House, and educational facilities in Victoria Gateway and Sutton Gateway neighbourhoods. Providing community infrastructure is an essential part of ensuring thriving local communities in the Central Area.
- 6.3.9 As identified through policy delivering new housing and community facilities at Queensway will need to take into account the emerging strategy of the 'Better Queensway' initiative. The Queensway policy recognises the need for no net loss of affordable housing and as this allocation has the greatest quantity of development capacity (in the plan period) yet to get permission it presents a good opportunity to delivery affordable homes to meet needs. Development at Baxter Avenue (PA8.2) also identifies the site's suitability for social housing. In all other locations achieving new affordable housing will be managed through Core Strategy and DMD policy requirements, which should be rigorously applied to ensure that new housing helps contribute to meeting the need for affordable homes.
- 6.3.10 The SCAAP includes policies for the development of new student accommodation in the Central Area, and in particular Elmer Square. Due to the short tenancy of these type of properties they can occasionally have an adverse impact on local environment character, for example through poor storage of refuse. Therefore, policy criteria could be included that requires planning applications for this type of development to be accompanied by a management plan for their operation.
- 6.3.11 The SCAAP clearly sets out the anticipated housing yield in each Policy Area. The table also shows that out of all allocated sites, based on anticipated yield, there is only a residual of just under 700 homes that have yet to receive planning permission. This inclusion provides useful clarity on the role of each area and appreciation of the likely growth planned for and that already ready for delivery. However, also included in SCAAP is the long-term potential on some areas beyond the plan period. This consideration of future growth is important as the plan period is only to 2021 and to achieve sustainable growth it is necessary to consider the long-term potential in the area.
- 6.3.12 **The built and heritage environment:** The principle focus of the SCAAP is how improvements can be made to the built environment of the Central Area, through new development and enhancement. This will have positive sustainability impacts related to improving the image of the centre. A better 'sense of place' can help support the community's pride where they live, which can have positive impacts on social sustainability. The town centre is also the showcase for the rest of the town, and therefore if this area has a high quality image it can encourage local and national investment in the whole town.
- 6.3.13 The SCAAP has the potential to deliver significant environmental benefits from supporting development that improve the built environment; this can include benefits for the economy and the communities of the town. There are many instances where proposals and policies of the SCAAP are likely to help deliver a higher quality built environment. This includes specific regeneration and renewal schemes, measures such as new planting and public art and policies to help ensure new and existing car parks are designed reduce their impact on the built environment, for instance through use of green walls on multi-storey car parks.

- 6.3.14 There may be an opportunity for the SCAAP or other mechanisms (such as Article 4 directions) to help control change of use from office to residential development through permitted development rights. The current approvals for this change of use may be undermining a policy led approach to sustainable development in some part of the Central Area.
- 6.3.15 **Education and culture:** Support for education in the town centre will have positive sustainability impacts, not only from improving availability of learning sites but also from the vibrancy a student population can bring to the Central Area.
- 6.3.16 The SCAAP has the potential to deliver benefits relating to education, supporting local communities and the economy.
- 6.3.17 The SA identifies that infrastructure studies show there is may be a need for new school space in Central Area. Victoria and Sutton Gateway Neighbourhoods are identified in the SCAAP as possible locations for new schools, although no specific locations are identified. Without necessary provision to meet demand there may be a detrimental impact on local communities, particularly affecting the more deprived communities of the Central Area. Therefore, it will be important to ensure demand and supply of school places is monitored and new facilities provided as necessary.
- 6.3.18 Consideration needs to be given to new student accommodation proposals to ensure they do not hinder other types of development. For example, some locations may be preferable for new homes rather than student accommodation. Concentration of student accommodation can also have detrimental impacts on neighbourhoods from a high transient population, although there can be benefits of creating vibrancy, management considerations are noted above in relations to 'residential development and communities'.
- 6.3.19 **Employment and retail:** The SCAAP identifies that of central Southend is the preferred area for new retail and office development. The SA finds that the SCAAP should have a beneficial impact on supporting a sustainable economy in the Central Area as well maintaining its retail role.
- 6.3.20 The Central Area is the most sustainable place for high trip generating office uses. There is a need to make sure that the availability of office and business space is not compromised in favour of other uses, such as residential or education use. Policies in the Core Strategy and DMD policy set out measures to manage supply and protection of existing uses. There is currently an over-supply of floorspace and loss of some of this use will remain compatible with sustainability objectives for the economy. However, new or renovated provision to meet the specific needs of modern business is likely to be necessary, including more flexible and higher quality space. Monitoring the demand and supply of Central Area office floorspace will be necessary to ensure that they are matched and to ensure economic growth in the town centre is not constrained to the benefit of out-of-centre locations that may be less accessible and therefore less environmental sustainable.
- 6.3.21 Protection of the retail use in the primary and shopping areas is important to maintain the retail role of the town centre and High Street. Too high a proportion of non- A1 retail uses can change the character of a retail area, which could lead to its further decline as a place to shop. However, there can also be benefits from reducing the number of vacant units through temporary retail of other uses. The protection of A1 uses from unsuitable change of use,

where there is actually a demand for this use, is enhanced through the inclusion details in the SCAAP appendix of the marketing evidence that will have to be provided as part of any application.

- 6.3.22 **Leisure, recreation and open space:** The SCAPP polices relating to leisure and recreation should have beneficial impacts on sustainable development from supporting the provision of improved space for leisure and recreation in the Central Area.
- 6.3.23 The SA does note that new seafront and waterfront leisure and recreation development will need to take into account the potential conflict of uses. There will be different demands on the area from areas of quiet enjoyment of the natural environment to places for active water-sports and seaside attractions. For everyone's enjoyment of different uses will need to be managed to ensure high quality leisure opportunities for all.
- 6.3.24 There is also the possibility that new development for waterfront tourism and leisure will conflict with the nature conservation interest of the site. This will need to be managed on a site-by-site basis to ensure no harm comes to internationally designated sites, in keeping with the Habitats Regulations.
- 6.3.25 **Sustainable construction and flooding:** The SCAAP has the potential to have some beneficial effects on sustainable development. In relation to sustainable construction and the more efficient use of resources.
- 6.3.26 The SA notes the potential for large mixed use and landmark sites proposed have real potential to deliver buildings to exemplar sustainability standards, both in construction and use of resources. Building to high standards can have benefits for the resource use of the individual buildings as well as providing an example of standards that can be achieved. This can help guide the delivery of other development in the Borough, helping guide the way for sustainable construction. Sustainable construction policy is primary covered by higher tiers of policy including the DMD.
- 6.3.27 **Natural environment:** Polices of the SCAAP are likely to be beneficial in protecting the natural environment, particularly areas of high designated quality with likely beneficial effects on sustainable development.
- 6.3.28 The SCAAP aims for the delivery of new urban greenspace, with some areas needing coordination to help deliver benefits in a unified way. For instance, the proposed Urban Park at Queensway has the potential to bring the natural environment into the heart of the town. Securing delivery of these aims could be improved through the preparation of a masterplan for the site. This could be used to secure funding for the scheme from developers, CIL etc. Similarly, any masterplan prepared for Victoria Avenue could include details of the linked green spaces at Victoria Avenue where an overarching strategy may be needed to coordinate piecemeal development to help create high quality, cohesive area of open space.
- 6.3.29 New open space is expected to help provide an alternative for recreation to the foreshore, to help reduce visitor pressure in the designated area, as part of the 'green grid' details are included in the SCAAP. This may help reduce pressure on the seafront area. The Policies Map will need to be clear on how these routes are linked and work with partners to deliver signage and information to encourage people to use these linked areas.

- 6.3.30 There are several references to lighting strategies in the SCAAP. These can help create a more attractive night-time environment and lighting of 'green grid' links could help improve safety. However, for nocturnal wildlife lighting can create barriers to movement. Therefore, lighting schemes need to take potential impacts into account, using suitable wattage, timings and low level lighting to avoid adverse impacts. Impacts will need to be considered on a site-by-site basis.
- 6.3.31 **Implementation:** It is evident that no sustainability benefits can be realised if development cannot be implemented. The implementation plan will have to show how the policies and proposals might be secured, and includes details of targets and delivery partners.
- 6.3.32 The SA identifies that the SCAAP is a succinct plan and clearly sets out the priorities for development in in Policy Area, with a limited number of development strategy policies specific to the Central Area.

# Appendix A    Baseline and Policy Context

# 1 Baseline and Policy Context

- 1.1.1 This appendix identifies the issues that should be taken into account in undertaking the Sustainability Appraisal (SA) of the Southend Central Area Action Plan (SCAAP) as the context for appraisal. It covers relevant policies and strategies as well as the baseline context. More information can be found in the Scoping Report for the Core Strategy.

# 2 Plans and Strategies

- 2.1.1 This section concentrates on the main plans, policies and programmes that set the context for the SCAAP, particularly at the local level. There are a great number of plans, strategies and regulations that exist with an international to local coverage. However, of most use to understanding what the SCAAP should be aiming to achieve and the sustainability priorities for the area are local plans and strategies. For this reason the focus of the plan and strategy review are these local plans and strategies.

## 2.2 National Planning Policy and Guidance

- 2.2.1 The UK planning system is a plan led approach as imbedded in Section 38 (6) of the Planning and Compulsory Purchase Act 2004. This requires planning applications to be determined in accordance with the development plan and unless material considerations indicate otherwise. Thus, it is essential the planning policies covering Southend will have the delivery of sustainable development embedded within them. National policy is set through the **National Planning Policy Framework (NPPF)**, which was adopted in March 2012.
- 2.2.2 The NPPF therefore constitutes policy which planning authorities and developers must take into account in the preparation of local and neighbourhood plans and is a material consideration in planning decisions.
- 2.2.3 The NPPF does not change the statutory status for the development plan as the starting point for decision making. Proposed development that accords with an up to date local plan should be approved and proposed development which conflicts should be refused unless other material considerations indicate otherwise (Paragraph 12).
- 2.2.4 The document identifies that the purpose of planning is to help achieve sustainable development, with 'sustainable' meaning the need to ensure that securing better lives for current generations does not mean worse lives for future generations and 'development' meaning growth. The key objectives within the guidance that are relevant to the current development proposals are summarised as:
- A presumption in favour of sustainable development;
  - Building a strong and competitive economy;
  - Promoting sustainable transport;
  - Climate change, flooding and coastal change;
  - Conserve and enhance the natural environment; and
  - Conserve and enhance the historic environment.



### *The Presumption in Favour of Sustainable Development*

- 2.2.5 The NPPF identifies that at the heart of the planning system there is a presumption in favour of sustainable development which should be seen as the golden thread running through plan making and decision making. The presumption means ‘decision takers at every level should approve development proposals that accord with the development plan without delay’ (Paragraph 14).
- 2.2.6 The three dimensions of sustainable development that give rise to the need for the planning system to perform a number of roles are identified in paragraph 7 as:
- **An economic role** – contributing to building a strong, responsive and competitive economy by ensuring sufficient land of the right type is available in the right places and at the right time to support growth and innovation; and by identifying and co-ordinating development requirements, including the provision of infrastructure
  - **A social role** – supporting strong vibrant and healthy communities providing the supply of housing required to meet the needs of the present and future generations; and by creating a high quality built environment, with accessible local services that reflect the communities needs and support and its health, social and cultural wellbeing
  - **An environmental role** – contributing to protecting and enhancing the natural built and historic environment and as part of this helping to improve biodiversity, use natural resources prudential, minimise waste and pollution and mitigate and adapt climate change including moving to a lower carbon economy.

### *Ensuring the vitality of town centres*

- 2.2.7 In the NPPF the focus is on recognising and protecting the importance of town centres as the focus for retail, leisure and business development. At paragraph 23 it states that local planning authorities should
- recognise town centres as the heart of their communities and pursue policies to support their viability and vitality;
  - define the extent of town centres and primary shopping areas, based on a clear definition of primary and secondary frontages in designated centres, and set policies that make clear which uses will be permitted in such locations;
  - promote competitive town centres that provide customer choice and a diverse retail offer and which reflect the individuality of town centres;
  - retain and enhance existing markets and, where appropriate, re-introduce or create new ones, ensuring that markets remain attractive and competitive;
  - allocate a range of suitable sites to meet the scale and type of retail, leisure, commercial, office, tourism, cultural, community and residential development needed in town centres.
  - recognise that residential development can play an important role in ensuring the vitality of centres and set out policies to encourage residential development on appropriate sites; and
  - where town centres are in decline, local planning authorities should plan positively for their future to encourage economic activity.

### *Building a Strong Competitive Economy*

- 2.2.8 The NPPF supports the growth agenda with the Government committed to securing economic growth in order to create jobs and prosperity and meet the challenges of global competition

and of a low carbon agenda. The focus on economic growth has been one of the major drivers of the planning reforms in delivering an economic recovery.

- 2.2.9 The guidance states that 'planning should operate to encourage and not act as an impediment to sustainable growth. Therefore significant weight should be placed on the need to support economic growth through the planning system (paragraph 19).

#### *Promoting Sustainable Transport*

- 2.2.10 The guidance identifies that the transport system needs to be balanced in favour of sustainable transport modes giving people a real choice about the way they travel. In preparing local plans local authorities should therefore support a pattern of development which, where reasonable, facilitates the use of sustainable modes of travel.

#### *Climate Change, Flooding and Coastal Change*

- 2.2.11 The NPPF recognises that planning plays a key role in reducing greenhouse gas, emissions, minimising the impacts of climatic change and supporting the delivery of renewable and low carbon energy and associated infrastructure.
- 2.2.12 New development is also required to be planned to avoid increased vulnerability to a range of impacts arising from climate change (including factors such as flood risk, water supply and changes to biodiversity and landscape). New developments brought forward in areas which are vulnerable, therefore should ensure the risks can be managed through suitable adaption measures, including the planning of green infrastructure.
- 2.2.13 In relation to flood risk, the NPPF identifies that inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk but where development is necessary, making it safe without increasing flood risk elsewhere.
- 2.2.14 When determining applications, local authorities should ensure that the flood risk is not increased elsewhere and only consider development appropriate in areas at risk of flooding where, informed by site specific flood risk assessment following the sequential test and, if required, the exception of test (Paragraph 103).

#### *Conserve and Enhance the Natural Environment*

- 2.2.15 The guidance sets out that the planning system should contribute to and enhance the natural and local environment by:
- Protecting and enhancing valued landscapes
  - Recognising the wider benefits of the eco-system services; and
  - Minimising impact on biodiversity and providing net gains and biodiversity where possible.
- 2.2.16 The NPPF identifies that planning policies and decisions should also ensure that any site is suitable for the new use taking into account of ground conditions and land instability, including natural hazard and mitigated accordingly.
- 2.2.17 Similarly, planning policies and decisions should aim to avoid noise from giving rise to significant adverse effects and mitigate and reduce to a minimum other adverse impacts on health and quality of life arising from noise and new development including through the use of conditions.

### *Conserving and Enhancing the Historic Environment*

- 2.2.18 In determining applications the local planning authorities should require an applicant to describe the significance that any heritage assets affected including any contribution made to their setting. The guidance highlights that the level of detail should be proportionate to the assets importance in no more than is sufficient to understand the potential impact of the proposal on their significance (Paragraph 128).
- 2.2.19 The **Planning Practice Guidance** was developed by government to provide additional detail to support the implementation of the NPPF and aid local planning authorities in its implementation. The PPG includes specific measures for improving town centres including a 'town centre first' policy (March 2014), where new development is promoted in the town centre first before it is considered in other areas.
- 2.2.20 The **Habitats Directive and Conservation (Natural Habitats &c) Regulations 1994** (as amended), have relevance to the SCAAP. This is because the area covered by the SCAAP is in close proximity, and in some instances overlapping, with areas designated as being of international significance for nature conservation. These designated areas are collectively known under European legislation as Natura 2000 sites. Any potential impact of planning policy, or specific proposals, on these areas needs assessment to determine the nature of these impacts to ensure that they will mitigate or avoid completely harm to the designated features on the site. A separate process of screening the SCAAP under the Habitats Directive has taken place to establish the potential for effects on the Natura 2000 sites.
- 2.2.21 The Habitats Directive and Conservation (Natural Habitats &c) Regulations 1994 (as amended), have relevance to the SCAAP. This is because the area covered by the SCAAP is in close proximity, and in some instances overlapping, with areas designated as being of international significance for nature conservation. These designated areas are collectively known under European legislation as Natura 2000 sites. Any potential impact of planning policy, or specific proposals, on these areas needs assessment to determine the nature of these impacts to ensure that they will mitigate or avoid completely harm to the designated features on the site. A separate process of screening the SCAAP under the Habitats Directive has taken place to establish the potential for effects on the Natura 2000 sites.

## **2.3 Local and Regional Plans and Strategies**

### *Guiding principles*

- 2.3.1 A masterplan was prepared in 2007 that forms the basis to the proposals in the SCAAP. This is the Southend **Central Area Masterplan**, by Renaissance Southend Limited (RSL). The purpose of the masterplan is to set a vision for central Southend and the seafront, as part of the major scheme for Renaissance Southend. The aim is to:
- To act as a catalyst in realising the vision and objectives of RSL for the revitalisation of Southend;
  - To develop confidence amongst public and private sector landowners;
  - To encourage investment; and
  - To contribute to the development of civic pride and local ownership of proposals and guidance set out in the Masterplan.
- 2.3.2 This masterplan formed the basis for the SCAAP, which takes forward many of the projects and proposals of the masterplan so they become planning policy, rather than a more open

framework for delivery. However, the masterplan area only extends up Victoria Avenue to Harcourt Avenue, and none of the 'Sutton' Gateway.

2.3.3 The **Community Plan** and **SBC Corporate Plan** are both important drivers for local planning policy.

2.3.4 The aspirations for the local community are set out in **The Southend-on-Sea Community Plan 2010-2020**, prepared by the Southend Together Partnership. Although the partnership is now largely dissolved the strategy does provide a useful insight into the full range of sustainability issues that need to be addressed in the borough, covering issues that go beyond planning policy. The Southend development plan will have an essential role to play in building the communities and delivering infrastructure to support more sustainable communities. The strategy sets ambitions for the future of Southend these include:

- To be a borough with decent housing, in safe and attractive residential area, that meets the needs of those who want to live here;
- To provide opportunities, support and information to people of all ages and abilities to enable them to take responsibility for their health and choose a healthy lifestyle;
- To be a borough that has safe, more accessible, and affordable means of getting about, which supports the potential for regeneration and growth;
- To protect the borough for current and future generations and to remain an attractive place for residents, businesses and visitors;
- Create a safer town, where people feel secure and confident to live;
- To reduce inequalities in health and wellbeing across Southend and support all ages to lead independent lives and choose a healthy lifestyle;
- To build strong, confident and proud communities who are empowered to take an active part in local decision making and have positive relationships with each other; and
- To create a thriving and sustainable local economy that will extend opportunity for local residents and promotes prosperity throughout the borough.

2.3.5 The **Southend Corporate Plan** contains 10 corporate priorities and related actions 2013/14. Relevant priorities for the Local Development Framework (LDF) include:

- Ensure a well-maintained and attractive street scene, parks and open spaces;
- Where possible minimise our impact on the natural environment;
- Encourage the prosperity of Southend and its residents; and
- Enable well planned housing and developments that meet the needs of Southend's residents and businesses.

#### *Flooding and Shoreline Management*

2.3.6 The **Strategic Flood Risk Assessment Level 1** (September 2010) prepared by Southend-on-Sea Borough Council provides an overview of flood risk issues throughout the borough in order to facilitate a sequential approach during the allocation of sites for future development.

2.3.7 The assessment identifies that parts of the town centre may be at risk of surface water flooding. Proposed developments within the town centre, amongst the other areas listed within the document, should take consideration for the impact of surface water flow paths to the development and ensure effective management of surface water on site.

- 2.3.8 There has also been evidence of groundwater flooding in the south seafront area. The assessment recommends that more site-specific desk studies should take place for development areas where there is thought to be potential for groundwater flooding.
- 2.3.9 The report also makes reference to several future flood risk management plans relevant to the borough. These include:
- **Essex and South Suffolk Shoreline Management Plan 2 (October 2010)** This provides a large-scale assessment of the risks associated with coastal evolution and presents a policy framework to address these risks to people and the developed, historic and natural environment in a sustainable manner. The Shoreline Management Plan (SMP) is a high-level document that forms an important part of the Department for Environment, Food and Rural Affairs (Defra) strategy for managing flooding and coastal erosion. The key aims of this strategy is to reduce the threat of flooding and erosion to people and their property and to benefit the environment, society and economy as far as possible in line with the Government's sustainable development principles.
  - The Thames Estuary 2100 group **TE2100 Plan (November 2012)**. This plan seeks to find ways of managing flood risk on the Thames Estuary. The Southend area extending round the end of the estuary and including Leigh on Sea is identified as Action Zone 8 where policy is: *'To ensure the communities and local economies in Canvey Island, Southend-on-Sea and Isle of Grain continue to thrive, we and others will need to do more to prevent flood risk increasing as a result of climate change'*.
- 2.3.10 **Strategic Flood Risk Assessment Level 2 (November 2010)** this report provides increased scope of the strategic flood risk assessment for sites where the exception test is required. Hydrodynamic modelling has shown that the seafront and southern extent of the Central Area are at residual risk of flooding in the event of a breach in the flood defences and via overtopping of the existing defences.
- 2.3.11 The document advises future developer to make reference to:
- Hazard and depth maps produced as part of the study;
  - Southend-on-Sea Borough Council Surface Water Management Plan for detailed surface water modelling results;
  - Site-specific Flood Risk Assessments (required for all development proposals on sites greater than 1ha) should refer to council and water utility historic flood records to establish the level of potential surface water flood risk to any future development in these locations.

#### *Transport and Movement*

- 2.3.12 Transport issues for the area are covered in the **Local Transport Plan 3 (2011-2026) (LTP3)** – revised January 2015. The Plan builds on the existing long term strategy set out in the preceding Transport Plans and encompasses key local and national developments and changes in policy. The key themes that need to be addressed by LTP3 are:
- A thriving and sustainable local economy in the Borough.
  - Minimise environmental impact, promote sustainability for a greener Borough.
  - A safer Borough.
  - Reduce inequalities in health and wellbeing and for a more accessible Borough.
- 2.3.13 The Southend Central area contains several strategic transport routes and priority areas. This includes the A127 Freight Corridor, with two strategic junction improvement areas; three

of the Proposed Metro and Sustainable Station Hubs at Southend Central, Southend Victoria and Prittlewell stations and the location of the Travel Centre. Passing through Central Southend is also the route of the Sustrans cycle network a Green Grid corridor.

- 2.3.14 The general principles of the LTP3 are to encourage more sustainable transport choices for all people in the borough, with an emphasis on walking and cycling especially for shorter journeys. However, the importance of car travel is also recognised with the need to take measures to increase capacity and reduce congestion.
- 2.3.15 The LTP3 identifies the seasonal shortfall of parking capacity in the certain car parks in the summer and that rationalisation of Town Centre car parks is being investigated and taken forward as part of the SCAAP. Priorities for improvements include improving the public realm in the town centre and visitor destinations and the seafront to give greater priority to pedestrian infrastructure and better management of on-street parking.
- 2.3.16 The LTP3 identifies the key transport challenges identified in the SCAAP area as:
- To maintain the quality and sustainability of connections in the central area in order to retain Southend's status as a key transport interchange;
  - Vehicular movement to be enhanced through a package of improvements which would include reallocation of parking;
  - Reinvigoration through selective redevelopment and re-provision of surface car parking;
  - Realignment of key infrastructure requirements identified in the LTP with the AAP; and
  - Enhanced pedestrian and cycling connections and upgrade of the public realm.

### *Economy*

- 2.3.17 The **Southend-on-Sea Local Economic Assessment** (December 2013) refreshes the original Local Economic Assessment (LEA) produced in 2010. The document is intended to provide:
- A summary update on the developments in terms of milestones achieved and changes to the economic development policy and landscape in England;
  - A detailed review of the Southend-on-Sea economy in comparison to its nearest neighbours and its wider South East Local Enterprise Partnerships (SELEP) partners.
  - Policy makers and public and private sector partners with a robust and up-to-date evidence base to support the allocation of resources and any subsequent funding allocations to central government.
- 2.3.18 The LEA identifies several important milestones for the Town Centre, these are:
- A new £27m state-of-the-art library and learning facility ("The Forum Southend") located in a new public square in the heart of Southend-on-Sea Town Centre (opened in September 2013).
  - A new £3m Cultural Centre opened in July 2012 and renamed the Royal Pavilion in July 2013. Located on Southend Pier, the Royal Pavilion hosts a variety of events, concerts and exhibitions including music, theatre, art and photography.
  - The creation of Southend-on-Sea Business Improvement District (BID) in November 2012 covering the Town Centre and the Seafront part-funding and unlocking significant investment for the town.

- The Purple Flag accreditation award for Southend-on-Sea's High Street and sea front in recognition of excellence in the town and city centres at night-time management and its contribution to the town's expanding post 6pm economy.

2.3.19 The LEA includes a Strengths, Weaknesses, Opportunities and Threats analysis for the Borough. Strengths include:

- Southend-on-Sea's proximity and good transport links to London means that it is able to supplement local employment with highly paid and highly skilled out commuters.
- Public and private sector investment has continued even during the recession, showing a confidence in the prospects for the town's on-going growth and success.

2.3.20 Weaknesses include:

- GVA remains lower than the regional and national averages and Southend-on-Sea is an exporter of skills to London and other locations.
- The tourism industry still survives on low levels of overnight stay, and the short supply of high quality hotels and restaurants in the central area may not attract those with money to spend.
- Deep rooted and long standing inequalities are present within the borough: Kursaal, Victoria and Milton wards are repeatedly highlighted as the wards in Southend-on-Sea that are the most deprived.
- High levels of car usage, together with poor public transport links around the sub-region, mean congestion on major trunk roads.
- There exists a large concentration of poor quality commercial stock in the centre of town, particularly around Victoria Avenue.

2.3.21 Opportunities include:

- Exciting regeneration plans are in place to further develop Southend-on-Sea's offer and improve its image. These will support the creation and attraction of new businesses, the enhancement of the quality of the tourism offer and number of overnight stays, and continue to make Southend-on-Sea an attractive option for London workers and their families.
- There are sectors showing the clear potential for growth including creative and cultural technology, aerospace and medical technologies.

2.3.22 Threat include:

- Southend-on-Sea is reliant on a number of regeneration projects, many of which may be under further scrutiny following the recent Comprehensive Spending Review.

2.3.23 SBC has produced the **Culture-on-Sea: A Cultural Strategy for Southend on Sea 2012 – 2020** document for the Borough. This is an overarching strategy for the cultural services for the council and beyond. Supporting the delivery of the strategy are a suite of related strategies and delivery / action plans covering the key areas of Sport & Leisure, Museums & Heritage, The Arts, Libraries, Green Space and the Southend Regeneration Framework 2007-21 and the Economic Development and Tourism Strategy.

2.3.24 The vision of the strategy is: *"To be recognised as the cultural and leisure capital of the East of England"*. It is proposed to deliver this through a number of strategic objectives for the area, those most relevant to the SCAAP are:

- increase the levels of engagement and participation in the cultural activities of the local population, including sport & leisure;
- develop new opportunities for all, including the disadvantaged, to participate in cultural activities;
- deliver a new municipal 21<sup>st</sup> Century Library & Library Service (note the new Forum library open in the town centre in 2013);
- deliver a new Public Square with an outdoor Big Screen for screenings and viewings and cultural activity within the Town Centre;
- support the Council in the delivery of its Public Health function, specifically in relation to physical activity and supporting general wellbeing;
- support the Borough in becoming an innovative and resilient economy that attracts high quality businesses, retains knowledge and nurtures a diverse and sustainable economic base;
- raise the profile of Cultural Services in Southend-on-Sea by creating a thriving and creative regional centre where people want to live, work and visit;
- maintain and develop our cultural heritage, museums and award winning parks and open spaces
- pursue opportunities which further our ambition for the provision of a new museum along the Cliffs.

2.3.25 The **Economic Development & Tourism Strategy Refresh** (November 2010) updates the 2007-2013 Economic Development and Tourism Strategy. The revision contains a vision for the borough as:

*“An innovative and resilient economy that attracts high quality businesses, retains knowledge and nurtures a diverse and sustainable economic base. A thriving and creative regional centre where people want to live, work and visit.”*

2.3.26 This refreshed, single economic vision reflects:

- The increasing need to innovate, evolve and diversify in order to succeed and mitigate the risks of macroeconomic events or fiscal policy;
- The retention of skills and knowledge to drive new business creation, building on Southend’s entrepreneurship and educational assets;
- The required balance between encouraging and supporting start-ups, where Southend is traditionally strong, helping enterprise survive and grow, where it is not as strong, and securing its position in terms of large employers; and
- Southend’s strength in terms of location, accessibility and quality of life.

2.3.27 The vision is supported by 13 objectives that will be used to implement the vision. The objectives include successfully delivering the Central Area Masterplan to enhance the retail offer and size of retail catchment.

2.3.28 The **Management of Designated Shopping Frontages in Southend-on-Sea: Technical Report** (October 2013) and **Addendum: Management of Designated Shopping Frontages in Southend-on-Sea** (July 2014).

2.3.29 This technical report addendum reports on a July 2014 survey of ‘Secondary Shopping Frontages’ as designated through the DMDPD 2014. Overall, there is a low vacancy rate in



the Town Centre. The exception is Victoria Avenue/Fairfax Drive with a very vacancy (5 of 7 units) but was strongly dependent on a single tenant. West Street/Victoria Avenue also has relative high vacancy at 6 of 22 units being vacant.

- 2.3.30 Primary shopping frontages were surveyed in March 2013 and also show a generally low vacancy. However, the proportion of vacant units within the town centre Primary Shopping Frontages is remains relatively high at 20% compared with the national average of 14.1%. The majority of this vacancy is in the Victoria Plaza that was refurbished in 2008 as the recession started. The Town Centre Primary Shopping Frontage is the largest in Southend, consisting of over 200 units. It is located on, or just off, a traditional linear High Street, which is anchored to the north by The Victoria Shopping Centre and to the south by The Royals. The town centre is classified in the Southend Core Strategy as a regional centre and will remain the first preference for all forms of retail development and for other town centre uses occurring in the borough.
- 2.3.31 The technical report concludes with recommendations that the SCAAP should continue to seek a minimum target of 70% of the identified Primary Frontage as retail.
- 2.3.32 Southend is part of **the South East Local Enterprise Partnership (SELEP)** consisting of other Essex authorities, Thurrock, Kent, Medway and East Sussex. Southend in within the Thames Gateway South Essex (TGSE) federal area of the LEP, which stretches through Essex to the outskirts of London. As part of the SELEP Southend has secured a range of measures to support the regeneration and growth of the Borough, including the City Deal to support newly formed businesses and incubator space, supporting for the regeneration of Victoria Avenue and public realm enhancements proposed by the SCAAP as part of the Growth Deal.
- 2.3.33 The A127 is identified as a key growth corridor for the LEP area from Basildon to Southend. With the right investment the Plan identifies the corridor there is the opportunity to directly enable the creation of 8,775 jobs and 1,451 new homes by 2021 and a further 48,927 jobs and 32,665 new homes over the longer term.
- 2.3.34 The Growth Plan states that Central Southend:

*Southend Central Area has already seen significant public and private sector investment including £25m of infrastructure and public realm works; the UK's first joint municipal-academic library (The Forum £27m); and the University campus development. A package of transport and public realm works designed to unlock potential development sites and accelerate delivery along Victoria Avenue and in the Central Area are expected to contribute to the delivery of around 2,000 new homes and the creation of up to 6,500 new jobs. Through the recently signed Southend City Deal, Southend central will host new employment space created by bringing an unused building back into use demonstrating local and central Government working in partnership to take a proactive lead on regeneration to lever private sector investment.*

*There is a clear opportunity to promote better connectivity across the area through improved utilisation of public transport infrastructure and services, enabling people to gain access to employment, education and leisure opportunities using public transport. The focus of the joint TGSE Local Sustainability Transport Fund application, supported by revenue measures, will be to continue the roll out of the bus real-time system and vehicle location, together with smart ticketing (linking with the Essex Thameside franchise) and associated marketing and promotion. This supports all the growth points and corridors with access to public transport.*

*This joint initiative between the local authorities, transport operators and businesses/ education providers demonstrates strong cross boundary partnership working. (paragraphs 4.289-4.290).*

- 2.3.35 The **A127 – Corridor for Growth** (March 2014) by Essex County Council and SBC identifies the growth needs for the A127, which links to Southend Central. This road corridor is considered to be a vital artery to economic competitiveness of the Thames Gateway South Essex sub-region and the economy of the County of Essex and beyond. Southend has been awarded £50 million of Local Growth funding over the next six years of which £35.6 million will be designated to the A127 Growth Corridor and capacity enhancements. The funding will encourage future economic prosperity and regenerate important gateways to the town.
- 2.3.36 There is a **Business Improvement District** (BID) established in Southend town centre and the seafront. The BID has helped unlock £27 million of investment for the area and helped enhanced visitor experience of the area, including new street wardens as well as planters to improve the public realm.

#### *Open Space*

- 2.3.37 **South Essex Green Grid Strategy** is a long-term project to deliver a network of open spaces and green links throughout Thames Gateway South Essex, as part of The Thames Gateway regeneration area. This aims to bring significant environmental improvements to this part of Essex, through the provision of combined recreational open spaces, wildlife corridors and improving the appearance of the landscape. The purpose of the Green Grid strategy is to:
- Provide a holistic and long-term vision for the sustainable future development and management of the south Essex area;
  - Define an environmental infrastructure that promotes the establishment and managements of appropriate character settings; and
  - Provide the context for development over the long term.
- 2.3.38 Therefore, the Green Grid strategy will have particular implications for the LDF by ensuring improvements to the ‘green’ character of the borough are taken into account in a strategic way – with long term planning for this change and how development can contribute to this.
- 2.3.39 The Green Grid scheme is part of the wider **Parklands South Essex** scheme, reported in *Thames Gateway Parklands – Delivering Environmental Transformation* (November 2010). This has government funding as part of the Maxigreen project for access and landscape improvements in the South Essex Marshes. The restored marshes will be part of the wider green grid of public open space, greenspaces, footpaths and nature reserves that cover the area.

#### *Air Quality*

- 2.3.40 SBC has produced a 2015 **LAQM Updating and Screening Assessment** as part of their statutory duty to review and assess air quality as stipulated in Part IV of the Environment Act 1995. Annual NO<sub>2</sub> concentrations have been exceeded at one location and the borough has declared no air quality management areas (AQMA). The air quality of the borough can therefore be considered to be good.

- 2.3.41 The report identifies that the main source of air pollution in the borough is road traffic emissions from major roads including the A13, A127 and A1159. Therefore in the Town Centre, controlling traffic levels will be essential to maintaining air quality.

## 3 Baseline information

- 3.1.1 During preparation of the SA of the Core Strategy information was collected on sustainability issues on a Borough-wide basis. At this stage in scoping for the SA of the AAP it is necessary to add to layer of detail to the more generic information collected previously in order to better inform the SA of issues of significance to central area.
- 3.1.2 The SEA Directive is concerned with the assessment of '*the likely significant effects on the environment of implementing the plan*', and this requires where possible some understanding of the 'baseline' situation so that the change that might arise from the influence of the plan can be considered.
- 3.1.3 The SA Report of the Core Strategy submission draft contains as Appendix 3 baseline information for the Borough. The following section includes updated information that is relevant to Southend Central.
- 3.1.4 For the purposes of collecting further evidence for the SCAAP, the council have defined the boundary of the town centre as the in the masterplan, to include administrative wards of Milton and Victoria. The SA uses data from these two wards as the basis for data collection on the social and economic characteristics of the area.

### 3.2 Role of the Town Centre

- 3.2.1 Southend-on-Sea Town Centre is a major retail, employment and commercial centre serving a catchment population of over 325,000 people. It lies at the heart of the Borough of Southend-on-Sea. The Milton and Victoria wards have a population of approximately 22,000<sup>1</sup> people. The town centre is the Borough's most important commercial area and largest shopping centre. The Inter-Departmental Business Register (IDBR), produced by the Office for National Statistics, provides robust and comprehensive dataset of employment at a district level. The report suggests that in 2013 the town centre (Milton and Victoria) provided 33% of the jobs in the Borough. Retail is an important role of the central area, with the shops focused on the High Street, forming a central spine through the centre from north to south. The High Street is pedestrianised linking the refurbished Victoria Plaza (1960s) and Royals (1980s) retail centres. On the periphery of the northern part of the High Street is the Town Centres only large food retailer and a major retail outlet offering non-food goods.
- 3.2.2 The South Essex College and new University of Essex complex is adjacent to the High Street, in 2013 the integrated municipal and academic public library opened in the town centre. Development of a multi-screen cinema, restaurants, cafés and bars mainly along High Street side streets has given the central area a complimentary leisure offer.
- 3.2.3 Victoria Avenue is the main area for office accommodation. The Council views that Victoria Avenue has a number of 1960's office developments, some of which are outmoded for modern requirements and are long-term vacant.

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<sup>1</sup> SBC, 2011 Census Profile (Milton and Victoria Wards)

- 3.2.4 The central area of the town also is the focus for much of the seaside leisure activity. With the entrance to the Pier at Pier Hill at the southern end of the High Street as well as the Adventure Island fun park. The seafront area also includes the Eastern and Western Esplanade and formal parks of the Southend cliffs.

### **3.3 Housing**

- 3.3.1 The central area contains extensive areas of high density housing providing homes for some 22,000<sup>2</sup> people (13% of the Borough total) in 11,000 households<sup>3</sup>.
- 3.3.2 Census data from 2011 shows that flats/maisonettes are the predominant form of dwellings in contrast to Southend and the rest of England & Wales where a house/bungalow are more prevalent. Additionally, one-bedroom dwellings are the most common form of dwelling in the Town Centre as opposed to three-bedroom dwellings in Southend and England & Wales.

### **3.4 Travel and transport**

- 3.4.1 The central area has very good travel and transport connections. In the Southend Central area there are three railway stations, Southend Victoria at the north end of the High Street and Central Station in the main shopping area and further north Prittlewell station. The newly refurbished bus station, the 'Travel Centre', is also in the town centre, on Chichester Road adjacent to the High Street. The main access by car is the A127 dual carriageway via Victoria Avenue and the A13 London Road, which has smaller and independent retail along it.
- 3.4.2 The Town Centre has parking facilities for around 4,000 public off-street spaces comprising 2,100 Council owned and 1,900 privately owned parking spaces with several sites being large surface car parks. Car parking in the central area is under a process of reorganisation and renewal. For example, Farringdon multi storey car park was demolished and replaced at University Square. The demolition of Queensway House has resulted in the creation of a new temporary car park at this site as well as a temporary privately run car park on Victoria Avenue (Portcullis House). Since preparation on the SCAAP started this has led to an increased parking provision within the town centre.
- 3.4.3 Cycling and walking routes are adequate, although there is potential for greater connectivity. The relatively flat character of the Southend topography means there is very good potential for more trips to be made by this mode. The seafront provides a particularly valuable connection of coastal neighbourhoods to the central Southend. A Sustrans cycle route passes along the Seafront and there is a network of local routes in the town centre, although not very comprehensive.
- 3.4.4 As noted in in the plans and strategies section there are also various schemes proposed through the Local Transport Plan 3 to bring enhancements to the public transport provision of the area.
- 3.4.5 Data shows that the borough has quite high levels of people walking to work or using public transport compared to national averages<sup>4</sup>. Census data 2011 shows that the 16% of the residents of the central area travel to work on foot compared to 7% in England and Wales and 9% in the Borough, 22% of the residents of the central area travel to work by car which

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<sup>2</sup> Census 2011

<sup>3</sup> SBC, 2011 Census Profile (Milton and Victoria Wards)

<sup>4</sup> SBC 2015, *Southend Local Transport Plan 3 Strategy Document 2011-2026*

represents the most prevalent form of transport for the central area. However, this is still below the average for England and Wales and the Borough that are 37% and 33% respectively.

- 3.4.6 Approximately 53% of households in the Central Area own a car, this is significantly lower than for the rest of the Borough (72.7%). Low car ownership in the centre may reflect good transport connections but is also likely to be characteristic of income deprivation in parts of the centre.

### 3.5 Population

- 3.5.1 The 2011 Census of resident population provides the best population record at ward level. The census records a small increase in population since 2011 increasing by approximately 3,000 people. The Town Centre now makes up 12.7 % of the total Borough's resident population, see table 4.1.

Table 4.1 Resident Population

Area	Census 2011
Southend-on-Sea	173,658
Town Centre	22,067
<i>Town Centre %</i>	<i>12.7</i>

Source: Census 2011

### 3.6 Employment and Economy

- 3.6.1 In 2011, Census data shows that 75.6% of residents in the central area were economically active, similar to the figure of 78% for Southend and 76.8% for England and Wales<sup>5</sup>.
- 3.6.2 In 2013, the Town Centre provides 33% of all the jobs in the Borough (see table 4.2). This proportion has been decreasing over time as shown in table 4.2. However, this is in the context of an overall growth in employment in the Borough since 2009 of around 1,500 jobs and a loss of around 2,000 jobs in the central area over the same period. However, there is an indication according to SBC that jobs in the Central Area have increased in 2014.

Table 4.2: Employment in Southend – central area vs rest of the Borough

Employment in Southend	2009	2010	2011	2012	2013
Central Area (Milton+ Victoria)	23,687	22,015	20,912	19,661	19,965
Central area as a %	38%	36%	35%	32%	33%
Rest of Borough	39,281	38,684	39,933	40,857	42,279
Rest of Borough as a %	62%	64%	66%	68%	70%
<b>Total</b>	<b>62,968</b>	<b>60,699</b>	<b>60,845</b>	<b>60,518</b>	<b>62,244</b>

Source: The Inter-Departmental Business Register (IDBR) produced by the Office for National Statistics

<sup>5</sup> NOMIS 2011 Ward Labour Market Profile E36000778 : Victoria & 2011 Ward Labour Market Profile E36000771 : Milton

- 3.6.3 The town centre contains a mix of employment types, and some sectors are proportionately more significant than in the Borough as a whole. These sectors include financial sector, real estate and business and 'other', retail is included in the 'other' category. In the town centre there is a notable division in the type of jobs held by residents with the greatest proportion in the highest group of 'professional' occupations and the second most prevalent is 'elementary occupations'<sup>6</sup>. In contrast, there are a number of sectors which are less important in the town centre than the Borough as a whole such as health and social work, which is dependent on the location of hospitals, and manufacturing as only one industrial site is found in the area.
- 3.6.4 The average unemployment rates for the Town Centre are approximately 12% and higher than those for Southend (8.1%) and England and Wales (7.6%)<sup>6</sup>.
- 3.6.5 A recent development in the central area is the 'The Hive' is a business enterprise centre, which opened in spring 2015. The enterprise centre is designed to support new and innovative businesses in the Southend.
- 3.6.6 The Local Economic Assessment for the Southend identifies that town centre is the focus of retail development as well as the location of the majority of 'creative' employment in the borough.
- 3.6.7 Tourism brings in around 6 million visitors a year (2011)<sup>6</sup> the majority of these visitors are day visitors (95.8%) who spend around £330 million per year.
- 3.6.8 To the north of the Central Area is the area that is the focus of the Southend Airport and Environs Joint Area Action Plan 2014. The focus of the AAP for this area was using London Southend Airport as a catalyst for new employment growth in Southend (and extending into neighbouring Rochford). Therefore, some uses that may have previously favoured by a Central Southend location may be attracted to new sites in this area.

### **3.7 Retail**

- 3.7.1 The Town Centre Primary Shopping Frontage is the largest in Southend, consisting of over 200 units. It is located on, or just off, a traditional linear High Street, which is anchored to the north by The Victoria Shopping Centre and to the south by The Royals. The Town Centre is classified in the Southend Core Strategy as a regional centre and will remain the first preference for all forms of retail development and for other town centre uses occurring in the borough.
- 3.7.2 There is strong retail economy the town centre. According to the Focus database in 2009 Southend town centre ranked 192 out of 711 UK retail centres. Despite the recession constraining consumer spending and trading conditions needing some time to recover, regeneration activity is improving the investment potential of the town centre. In the long term, Southend's retail sector is robust, and provides an important sub-regional focus, though its growth potential is constrained by the proximity of Lakeside and Bluewater shopping centres.
- 3.7.3 A town centre study in 2015 showed that the proportion vacant town centre primary shopping frontages in 2015 is relatively high at 18% compared to the national average of 10.4%. The majority of vacancies are within the Victoria Plaza as its refurbishment coincided with the start of the recession. For secondary shopping frontages the greatest vacancy is on Victoria

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<sup>6</sup> Tourism South East Volume and Value Report, October 2011

Avenue/Fairfax Drive where only 5 of 7 units are occupied (4 of these units were tenanted by a single occupant who left). The other area of higher vacancy is West Street/Victoria Avenue where 6 of 22 units being vacant<sup>7</sup> - a 27% vacancy.

### 3.8 Social characteristics

- 3.8.1 Education rates show that although the rates of adults with no qualifications are higher in central Southend than for the Borough as a whole, there are also more residents with higher level qualifications. This may be as a result of younger professional people with qualifications living close to or in the Town Centre juxtaposed with pockets of deprivation, although without further investigation this cannot be confirmed.
- 3.8.2 Public Health England<sup>8</sup> has reported, based on census data and the Indices of Deprivation 2010, which combined the wards of Milton and Victoria have high levels of deprivation. The data shows that the percentage of income deprivation, child poverty and older people in deprivation is significantly worse in this area than the England average. Nearby wards also experience higher levels of deprivation, including Kursaal ward. The Public Health England data also shows child development at the age of 5 and GCSE achievement of 5A\*-C (including English and Maths) is significantly worse in Southend Town Centre than in England.
- 3.8.3 The Indices of Deprivation 2015 show that parts of Milton, Victoria and Kursaal wards are in the 10% most deprived (all domains). The highest area of deprivation is in Victoria ward around Queensway and Coleman Road, which is ranked the 514 most deprived area in the England out of 32,855 nationally. These areas were ranked more deprived in the 2010 Index. However, this does not necessarily mean an improvement in the area as it may indicate an overall increase in deprivation nationally. Figure 3.1 shows the most up-to-date deprivation information for the index of multiple deprivation.

NEETs refer to 16-18 year olds who are Not in Education, Employment or Training. The town's NEET figure (5.1%) is lower than the English average (5.7%). Southend's NEET figure has improved from 5.6% in 2009 which can be attributed to the substantial number of learning and training providers. However one part of the Town Centre, Victoria, had the highest recorded number of NEETs in the Borough.

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<sup>7</sup> Management of Designated Shopping Frontages in Southend-on-Sea: Technical Report 2013

<sup>8</sup> [www.localhealth.org](http://www.localhealth.org) accessed 11<sup>th</sup> September 2015

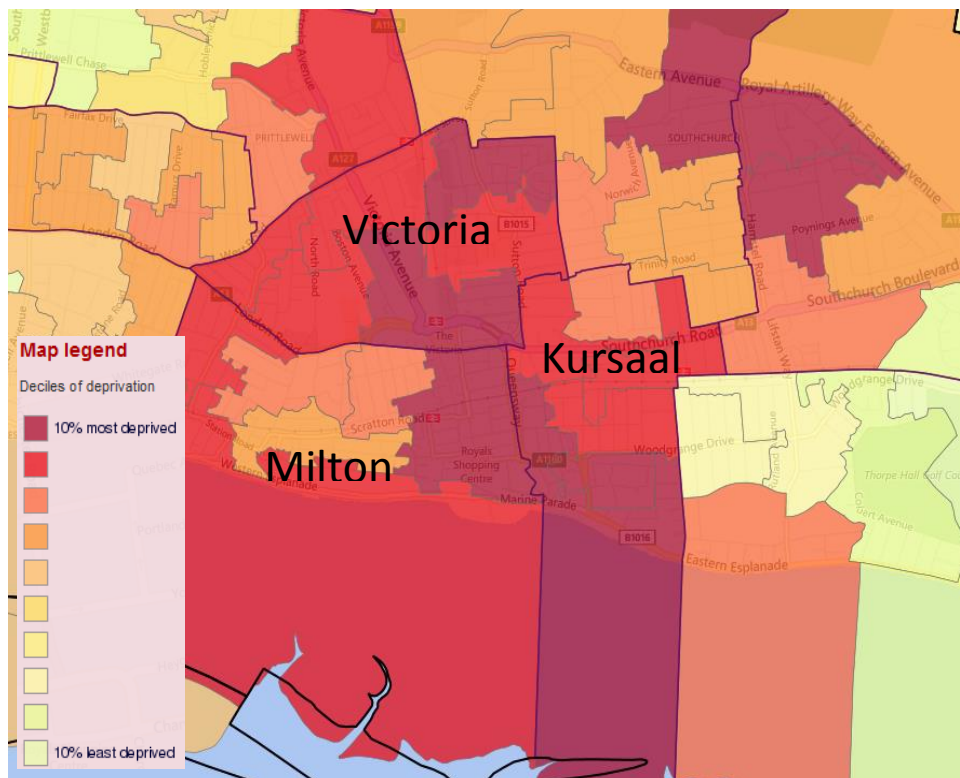


Figure 3.1: Index of Multiple Deprivation 2015

### 3.9 Built environment quality

- 3.9.1 The town centre contains area designated for their historical and architectural quality as well as areas that have poorer architectural quality or have become dilapidated, for example developments along Victoria Avenue. There is also recent regeneration, including the South East Essex College and University of Essex buildings, Pier Hill and the Travel Centre which have brought improvements to parts of the central area. However, there is scope for further environmental improvements and making land available for alternative uses.
- 3.9.2 Phase 1 of the Victoria Gateway and City Beach public realm scheme is complete. The Victoria Gateway Scheme provides a considerable uplift to the public realm at this important entrance to the central area and also supports a more integrated approach to public transport and surface access across a major thoroughfare into the central area. In addition along the seafront, the City Beach scheme has realigned the carriageway to create wider pavements and space for cafes, activities and created attractive features such as fountains and lighting within a new public space.
- 3.9.3 The Council are committed to improving the Queensway area including the social housing and surrounding streets, this is part of the Better Queensway strategy.
- 3.9.4 Southend central area also contains many listed buildings and five conservation areas consisting of Prittlewell in the north, Clifftown in the south west, Kursaal and Eastern Esplanade in the south east and Warrior Square located in the middle of the centre<sup>9</sup>. The conservation areas are all predominantly residential neighbourhoods, and Clifftown directly borders the retail core of the town as well as the seafront. There are many Listed buildings within the central area, particularly within the conservation areas, although are also found

<sup>9</sup> Southend Central Area Action Plan Evidence Base - Part 3 Conservation Areas



beyond the boundaries of these areas. Many of the listed buildings reflect Southend's heritage as a seaside holiday destination. A full list of all national and local listed buildings are part of the evidence base for the Central Area, these are the SCAAP Evidence base Part1, Listed buildings, Part 2 Locally listed buildings and Part 3 Conservation Areas.

- 3.9.5 The town centre also contains locally identified 'Frontages of Townscape Merit', these are street frontages that add to the character of the central area, including along Alexandra Street, Clarence Street, High Street, Weston Road and Clifftown Road.

### **3.10 Open space**

- 3.10.1 There are only very limited areas of public open space, particularly green space, in the central area, with the exception of the seafront. Areas of importance include the Southend Cliffs formal gardens.
- 3.10.2 However, within the main commercial and retail areas of the town centre green space provision is poor, and includes the recently improved cemetery / open space at St. John's church behind the Royals shopping centre and Warrior Square and Prittlewell Square Gardens – a high quality formal garden set within a conservation area in the south west of the central area. An enhancement scheme for Warrior Square Gardens has recently been completed including the provision of an architecturally designed café and remodelled public green space.
- 3.10.3 Churchill Gardens in the north of the central area provides additional open space, although is part of a more residential neighbourhood. Green spaces are needed throughout the urban area as demand will increase with a warming climate and these areas can help cool built urban areas, preventing 'heat island' impacts. Therefore, provision of green open spaces may be a matter to be addressed by the SCAAP.
- 3.10.4 In June 2015, SBC announced a draft Parks and Green Spaces Strategy which is a five year plan to safeguard and enhance Southend's open spaces.
- 3.10.5 Redevelopment of the centre and proposals of the SCAAP should take into account ways in which open spaces in this location can contribute to the Thames Gateway, South Essex Green Grid strategy and the Parks and Green Spaces Strategy.

### **3.11 Flood**

- 3.11.1 The seafront, south of the Town Centre, is located in Flood Zone 3 and a small area to the north of the Town Centre is located in Flood Zone 2. These areas will be vulnerable to flooding. There is also the risk of surface water flooding throughout the central area. Areas of flood are associated with Prittle Brook which will need to be managed through design and drainage of new development. There is also an increased fluvial flood risk at the Kursaal area east of Southchurch Avenue which is at greater risk of flood according to Environment Agency maps.

### **3.12 Air quality**

- 3.12.1 Annual NO<sub>2</sub> concentrations have been exceeded at one location in SBC and the borough has declared no air quality management areas (AQMA). The air quality of the borough can therefore be considered to be good.

3.12.2 The main source of air pollution in the borough is road traffic emissions from major roads in the Town Centre including the A13, A127 and A1159. Therefore in the Town Centre, controlling traffic levels will be essential to maintaining air quality.

### **3.13 Nature conservation**

3.13.1 There are no sites of identified nature conservation importance in the central area. However, the potential for nature conservation enhancement should be a consideration of all development sites in the area.

3.13.2 The Town Centre is also near the internationally designated Natura 2000 sites, as referred to in Section 3. Therefore, development in these areas will have to ensure it will not have an adverse impact on these nature conservation sites. Potential impact pathways include sewerage, rainwater run-off, or pollution impacts of large scale new development, as well as any direct impact on the birds for which these areas are designated.

3.13.3 The Benfleet and Southend Marshes located to the south of the central area outside of the built environment boundary, are internationally designated as a Ramsar Site, Site of Special Scientific Interest and Special Protection Area. These must be protected not only from direct disturbance from development but also change that would threaten their integrity, such as increased pollution or changes in water availability. However the key threat is largely beyond the control of the LDF is caused by built development causing 'coastal squeeze' which will be exacerbated by climate change and sea level rise.

### **3.14 Climate Change and Sea Level Rise**

3.14.1 Most recent predictions of the climate change for the East of England come from the UK Climate Projections (UKCP09). The predictions are all shown for the 2050s under a medium emissions scenarios, under low or higher scenarios emissions will be correspondingly lower and higher:

- increased summer mean temperatures, with higher peak temperatures as well as prolonged periods of high temperature
- in summer there is likely to be at least a 17% reduction in rainfall (could be as much as a 38% reduction), but an increase of 14% winter precipitation levels (or as much as 31% increase)

3.14.2 Predictions of sea level rise in the London area are included in the UK Climate Projections *Marine and Coastal Projections Report* (June, 2009). These show that by 2050 sea level rise could be up to 25.8cm (high emissions scenarios) but even under low scenarios could be 18.4cm.

3.14.3 Sea level rise could lead to issues such as:

- water resource deficiencies, which may lead to serious issues in the area particularly with the levels of development set for the Thames Gateway
- increased flood risk, including for sea defence overtopping, and also from rivers
- a risk of subsidence through changing soil moisture levels.

## Appendix B Opportunity Site Iteration

Proposed Submission – June 2016	Preferred Approach 2015: Policy Area, Opportunity Sites and Major Full Permissions	2011 Proposed Submission: Quarters and Gateway Neighbourhoods	Sustainability appraisal of ‘Quarters’ and ‘Gateway Neighbourhoods’ Polices: SA Report of the Proposed Submission 2011	Southend-on-Sea Justification for changes from the 2015 Preferred Approach to Proposed Submission 2016*
<p><b>Policy Area:</b> Central Seafront</p> <p><b>Opportunity Sites:</b> CS1.1 Southend Pier CS1.2 Seaways CS1.3 Marine Plaza CS1.4 New Southend Museum</p>	<p><b>Policy Area:</b> Central Seafront</p> <p><b>Opportunity sites:</b> OS7 Southend Pier OS8 Seaway Carpark and Marine Parade OS9 New Southend Museum OS10 Woodgrange Drive (Kursaal) Estate (CS1.1 Esplanade House)</p>	<p>Name: Central Seafront (8)</p> <p>Same area</p> <p><b>Proposal Sites:</b> CS6a Southend Pier CS6b Seaway Car Park and Marine Parade CS7a Cultural Centre and New Southend Museum CS8a Woodgrange Drive (Kursaal) Estate</p>	<ul style="list-style-type: none"> <li>▪ The policies address many issues of importance to delivering sustainable development. This includes improving the quality of the built environment, protecting heritage and biodiversity, improving non-car access and enhancing the potential for the tourism economy.</li> <li>▪ Some criteria set positive steps for the future of the area that need to be implemented by the Council or other public / community groups. This includes an Urban Greening Strategy, Art Trail, and Creative Lighting Scheme.</li> <li>▪ Requirements for ‘visual impact assessment’ will help to make sure development takes into account its context and views to and from the site.</li> <li>▪ There are many aspects of the policy that could help secure public realm improvements and in doing so have benefits for the tourism economy of the Borough, as well as benefits for local communities.</li> <li>▪ There is a need to ensure new development does not harm the biodiversity assets of the foreshore.</li> </ul>	<p>Opportunity Sites have been renumbered:</p> <p>CS1.1 – formerly OS7 CS1.2 – formerly OS8 (part) CS1.3 – formerly OS8 (part) CS1.4 – formerly OS9</p> <p>The former OS8 has been divided into two distinct sites, with amended boundaries and the removal of the linking section along Marine Parade. This allows for the residential and leisure elements be more clearly defined as the location of Seaways more suited to a leisure and tourism development and improvements to the public realm.</p> <p>OS10 has been removed as there is no evidence to present to the Planning Inspector that it can be delivered prior to the end of the plan period (2021) and therefore cannot yet be allocated.</p> <p>CS1.1 Esplanade house has been removed as it is committed development with planning permission, which has not been promoted for inclusion within the SCAAP.</p> <p>The overall area of CS1 remains</p>

				largely the same as in the previous version with some update to take into account the car park for Cliffs Pavilion.
<p><b>Policy Area:</b> Clifftown</p>	<p><b>Policy Area:</b> Clifftown</p> <p><b>Opportunity Sites:</b> OS16 Clarence (post-2021) OS17 Alexander (post-2021)</p>	<p>Name: Clifftown (6) Same area</p> <p><b>Proposal Sites:</b> PS6a Clarence Road Car Park  PS6b: Alexandra Street Car Park</p>	<ul style="list-style-type: none"> <li>▪ The policy supports the future role of Clifftown as an area of strong cultural identity. The policy includes criteria to make sure that the historic and heritage character of the area is taken into account in new development. There is also the need to restore areas that have become degraded.</li> <li>▪ Access improvements are promoted. This includes changes to car access to a single new car park on one of the development sites, so that less traffic is created on streets of the area. Other improvements are to the retail circuits in the area, linking to the Central Station, Cliff Gardens and Pier Hill. This will help support the economy through tourism and visitor spend. Shops in this area are more likely to be independent and therefore these retail circuits will aid the local economy especially.</li> <li>▪ Regenerating the Empire Theatre will also add to the cultural character of the area. Other cultural uses should be promoted such as galleries and performance spaces.</li> <li>▪ Central House is identified for redevelopment, for more information it could be included as a separate allocation, especially as this is identified as the site of a possible tall building.</li> </ul>	<p>OS16 and OS17 have been removed as there is no evidence to present to the Planning Inspector that they could be delivered prior to the end of the plan period (2021). However, there may be potential for redevelopment in future subject to car parking needs.</p> <p>The overall area of PA6 remains the same as in the previous version.</p>

<p><b>Policy Area:</b> PA3 Elmer Square</p> <p><b>Opportunity Sites:</b> PA3.1 Elmer Square Phase 2</p>	<p><b>Policy Area:</b> Elmer Square</p> <p><b>Opportunity Sites:</b> OS3 Elmer Square Phase 2</p>	<p>Name: Elmer Square (3) Same area.</p> <p>Proposal Site: PS3a Elmer Square</p>	<ul style="list-style-type: none"> <li>▪ Policy should aid the image of a town as a place to do business due to the links with a modern university.</li> <li>▪ Improvements through the policy may benefit the community and businesses through shared use facilities, including the new Central Library and exhibition space.</li> <li>▪ A new public square will also benefit those who live, work and visit the central area. There will be good access from the High Street giving shoppers a place to relax.</li> </ul>	<p>Opportunity Sites have been renumbered:</p> <p>PA3.1 – formerly OS3. The site boundary has slightly reduced.</p>
<p><b>Policy Area:</b> PA1 High Street</p>	<p><b>Policy Area:</b> High Street</p> <p><b>Opportunity sites:</b> OS1 Whitegate Road (post 2021) OS2 Pitman’s Close (post 2021) (PA1.1 – Maitland House) (PA1.2 – 4 Southchurch Road)</p>	<p>Name: High Street (1) Excluded both the Victorias and Royals Shopping Centres</p> <p><b>Proposal sites:</b> None (see Warrior Sq and Tylers)</p>	<ul style="list-style-type: none"> <li>▪ Benefits to the public realm and the attractiveness of the area to all users;</li> <li>▪ Positive emphasis on increased pedestrianisation and cycling improvements to support non-car travel;</li> <li>▪ Policy requirement for new High Street planting can help bring visual diversity to the area, support biodiversity and can help reduce urban heating;</li> <li>▪ Better use of the currently under-utilised Victoria Circus and other public spaces can have many benefits, including for community events and for the tourism economy.</li> </ul>	<p>All of the specific sites included in the Preferred Approach version have been deleted.</p> <p>OS1 and OS2 have been removed as there is no evidence to present to the Planning Inspector that they could be delivered prior to the end of the plan period (2021). However, there may be potential for redevelopment in future.</p> <p>PA1.1 and PA1.2 have both been removed as even though they had planning permission the sites were not promoted through the SCAAP preparation process and therefore there is no clear evidence to suggest they could be delivered by 2021. They may be suitable for development in future.</p> <p>The overall area of PA1 remains the same as in the previous version.</p>

				However, it is useful to note that between earlier versions of the SCAAP the borders of the High Street are changed to ensure that the two shopping centres, which are retail anchors for the High Street, are included so these form part of the High Street policy area.
<p><b>Policy Area:</b> PA2 London Road</p>	<p><b>Policy Area:</b> London Road</p> <p><b>Opportunity Sites:</b> OS15 Sainsbury's &amp; Adjacent Buildings (post-2021)</p>	<p>Name: Queensway and London Road /Broadway (2)</p> <p>Excluded the A13 from the A127/A13 junction to Short Street, as at the Preferred Approach this includes the revised A127/A13 junction.</p> <p><b>Proposal Site:</b> PS2a Sainsbury's and adjacent buildings</p>	<ul style="list-style-type: none"> <li>▪ Enhancing Victoria Circus will have benefits for the character of this currently underutilised space so that it can actively contribute to the attractiveness of the area and the local economy.</li> <li>▪ Other beneficial policy aspects include promotion of urban greening, the potential for a street market, potential for higher/further education use, permitting a mix of uses including residential.</li> <li>▪ The policy should help improve the character of the area, while retaining some of the existing uses such as car parking. Much of the improvements relate to better links across the site and to other parts of the central area for cyclists and pedestrians.</li> <li>▪ This will help make the site more accessible including links across Queensway to Victoria Rail Station.</li> </ul>	<p>OS15 has been removed as there is no evidence to present to the Planning Inspector that it could be delivered prior to the end of the plan period (2021). However, there may be potential for redevelopment in the future.</p> <p>The overall area of PA2 remains the same as in the previous version.</p>
<p><b>Policy Area:</b> PA4 Queensway</p> <p><b>Opportunity</b></p>	<p><b>Policy Area:</b> Queensway</p>	<p>Name: Queensway and Southchurch Road (4)</p> <p>Included Victoria Shopping Centre and</p>	<ul style="list-style-type: none"> <li>▪ Redevelopment of the area for housing, office and secondary retail space would help meet objectives for the central area of providing development to meet the needs of existing and future residents, as well as</li> </ul>	<p>The former OS4 has been renamed to PA4.1 'Better Queensway' Project.</p> <p>The Policy Area has been increased to an area larger than the Opportunity</p>

<p><b>Sites:</b></p> <p>PA4.1 'Better Queensway' Project</p>	<p><b>Opportunity Sites:</b></p> <p>OS4 Queensway</p>	<p>excluded flats and houses south of Coleman Street and Short Street Car Park.</p> <p><b>Proposal Site:</b></p> <p>PS4a Queensway House and adjacent buildings</p>	<p>encouraging economic growth in the area.</p> <ul style="list-style-type: none"> <li>▪ Changes in access are promoted through the policy may help overcome the barrier created by Queensway.</li> <li>▪ The 'urban forest' scheme could have many benefits for sustainable development from bringing nature into a heavily built-up area. Benefits will not only be for wildlife but also the wellbeing of residents.</li> <li>▪ The policy recognises the surface water flood potential in this area.</li> </ul>	<p>Site, this creates a more comprehensive scheme that includes the car park and retail unit to the east of Southend Victoria station, the health centre on the corner of Short Street and Queensway and also the buildings between Southchurch Road and Essex Street that are adjacent to the cleared area that is part of the PA4.1</p>
<p><b>Policy Area:</b></p> <p>PA9 Sutton Gateway Neighbourhood</p> <p><b>Opportunity Sites:</b></p> <p>PA9.1 Sutton Road</p> <p>PA9.2: Guildford Road</p>	<p><b>Policy Area:</b></p> <p>Sutton Gateway Neighbourhood</p> <p><b>Opportunity Sites:</b></p> <p>OS14 Sutton Road</p> <p>(PA9.1 – Kenway</p> <p>PA9.2 257-285 Sutton Road</p> <p>PA9.3 319 to 321 Sutton Road)</p>	<p>Name: 'Sutton' Gateway Neighbourhood (10)</p> <p>Included flats and houses south of Coleman Street</p> <p><b>Proposal site:</b></p> <p>PS10a Former B&amp;Q Site</p> <p>PS10b Sutton Road</p> <p>PS10c Coleman Street</p>	<ul style="list-style-type: none"> <li>▪ The policy sets out succinctly the principles that will guide the redevelopment of the area. The majority of changes related to the three allocated sites as well that need to improve access through the area, including changes to Sutton Road and safe walking and cycling routes along Short Street to Queensway.</li> <li>▪ The site will also need to accommodate some additional open space and this is likely to be in the north of the area at the Sutton Road proposal site.</li> <li>▪ As with many of the quarters this area could accommodate new higher and further education facilities if required.</li> <li>▪ Planning briefs are to be prepared for the two employment areas and this should help their renewal over time in a coordinated way.</li> </ul>	<p>OS14 is renamed as PA9.1 and makes provision for fewer dwellings. However, it still covers the same area but removes sites former PA9.2 and PA9.3 both of which are committed development and have planning permission. The overall housing yield has been reduced to take account of dwelling completions in the area.</p> <p>A new opportunity site, new PA9.2: Guildford Road, has been included as it is newly identified as a site that can be shown to be deliverable by 2021. The current surface level car park and existing building is to be redeveloped to provide 50 dwellings and a convenience store.</p> <p>PA4 area has been amended to exclude the car park and retail store now part of the PA4 Queensway.</p>



<p><b>Policy Area:</b> PA7.1 Tylers</p> <p><b>Opportunity Site:</b> PA7.1: Tylers Avenue</p>	<p><b>Policy Area:</b> Tylers</p> <p><b>Opportunity sites:</b> OS6: Tylers</p>	<p>Name: Tylers Avenue (7)</p> <p>Included The Royals Shopping Centre</p> <p><b>Proposals Sites:</b> PS7a Tylers Avenue PS7b Pitman's Close</p>	<ul style="list-style-type: none"> <li>▪ Of particular importance will be bringing built environment and road safety enhancements to Chichester Road that currently provides an unattractive link route to the east of the High Street. Also, improved crossing points over Queensway will help improve the connectivity of the area to the wider residential areas to the east.</li> <li>▪ Open space and natural environment changes are also proposed, enhancing the quality of space at St. John's Church and as part of the greening of Queensway.</li> <li>▪ The policy sets out a number of road improvement schemes that may be dependent on the car park being redeveloped, such as creation of 'home-zones' or mixed-mode routes. The delivery of these schemes, or similar, should be considered even if the car park is to stay. This will help make the area more attractive for visitors and residents.</li> <li>▪ Specific schemes have the potential to deliver economic benefits of to the area, including expansion of retail units at the south of the High Street. Design will need to be of a high quality to complement the existing area and the importance of the location for attracting visitors and tourists.</li> <li>▪ The intention is to see the area made much more permeable to the public. This change will help enhance connectivity in the area, providing clearer links to the seafront and development proposals as part of the</li> </ul>	<p>The former OS6 has been renamed PA7.1 and the site boundary has been extended to include the Travel Centre to the south.</p> <p>The overall area of PA7 remains the same as in the previous version.</p>
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			'Central Seafront' quarter.	
<p><b>Policy Area:</b></p> <p>PA8 Victoria Gateway Neighbourhood</p> <p><b>Opportunity Sites:</b></p> <p>PA8.1: Victoria Avenue</p> <p>PA8.2: Baxter Avenue</p>	<p><b>Policy Area:</b></p> <p>Victoria Gateway Neighbourhood</p> <p><b>Opportunity sites:</b></p> <p>OS11 Victoria Avenue</p> <p>OS12 Former Essex and Suffolk Water Board Site</p> <p>OS13 Roots Hall Football ground and environs</p> <p>(PA8.1 Heath House and Carby House)</p> <p>(PA8.7 Carnarvon Road)</p> <p>(PA8.8 Victoria House)</p> <p>(PA8.2 Roots Hall Stadium)</p> <p>(PA8.3 297 Victoria Avenue)</p> <p>(PA8.3 25 Roots Hall Avenue)</p> <p>(PA8.5 Salisbury Avenue and North Road)</p> <p>(PA8.6 175 London</p>	<p>Name: 'Victorias' Gateway Neighbourhood (9)</p> <p>Same area</p> <p><b>Proposal Sites:</b></p> <p>PS9a The Victoria Office Area Site</p> <p>PS9b Former Essex and Suffolk Water Board Site</p> <p>PS9c Roots Hall Football ground and environs</p>	<ul style="list-style-type: none"> <li>▪ The policy contains many design principles that should help make sure redevelopment makes this area more sustainable. There are specific proposals to bring enhancements to the built and historic environment, such as redevelopment of the water board and stadium sites.</li> <li>▪ The policy also contains provisions to improve the transport and travel access through the area, including making use of Victoria Avenue as a transport corridor.</li> <li>▪ It will be important that new development does not harm the nature conservation value of any existing sites, and new landscaping and open spaces help encourage increased local biodiversity. The role of new open space in this area to help mitigate visitor pressure on the foreshore should be considered, as part of the Southend 'green grid'.</li> <li>▪ The policy includes the need to deliver more community facilities in the area. A new health centre, primary school and open space could be located in this part of the development area.</li> <li>▪ Development may also include a new combined heat and power plant to provide more efficient energy to new mixed use development.</li> </ul>	<p>The former OS11 has been renamed to PA8.1. PA8.1 still covers the same area as OS11 but the anticipated housing yield has been rounded down.</p> <p>PA8.5 and PA8.6 have also been deleted as they are also committed development with planning permission and have not been promoted during the SCAAP preparation process.</p> <p>Site OS13 has been removed along with the former PS8.2 and PS8.3 that were part of OS13. Site OS12 has also been removed. All of these sites have been removed as there is no evidence to present to the Planning Inspector that they could be delivered prior to the end of the plan period (2021). However, there may be potential for redevelopment in future.</p> <p>The policy includes a new Policy Area, PA8.2: Baxter Square, which is to provide 500 high quality mixed tenure dwellings to west of PA8.1, there is reasonable evidence to suggest it could be delivered by 2021 and the end of the plan period.</p> <p>The overall area of PA8 remains the same as in the previous version.</p>

	Road)		<ul style="list-style-type: none"> <li>As well providing employment as part of the neighbourhood redevelopment in this location could have benefits to the provision of new modern offices in other parts of the centre. This may be achieved by removing a poor quality office stock that hinders the office development market.</li> </ul>	
<p><b>Policy Area:</b> PA5 Warrior Square</p>	<p><b>Policy Area:</b> Warrior Square</p> <p><b>Opportunity sites:</b> OS5 Warrior Square  (PA5.1 18-20 Southchurch Road)</p>	<p>Name: Warrior Square (5) Same area</p> <p><b>Proposal sites:</b> PS5a Warrior Square Car Park  PS5b Whitegate Road</p>	<ul style="list-style-type: none"> <li>The policy does set out a wide range of uses that the site could be redeveloped for, including higher and further education. The SA notes the importance of maintaining housing in the Town Centre and offices in the town centre as well as education and therefore the need to set policy for the most appropriate use of the space.</li> <li>The principles of the policy are compatible with more sustainable transport access.</li> <li>The policy should give greater emphasis to other aspects of 'greening' the environment, particularly given the location of the site adjacent to the Warrior Square Park. The site could be used to link the 'urban forest' with the park and the railway line embankments, creating routes for wildlife movement.</li> </ul>	<p>OS5 has been removed as there is no evidence to present to the Planning Inspector that it could be delivered prior to the end of the plan period (2021). However, there may be potential for redevelopment in future.</p> <p>PA5.1 has been removed as it is committed development with planning permission, which has not been promoted as part of the SCAAP preparation process.</p> <p>The area of PA5 has also been amended to remove the buildings on Southchurch Road, now part of PA4 Queensway.</p>

\* Refer to Appendix B of the SA Report of the Preferred Approach SCAAP for a record of changes between the Preferred Approach SCAAP 2015 and earlier versions of the SCAAP.

# Appendix C Sustainability Objectives

Appendix C  
Sustainability appraisal of the Area Action Plan Objectives

	Access	Housing	Education	Health	Community	Biodiversity	Landscape	Built envt.	Air	Water	Land	Soil	Minerals / waste	Energy	Local econ.	Employment	Wealth creation
1. Economy	?	●	-	-	?	-	-	-	-	-	-	-	-	-	●	●	●
2. Design excellence	●	-	-	-	●	-	●	●	●	-	-	-	-	?	?	-	●
3. Homes and infrastructure	●	●	-	?	●	-	-	-	-	-	?	-	-	-	?	?	-
4. Business development	●	-	-	-	-	-	-	-	?	-	-	-	-	-	●	●	●
5. Leisure and tourism	-	-	-	?	●	?	-	●	-	-	-	-	-	-	●	●	●
6. Education	-	?	●	-	?	-	-	?	-	-	-	-	-	-	?	?	-
7. Access and streetscapes	●	-	-	●	●	●	?	●	●	?	-	-	-	-	?	-	●
8. Car parking	●	-	-	-	-	-	-	?	?		?	-	-	-	●	?	●
9. Climate change and flood risk	-	?	-	-	-	-	-	●	-	●	-	-	-	-	-	-	?
10. Green and open spaces	●	-	-	●	?	●	●	-	●	●	●	-	-	-	-	-	-
11. Heritage assets	-	-	-	-	●	-	●	●	-	-	-	-	-	-	?	-	●

Sustainability Objective	Sustainability Comment
<b>1. Economic vitality and viability</b>	<p>This objective is to see new development deliver increased economic performance in the central area. The objective covers a range of development types include homes, shops and businesses. The objective includes the need also to cater to visitors to the area.</p> <p>Wording could be altered to remove 'whilst' as recreation and leisure are components of the Southend economy.</p>
<b>2. Design excellence</b>	<p>This objective contains many elements that are very positive for bringing about more sustainable development in the Central Area. The objective aims to make improvements to the quality of the public realm, reinforce a sense of place and promote development that complements new and existing development, helping to make the Central Area a more attractive place. This can have advantages for creating a Southend attractive to investors, somewhere that residents feel proud of and a good place to visit. Design excellence that contributes to helping establish Southend as a Low Carbon City ensures that the central area is both socially and environmentally sustainable, with global benefits relating to climate change.</p>
<b>3. Homes and community infrastructure</b>	<p>This objective relates to the development of new homes in the town centre, as expected through the Core Strategy. The objective should aid in the delivery of homes to meet needs in this central location, raising the potential for homes to be provided and supported by community and social infrastructure. This will contribute to reducing inequalities in health and wellbeing to support all ages to lead independent and healthy lifestyles.</p> <p>The objective also aims for the town centre to be a suitable location for families to live, this will including provision of necessary facilities and services, as well as suitably sized homes. A mix of housing types and tenures in the town centre can build communities in these locations.</p>

<b>4. Business development</b>	This objective introduces the necessity to expand the businesses in central Southend. The aim is to identify, promote and actively bring forward suitable sites for development with the intention to provide infrastructure that is attractive to investors which will foster economic activity and hence contribute to wealth creation in the Central Area. These sites identified will contribute to enhancing employment opportunities and reducing disparities arising from unequal access to jobs. The central area is a sustainable location for business growth as it is well connected to the majority of the Borough's residents by public transport and is close to large residential areas where access can be by foot or bicycle.
<b>5. Leisure, tourism and education/</b>	This objective is to encourage development that enhances leisure and tourism in the Central Area and particularly focusing on the assets of the Seafront to attract visitors and support growth. These assets have wellbeing advantages for residents while maintaining the distinctive character and cultural heritage of the area. Enhancing leisure and tourism facilities and seafront assets allows Southend to build on their local strength and enhance employment opportunities which are conducive to wealth creation. Encouraging more overnight stays having the potential to increase the spend per visitor and improve the tourism economy without increasing pressure on assets through achieving the same level of spend through increasing day visitors.
<b>6. Education</b>	Over recent years Central Southend has increasingly become the focus of further and higher education, with new buildings for South Essex College and Essex University based in the area. Continuing to promote this use has benefits for the vibrancy and perception of the town, as well as supporting the local economy. However, there is a risk that large amounts or poorly designed and managed student housing can undermine the character of an area from a large transient population and student accommodation provided at the expense of family homes.
<b>7. Access and streetscapes</b>	This objective aims for better connectivity of public and green spaces and to create quality streetscapes using co-ordinated materials and furniture. This enables better access, improves opportunities for walking, cycling and use of sustainable modes of transport providing health benefits to residents of the Central Area and the wider Borough. Well-designed streetscapes will also enhance the quality of the built environment, create a cohesive public realm and improve pedestrian and cyclist safety.



<b>8. Car parking</b>	This objective introduces the need for the SCAAP to take into consideration impacts of parking on the vitality of the town centre, concentrating on making improvements to the quality of access to public car parking so that it is convenient, well sign posted, safe and secure and reduces the impact on non-car users. These improvements enable residents to access services, facilities and opportunities while encouraging visitors and investors to the town centre. This can help retain the role of the town centre as the focus for Southend and enable it to compete with other nearby retail centres. However, to achieve more sustainable development there may be a need to manage overall levels of parking as part of a strategy to encourage more sustainable travel. In the medium to longer term there may be a need to reduce overall level and free up land for alternative uses.
<b>9. Climate change and flood risk</b>	This objective aims 'address climate change' although this is limited to manage and mitigate flood risk and encourage the provision of Sustainable Drainage Systems and urban greening to reduce surface run-off. By appropriately managing flood risk and surface run-off, the built environment and cultural heritage of the Central Area are safeguarded providing greater ability for housing needs to be met. Considerations of other potential impacts of climate change could be included, including increasing water scarcity and urban heating effects in the town centre.
<b>10. Green and open spaces</b>	This objective is to increase the connectivity of the town centre and seafront to green and open spaces to relieve pressure on the area's environmental designations and the biodiversity it contains. Improving access through sustainable modes of transport will have associated health benefits for residents. This objective could provide a plethora of benefits for the natural environment - biodiversity will be maintained, protected and hopefully enhanced, air quality will be improved and flood risk reduced.
<b>11. Heritage assets</b>	The objective aims to celebrate Southend's unique heritage assets, including their setting. Conservation of heritage assets in Central Southend is vital due to various advantages that they bring to the area. These assets help to create a sense of place for the community, create distinctiveness of the built environment and enhance the cultural significance of the landscape. Aside from the aesthetic benefits of these assets they also encourage economic growth by providing features that are attractive to visitors and that encourage inward investment and wealth creation.

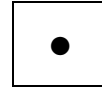
# Appendix D Sustainability Appraisal of Policies

# 1 Explanation

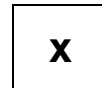
These matrices show an appraisal of the policies submission version against the full set of sustainability objectives developed for the SA process, as shown in the SA Report, Section 2.

## Key to appraisal symbols

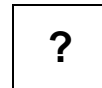
Likely to contribute to the achievement of greater sustainability according to the identified objective



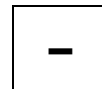
Likely to detract from the achievement of greater sustainability according to the identified objective



Likely effect but too unpredictable to specify, or multiple impacts potentially both positive and negative



No identifiable relationship between the topic covered in the policy and the sustainability concern



Concern	Ref	Explanation and desirable direction of change
<b>Social progress which recognises the needs of everyone</b>		
Accessibility	<b>SP1</b>	<ul style="list-style-type: none"> <li>enable all to have similar and sufficient levels of access to services, facilities and opportunities</li> </ul>
Housing	<b>SP2</b>	<ul style="list-style-type: none"> <li>to provide the opportunity for people to meet their housing need</li> </ul>
Education & Skills	<b>SP3</b>	<ul style="list-style-type: none"> <li>to assist people in gaining the skills to fulfil their potential and increase their contribution to the community</li> </ul>
Health, safety and security	<b>SP4</b>	<ul style="list-style-type: none"> <li>to improve overall levels of health, reduce the disparities between different groups and different areas, and reduce crime and the fear of crime</li> </ul>
Community	<b>SP5</b>	<ul style="list-style-type: none"> <li>to value and nurture a sense of belonging in a cohesive community, whilst respecting diversity</li> </ul>
<b>Effective protection of the environment</b>		
Biodiversity	<b>EP1</b>	<ul style="list-style-type: none"> <li>to maintain and enhance the diversity and abundance of species, and safeguard these areas of significant nature conservation value</li> </ul>
Landscape character	<b>EP2</b>	<ul style="list-style-type: none"> <li>to maintain and enhance the quality and character and cultural significance of the landscape, including the setting and character of the settlement</li> </ul>
Built environment	<b>EP3</b>	<ul style="list-style-type: none"> <li>to maintain and enhance the quality, safety and distinctiveness of the built environment and the cultural heritage</li> </ul>
<b>Prudent use of natural resources</b>		
Air	<b>NR1</b>	<ul style="list-style-type: none"> <li>to reduce all forms of air pollution in the interests of local air quality and the integrity of the atmosphere</li> </ul>
Water	<b>NR2</b>	<ul style="list-style-type: none"> <li>to maintain and improve the quantity and quality of ground, sea and river waters, and minimise the risk of flooding</li> </ul>
Land	<b>NR3</b>	<ul style="list-style-type: none"> <li>to use land efficiently, retaining undeveloped land and bringing contaminated land back into use</li> </ul>
Soil	<b>NR4</b>	<ul style="list-style-type: none"> <li>to maintain the resource of productive soil</li> </ul>
Minerals and other raw materials	<b>NR5</b>	<ul style="list-style-type: none"> <li>to maintain the stock of minerals and other raw materials</li> </ul>
Energy sources	<b>NR6</b>	<ul style="list-style-type: none"> <li>to increase the opportunities for energy generation from renewable energy sources, maintain the stock of non renewable energy sources and make the best use of the materials, energy and effort embodied in the product of previous activity</li> </ul>
<b>Maintenance of high and stable levels of economic growth and employment</b>		
Local economy	<b>EG1</b>	<ul style="list-style-type: none"> <li>to achieve a clear connection between effort and benefit, by making the most of local strengths, seeking community regeneration, and fostering economic activity</li> </ul>
Employment	<b>EG2</b>	<ul style="list-style-type: none"> <li>to maintain and enhance employment opportunities matched to the size of the local labour force and its various skills, and to reduce the disparities arising from unequal access to jobs</li> </ul>
Wealth creation	<b>EG3</b>	<ul style="list-style-type: none"> <li>to retain and enhance the factors which are conducive to wealth creation, including personal creativity, infrastructure, accessibility and the local strengths and qualities that are attractive to visitors and investors</li> </ul>

## 2 Part B: Development Strategy and Policies

### 2.1 Retail

#### DS1: A Prosperous Retail Centre

##### Policy summary

The policy sets the general principles for delivering a prosperous retail centre, including uses in primary and secondary frontages.

##### Relationship of policy with sustainable development

Focusing shopping facilities in one central location can help reduce peoples' need to travel. Therefore, these benefits related to reduced car use, aided by the fact the Central Area has good public transport access and is within easy walking and cycling distance from many peoples' homes. A diverse retail offer can also make the area attractive to visitors and be part of the economy. However, retail should be compatible with the size of the town and ideally not harm nearby towns ability to retain viable retail offer.

A well designed shopping area and where existing heritage is protected can help make Southend town centre unique, differentiating it from other generic shopping areas.

##### Policy DS1: Maintaining a prosperous retail centre (not including principle 4 options)

SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
•	•	-	-	•	-	-	•	•	-	?	-	-	-	•	•	•

##### DS1: Sustainability appraisal comment

This policy should help to retain the retail core of the Central Area as an attractive place to visit. The policy should help protect those areas of primary importance for shopping from alternative uses. Focused retail development may be required to protect the viability of shopping areas.

The policy also specifies the type of uses that would be suitable in the area to maintain an active frontage on ground floor level, as well as allowing a mix of uses on upper stories, including a residential element.

The policy also allows for the use of upper floors of existing retail areas to be used for other uses, such as residential development. This should help improve maintain and improve the vibrancy of the area, although it will be important to ensure that new residential uses are compatible with the location. This is important to protect the amenity of new and existing residents, but also to protect existing businesses and facilities in the Central Area. For instance, new residential development given permission for development in inappropriate locations or with insufficient noise mitigation in their design can lead to existing uses in town centres having to close or change the way they operate e.g. bars, restaurants or music venues as well as shops with early morning or late night deliveries.

There is a strong emphasis on protection of the public realm through the standard of design expected of new buildings, as well as protecting the loss of traditional shopfronts.

The promotion of new street markets is likely to have beneficial effects, many towns and cities have thriving new retail markets that add vibrancy to towns and can support other sustainability objectives, such as access to healthy food, supporting small business and selling local goods.

The policy identifies the defined 'primary shopping frontage' in the central area where the aim is for this to remain 60% in A1 (retail use). This should help to maintain the retail character of the area, as other uses in these areas can detract from the overall role and character. The loss of character in these areas can have reinforcing effects on shoppers and retailers that could lead to further decline.

The current version of the SCAAP has seen a reduction in the overall length of the primary shopping areas from earlier versions. However, a high level of protection has been assigned (60%). Retail outside these primary areas will still be protected, but to a lesser extent. This approach should help support a

sustainable retail economy, while allowing for some change to help retain vibrancy and reduce vacancy in the central area, as a result of the national change in shopping habitats e.g. more online shopping and retail superstores. There will be a need to monitor these shopping areas, and put remedial policies in place if necessary, if this strategy sees an unexpected rapid decline in A1 uses.

The policy also now links to an Appendix that sets out in detail the marketing evidence that will have to be presented to secure a change of use application. This should help support the retention of these uses where there is a genuine market for retail use.

### **Recommendations and potential for significant impacts**

This policy is likely to have a positive impact on achieving sustainable development. This is as long as the retail offer remains appropriate to the size of the town and other built environment improvements are achieved.

Monitoring the Primary and Secondary Shopping areas to ensure the change in policy approach does should the strategy cause a rapid decline in A1 uses remedial measures can be put in place.

The policy could include support of temporary use for long-term vacant units for other use-classes or pop-up shops selling locally made goods. This will require working with the relevant landlords and leaseholders to allow appropriate tenancies.

### **Policy Trail**

**Update to Preferred Approach 2015:** The policy has seen a substantial change from the previous version. The current policy is an amalgamation of several distinct policies from the previous version of the SCAAP, these are: DS1 New and enhanced shopping facilities, DS2 Shopping frontages and use of floors above shops and DS3 Retail markets.

These former policies were much more detailed but repeated other policies of the SCAAP and the LDF. Therefore the sustainability impacts of the revisions are not assessed as different.

Previously the SCAAP policies specified the quantum of additional retail floorspace required. This may have allowed some indication of the scale of new development required. However, it was not location specific and would not necessarily have resulted in the delivery of this quantum. The previous SCAAP version also set out general locations for defined types of retail growth, which is now predominantly covered in area specific policies. It also included sites that are no longer included as Opportunity Sites in the SCAAP.

Previously a 30% non-A1 use was included in policy, in keeping with Option A.

**Update to Revised Submission Version 2016:** Options have been removed and the preferred approach chosen of a high level of protection in the A1 retail frontage (60%) in the primary shopping frontage. However, the overall length of this primary frontage has been reduced to about 40% when compared to the existing Local Plan designation (1994) to reflect the focused retail core. This reduction should aid in protecting this retail area.

### **Iteration of policy following SA of the draft Revised Proposed Submission:**

The strategy has been updated to from an original retention of 70%. This may have help protect A1 use further, but could have resulted in lower quality A1 use in place of higher quality alternative town centre uses. Additional detail on marketing evidence has been added to the SCAAP to support the policy.

## Other relevant LDF Policies – covered by separate Sustainability Appraisal

### 2.2.1 Core Strategy:

- CP2: Town and Centre Retail Development.

## 2.3 Employment

2.3.1 Non site specific principles relating to this type of development are covered in other parts of the LDF and to avoid repetition are not repeated in the SCAAP. The Core Strategy and Development Management DPD are the relevant component documents of the LDF and all policies within them have already been subject to SA.

2.3.2 In addition, area specific policies of the SCAAP identify areas of focus for employment and the sustainability appraisal of these policies is covered in this appendix.

2.3.3 Policies of most relevance are:

- Core Strategy: CP1: Employment generating development;
- Development Management DPD: DM10: Employment Sectors;
- Development Management DPD: DM11: Employment Areas.

2.3.4 In the SCAAP the following areas have an employment focus:

- PA2: London Road;
- PS5: Warrior Square;
- PA6: Clifftown;
- PS8: Victoria Gateway Neighbourhood; and
- PA9: Sutton Gateway Neighbourhood.

2.3.5 Previous policy DS4 'Employment development in the Central Area' proposed for the SCAAP has been removed to avoid duplication of policy with the LDF as a whole and SCAAP area specific policies.

## 2.5 Housing

- 2.5.1 Non site specific principles relating to this type of development are covered in other parts of the LDF and to avoid repetition are not repeated in the SCAAP. The Core Strategy and Development Management DPD are the relevant component documents of the LDF and all policies within them have already been subject to SA.
- 2.5.2 In addition, area specific policies of the SCAAP identify areas of focus for housing and the sustainability appraisal of these policies is covered in this appendix.
- 2.5.3 Policies of most relevance are:
- Core Strategy: KP1: Spatial Strategy;
  - Core Strategy: CP8: Dwelling Provision;
  - Development Management DPD: DM7 Dwelling mix, size and type;
  - Development Management DPD: DM8: Residential standards;
  - Development Management DPD: DM9 Specialist residential accommodation.
- 2.5.4 In the SCAAP the following areas have a housing focus:
- PA1: High Street Policy Area;
  - PA2: London Road;
  - PA4: Queensway;
  - PS5: Warrior Square;
  - PA6: Clifftown;
  - CS1: Central Seafront;
  - PS8: Victoria Gateway Neighbourhood; and
  - PA9: Sutton Gateway Neighbourhood.
- 2.5.5 Previous policy DM8 'Housing' proposed for the SCAAP has been removed to avoid duplication of policy with the LDF as a whole and SCAAP area specific policies.



## **2.7 Culture, leisure, tourism and recreational facilities**

- 2.7.1 Non site specific principles relating to this type of development are covered in other parts of the LDF and to avoid repetition are not repeated in the SCAAP. The Core Strategy and Development Management DPD are the relevant component documents of the LDF and all policies within them have already been subject to SA.
- 2.7.2 In addition, area specific policies of the SCAAP identify areas of focus for cultural, leisure, tourism and recreation development and their sustainability appraisal is contained within this appendix.
- 2.7.3 Policies of most relevance are:
- Core Strategy: KP1: Spatial Strategy;
  - Core Strategy: CP1: Employment generating development;
  - Core Strategy CP6: Community infrastructure;
  - Core Strategy CP7: Sport, recreation and green space;
  - Development Management DPD: DM6 The Seafront;
  - Development Management DPD: DM12 Visitor Accommodation.
- 2.7.4 In the SCAAP the following areas have a cultural, leisure, tourism or recreation focus:
- PA1: High Street Policy Area;
  - PA2: London Road;
  - PA3: Elmer Square;
  - PA4: Queensway;
  - PS5: Warrior Square;
  - PA6: Clifftown;
  - PA7: Tylers;
  - CS1: Central Seafront;
  - CS4: The Waterfront;
  - PS8: Victoria Gateway Neighbourhood; and
  - PA9: Sutton Gateway Neighbourhood.
- 2.7.5 Previous policy DS6 proposed for the SCAAP has been removed to avoid duplication of policy with the LDF as a whole and SCAAP area specific policies.

## 2.9 The Historic Environment

- 2.9.1 Non site specific principles relating to this type of development are covered in other parts of the LDF and to avoid repetition are not repeated in the SCAAP. The Core Strategy and Development Management DPD are the relevant component documents of the LDF and all policies within them have already been subject to SA.
- 2.9.2 In addition, area specific policies of the SCAAP identify areas of importance for protection of the historic environment, including archaeology and their sustainability appraisal is contained within this appendix.
- 2.9.3 Policies of most relevance are:
- Core Strategy: KP1: Spatial Strategy;
  - Core Strategy: CP4: The environment and urban renaissance;
  - Development Management DPD: DM5 Southend-on-Sea's Historic Environment.
- 2.9.4 In the SCAAP the following areas have an historic environment focus:
- PA1: High Street Policy Area;
  - PS5: Warrior Square;
  - PA6: Clifftown;
  - CS1: Central Seafront; and
  - PS8: Victoria Gateway Neighbourhood.
- 2.9.5 Previous policies proposed for the SCAAP HE1 to HE7 have been removed to avoid duplication of policy with the LDF as a whole and SCAAP area specific policies.

## 2.11 Open and green spaces

2.11.1 Non site specific principles relating to this type of development are covered in other parts of the LDF and to avoid repetition are not repeated in the SCAAP. The Core Strategy and Development Management DPD are the relevant component documents of the LDF and all policies within them have already been subject to SA.

2.11.2 In addition, area specific policies of the SCAAP identify areas of focus for open and green spaces and their sustainability appraisal is contained within this appendix.

2.11.3 Policies of most relevance are:

- Core Strategy: KP2: Development principles;
- Core Strategy CP4: The environment and urban renaissance;
- Core Strategy CP7: Sport, recreation and green space.

2.11.4 In the SCAAP the following areas have coverage of open and green space protection::

- DS5: Transport, access and the public realm;
- PA1: High Street Policy Area;
- PA3: Elmer Square;
- PA4: Queensway;
- PS5: Warrior Square;
- PA6: Clifftown;
- CS1: Central Seafront;
- PS8: Victoria Gateway Neighbourhood; and
- PA9: Sutton Gateway Neighbourhood.

2.11.5 Previous policy PR1 'Open space provision' proposed for the SCAAP has been removed to avoid duplication of policy with the LDF as a whole and SCAAP area specific policies.

## 2.13 Key Views

DS2: Key Views																
<b>Policy summary</b>																
This policy identifies seven key views that should be considered for all relevant new development in the Central Area.																
<b>Relationship of policy with sustainable development</b>																
Views and protecting the unique characteristics of Southend can help ensure a sense of place and people's pride in where they live and the attractiveness to visitors.																
DS2: Key Views																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
-	-	-	-	?	-	?	●	-	-	-	-	-	-	?	-	?
<b>Sustainability appraisal comment</b>																
The policy should help in protecting the character of the area. However, every application should be judged on its merit, particularly where new development makes innovative use of views or provide other sustainability benefits.																
<b>Recommendations and potential for significant impacts</b>																
The policy is likely to have a positive impact on sustainable development.																
<b>Policy Trail</b>																
<b>Update to Preferred Approach 2015:</b> This policy is a more positively worded version of the previous PR4 and in addition now names important views.																
<b>Update to Revised Proposed Submission Version 2016:</b> No change.																
<b>Iteration of policy following SA of the draft Revised Proposed Submission:</b>																
None.																

### Relevant LDF Policies – covered by separate Sustainability Appraisal

#### 2.13.1 Core Strategy:

- KP2: Development Principles;
- CP4: The Environment and Urban Renaissance.

#### 2.13.2 Development Management Development Plan Document:

- DM1: Design Quality;
- DM4: Tall and Large Buildings;
- DM5: Southend-on-Sea's Historic Environment;
- DM6: The Seafront.

## 2.15 Landmarks and Landmark Buildings

DS3: Landmarks and Landmark Buildings																
<b>Policy summary</b>																
This policy seeks to protect landmark buildings and set criteria for developing new ones.																
<b>Relationship of policy with sustainable development</b>																
Landmark buildings can help give an area character, supporting a unique local built character that people can have pride in. These buildings are also important in creating a legible town, providing focal points as residents and visitors move through the Central Area.																
Policy DS3: Landmarks and Landmark Buildings																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
-	-	-	-	?	-	•	•	-	-	-	-	-	-	?	-	?
<b>Sustainability appraisal comment</b>																
The policy is compatible with achieving sustainable development.																
Landmark buildings in the Borough should be kept under review to reflect any newly constructed or restored buildings that may become a landmark feature e.g. a new museum or tall buildings that can be added to the list in future.																
<b>Recommendations and potential for significant impacts</b>																
The policy is likely to have a positive impact on sustainable development.																
<b>Policy Trail</b>																
<b>Update to Preferred Approach 2015:</b> This policy similar to the previous policy PR5, as well as CS1 relevant to landmark buildings in the seafront, although the policy now contains an additional principle to protect the setting of landmark buildings.																
<b>Update to Revised Proposed Submission Version 2016:</b> This policy now contains an additional principle to protect the setting of nearby heritage assets in the interest of sustainable development.																
<b>Iteration of policy following SA of the draft Revised Proposed Submission:</b>																
None relevant.																

### Relevant LDF Policies – covered by separate Sustainability Appraisal

#### 2.15.1 Core Strategy:

- KP2: Development Principles;
- CP4: The Environment and Urban Renaissance.

#### 2.15.2 Development Management Development Plan Document:

- DM1: Design Quality;
- DM4: Tall and Large Buildings;
- DM5: Southend-on-Sea's Historic Environment;
- DM6: The Seafront.

## 2.16 Flood Risk Management and Sustainable Drainage

DS4: Flood Risk Management and Sustainable Drainage																
<b>Policy summary</b>																
The policy sets out principles to manage flood risk and drainage to new and existing residents and visitors to the Central Area.																
<b>Relationship of policy with sustainable development</b>																
Protecting people from flood is essential to protect wellbeing, both in terms of direct physical risk as well as in relation to the effects on mental wellbeing related to the experience of flooding and anxiety of repeat flooding. Flooding can also adversely impact on local businesses that are at risk. Through providing safe and sufficient capacity for drainage it will ensure water quality is protected.																
DS4: Flood Risk Management and Sustainable Drainage																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
-	?	-	?	-	-	-	-	-	●	-	-	-	-	?	-	-
<b>Sustainability appraisal comment</b>																
The policy is compatible with achieving sustainable development.																
<b>Recommendations and potential for significant impacts</b>																
The policy is likely to have a positive impact on sustainable development. The policy, and supporting information in Appendix 4, should ensure it maintains flexibility in its approach to allow changes to national policy and guidance to be taken into account, which may change over the plan period.																
<b>Policy Trail</b>																
<p><b>Update to Preferred Approach 2015:</b> Previously versions of the SCAAP had an overarching flood risk policy, a policy for the central area and policies covering specific policy areas also identified area of surface water flood risk. The revisions to policy have simplified the approach, although the importance of consideration of suitable drainage on all sites previously included may have had greater benefits for sustainable development.</p> <p><b>Update to Revised Proposed Submission Version 2016:</b> The revisions to the policy have added further detail to flood risk assessments and floor level specifications. The policy includes further criteria stating surface water will not be permitted to discharge to a foul sewer but to a combined sewer system.</p>																
<b>Iteration of policy following SA of the draft Revised Proposed Submission:</b>																
Additional detail on freeboard definition has been added that may aid delivery.																

### Relevant LDF Policies – covered by separate Sustainability Appraisal

#### 2.16.1 Core Strategy:

- KP2: Development Principles;
- KP3: Development Principles;
- CP4: The Environment and Urban Renaissance.

#### 2.16.2 Development Management Development Plan Document:

- DM6: The Seafront.

## 2.17 Transport, Access and the Public Realm

DS5: Transport, Access and Public Transport																
<b>Policy summary</b>																
The policy sets out principles to manage travel and access in the central area, with a focus on more sustainable modes.																
<b>Relationship of policy with sustainable development</b>																
Travel and transport are issues with strong links to achieving sustainable development. Measures that reduce reliance on car driving and help make services accessible by foot, cycling or public transport from where people live will have environment, social as well as economic benefits.																
DS5: Transport, access and public transport																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
●	-	-	●	-	?	-	●	●	-	-	-	-	-	●	?	?
<b>Sustainability appraisal comment</b>																
The policy is compatible with achieving sustainable development. There are likely to be beneficial effects in relation to air quality from encouraging more sustainable travel that may help reduce car use, congestion. Improvements that encourage more walking and cycling can also have health benefits from 'Active Travel'. There may also be built environment benefits from enhancing the public realm to improve walking routes.																
The policy will also help ensure equitable access to services and facilities in the Central Area as it will maintain and improve access for those who do not drive, for whatever reason. The cross reference to the implementation of sustainable access measures in Appendix 5 and development management policies relating to sustainable transport will support a modal shift away from car use. The reference to a freight management plan may also help reduce the adverse effects on local highways and congestion from the delivery of goods.																
Measures to improve information and signage regarding car parking availability may remove the need for further car parks in the centre which may indirectly increase sustainable travel. Protection is given to car parking in the Central Area, in particular south of the railway line where car parks more frequently near capacity.																
The policy criteria related to the importance of ensuring new car parks contribute to the character of the built environment can also have benefits for this sustainability objective, with possible associated benefits for enhancing biodiversity.																
<b>Recommendations and potential for significant impacts</b>																
The policy is likely to have a positive impact on sustainable development in helping to encourage more sustainable travel choices.																
It is hoped that the policy will support a reduction in the medium to long term in the number of car parking spaces in the Central Area as part of a strategy to promote more sustainable transport choices, improve the character of some areas release land for alternative uses.																
Encouraging more visitor to park north of the railway line, for instance through signage or pricing, may help support the town centre as people walk through to access the seafront.																
<b>Policy Trail</b>																
<b>Update to Preferred Approach 2015:</b> This policy is new to the plan previously there were seven distinct policies covering transport and these have been rationalised into this single policy to avoid repetition within the SCAAP and with other LDF documents. Specific improvement schemes are also now only contained																

in relevant area development principle policies.

**Update to Revised Proposed Submission Version 2016:** The policy has been updated to reflect the outcomes of the Draft Southend Town Centre Car Parking Study (May 2016). As a result new criteria have been added to the policy to ensure no net loss of car parking south of the central railway line and to rebalance parking supply both sides of the railway to ensure maximum usage.

New criteria have been added to improve strategic junctions, safety and car parking pressure. A new principle has been added to provide facilities for charging electric vehicles.

**Iteration of policy following SA of the draft Revised Proposed Submission:**

More detail is added on the need for transport assessment and travel plans in for development in the Central Area that will help in a strategy of reducing congestion and encouraging more sustainable travel.

## Relevant LDF Policies – covered by separate Sustainability Appraisal

### 2.17.1 Core Strategy:

- KP2: Development Principles;
- CP3: Transport and Accessibility;
- CP4: The Environment and Urban Renaissance.

### 2.17.2 Development Management Development Plan Document:

- DM15: Sustainable Transport Management.

## 2.18 Infrastructure Provision

2.18.1 Non site specific principles relating to this type of development are covered in other parts of the LDF and to avoid repetition are not repeated in the SCAAP. The Core Strategy and Development Management DPD are the relevant component documents of the LDF and all policies within them have already been subject to SA.

2.18.2 Where relevant area specific policies of the SCAAP identify areas that may be focus for new infrastructure, in particular new schools. These policies are appraised elsewhere in this Appendix.

### 2.18.3 Policies of most relevance are:

- Core Strategy KP1: Spatial Strategy
- Core Strategy: KP2: Development principles;
- Core Strategy CP4: The environment and urban renaissance;
- Core Strategy CP6: Community Infrastructure
- Core Strategy CP7: Sport, recreation and green space
- Development Management DPD: DM2 Low Carbon Development and Efficient Use of Resources
- Development Management DPD: DM14 Environmental Management.

2.18.4 In addition SBC now prepares the 'Southend Infrastructure Delivery Plan', a live document produced to identify the range of infrastructure types and projects required to supported



growth. It identifies funding sources, delivery bodies, timescales and priorities and supports the application of the Community Infrastructure Levy applied to new development.

2.18.5 In the SCAAP the following areas have coverage of open and green space protection:

- DS5: Transport, access and the public realm;
- PA1: High Street;
- PA2: London Road;
- PA3: Elmer Square;
- PA4: Queensway;
- PS5: Warrior Square;
- PA6: Clifftown;
- PA7: Tylers;
- CS1: Central Seafront;
- PS8: Victoria Gateway Neighbourhood; and
- PA9: Sutton Gateway Neighbourhood.

2.18.6 Previous policies of the SCAAP have been removed to take into account the policy criteria already in the LDF, these are DS5: Education, higher and Further Education; DS7: Social and Community Infrastructure; IF1: Central Area Infrastructure; IF2: Section 106 and Developer Contributions; IF3 Flood Risk Management.

## 3 Part C: Policy Areas and Opportunity Site Policies

### 3.1 High Street Policy Area

#### PA1: High Street Area

##### Policy summary

The policy sets the criteria for developing and regeneration of the High Street. This includes the mix of uses suitable and treatment of the public realm.

##### Relationship of policy with sustainable development

The High Street is a high trip generating focus for the Central Area. Built environment conservation and improvements can help encourage more people to spend more time in the area and a mix of uses can bring further vitality to the area and maintain its character.

##### Policy PA1: High Street Area Development Principles

SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
•	•	-	•	•	?	?	•	•	-	?	-	-	?	•	•	•

##### PA1: Sustainability appraisal comment

The High Street is the core of the Central Area, providing a range of shopping and leisure uses. This policy should help to make sure the High Street is an attractive place to visit. The policy also specifies the type of uses that would be suitable in the area to maintain and encourage an active frontage on ground floor level, as well as allowing a mix of uses on upper stories, including a stronger residential element.

There is a strong emphasis on improving the public realm to enhance the character and the quality of the High Street, The Royals Shopping Centre, the setting of the Victoria's Shopping Centre and Queensway dual carriageway with potential for better use of Victoria Circus as a public space for events.

##### Recommendations and potential for significant impacts

The policy is likely to help achieve beneficial sustainability impacts for the Central Area and wider Borough.

##### Policy Trail:

**Update to Preferred Approach 2015:** The policy has been substantially revised from the 2011 version removing the detail on the public realm improvements and increasing emphasis on the mix of uses that should be delivered, it is a more succinct policy. This will have no identifiable sustainable development effects as these matters are covered elsewhere in the SCAAP and LDF, although the references to urban greening could be reinstated. There may be beneficial effects from the inclusion of the decentralised energy policy principle.

OS1 was previously included in 'Warrior Square' area (PS5b) and more detail has been added on the type of development that should be delivered.

OS2 was previously in the Tylers area (PS7a) and more detail has been added on the type of development that should be delivered.

**Update to Revised Proposed Submission Version 2016:** The policy includes further areas for active frontages.

Similar to Policy DS3, criteria has been added to conserve existing landmarks including views, character and setting.

PA1.1, PA1.2, OS1 and OS2 have been removed as they were not promoted in the SCAAP process.

**Iteration of policy following SA of the draft Revised Proposed Submission:**

Additional detail has been added in on urban greening projects and reference to the Streetscape Manual.

## 3.2 London Road Policy Area

PA2: London Road Policy Area																
<p><b>Policy Summary</b></p> <p>The policy sets the general principles for the enhancing the Queensway and London Road area. The proposal site is the large area of land currently occupied by Sainsbury's and a car park and a London Road that is fully accessible to vehicles as far as Victoria Circus.</p>																
<p><b>Relationship of policy with sustainable development</b></p> <p>This part of the Central Area contains a number of bulky buildings that dominate the area. There is a low quality frontage and the built environment is of a poor quality with little provision for cyclists and pedestrians. Regeneration of the area through wholesale redevelopment or partial renewal could have substantial benefits to its quality and attractiveness to visitors. Increasing pedestrianisation and public realm improvements could have benefits for the character of this currently underutilised space so that it can actively contribute to the attractiveness of the area and the local economy.</p>																
Policy PA2: London Road Policy Area Development Principles																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
•	?	?	?	•	?	?	•	?	-	?	-	-	?	•	?	•
<p><b>Sustainability appraisal summary and comment</b></p> <p>The policy should help improve the character of the area, while retaining some of the existing uses such as car parking. Much of the improvements relate to better links across the site and to other parts of the Central Area for cyclists and pedestrians. This will help make the site more accessible including links across Queensway to Victoria Rail Station.</p> <p>The principles for development also contain some other positive aspects, such as promoting urban greening, the potential for a street market, potential for higher/further education use, permitting a mix of uses including office and residential to upper floors, and the potential for decentralised and low carbon energy.</p>																
<p><b>Recommendations and potential for significant impacts</b></p> <p>The policy is likely to help achieve beneficial sustainability impacts for the Central Area and wider Borough. Site OS15 has been removed as it was not considered in the SCAAP process. Any development coming forward on this site in the plan period will need to be guided by this policy as well as the Core Strategy and DMD, but for the longer term a cohesive set of criteria to guide delivery should be included in policy (this could be as part of any future Local Plan).</p>																
<p><b>Policy trail:</b></p> <p><b>Update to Preferred Approach 2015:</b> The policy for the area has changed in name although covers a broadly similar area. The policy recognises that the Sainsbury's site has potential for redevelopment, but based on available information this is phased to post-2021, the policy recognises that if the site does come forward it needs to be managed through a Development Brief.</p> <p><b>Update to Revised Proposed Submission Version 2016:</b> Similar to Policy DS3, criterion has been added to ensure landmark buildings are conserved contributing to sustainability principles by protecting the character of the settlement.</p> <p>OS15 has been removed as the site was not promoted in the SCAAP process and no evidence of delivery before 2021.</p>																
<p><b>Iteration of policy following SA of the draft Revised Proposed Submission:</b></p> <p>Additional detail has been added in on urban greening projects and reference to the Streetscape Manual.</p>																

### 3.3 Elmer Square Policy Area

<b>PA3: Elmer Square</b>																	
<b>Policy Summary</b>																	
The policies set out the general principles and specifics for development in and around Elmer Square, including the Phase 2 of the Elmer Square opportunity area.																	
<b>Relationship of policy with sustainable development</b>																	
This is an important site near the High Street already associated with further and higher education. Enhancing this education role, as well as public realm improvements, could improve the attractiveness of Southend for those choosing a place to study. These improvements could also aid the image of a town as a place to do business due to the links with a modern university complementing the recently completed Phase 1 enhancements to the university and public realm.																	
<b>Policy PA3: Elmer Square Development Principles</b>																	
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3	
●	?	●	?	●	?	?	●	?	-	?	-	-	?	●	●	●	
<b>Sustainability appraisal summary and comment</b>																	
The policy clearly sets that educational uses will be favoured in this part of the Central Area with further improvements to the public realm and cycling and walking routes. There is the potential for benefits to the built environment that will have a positive impact on the image of the Central Area. The results of improvements are likely to have economic benefits for Southend through improvements to the image and attractiveness of the Central Area, and links to a modern university.																	
Conserving existing landmarks will preserve the character of the settlement and will provide relaxation space, contributing to residents' sense of place.																	
Access upgrades will help encourage walking and cycling access, this will be part of securing a mode shift away from car use, with benefits for the wider Central Area. The policy includes principles to ensure that development delivery offer enhancements such as urban greening and consideration of more efficient energy use.																	
The policy requires that consideration is given a long term management and maintenance plan for any new student accommodation. Given the transient populations occupying these blocks they can have the potential to cause detrimental effects on the local environment, including poor refuse storage and disposal, lack of maintenance of planted areas and out of term time can result in loss of activity around the block. This can impact on the image of areas with possible economic impacts and adverse impacts on local residents. Therefore, this policy inclusion should help protect the local environment from potential harm from this type of development.																	
<b>PA3.1: Elmer Square Phase 2</b>																	
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3	
●	?	●	-	?	-	-	●	-	-	-	-	-	-	●	?	?	
<b>PA3.1: Elmer Square Phase 2</b>																	
The opportunity sites policy is quite simple and relates to Phase 2 of the Elmer Square development, specifying that educational and supporting uses will be suitable in this location. This should help to enhance Elmer Square and the 'Education Quarter' of the Central Area.																	
<b>Recommendations and potential for significant impacts</b>																	
The policy should help deliver more sustainable development in this area.																	

**Policy Trail:**

**Update to Preferred Approach 2015:** The policy has been updated to reflect that Phase 1 of Elmer Square has been completed and Phase 2 still has to be delivered, including the redevelopment of the Prudential block that is long-term vacant and detracts from other environmental improvements the area. The policy also now includes more on energy efficiency and removes the detail on flooding for coverage by the generic policy.

**Update to Revised Proposed Submission Version 2016:** Similar to Policy DS3, criterion has been added to ensure landmark buildings are conserved contributing to sustainability principles by protecting the character of the settlement.

The policy now includes more on public realm improvements encouraging sustainable connection to Phase 1 and creating new vistas within the high street area.

OS3 is now referred to as PA3.1. Planning permissions within this area will now be granted for further uses. The site boundary of the policy area has reduced slightly and now excludes an area of hardstanding.

**Iteration of policy following SA of the draft Revised Proposed Submission:**

Additional detail has been added in on urban greening projects and reference to the Streetscape Manual.

### 3.4 Queensway Policy Area

#### PA4: Queensway

##### Policy Summary

This policy sets the general principles for the redevelopment and enhancements to the Queensway and Southchurch Avenue area. The proposal policy sets the principles for redevelopment of the Queensway House site, improvements to non-car users to Queensway and new areas of planting to improve the setting of listed buildings.

##### Relationship of policy with sustainable development

Queensway dual carriageway currently creates a barrier for those walking or cycling to the town centre from the north and east. New and improved cycle and foot links can have environmental benefits from reduced car use. They will also help people feel more linked to the Central Area, reducing the barriers created by the road, which could have benefits related to sustainable communities and could also improve perceived character of the area encouraging a more diverse community and economy.

The poor quality of the built environment here can be enhanced through the design of new spaces and other changes such as new public art.

Replacement of the social housing lost on site will be necessary to maintain the affordable housing supply of Southend.

There is the potential for new planting and open space to bring nature into the Central Area, with benefits for the visual appearance, biodiversity and wellbeing created through access to 'green' infrastructure.

Queensway House has already been cleared and the current use temporary as a car park and therefore development needs to be progressed quickly on the site.

#### Policy PA4: Queensway Policy Area OS4.1: 'Better Queensway' Project'

SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
●	?	-	?	?	?	?	●	?	-	-	-	-	?	-	-	?

This area is to be redeveloped to provide a housing led development with supporting community and secondary town centre uses. The area is identified as one part of the Central Area that has the potential to deliver a range of new residential development. This will help meet objectives for the Central Area of providing development to meet the needs of existing and future residents, as well as encouraging economic growth in the area.

Changes in access are promoted through the policy. This should help overcome the barrier created by Queensway. There is also potential for new mixed mode – shared priority access from this area to other parts of the Central Area.

New open space provision will have benefits for residents, providing space for outdoor leisure and relaxation. Design of these spaces should make them useable by residents, visitors and local workers and not simply be grassed areas as the setting for buildings. Well landscaped areas should also enhance the character of this area. New open space should have 'soft' landscaping wherever possible as this can have greater sustainability benefits than hard landscaped areas, for instance for wildlife and to help absorb water and heat. Improvements to Queensway for pedestrians and cyclists will also bring benefits to the area.

All of these changes could improve the setting of the listed buildings at Porters and All Saints Church.

There has been a loss of community facilities at the site, so it is essential that there is some re-provision of community services either on this site or nearby, with the same level of accessibility for local users. The importance of the provision of services is set out in policy and the need for these to help contribute with any planning vision for the area.

There is also the potential for a large amount of the affordable housing to be lost through development.

Therefore, the policy includes a criterion to ensure no net loss of affordable housing provision at the site. Affordable housing should be provided in a range of tenure types and in a range of housing sizes, compatible with the policies of the Development Plan Document and Core Strategy.

A new urban park, including links to Warrior Square, should help achieve a coordinated delivery of this space. Linking spaces can have significant benefits for biodiversity, as well as providing attractive walking and cycling routes encouraging active travel.

### **Recommendations and potential for significant impacts**

The policy should help deliver more sustainable development in this area.

The preparation of a site masterplan or development brief by SBC, or 'Better Queensway' group, would be useful in helping deliver a coordinated scheme for the area. This will help maximise the benefits for sustainable development, as this large site that has the potential to deliver significant change, such as in road layout, a large amount of new housing (including affordable), community and other uses and therefore needs a unified approach to development.

The existing site includes a large amount of affordable housing. Therefore, the inclusion in the policy of a requirement to ensure no net loss should help protect housing in this location to meet the needs of local residents and the wider needs of Southend. In delivering new affordable housing consideration may need to be given to the type (rented or shared ownership), although this may depend on national policy definitions.

Any plans for a new urban park should include details of suitable types of planting that will help support urban wildlife, for instance use of flower and fruiting species to support bees and other invertebrates, birds and bats.

### **Policy Trail:**

**Update to Preferred Approach 2015:** The policy area has been amended to include the Coleman Street flats and houses. This has helped provide a more coherent redevelopment site each side of the dual carriageway Queensway. The policy also now notes the importance of the area in providing the setting to the listed buildings that site on the Queensway roundabout just outside the Central Area.

The policy has been changed to change the emphasis of open space in this area from an 'urban forest' to an 'urban park'. This may be more suitable for this location, although tree planting remains part of the principles for development. The policy also now includes more on energy efficiency and removes the detail on flooding for coverage by the generic policy.

The former Proposal Site PS4a included Queensway House that has now been demolished.

**Update to Revised Proposed Submission Version 2016:** OS4 is now PA4.1, the policy has been amended and increased the number of indicative dwellings and brought forward the timescale for delivery to before 2021.

The policy identifies what PA4.1 aims to deliver. The area of PA4 has been expanded and now extends beyond the Opportunity Site. This creates a more cohesive development area by including all land to the north of Queensway, east of Southend Victoria Station.

### **Iteration of policy following SA of the draft Revised Proposed Submission:**

Additional detail has been added in on urban greening projects and reference to the Streetscape Manual.

Details of the need to protect heritage assets are now included in the policy, this is an important consideration given proximity to the Grade I 'Porters'.



### 3.6 Warrior Square Policy Area

PA5 Warrior Square																	
<p><b>Policy Summary</b></p> <p>The policies set out the general principles for development at Warrior Square. Specific development requirements are set for the allocated site on the existing Warrior Square car park.</p>																	
<p><b>Relationship of policy with sustainable development</b></p> <p>The land at Warrior Square is currently underutilised as a surface car park and the cleared site of the former swimming pool, which detracts from the quality of the conservation area and Warrior Square open space. Development here is well linked to the High Street and has the potential to deliver a variety of benefits in the Central Area, with a residential focus. Re-organisation of the car park, including vehicle and pedestrian access could improve movements to and from the site, increasing the attractiveness of routes for walkers and reducing town centre traffic congestion.</p>																	
PA5: Warrior Square Policy Area Development Principles																	
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3	
●	●	-	●	?	?	?	●	?	-	-	-	-	?	●	●	?	
<p><b>Sustainability appraisal summary and comment</b></p> <p>The policy sets the principles for delivering development in this location with a focus on residential led mixed use development.</p> <p>The principles of the policy are compatible with more sustainable transport access and also are likely to help deliver nature conservation benefits through implementation of improvements to the urban greening.</p> <p>The principles of the policy also make clear the benefits the redevelopment of this area bring to the quality of the built environment, this includes the setting of the Conservation Area, relationships with the Warrior Square gardens and use of public art.</p> <p>The specification of soft planting is also positive in terms of securing sustainable development as it will have greater benefits for sustainability than hard landscape, for instance biodiversity, surface water drainage and reducing urban heating.</p> <p>The policy includes the need to ensure the more efficient use of energy in new development in the town centre as well as ensuring development respects the character of landmark buildings.</p>																	
<p><b>Recommendations and potential for significant impacts</b></p> <p>The policy should help deliver more sustainable development in this area. This includes the delivery of new community uses and urban greening related to the existing open space. In the longer term opportunity to have a more detailed policy for delivery of development on the site to ensure it is delivered in a way that is compatible with the long-term vision for this part of the Central Area (this could be as part of any future Local Plan).</p>																	
<p><b>Policy Trail:</b></p> <p><b>Update to Preferred Approach 2015:</b> The reference to a new multi-storey car park has been removed from the policy as this no longer reflects the parking strategy of SBC. The area is also no longer identified for educational uses as these should be concentrated in the Elmer Square area than this location is better suited for residential development.</p> <p>Some policy specifics have been removed from the Opportunity Site that could have had beneficial impacts for sustainable development, including the delivery of new cycle focused services.</p> <p>The policy also now includes more on energy efficiency.</p> <p><b>Update to Revised Proposed Submission Version 2016:</b> Similar to Policy DS3, criterion has been added to ensure landmark buildings are conserved contributing to sustainability principles by protecting the</p>																	

character of the settlement.

Policy specifics have been included regarding access and public realm improvements.

PA5.1 and OS5 have been removed as they have not been promoted during the SCAAP process and therefore there is no clear evidence that they will come forward for development during the plan period to 2021. ..

**Iteration of policy following SA of the draft Revised Proposed Submission:**

Additional detail has been added in to the policy to reflect the potential for community infrastructure in this location, specifically a doctor and dentist.

Additional detail has been added in on urban greening projects and reference to the Streetscape Manual.

### 3.8 Clifftown Policy Area

Clifftown Policy Area																	
<p><b>Policy Summary</b></p> <p>These policies set the general vision for the continued enhancement of Clifftown and its role as a cultural quarter for Southend, with specific policy principles set out for the redevelopment of identified sites.</p>																	
<p><b>Relationship of policy with sustainable development</b></p> <p>The area has the potential to bring benefits to the Southend tourism and visitor economy, as the area is of distinctive built environment character, although this could be enhanced. The area provides a link to the main seafront areas to the west of the pier. Continued built environment enhancements and a diversification of cultural and retail options are an important part of the Southend as a high quality visitor destination.</p> <p>Securing good walking and cycling links to the town centre could help to encourage non-car access to the Central Area from those who live to the east. This will have benefits in reducing town centre congestion and the adverse impacts this causes.</p>																	
<b>PA6: Clifftown Policy Area Development Principles</b>																	
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3	
●	-	-	-	?	-	●	●	?	-	-	-	-	?	●	?	?	
<p><b>Sustainability appraisal summary and comment</b></p> <p>The policy supports the future role of Clifftown as an area of strong cultural identity. The policy includes criteria to make sure that the historic and heritage character of the area is taken into account in new development. There is also the need to restore areas that have become degraded.</p> <p>Other improvements are to the retail circuits in the area, linking to the Central Station, Cliff Gardens and Pier Hill. This will help support the economy through tourism and visitor spend. Shops in this area are more likely to be independent and therefore these retail circuits will aid the local economy especially.</p> <p>Regenerating the Empire Theatre will also add to the cultural character of the area, including as set out in policy for uses such as art galleries or performance spaces. Central House is identified for redevelopment, for more information it could be included as a separate allocation, especially as this is identified as the site of a possible tall building.</p> <p>The policy includes requirements for urban greening, with potential benefits for biodiversity and the quality of the built environment.</p> <p>Specific recognition is given in the policy on the importance of protecting both designated and non-designated heritage assets in the area, which should help in conserving the built heritage of Clifftown, one of its principal assets.</p>																	
<p><b>Recommendations and potential for significant impacts</b></p> <p>The policy should help deliver more sustainable development in this area.</p>																	
<p><b>Policy Trail:</b></p> <p><b>Update to Preferred Approach 2015:</b> The policy was previously split over a heritage policy (HE1) and one on development principles. The unification into one policy makes understanding the aspirations for the area clearer. Education has also been removed as a potential part of the mix of uses, which should help ensure Elmer Square remains the focus for education. The two car parks are no longer identified as sites for development within the plan period as there is insufficient certainty that they would be available for the development.</p> <p><b>Update to Revised Proposed Submission Version 2016:</b> In line with Policy DS2, the policy includes a principle to conserve and enhance heritage assets, Conservation Areas and listed buildings.</p>																	

The policy includes further public realm improvements which seek to enhance the quality of the built environment in line with sustainability principles.

OS16 and OS17 have been removed as they were not promoted during the SCAAP process and therefore there is no evidence they are deliverable by the end of the plan period 2021.

**Iteration of policy following SA of the draft Revised Proposed Submission:**

Additional detail has been added in on urban greening projects and reference to the Streetscape Manual.

### 3.9 Tylers Policy Area

#### PA7 Tylers Policy Area

##### Policy Summary

These policies set out the principles for developing the south eastern side of the High Street. The area includes a site allocation.

##### Relationship of policy with sustainable development

This part of the Central Area currently contains a mix of uses that includes a large surface car park as well as residential development and The Royals shopping centre. The area provides the potential to link the town and end of the High Street with the Eastern Esplanade and create improved retail circuits at the bottom of the High Street.

Changes to the car park and the quality of walking routes have the potential to enhance the built environment quality in the area. These improvements will make the area more attractive to shoppers and visitors, as well as make it a more attractive place to live. Fewer cars on the streets will help improve the attractiveness of the area for residents and has the potential to deliver improved road safety for existing residents.

Re-use of the car park site will allow the re-use of this prominent site, although it is likely that car parking will be re-provided with no net reduction.

##### PA7: Tylers Policy Area Development Principles

SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
•	?	-	?	?	?	?	•	?	-	•	-	-	?	•	?	•

##### Sustainability appraisal summary and comment

The policy sets out the desired changes to the area that would help enhance the built environment quality of this area and make more efficient use of land. The policy promotes mixed-use including active uses on the ground floor and energy efficiency measures where appropriate. The development of the area to include a travel centre could enhance facilities and connectivity as well as releasing the existing Travel Centre site for possibly new open space, with benefits for health of community and possibly biodiversity if delivered.

Of particular importance will be bringing built environment and road safety enhancements to Chichester Avenue that currently provides an unattractive link route to the east of the High Street. Also, improved crossing points over Queensway will help improve the connectivity of the area to the wider residential areas to the east.

The policy sets out a number of road improvement schemes that may be dependent on the car park being redeveloped, such as creation of 'home-zones' or mixed-mode routes. This will help make the area more attractive for visitors and residents.

Specific schemes have the potential to deliver economic benefits of to the area, including expansion of retail units at the south of the High Street. Design will need to be of a high quality to complement the existing area and the importance of the location for attracting visitors and tourists.

The intention is to see the area made much more permeable to the public. This change will help enhance connectivity in the area, providing clearer links to the seafront and development proposals as part of the 'Central Seafront'.

PA7.1: Tylers Avenue																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
●	?	-	-	?	-	-	?	-	-	●	-	-	-	●	?	●
<p><b>Sustainability appraisal summary and comment</b></p> <p>There is an intention to secure the re-use of the Tylers car park site. However, all car parking would need to be re-provided either on or off site, with implications for the use of other sites.</p> <p>The site is to provide a mix of uses including active frontages and residential uses with the potential for office use making more suitable and desired use of the land. The development is to complement its surroundings and improve linkages to surrounding areas.</p> <p>The development of the area to include a travel centre could enhance transport facilities and connectivity to the town centre.</p> <p>The policy may have benefits for the economy not only from improved built environment quality but also from the provision of new office and retail development.</p>																
<p><b>Recommendations and potential for significant impacts</b></p> <p>The policy should help deliver more sustainable development in this area.</p>																
<p><b>Policy Trail:</b></p> <p><b>Update to Preferred Approach 2015:</b> The policy has had some amendments from the previous version. This includes removing the potential of the site for Higher and Further education uses, which may be beneficial in focusing these uses on Elmer Square. Reference has also been removed to specific improvements to St John’s churchyard, although general improvements related to the new urban park are included. Specifics relating to flooding have been removed to a generic policy for the area.</p> <p><b>Update to Revised Proposed Submission Version 2016:</b> The policy includes a principle to improve pedestrian accessibility, enhance passenger transport facilities and public realm enhancements to improving connectivity to the town centre.</p> <p>OS6 is now referred to as PA7.1. The site boundary now includes the Travel Centre to the south of the original boundary and the number of indicative dwellings has increased.</p>																
<p><b>Iteration of policy following SA of the draft Revised Proposed Submission:</b></p> <p>Additional detail has been on application of the Streetscape Manual.</p>																

### 3.11 Central Seafront

Central Seafront Policy Area																
<p><b>Policy Summary</b></p> <p>These policies set out the development principles and Opportunity Site development management considerations for the Central Seafront area. There are also specific policies relating to managing the special qualities of the area.</p>																
<p><b>Relationship of policy with sustainable development</b></p> <p>The central Seafront is one of Southend's major assets. The area makes a significant contribution to the Borough's economy as a prime tourism destination, the character and buildings also give the town its distinctive character. Continued change is necessary in this location to keep Southend as a modern tourist destination for day trippers and with an aim of increasing overnight visitors for leisure and business. Change needs to take place in a way that protects and enhances the best of its historic and heritage, and redeveloping those areas that have become degraded or were poorly planned.</p> <p>The quality of the Seafront also raises the potential for it to draw more people into other parts of the Central Area, such as the High Street and Clifftown, aided by access and public realm improvements in these areas.</p> <p>Development in the Seafront area needs to protect the high quality natural/semi-natural environment. The foreshore areas are internationally designated due to their importance for nature conservation, and this asset must be protected for its own sake and the value it gives the area and attractiveness to tourists.</p>																
CS1 Central Seafront Policy Area Development Principles																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
•	?	-	?	•	•	•	•	?	?	?	-	-	?	•	?	•
<p><b>Sustainability appraisal summary and comment</b></p> <p>The policies address many issues of importance to delivering sustainable development. This includes improving the quality of the built environment, protection heritage and biodiversity, improving access and enhancing the potential for the tourism economy.</p> <p>The policies set out what is anticipated from development in the Central Seafront area. The principles set general aspirations for development, with some generic statements of what is desired from development in the area, and some more specific criteria – such as where new access routes are required and defines development sites.</p> <p>Principles in the policy set positive steps for the future of the area that need to be implemented by the Council or other public / community groups and development partner. This includes increasing links to the 'green-grid', improving the Sustrans Route, ongoing City Beach, as well as with the potential to work with the owners of Adventure Island to increase permeability to the seafront.</p> <p>The policy should have positive effects on the economy through helping encouraging tourism and enhancing the character of the area. The importance of maintaining and enhancing the built environment character is identified through several policy principles, including the importance of sea views and the need for new development to respect local Conservation Areas and Listed Buildings.</p> <p>The importance of the seafront as an open space is also identified and principles put in place to protect it as part of the tourism asset, but also recognising the nature conservation importance and connections to the 'green-grid'.</p> <p>Requirements for consideration of decentralised energy in this location may help in reducing carbon emissions from development. In particular solar or wind energy may be well suited to the location.</p>																

CS1.1: Southend Pier																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
-	-	-	-	●	?	?	●	-	-	-	-	-	-	●	?	●
<p><b>Sustainability comment</b></p> <p>This policy sets the principles for the continued enhancement of the Pier. This is an important feature that characterises the town and is an important tourist attraction.</p> <p>The policy principles set out measures to bring general improvements to the offer for visitors at the Pier, although much will need to be funded through working with partners and private business. The changes proposed are likely to be relatively minor and many may not require planning permission.</p> <p>As the Pier extends into the internationally protected nature conservation areas it is essential that changes do not conflict with their nature conservation objectives. New developments, including minor changes, are likely to require appropriate assessment to demonstrate no significant harm. It is very unlikely that development here would be permitted if harm is identified, as it will not fit the 'overriding' need criteria.</p>																
CS1.2: Seaway Car Park																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
?	?	-	●	●	?	?	●	-	-	●	-	-	-	●	?	●
<p><b>Sustainability comment</b></p> <p>This site links well with The Royals, High Street and Tylers Avenue area to the north. The mixed use redevelopment should also help ensure the good use of land and contribute to improved connectivity between the seafront and the rest of the Central Area.</p> <p>As set out in the policy, the redevelopment has great potential to make use of the elevation and the views available of the seafront in this area to encourage visitors to walk up through the site and onward linking to the High Street.</p> <p>The policy makes clear that proposed development should be mixed-use, this can include residential so long as it is part of a scheme that contributes to the visitor economy and cultural value of the area.</p> <p>New buildings on the site, especially new tall buildings, will be highly visible and therefore it essential that they are delivered of a high visual quality and to provide a legacy building for the future. The policy stipulates a 'palette of good quality materials' and this will support sustainability objectives relating to a good quality built environment.</p> <p>Development in this location has great potential to deliver a high quality scheme that improves the character of this part of the seafront to the east of the High Street.</p> <p>The measures proposed in the policy are likely to increase pedestrian movements in this area. North south pedestrian links will help improve access from the seafront to the town centre and encourage more visitors to move between the two, with benefits for the wider town centre.</p> <p>New open space will also benefit the area especially if it has good access to the residential neighbourhoods to the north.</p>																
CS1.3: Marine Plaza																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
?	?	?	●	●	?	?	●	-	?	-	-	-	-	●	?	●
<p><b>Sustainability comment</b></p> <p>This site links well with The Royals, High Street and Tylers Avenue area to the north. There is significant potential for redevelopment of this site to be a major asset for the area by providing high quality housing in a central location. It should also help ensure the good use of land to help meet the housing need of the area and contribute to improved connectivity between the seafront and the rest of the Central Area.</p> <p>As set out in the policy, the redevelopment has great potential to make use of the elevation and the views</p>																



available of the seafront in this area to encourage visitors to walk up through the site.

The site is to provide active frontages with leisure and supporting uses on the ground floor.

New open space will benefit the area especially if it has good access to the residential neighbourhoods to the north.

The site has planning permission.

CS1.4: New Southend Museum																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
?	-	•	?	•	?	?	•	-	-	-	-	-	?	•	•	•

**Sustainability comment**

A new museum of high architectural quality could have benefits for the whole of the Borough through increasing visitors to the town. Any building would need to be of exemplary quality, both in its design and with the potential to also in sustainability construction, as is set out in policy. The policy also includes the importance of high quality new planting in the area to support biodiversity, which will need to a consideration of future development management decisions on the site.

**Recommendations and potential for significant impacts**

This policy should have a largely beneficial impact on securing sustainable development in the Central Seafront.

The policy sets out measures to enhance the visitor economy through public realm, built environment and connectivity improvements in this part of the Central Area. This includes policy for improved visitor provision through the development of new museum and improvements to the pier.

The Seaway Car Park and Marine Plaza improvements also have the potential to significantly improve this area and policy. It may help improve the delivery of sustainable development if a similar set of clear policy criteria was included in the policy to deliver site CS1.2 Marine Plaza as there are for CS1.2 Seaways. The policy does include provisions for measures to provide the environment between the two sites and wayfinding between them, such as signage and seating on the seafront. However, as the Marine Plaza site has planning permission there may be a limited scope for a more fundamental coordination of the two sites, although future development of Seaways should consider the context including any design of Marine Plaza, either as built or in design (as relevant).

**Policy Trail**

**Update to Preferred Approach 2015:** The policy has undergone some changes from previous versions. In particular the principles were formerly part of several distinct and this unified approach much improves the clarity of the message on what Central Seafront development should look like.

Previously, the policy set a requirement of a 'visual appraisal' of all development in this area, which is no longer specified. However, the importance of views is identified in policy and therefore where suitable it will still be necessary for development to show how this has been taken into account in planning applications.

There are no longer specific policies covering the Eastern and Western Esplanades, this change is unlikely to result in a notable change in sustainability effects, although they did add some detail on how development should be delivered in these areas.

Flooding issues are now covered through the generic SCAAP flooding policy.

**Update to Revised Proposed Submission Version 2016:** In line with Policy DS2 and Policy DS3, the policy includes a principle that seeks to conserve landmarks and their setting.

The policy includes additional specifics of how improvement to traffic management will be achieved. These specifics focus on sustainable transport.

Regarding opportunity sites, former CS1.1 has been removed as it has been granted full planning permission but not actively promoted for inclusion in the SCAAP and therefore no evidence of delivery by the end of the plan period. OS10 has been removed as the site will not be delivered before the end of the

plan period in 2021.

The former site OS8 has been divided into two sites separating out CS1.2: Seaway Car Park from CS1.3: Marine Plaza. The leisure element of the former OS8 is to be provided on CS1.2 and the residential element is to be provided on CS1.3. The site boundary has also been amended and there is now a notable separation between the two sites along Marine Parade.

Opportunity Site 9 has been renamed CS1.4.

**Iteration of policy following SA of the draft Revised Proposed Submission:**

CS1 now includes reference to the Streetscape Manual and more on pedestrian and cycling connectivity. There is also additional detail on the ways that street furniture and signage can be used to improved connectivity to the seafront and town centre.

**CS2: Nature Conservation and Biodiversity**

**Policy summary**

The policy sets the criteria that will be used to make sure development in the Central Seafront does not harm the nature conservation assets in the area.

**Relationship of policy with sustainable development**

Protecting the nature conservation value will have benefits for biodiversity as well as helping protect the visual character of the area.

SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
-	-	?	-	-	●	?	-	-	-	-	-	-	-	-	-	?

**Sustainability appraisal summary and comment**

The policy should help in protecting the high quality nature conservation assets in this area, identifying those locations where greening is particularly important and supported by the policies map.

The policy allows for new development that would aid people’s understanding of the nature conservation value of the area. Improved understanding can help in protecting the area and aid visitors’ recognition of how they can help avoid adverse impacts – although all development will need to be subject to assessment and the same tests apply.

**Recommendations and potential for significant impacts**

The policy is compatible with sustainable development and the need to protect the high quality nature conservation assets of the area.

The policy repeats some aspects of national policy and the Habitats Regulations and this may be unnecessary. Depending on the type and scale of development that comes forward in proximity to the foreshore in future there may need to be consideration of future strategies to help avoid increased recreational pressure from multiple sources in this location. However, individual applications will be required to undergo HRA screening that must look at the implications of development individually and in combination.

**Policy Trail:**

**Update to Preferred Approach 2015:** No significant changes.

**Update to Revised Proposed Submission Version 2016:** No significant changes.

**Iteration of policy following SA of the draft Revised Proposed Submission:**

None relevant

## CS3: The Waterfront

### Policy summary

The policy sets the criteria for developing the waterfront area and the mix of uses that need to be accommodated.

### Relationship of policy with sustainable development

The Waterfront is a prime asset for residents and visitors to Southend on Sea, with the need to protect built and natural assets in the area from harm.

SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
-	-	-	-	?	●	-	?	-	-	-	-	-	-	?	-	?

### Sustainability comment

The waterfront area is a community and economic asset for Southend, although it also is internationally important for its nature conservation value. Enhancing the quality of the public realm and peoples' enjoyment of the area can have substantial sustainability benefits for the town. There is much in this policy that should help improve the area, including development of allocated sites and other programmes that will need to be implemented alongside development.

Development must ensure it does not harm the biodiversity assets of the foreshore, as set out in the policy. Especially where development gives rise to an increase in visitor pressure in this area, for example new jetties and slipways.

### Recommendations and potential for significant impacts

This policy should help to achieve more sustainable development in the waterfront area.

Much of this policy will be reliant on the plans and strategies of other parts of the Council and other groups and organisations. For instance through new tourism strategies for the area and investment by private businesses.

### Policy Trail:

**Update to Preferred Approach 2015:** The policy has had minor amendments to remove duplication with other policies.

**Update to Revised Proposed Submission Version 2016:** No changes.

### Iteration of policy following SA of the draft Revised Proposed Submission:

None.

### 3.13 The Victoria Gateway Neighbourhood

Victoria Gateway Neighbourhood																
<p><b>Policy Summary</b></p> <p>This policy addresses the redevelopment potential of the 'Victoria' Gateway. Proposals include a substantial mixed use redevelopment site along Victoria Avenue and a mixed tenure housing development along Baxter Avenue.</p>																
<p><b>Relationship of policy with sustainable development</b></p> <p>There is a substantial potential for comprehensive redevelopment of this part of the town. Redevelopment will make better use of the limited available land resources in the Borough and can help create a new sustainable mixed use community. There is potential for a new sustainable residential neighbourhood with good links to the town centre, as well as good public transport access to a wider area. New development should also continue to promote the establishment of good pedestrian and cycle links to the town centre through enhancing Victoria Avenue.</p> <p>Development will also help bring improvements to the built environment by removing poor quality office space, some of which is long-term vacant and is disrepair.</p> <p>As part of making new communities it will be important to ensure there are good access and sufficient capacity in accessible community facilities as set out in policy.</p> <p>New development in this area has the potential to enhance the historic heritage value of the Prittlewell conservation area, which has become degraded in some parts. Other heritage should be protected such as the brick built buildings of the water board.</p>																
Policy PA8: Victoria Gateway Neighbourhood Development Principles																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
•	?	?	-	•	?	?	•	?	-	•	-	-	?	•	•	•
<p><b>Sustainability comment</b></p> <p>The policy contains many principles that should have beneficial effects for sustainable development. There is a specific proposal to bring enhancements to the built environment.</p> <p>The policy also contains provisions to improve the transport and travel access through the area, including integrating Victoria Avenue with the surrounding area through improved pedestrian and cycle links.</p> <p>The role of new open space in this area to help mitigate visitor pressure on the foreshore is referred to in policy, including links with the 'green-grid', this should have a positive relationship with sustainable development objectives relating to biodiversity.</p> <p>The policy includes the need to deliver more community facilities in the area, with examples of the suitable types set out in policy, in particular a new school. There is also recognition of the importance of the area for cultural facilities, such as the arts centre and uses at the former Water Board site.</p> <p>As well providing employment as part of the neighbourhood redevelopment in this location could have benefits to the provision of new modern offices in other parts of the centre. This may be achieved by removing a poor quality office stock that hinders the office development market.</p>																

PA8.1: Victoria Avenue Office Area																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
●	●	?	●	●	?	-	●	?	?	●	-	?	?	●	●	●
<p><b>Sustainability Comment</b></p> <p>This is an area allocated for comprehensive renewal. A design brief prepared for the area would help to ensure that it is delivered in a coordinated way.</p> <p>There are many aspects of the delivery of the site that are compatible with sustainable development. Redevelopment will see this area of under-occupied land in central Southend bought back into use. In Southend making the most of available land is essential as the urbanised area almost reaches Borough boundaries in all directions. The main aim is to create a more sustainable community in this area, integrating residential, offices, community facilities and open space.</p> <p>At the moment planning applications and permitted development change of use for the site are being progressed for individual plots, this raises the risk of disjointed development in the Opportunity Site that does not achieve the improvements needed for the area and loses opportunities for sustainable development. To help overcome this, consideration could be given to implementing measures that remove permitted development rights for the area, which allow change of use from office to residential. This could stop the piecemeal conversion and allow a more considered policy led approach to the form and type of development, which is needed to help deliver development as a cohesive whole with a joined up approach to issues such as open space and visual appearance.</p> <p>There is a need to ensure that the supply of office space in Southend, particular in the central area meets demand. The central area is a more sustainable location for this type of high trip generating use, as more commuting trips can be made by sustainable transport modes. Peripheral office development and business parks are much more likely to increase car use and congestion on local roads, as many employees will be reliant on driving to work due to lack of public transport services and further distances to walk or cycle. Therefore, although there may be a need for a reduction in the overall floorspace new and existing office space needs to be provided to help meet modern business needs, for instance open plan and flexible space.</p>																
PA8.2: Baxter Avenue																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
-	●	-	?	?	?	-	?	-	-	●	-	-	-	-	●	-
<p><b>Sustainability Commentary</b></p> <p>This is a development site is allocated for regeneration to provide high quality mixed tenure residential development. This is to include new sheltered and affordable accommodation. Such development will ensure all types of housing are available in the area and all residents will have a positive relationship with the housing sustainability objective.</p> <p>Policy also requires development to incorporate open space, urban greening and sustainability measures having a positive impact on biodiversity and providing areas for residents' relaxation.</p> <p>The site is to provide pedestrian access and linkages between Victoria, Baxter and Boston Avenue providing greater connectivity throughout the area and reducing car dependency.</p>																
<p><b>Recommendations and potential for significant impacts</b></p> <p>The policy should help deliver more sustainable development in this area.</p> <p>Consideration could be given, in policy or through other mechanisms, to removing permitted development rights at Victoria Avenue to prevent piecemeal change of use undermining a policy led approach to sustainable regeneration. The policy refers to the potential for a masterplan.</p> <p>Availability of office space here and in the wider Central Area should be monitored to ensure there is a good supply of the type of space that is demanded by the market, even if there is a loss of overall space.</p>																

## **Policy Trail**

**Update to Preferred Approach 2015:** The policy principles relating to the area has had some iteration to reduce duplication with other parts of the SCAAP. The policy includes a new principle relating to energy efficiency and decentralised supply. The archaeological potential of the site is also recognised in the revised policy.

OS11 has undergone a simplification and removal of some elements that may have had additional beneficial sustainability benefits. For example, there is no commitment to prepare a Supplementary Planning Document for the area and no new school is included as part of the site.

**Update to Revised Proposed Submission Version 2016:** The policy aims remain the same as the earlier version however there have been changes to the Opportunity Sites and Policy Areas. However, the policy now recognises potential for the area to be a focus of cultural activities in association with the existing Beecroft Centre and Central Museum Building.

The former Opportunity Area 11: Victoria Avenue Office Area has been replaced by PA8.1: Victoria Avenue.

PA8.1 - 8.8, OS12 and OS13 have been removed as they have not been promoted within the SCAAP and therefore there is no evidence that they would be deliverable by 2021 and the end of the plan period.

The policy includes a new Opportunity Site, PA8.2: Baxter Square, which is to provide 500 high quality mixed tenure dwellings to west of PA8.1. The scheme will also provide open space, urban greening, pedestrian access and linkages.

### **Iteration of policy following SA of the draft Revised Proposed Submission:**

Additional detail has been added in on urban greening projects and reference to the Streetscape Manual. There is additional reference to the importance of the area for community and cultural development.

### 3.15 Sutton Gateway Neighbourhood Policy Area

PA9 Sutton Gateway Neighbourhood																
<b>Policy Summary</b>																
This policy addresses the redevelopment potential of the Sutton Gateway Neighbourhood, currently mix use area including a substantial residential area as well as large employment areas. The proposal site is an area of low quality commercial buildings, including sites with planning permission for redevelopment.																
<b>Relationship of policy with sustainable development</b>																
This is one of the 'gateway' neighbourhoods to Southend Central Area. The area contains a mix of uses that do not necessarily work well together and have resulted in a degraded townscape.																
There are opportunities through the upgrading of some sites to help improve the overall quality of this gateway neighbourhood. This will involve making sure the economic and residential uses do not conflict, yet retaining both these uses in the area.																
There is also the potential to help overcome the access barriers to the town centre. This will help the residential areas in the gateway feel more part of Southend's centre, as currently the area is cut-off to the west and south by the railway line and road respectively.																
PA9: Sutton Gateway Neighbourhood Policy Area Development Principles																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
●	●	?	-	●	-	-	?	-	-	-	-	-	-	?	-	?
<b>Sustainability appraisal summary and comment</b>																
The policy sets out succinctly the principles that will guide the redevelopment of the area. The majority of changes related to the two allocated sites as well that need to improve access through the area, including changes to Sutton Road and enhancements to the public realm along Short Street to Queensway.																
PA9.1: Sutton Road																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
?	●	-	-	?	●	-	●	-	-	●	-	-	-	?	?	?
<b>Sustainability appraisal summary and comment</b>																
This is a linear allocation that covers the business and industrial units facing onto Sutton Road. Many of these units are dilapidated and / or vacant. The units face onto a largely residential neighbourhood and many of the buildings on the opposite side of the road contain shops.																
The policy promotes redevelopment of this area for housing. This will involve the loss of existing employment use, although Employment Land Review has shown that this is surplus to current needs in the Borough, and its loss will not result in the overall loss of jobs.																
There is the possibility that redevelopment may result in the loss of some local services in the immediate area.																
New housing should respect the context of the area and actively engage with properties on the opposite side of the road. However, new development could be of a higher quality than some of the more recent nearby development as a way of enhancing the built environment character.																
The delivery of this site as a unified whole could be improved through the development of a masterplan or development brief.																

PA9.2: Guildford Road																
SP1	SP2	SP3	SP4	SP5	EP1	EP2	EP3	NR1	NR2	NR3	NR4	NR5	NR6	EG1	EG2	EG3
-	●	-	-	?	?	-	●	-	-	●	-	-	-	●	-	●
<p><b>Sustainability appraisal summary and comment</b></p> <p>The policy relates to the redevelopment of the site to replace a convenience store. The policy aims to enhance the Secondary Shopping offer as well as providing residential accommodation. The allocated site will need to accommodate amenity space and urban greening. Development at this site will be expected to incorporate open space, urban greening and sustainability measures having a positive impact on biodiversity and providing areas for residents' relaxation. The proposed uses will increase footfall through the area and contribute to the economy of the area.</p> <p>Development at the site must retain the façade of the current building on Sutton Road that will help protect the built environment character.</p>																
<p><b>Recommendations and potential for significant impacts</b></p> <p>The policy should help deliver more sustainable development in this area.</p> <p>The employment land review identifies employment land in this area is surplus and could be better used for housing. However, to protect the local economy consideration should be given to existing tenants and availability of other sites at a similar cost. The Sutton Road site could include development of new small scale business premises.</p> <p>As set out in policy the greening of this site could achieve benefits for biodiversity, although scope will be limited to landscaping as there is no potential for new open space in the neighbourhood.</p> <p>Principles of development set out in a development brief or clear criteria could help ensure development at PA9.1 takes place in a co-ordinated and cohesive way, making the most of opportunities for sustainable development.</p>																
<p><b>Policy Trail:</b></p> <p><b>Update to Preferred Approach 2015:</b> The policies have been amended to remove the former B&amp;Q site, as this site is now in full use, although the potential for development here is retained in the policy. The Short Street car park site and the Coleman Street site are now part of the Queensway Policy Area as they provide a better fit with that area. The policy includes a principle for energy efficiency and potential for decentralised supply.</p> <p><b>Update to Revised Proposed Submission Version 2016:</b> Similar to Policy DS3, criteria have been added to conserve existing landmarks including views, character and setting.</p> <p>Further public realm improvements in the form of public art provisions have also been added.</p> <p>OS14 is now referred to as PA9.1 and makes provision for a reduced number of dwellings as well as A1 and D1 uses.</p> <p>PA9.1, PA9.2 and PA9.3 have been removed as they have not been promoted in the SCAAP process and therefore there is no evidence that they could be delivered within the plan period by 2021.</p> <p>A new PA9.2 has been included which is situated to the south of the Sutton Gateway Neighbourhood Area. The policy area makes provision to redevelop the site for 50 dwellings and a convenience store</p>																
<p><b>Iteration of policy following SA of the draft Revised Proposed Submission:</b></p> <p>Additional detail has been added in on application of the Streetscape Manual.</p> <p>Reference to a new area of public open space has been removed from the policy that may</p>																



## **Appendix E Meeting the requirements of the Strategic Environmental Assessment Regulations**

## Meeting the requirements of the Strategic Environmental Assessment Regulations

In order to satisfy the Strategic Environmental Assessment Regulations it is necessary for the SA report to fulfil certain requirements. Table 1 shows how these requirements are being met through this SA report, both as part of the main text and thorough appendices. For further detail on some matters, such as the full baseline, the SA reports from other parts of the LDF and original scoping will also provide a useful resource.

**Table 1: Requirements of the SEA Regulations and how they are met through the SA report**

<b>SEA Requirements</b>	<b>Covered in the SA report at:</b>
a) An outline of the contents, main objectives of the plan or programme and relationships with other relevant plans and programmes.	<b>Section 2 Appendix A</b>
b) The relevant aspects of the current state of the environment and the likely evolution thereof without implementation of the plan or programme	<b>Section 2 Appendix A LDF Scoping Report</b>
c) The environmental characteristics of areas likely to be significantly affected.	<b>Section 4 Appendix D</b>
d) Any existing environmental problems which are relevant to the plan or programme including, in particular, those relating to any areas of a particular environmental importance, such as areas designated pursuant to Directives 79/409/EEC and 92/43/EEC.	<b>Section 2 Appendix A</b>
e) The environmental protection objectives, established at international, community or national level which are relevant to the plan or programme and the way those objectives and any environmental considerations have been taken into account during its preparation.	<b>Appendix A</b>
f) The likely significant effects on the environment, including on issues such as: biodiversity; population; human health; fauna; flora; soil; water; air; climatic factors; material assets; cultural heritage, including architectural and archaeological heritage; landscape; and the interrelationship between the above factors. These effects should include secondary, cumulative, synergistic, short, medium and long term, permanent and temporary, positive and negative	<b>Section 4 Appendix D</b>
g) The measures envisaged to prevent, reduce and, as fully as possible, offset any significant adverse effects on the environment of implementing the plan or programme.	<b>Section 4 Appendix D</b>
h) An outline of the reasons for selecting the alternatives dealt with and a description of how the assessment was undertaken, including any difficulties (such as technical deficiencies or lack of know-how) encountered in compiling the required information.	<b>Section 3 (SA report on Issues and Options, March 2010) Appendix B Appendix D Appendix F</b>
i) A description of measures envisaged concerning monitoring in accordance with Article 10.	<b>Section 5 Appendix G</b>
j) A non-technical summary of the information provided under the above headings.	<b>Non-technical summary</b>

## **Appendix F    Issues and Options    SCAAP – SA recommendations**

## **1.1 Sustainability appraisal recommendations of the Issues and Options SCAAP**

- 1.1.1 The SA at that stage found that there was much in the SCAAP that had the potential to have a beneficial effect on sustainable development and the Central Area has an important role to play in in the sustainable development of the wider Borough, as a focus for employment, tourism and retail.
- 1.1.2 In most instances recommendations that were made at Issues and Options SA were taken into account by the plan makers in preparing the Proposed Submission version of the SCAAP. However, this was not always the case, the paragraphs 3.4.11 to 3.4.44 show recommendations made, as well how these have been addressed in the current Preferred Approach SA. The SA Report at Proposed Submission stage includes additional recommendations that arose as a result of the greater detail in this version of the SCAAP compared to Issues and Options.
- 1.1.3 This section of the SA Report sets out the recommendations made through the previous versions of the SA of the SCAAP, with a note on how they have been addressed through the current stage of the SCAAP, where necessary.

### ***Transport and movement***

- 1.1.4 To successfully achieve a modal shift away from car use, the SA recommended a need to ensure transport, movement and parking strategy presents a proactive and joined up approach to managing traffic in the town centre. Without this the regeneration of central Southend could be adversely affected by increasing congestion, with negative health and environmental impacts.
- 1.1.5** These matters were carried through into the SCAAP with a strategy that included improvements to walking and cycling routes as well as maintenance of car parking.

### ***Residential development***

- 1.1.6 Central Southend has a significant role to play in delivering new residential development for the Borough. The requirement is set through the Core Strategy for Southend.
- 1.1.7 The SA made recommendations that the SCAAP included specific information on housing delivery and the locations for growth. Information on this has been incorporated into relevant site area policies of the SCAAP with a summary provided of yield and timeframe for delivery at Table 5. Each area policy also shows the status of the site (i.e. under construction, expired application or new allocation). This clarity aids understanding of the role of sites in delivering the overall housing needs and will help in monitoring these sites so that SCAAP or Local Plan review can identify sites that are stalling and take remedial action as necessary. .
- 1.1.8 The SA also recommended seeking more equitable access to in housing the SCAAP could through policies that go beyond Core Strategy affordable housing targets. However, SBC believes it is more suitable to ensure the same approach to delivering affordable housing throughout the plan area without separate targets for the Central Area.
- 1.1.9 The SA also recommended that the SCAAP will also need to be clear on where and how the needs of new and existing residents will be met, through provision of community services,

such as health centres, community space and schools. This is essential to deliver health and wellbeing in these communities who live in central Southend.

- 1.1.10 **Preferred Approach 2015:** These measures have not been taken forward, although a new health centre has been delivered and policies include the need to provide new education facilities, although this is not site specific.

#### ***The built environment***

- 1.1.11 The SA identified the beneficial effects for sustainable development of the SCAAP Issues and Options version focus on regeneration and improvements to the built environment. This will have beneficial sustainability impacts related to improving the image of the centre for the benefit of residents and the economy.
- 1.1.12 The SA identified that it may be necessary for the SCAAP to contain specific design guidance polices for the Central Area. This will help ensure development creates a unified place, even if separate Quarters (now known as 'Opportunity Sites') have their own distinctive qualities.
- 1.1.13 The SA recommended design guidance may be particularly necessary to avoid adverse impacts from tall buildings.
- 1.1.14 **Preferred Approach 2015:** The principles of the Policy Area policies include some detail of public realm improvement to the Central Area. Design and in particular measure to manage tall buildings are covered in the DMD.

#### ***Leisure and recreation***

- 1.1.15 The SA identified that leisure and tourism are essential components of the economy of Southend. The SA notes that of particular importance will be securing higher visitor spend in the Southend, in particular Central Southend where much leisure and tourism development is located.
- 1.1.16 The SA identified the SCAAP should give consideration to the role of new hotels or conference facilities, including possible locations for development, to increase the number of overnight stays in the town.
- 1.1.17 The SA recommended that the SCAAP seafront and waterfront leisure and recreation development will need to take into account the potential conflict of uses. This is because there are likely to be different demands on the area from areas for quiet enjoyment of the natural environment to places for active water-sports. For everyone's enjoyment different uses will need to be managed to ensure high quality leisure opportunities for all.
- 1.1.18 The SA also identifies the potential for waterfront tourism and leisure to conflict with the nature conservation interest, that will need to be managed to ensure no harm comes to internationally designated sites.
- 1.1.19 **Preferred Approach 2015:** There is no specific policy covering new hotels and conference facilities in the Central Area, although there is a generic policy in the DMD. The SCAAP does recognise the potential conflict of interest of waterfront development and includes a policy to ensure the protection of the internationally designated sites.

#### ***Education and Culture***

- 1.1.20 The SA identified that the SCAAP could consider making it a requirement for larger new employment developments in the town centre to contribute to training associated with the university, to improve the skills of local residents and access to newly created employment.
- 1.1.21 The SA recommendations note that it may be necessary to consider the capacity of existing schools in the Central Area to ensure all new and existing resident children of the area have access to education. However, information provided by SBC note that there should be capacity in existing schools until 2021.
- 1.1.22 The SA noted that to ensure the best use of land a mix of development in the Central Area it is important to ensure a mix of uses, but educational uses may be best focused on Elmer Square.
- 1.1.23 **Preferred Approach 2015:** Although the SCAAP supports growth of the university there are no policies that specifically link this development with the wider economic growth of the Central Area. The need for schools is acknowledged in the SCAAP with Policy Areas identified that may be suitable. However, no commitment or specific sites are identified for new schools as capacity is identified to 2021 (the end of the plan period).

#### ***Employment and retail***

- 1.1.24 The SA identified that the SCAAP will need to ensure proposals are in place to meet the job growth requirements set through the Core Strategy. This will need to include premises for a range of business types from workshops, live/work units to larger office space. As the SA of the Issues and Options SCAAP was completed prior to the Employment Land Review (ELR) (published 2010) the SA recommended that the findings of the ELR were taken into account in moving forward with the SCAAP, including permitting change of uses where existing uses could be proven surplus.
- 1.1.25 The SA of the subsequent SCAAP shows a clear intention to provide for economic growth in the Central Area. However, there is a need to make sure that existing office and business space is not lost in favour of other uses, such as residential or education use. A quality office provision needs to be maintained in the town centre. Existing land that is currently in employment use should not be lost if there is an undersupply.
- 1.1.26 The Central Area is the most sustainable place for high trip generating office uses, based on transport and accessibility considerations. Also, other employment such as small industrial uses is an important source of local jobs and local services. Land availability in the Borough is limited, raising the importance of protecting what resources there are.
- 1.1.27 **Preferred Approach 2015:** The SCAAP maintains Central Southend as a primary location for new office development. However, since the previous version of the SCAAP the London Southend Airport AAP has been adopted are new modern office developments to be built in the north of the Borough that could compete with the Central Area. The SCAAP identifies suitable sites for office development throughout the Central Area, although focused more in the north and along the High Street and Victoria Avenue. The SCAAP has taken into account the ELR, which confirms the potential to deliver mixed use on former office and commercial uses at Sutton and Victoria Avenue.

#### ***Sustainable construction***

- 1.1.28 To reduce natural resource consumption created by the large scale of proposed restoration and redevelopment the SA suggested the potential for an area wide energy strategy to be prepared for the SCAAP. The SA also suggests the SCAAP consider the inclusion of targets for carbon reduction from new development and sustainable construction standards that go beyond national policy, subject to viability. Consideration could also be given to the potential for district-wide low carbon energy, e.g. combined heat and power.
- 1.1.29 The SA recommended that there could be greater consistency in the SCAAP in the way flood issues are managed. The control of flood is covered through several different policies in the LDF. Some sites' development principles policies refer to the need to manage surface water flooding and others do not. For sites where flood risk is not mentioned it is not clear if this is because there is little risk there, or an omission to policy. This may require clarification in the SCAAP.
- 1.1.30 **Preferred Approach 2015:** Energy efficiency and lower carbon energy is identified as important for development in the Policy Areas in the SCAAP. However, no additional targets are set for any sites.
- 1.1.31 A new single flood risk policy for the Central Seafront has been prepared.

#### ***Natural environment and open space***

- 1.1.32 The SA identified that issues related to protecting the natural environmental are absent from the SCAAP at the Issues and Options version. It is essential that new development does not harm the high quality internationally designated nature conservation sites that exist on the Southend foreshore and sandflats. The SCAAP will need to be able to demonstrate, through a Habitats Regulation Assessment how the potential for impacts has been assessed and how any impacts will be mitigated against (for the SCAAP as a whole and by individual applications). Possible impact routes include direct disturbance, storm water or waste water outfall, and coastal squeeze caused by sea defences.
- 1.1.33 Proposals or policies for landscaping, new parks and new planting can have sustainability benefits of enhancing the urban biodiversity, but also can help create shade within heavily developed areas essential in combating the urban heat impacts associated with global warming.
- 1.1.34 In providing new open space in the urban environment emphasis should be put on providing soft landscape rather than hard landscaping.
- 1.1.35 **Preferred Approach 2015:** The SCAAP addresses Habitats Regulations requirements as well as containing policy principles to improve urban greening and new planting in the Central Area.

#### ***Implementation***

- 1.1.36 In the SA of the Proposed Submission stage the SA Report that although the majority of SCAAP objectives, policies and proposals are highly compatible with achieving sustainable development, there were queries over the clarity of the SCAAP and therefore its usability.
- 1.1.37 The principle focus of recommendations was on the need to avoid repetition with the SCAAP and with other parts of the Southend LDF, for a more succinct user friendly document that sets out clear messages. The SA Report suggested simplification of policy for:

- Mixed-mode and shared priority routes;
- Historic environment;
- Protection of frontages / visually active frontages
- The need to deliver regeneration and new development in some locations is repeated between the policies and supporting text, for example areas such as Queensway and Victoria Circus that are covered in several policies;
- The Central Seafront policies break from the style of other policies and do not fit that easily within the plan.
- Some policies contain specific reference to the need to manage flooding on the site, whereas others do not, in addition to a generic flooding policy.
- All of the policies on the development quarters are repetitions with a design policy for each repeating elements of generic policies and could be combined.
- Repetition of development management policies and criteria.

1.1.38 **Preferred Approach 2015:** The SCAAP has been substantially simplified, reducing duplication within the document itself and also with other policies of the LDF. This has been aided by the adoption of the DMD that includes a standard set for policies for managing development throughout the Borough.



## **Appendix G Initial Proposed Monitoring Framework**

## Appendix G: Indicative Monitoring Framework

Concern	Explanation and desirable direction of change	Objectives	Potential indicators*
<b>Social progress which recognises the needs of everyone</b>			
Accessibility	<ul style="list-style-type: none"> <li>enable all to have similar and sufficient levels of access to services, facilities and opportunities</li> </ul>	<ul style="list-style-type: none"> <li>maintain Southend Central Area as the centre for all services, as the most accessible location</li> <li>improve accessibility to the town centre</li> <li>improvement in public transport accessibility along the entire length of the seafront</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>total number and location of residential units built within 30 minutes public transport time of key community facilities (CS CP3)</li> <li>total number of key transport infrastructure schemes completed within the plan period which improve accessibility and sustainable transport provision (CS CP3)</li> </ul> <p>Suggested indicators:</p> <ul style="list-style-type: none"> <li>travel to work mode</li> <li>bus use numbers</li> </ul>
Housing	<ul style="list-style-type: none"> <li>to provide the opportunity for people to meet their housing need</li> </ul>	<ul style="list-style-type: none"> <li>ensure a sufficient number of dwellings</li> <li>encourage a suitable mix of dwellings, including tenure and size</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>mix of dwelling sizes provided by new developments (DPD CP8.2 (i))</li> <li>total number of affordable dwelling completions by tenure (DPD DM7.1)</li> <li>total number of dwelling units, by size, lost to non-residential uses (CS CP8)</li> <li>total number of dwellings built on previously developed land and green field land per annum (CS CP8)</li> </ul>

Concern	Explanation and desirable direction of change	Objectives	Potential indicators*
Education & Skills	<ul style="list-style-type: none"> <li>■ to assist people in gaining the skills to fulfil their potential and increase their contribution to the community</li> </ul>	<ul style="list-style-type: none"> <li>■ improve accessibility to employment and education facilities</li> <li>■ support continued development of the University campus in the town centre</li> </ul>	<p>Suggested indicators:</p> <ul style="list-style-type: none"> <li>■ primary schools in central area operating consistently at or over capacity</li> <li>■ demand for school places</li> <li>■ student accommodation units given permission</li> <li>■ GP / patient ratio central areas clinics</li> <li>■ Floor area of new community facilities created in Queensway / Victoria Avenue / Sutton Gateway</li> </ul>
Health, safety and security	<ul style="list-style-type: none"> <li>■ to improve overall levels of health, reduce the disparities between different groups and different areas, and reduce crime and the fear of crime</li> </ul>	<ul style="list-style-type: none"> <li>■ improvements to reduce fear of crime in the town centre, especially at night</li> <li>■ improve pedestrian routes through the town centre and seafront to help design out crime</li> </ul>	<p>Suggested indicators:</p> <ul style="list-style-type: none"> <li>■ crime statistics including anti-social behaviour and vandalism</li> <li>■ local public health profile indicators e.g. obesity, coronary disease etc.</li> </ul>

Concern	Explanation and desirable direction of change	Objectives	Potential indicators*
Community	<ul style="list-style-type: none"> <li>■ to value and nurture a sense of belonging in a cohesive community, whilst respecting diversity</li> </ul>	<ul style="list-style-type: none"> <li>■ improve the viability and distinctive character of Southend town centre</li> <li>■ provide public art and improvements to the design of seafront tourist buildings, such as beach huts and kiosks to provide a recognisable unified approach for Southend</li> <li>■ provide new community open spaces in the town centre and seafront</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>■ total amount of additional or improved leisure, health, social care and education/lifelong learning floorspace/facilities developed (CS CP6)</li> </ul> <p>total amount of new provision or qualitative improvements to existing open space facilities (CS CP7)</p> <p>Suggested indicators:</p> <ul style="list-style-type: none"> <li>■ Floor area of new community facilities created in Queensway / Victoria Avenue / Sutton Gateway</li> <li>■ incidents of anti-social behaviour</li> <li>■ number of community events, festivals or markets</li> </ul>
<b>Effective protection of the environment</b>			
Biodiversity	<ul style="list-style-type: none"> <li>■ to maintain and enhance the diversity and abundance of species, and safeguard these areas of significant nature conservation value</li> </ul>	<ul style="list-style-type: none"> <li>■ protect undeveloped parts of the coastline</li> <li>■ protect key habitats directly or indirectly from developments which may harm them</li> <li>■ ensure new development brings enhancements to the built environment where appropriate</li> <li>■ ensure 'appropriate assessment' of all development is carried out where appropriate</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>■ total loss or gain (ha) due to impact of development on: <ul style="list-style-type: none"> <li>- priority habitats by priority species type (CS CP4)</li> </ul> </li> </ul> <p>Suggested Indicators:</p> <ul style="list-style-type: none"> <li>■ projects requiring Habitats Regulations Assessment / developments within European Designated sites (i.e. on the foreshore)</li> <li>■ Status of SSSIs</li> </ul>

Concern	Explanation and desirable direction of change	Objectives	Potential indicators*
Landscape character	<ul style="list-style-type: none"> <li>■ to maintain and enhance the quality and character and cultural significance of the landscape, including the setting and character of the settlement</li> </ul>	<ul style="list-style-type: none"> <li>■ protect undeveloped parts of the coastline</li> <li>■ retain notable features and areas of open space along the coast line</li> <li>■ protect views of the estuary</li> </ul>	<ul style="list-style-type: none"> <li>■ number of schemes that enhance visually important views – sightlines, access, open space and view improved to identified areas (SCAAP Policy DS2: Key Views)</li> <li>■ Developments effecting the foreshore</li> </ul>
Built environment	<ul style="list-style-type: none"> <li>■ to maintain and enhance the quality, safety and distinctiveness of the built environment and the cultural heritage</li> </ul>	<ul style="list-style-type: none"> <li>■ enhance and protect land mark and listed buildings on the sea front</li> <li>■ enhance and protect listed buildings and those of interest in the town centre</li> <li>■ improve urban design quality through policy</li> <li>■ protect existing and create new open and green space</li> </ul>	<p>Existing indicator:</p> <ul style="list-style-type: none"> <li>■ changes in the number of designated heritage assets identified as being at risk as per Essex building at risk register (DPD DM5.1)</li> <li>■ change in the number of Grade I and II Listed Buildings and scheduled monuments (DPD DM5.2)</li> <li>■ number of appropriately located new landmark buildings – increase in the visually important landmark buildings that signal gateways and or improve legibility for visitors using the central area (SCAAP Policy DS3: Landmarks and Landmark Buildings)</li> </ul> <p>Suggested indicators:</p> <ul style="list-style-type: none"> <li>■ important trees lost to development</li> <li>■ locally listed buildings lost to development</li> <li>■ area of new public open space created</li> </ul>
<b>Prudent use of natural resources</b>			
Air	<ul style="list-style-type: none"> <li>■ to reduce all forms of air pollution in the interests of local air quality and the integrity of the atmosphere</li> </ul>	<ul style="list-style-type: none"> <li>■ reduce traffic congestion in the town centre</li> <li>■ encourage freight modal shift and encourage a reduction in emissions of new buildings</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>■ floorspace built to BREEAM Very Good, Excellence or Outstanding (DPD DM2.1)</li> <li>■ traffic counts on defined traffic cordons in Borough (CS CP3)</li> <li>■ No exceedances in NO<sub>2</sub> and PM<sub>10</sub></li> </ul>

Concern	Explanation and desirable direction of change	Objectives	Potential indicators*
Water	<ul style="list-style-type: none"> <li>■ to maintain and improve the quantity and quality of ground, sea and river waters, and minimise the risk of flooding</li> </ul>	<ul style="list-style-type: none"> <li>■ ensure no increased risk of coastal flooding</li> <li>■ acknowledge the risk to water quality from on-shore developments</li> </ul>	<ul style="list-style-type: none"> <li>■ number of planning applications granted contrary to Environment Agency advice on flood risk (DPD CP5.5).</li> <li>■ number of applications refused due to DPD Policy DM6: Seafront in relation to Policy Table 1: (i) upheld at appeal; (ii) dismissed at appeal (DPD DM6.1)</li> <li>■ number of developments incorporating sustainable drainage systems (SuDS) - For all new development, new impermeable areas will be drained by SuDS (SCAAP DS4.1)</li> </ul>
Land	<ul style="list-style-type: none"> <li>■ to use land efficiently, retaining undeveloped land and bringing contaminated land back into use</li> </ul>	<ul style="list-style-type: none"> <li>■ protect undeveloped coastline in the Borough</li> <li>■ encourage development on previously developed land</li> <li>■ encourage high density residential development and mixed use development in the town centre</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>■ total number of dwellings built on previously developed land and green field land per annum (CS CP4)</li> <li>■ amount of contaminated or degraded land brought back into beneficial long-term use (DM CP5.3)</li> </ul>
Soil	<ul style="list-style-type: none"> <li>■ to maintain the resource of productive soil</li> </ul>	<ul style="list-style-type: none"> <li>■ protect productive soil where applicable (little overall impact likely)</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>■ amount of contaminated or degraded land brought back into beneficial long-term use (CS CP5)</li> </ul>
Minerals and other raw materials	<ul style="list-style-type: none"> <li>■ to maintain the stock of minerals and other raw materials</li> </ul>	<ul style="list-style-type: none"> <li>■ minimise use of aggregates for new development (relevance to sea defences)</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>■ capacity within the Borough to produce secondary/recycled aggregates on appropriate sites (CS CP5)</li> </ul>
Energy sources	<ul style="list-style-type: none"> <li>■ to increase the opportunities for energy generation from renewable energy sources, maintain the stock of non-renewable energy sources and make the best use of the materials, energy and effort embodied in the product of previous activity</li> </ul>	<ul style="list-style-type: none"> <li>■ encourage efficient use of energy</li> <li>■ use of more energy from low carbon sources</li> <li>■ encourage decentralised energy supply, including through renewable energy or CHP.</li> </ul>	<p>Existing indicators:</p> <ul style="list-style-type: none"> <li>■ Total number of applications granted for major schemes with renewable energy production technology (CS CP4)</li> </ul>

Concern	Explanation and desirable direction of change	Objectives	Potential indicators*
<b>Maintenance of high and stable levels of economic growth and employment</b>			
Local economy	<ul style="list-style-type: none"> <li>to achieve a clear connection between effort and benefit, by making the most of local strengths, seeking community regeneration, and fostering economic activity</li> </ul>	<ul style="list-style-type: none"> <li>improve the viability and vitality of the town centre as economic hub for the Borough</li> <li>improve the viability and vitality of the seafront as a major and flexible tourist destination</li> <li>identify sites for local business start-ups in accessible locations</li> </ul>	Existing indicators: <ul style="list-style-type: none"> <li>additions/losses of new hotels/visitor accommodation (DPD DM12.1)</li> <li>vacancy in Primary and Secondary shopfronts (DPD DM13.2)</li> <li>vacancy rate within 'Employment Areas' (DPD DM11.2)</li> </ul>
Employment	<ul style="list-style-type: none"> <li>to maintain and enhance employment opportunities matched to the size of the local labour force and its various skills, and to reduce the disparities arising from unequal access to jobs</li> </ul>	<ul style="list-style-type: none"> <li>work to create new jobs in a range of sectors within the Borough</li> <li>work to make the coast a major destination for conferences</li> <li>support a diverse range of businesses premises to meet different needs, as well as supporting existing business clusters</li> </ul>	Existing indicators: <ul style="list-style-type: none"> <li>total number of jobs by sector (DPD CP1.2)</li> <li>amount of employment land lost in employment and regeneration areas (CS CP1)</li> <li>amount of employment land lost to residential development (CS CP1)</li> </ul>
Wealth creation	<ul style="list-style-type: none"> <li>to retain and enhance the factors which are conducive to wealth creation, including personal creativity, infrastructure, accessibility and the local strengths and qualities that are attractive to visitors and investors</li> </ul>	<ul style="list-style-type: none"> <li>contribute to creating attractive environment for business to flourish</li> <li>improve access for all residents to a range of jobs</li> </ul>	Existing indicators: <ul style="list-style-type: none"> <li>net change in business floorspace within Employment Growth Areas (DPD DM11.2)</li> <li>additions/losses of new hotels/visitor accommodation (DPD DM12.1)</li> </ul>

\* These indicators include 'Existing indicators' set out in the Core Strategy (CS), Development Management Plan (DM) and as proposed for the SCAAP. 'Suggested indicators' are those suggested to supplement and fill gaps in existing indicators, subject to available information.

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## Southend Central Area Action Plan (SCAAP)

### Equality Analysis

August 2016



## Southend Central Area Action Plan (SCAAP): Equality Analysis

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### What is an Equality Analysis (EA)?

- An EA provides an assessment of the impact of decisions relating to a policy, service function or restructure on particular customers, residents and staff.

### Why do I need to do an EA?

- It helps to improve the quality of decision making by enabling equality considerations to be taken into account.
- It shows that 'due regard' is being given to the Public Sector Equality duty in decision making – a requirement of the Equality Act 2010.
- It reduces the potential of decisions being challenged, leading to delayed implementation and risk of costly processes like Judicial Review.



## Background Information

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1. **Name of policy, service function or restructure requiring an Equality Analysis:**

Southend Central Area Action Plan (SCAAP)

2. **Department:**

Place

3. **Service Area:**

Transport and Planning

4. **Date Equality Analysis undertaken:**

20 April 2016

5. **Names and roles of staff carrying out the Equality Analysis:**

Name	Role	Service Area
Matthew Thomas	Team Leader	Planning & Building Control
Mark Sheppard	Senior Policy Planner	Planning & Building Control
Michael Sargood	Senior Media Relations Advisor Project Manager	Policy, Engagement and Communication
Ashley Dalton	Project Officer	Major Projects & Strategic Transport Policy
Krithika Ramesh		Major Projects & Strategic Transport Policy

## 6. What are the aims or purpose of the policy, service function or restructure that is subject to the EA?

The Southend Central Area Action Plan (SCAAP), when adopted, will form part of the Southend-on-Sea (hereafter referred to as 'Southend') Local Planning Framework.

Building on more recent resurgence and growth, the SCAAP is considered to be an important catalyst and driver for investment and for the delivery of the remaining proportion of regeneration and growth in the Southend Central Area to meet or exceed Core Strategy targets up to 2021.

The vision for Southend Central Area, which includes the Town Centre and Central Seafront Area, is for it to be a City by the Sea. As a prosperous and thriving regional centre and resort with a rich heritage, it will be an area that is vibrant, safe and hospitable, rich in commerce, learning and culture and an attractive, diverse place where people want to live, work and visit for both day trips and overnight stays.

The aim is to transform the image of Southend through sustainable economic growth, development and social provision, and for it to be independently recognised as a popular location for businesses, residents, students and visitors.

The purpose of this Plan is to give more detailed consideration to how and where regeneration and growth can sustainably be accommodated in the Southend Central Area, including the Town Centre, Central Seafront Area and gateway neighbourhoods.

It contains proposals for Policy Areas and Opportunity Sites aimed at strengthening and transforming Southend Town Centre's sub-regional role as a successful retail and commercial destination, cultural hub and educational centre of excellence, leisure and tourist attraction, and a place to live.

The intention is also to seek to safeguard, conserve and enhance the significant biodiversity, green space and other environmental resources in the area and on the foreshore, as well as to bring about public realm and access improvements.

## 7. What are the main activities relating to the policy, service function or restructure?

To improve and transform the economic vitality, viability and diversity of Southend Central Area by encouraging the establishment of a wider range of homes, businesses and shops whilst providing new opportunities for learning, recreation and leisure.

To promote design excellence and good-quality development proposals and public realm improvements to reinforce a distinctive sense of place, complement new and existing development, and contribute towards the Council's aspirations to establish Southend as a Low Carbon City.

To improve accessibility to the area, ensuring streets, public and green spaces are connected, well-designed and safe, utilising a coordinated palette of materials and furniture that enhance the quality of the streetscape and improve opportunities for walking and cycling, and access to more sustainable modes of transport, such as rail and bus.

To promote a positive approach to public car parking provision that provides public car parking levels that support the vitality of the town centre and access to the seafront by encouraging improvements to the quality of access to parking so that it is convenient, well-signposted, safe and secure.

To appropriately manage and mitigate flood risk and to encourage the provision of Sustainable Drainage Systems and urban greening measures in order to reduce surface water run-off.

To enhance the quality of, and access to, Southend Central Area's green and open spaces, and to improve connectivity between the Town Centre and Central Seafront Area in order to relieve pressure on the Site of Special Scientific Interest (SSSI), Ramsar site, Special Protection Areas (SPA) and other environmental designations, to protect and enhance local biodiversity and nature conservation, and to encourage opportunity for linked trips.

To celebrate and have full regard of Southend's unique heritage assets, such as the Grade II listed Pier, to ensure these assets are appropriately conserved and enhanced and continue to form an integral part of how Southend Central Area is experienced by those who live, work and visit it.

To increase the number and diversity of people living within Southend Central Area and its Gateway Neighbourhoods by building more homes and ensure that living in the area becomes appealing to more families with children, supported by social and community infrastructure that contribute to reducing inequalities in health and wellbeing and support all ages to lead independent lives and live healthy lifestyles.

To encourage the establishment and expansion of businesses in Southend Central Area by identifying, promoting or actively bringing forward suitable sites for development to meet modern user and investor requirements.

To encourage new development, including visitor accommodation, that enhances Southend's leisure and tourism, having particular regard for the assets offered by the Central Seafront Area, in order to attract greater visitor numbers, promote more overnight stays and support growth, complemented by a thriving learning quarter that provides state of the art facilities and well-designed student accommodation.

## 8. Evidence Base

The SCAAP has been prepared in accordance with the Southend Local Development Scheme (LDS), is consistent with the overriding approach as set out by the Core Strategy (2007), and has been prepared in accordance with the Council's Statement of Community Involvement (SCI). It has also been informed by an extensive and robust evidence base.

### Statement of Community Involvement (SCI)

The Statement of Community Involvement (SCI) sets out how the community and other stakeholders will be consulted on planning policy documents and planning applications. It exceeds the minimum legal requirements for consultation set out in the Planning Acts and regulations. Southend Borough Council has applied some general principles to its planning consultations. These being that (i) involvement will be open to all regardless of gender, faith, race, disability, sexuality, age and social deprivation and (ii) we will seek views of interested parties as early as possible. It states that an extensive database has been built up of individuals and organisations wishing to be involved in Southend Borough Council planning consultations. Any individual or organisation wishing to be included may be added to the database at any time. We will contact appropriate organisations and individuals directly by post or electronic means. [http://www.southend.gov.uk/downloads/file/1533/statement\\_of\\_community\\_involvement\\_2013pdf](http://www.southend.gov.uk/downloads/file/1533/statement_of_community_involvement_2013pdf)

When we consult:

- If appropriate and helpful, we may publish a brief consultation statement outlining our intentions before we commence consultation.
- We will contact appropriate organisations and individuals directly by post or electronic means.
- We will include with this initial notification either an internet link to the consultation documents on the SBC website or a CD containing relevant material.



- We will leave consultation documents on display at locations open to the public such as council offices and libraries.
- We may publicise consultations by methods such as leaflets, newsletters, press release, public notice, social media, existing forums, community events, public exhibitions, workshops and joining with other consultations where feasible and appropriate.
- All consultation documents will be published on our website, and this will include all supporting documents. We will provide a hard copy of a document as soon as reasonably practicable after it has been requested. Documents will be sold at a price reflecting publication costs. We will make available comments received or a summary as soon as it is feasible after the close of the consultation.
- We will explain how consultation comments have been taken into account when decisions are taken. Arrangements will be made, on request to make all documents available in alternative formats, including Braille, should this be required. All documents will be made available in other languages on request.

### **Sustainability Appraisal (SA)**

The SCAAP has been fully informed by a Sustainability Appraisal (including Strategic Environmental Assessment), The Sustainability Appraisal is published alongside the SCAAP at each stage for public comment. A Sustainability Appraisal (SA) is an assessment of the potential significant social, environmental and economic impacts of development and forms an integral part of the plan making process. It ensures that all policies and proposals are prepared with a view to contributing to the achievement of sustainable development. The SA forms an iterative process with all stages of the SCAAP being assessed. These appraisals have been used to assess alternative policy options, assist decision-making and identification of the most sustainable policies to take forward. The latest assessment of the sustainability and the potential significant effects of this plan can be found in the SA Report which is available on the councils website at [www.southend.gov.uk/scaap](http://www.southend.gov.uk/scaap)

### **Preferred Approach SCAAP (December 2015)**

This Preferred Approach builds on the Proposed Submission version of the SCAAP (referred to hereafter as the Superseded Proposed Submission version), published in September 2011, and the Issues and Options version, published in June 2010. It has also been informed by representations made to the Issues and Options versions of the Seafront Area Action Plan and Town Centre Action Plan, which preceded the SCAAP, consulted on in 2007, and the Central Area Masterplan (CAM), adopted by the Council in 2008 as corporate policy.

Main consultation stages of the SCAAP:

- Issues and Options version (June 2010)
- (Superseded) Proposed Submission version (September 2011)
- Preferred Approach version (November 2015) [Presenting this Document]

The purpose of the Issues and Options stage (Consultation – 21st June 2010 to 9th August 2010) was to explore the spatial options for Southend Central Area and how detailed policies and proposals could guide regeneration in a sustainable manner. The Council wanted to gather the public and stakeholder’s views about the general direction of proposed policy to meet Southend’s specific issues.

The Borough Council put forward a suggested approach where development areas were referred to as ‘Quarters’, ‘Gateway Neighbourhoods’, and ‘Proposal Sites’ (referred to within this Preferred Approach version of the SCAAP as ‘Policy Areas’ and ‘Opportunity Sites’), as part of the consultation alongside alternative options. The process has provided local people with the opportunity to shape the look and feel of Southend Central Area and its communities, including consideration of environmental and social interests. The responses received at this stage informed the production of the SCAAP policies.

The last key stage, prior to this Preferred Approach version, in the preparation of the SCAAP was the (Superseded) Proposed Submission Version. Consultation took place between 5th September 2011 and 17<sup>th</sup> October 2011. The purpose of this consultation was to allow representations to be made in relation to soundness and legal compliance.

This Preferred Approach version of the SCAAP takes account of:

- Issues raised during the publication of the (Superseded) Proposed Submission version of the SCAAP in 2011, which itself built upon the Issues and Options version of the SCAAP;
- Changes in national policy and guidance, and removal of regional policy;
- Updates to the technical evidence base;
- Relevant progress on sites within the Plan area.

As well as the statutory public consultation described herein, Southend Borough Council also ran a number of public workshops with businesses and the community. These were held at the Park Inn which is located within the Southend central area. The workshops were well attended and provided interested parties with the opportunity to discuss issues with planning officers and feed in their comments. These comments have then been taken into account as the SCAAP has been progressed to the next stage (the Publication Version).

During all stages of public consultation all sections of the community within Southend were given equal opportunity to inform the draft Plan through the Issues and Options stage in June 2010 to the Preferred Options (December 2015) outlined above. Representations received in response to these consultations have been taken into account in this assessment.

The Council collates a range of data to inform Equality Analyses; and on the Council's website there is a section that provides access to key information, data and intelligence about the residents of Southend and the communities they live in, including details of the 2011 Census such as Ward Profiles:- [www.southend.gov.uk/info/200441/southend\\_insights](http://www.southend.gov.uk/info/200441/southend_insights). By understanding local needs in this way, the Council and our partners can ensure that planning policies are prepared that deliver sustainable communities for all.

## 9. Analysis

This section considers the potential impact (positive, negative or neutral) of proposals on key 'protected characteristics' (also known as 'Protected Groups') as outlined in the Equality Act 2010 and any mitigating actions to be taken. In addition, the Council has identified the need to assess the impact of policies or service functions on carers, looked after children (as part of the age characteristic) and socio economic impact on different groups such as employment classifications.

The policies have been tested against the following equality 'protected characteristics' as well as the additional two identified by Southend Borough Council:

- Age – refers to a person belonging to a particular age (e.g. 42 years old) or a range of ages (e.g. 21 – 24 year olds)
- Disability – a person has a disability if he/she has a physical or mental impairment which has a substantial or long-term effect on that person's ability to carry out normal day-to-day activities
- Gender – a man or a woman
- Gender Reassignment – the process of transitioning from one gender to another
- Marriage and Civil Partnership – a marriage is no longer restricted to the union of a man and a woman but now includes same-sex couples. Same-sex couples can have their relationship legally recognised as a 'civil partnership'; they must not be treated less favourably than married couples.
- Pregnancy and Maternity – pregnancy refers to the condition of being pregnant or expecting a baby whilst maternity refers to the period after birth and is linked to maternity leave in the employment context. In the non-work context, protection against maternity discrimination is for 26 weeks after giving birth.
- Race – refers to a group of people defined by their race, colour and nationality (including citizenship) ethnic and national origins.

- Religion and Belief – religion has the meaning usually given to it but belief includes religious and philosophical beliefs including lack of belief (e.g. Atheism). A belief should affect your life choices or the way you live for it to be included in the definition.
- Sexual Orientation – whether a person’s sexual attraction is towards their own sex, the opposite sex or both sexes.
- Carers - a carer is anyone who cares, unpaid, for a friend or family member who due to illness, disability, a mental health problem or an addiction cannot cope without their support.
- Socio-economic status - an individual’s socio-economic status as a person’s social and economic position in relation to others, based on income, education and occupation.

Protected Group	Positive	Negative	Neutral	Comments	Specific mitigating actions to be taken
Age (including looked after children)	Yes	No	No	<p>The policies also seeks to provide land for employment, education, healthcare, community facilities, public space, transport, leisure and recreation, among other things, which is intended to have a positive effect on people living in the SCAAP area as well as those in the wider Borough. It is considered that these policies, in combination with other policies in the suite of Local Plan documents, will have a positive effect on individuals of all age groups.</p> <p>A major element of the SCAAP is to make land available for new mixed use sustainable development in the town centre, central seafront and gateway neighbourhoods. The delivery of new residential development will help to meet housing targets in the Southend Core Strategy and assist with Government objectives to increase the supply of housing.</p> <p>There is a national issue with housing affordability, which is particularly an issue for first time buyers, under the age of 35. The SCAAP will not be able to solve the issue but it may be seen as a positive step towards contributing towards the provision of new dwellings to meet this need.</p> <p>There is expected to be an increasing demand for housing with a care or support element to it. It is not for the SCAAP to determine this type of development but the policies do not prevent a scheme coming forward if it meets the requirements of wider policies at the Council. There is also provision for new student housing as part of the educational</p>	<p>None at this time as there are no specific impacts have been identified that would negatively affect people differently according to their age.</p> <p>Moreover the policies in the SCAAP seek to promote good design, a mix of housing including affordable housing, employment land for retail and office development among other things, an improved public realm, health and education facilities, community facilities and an improved transport infrastructure.</p>

			<p>development.</p> <p>The SCAAP also aims to improve public safety through promoting good urban design and active frontages to create a vibrant public realm which benefits from natural surveillance. Public realm improvements will also ease pedestrian movement around the central area and seafront.</p> <p>New attractive and high quality public realm and public spaces, which are accessible to all, will increase the enjoyment of the Southend Central Area for users. Well-designed places with natural surveillance in the day and good lighting in the evenings will reduce the fear of crime for the elderly. Specific improvements to identified pieces of green infrastructure are likely to be of benefit to the young in particular through the provision of new play areas as well as spaces that may be used by older children.</p> <p>Improvements to existing pedestrian links and creating new ones will improve the mobility of the elderly and young around the Central Area. An increase in cycle paths and cycle networks within the central area linking with other parts of the Borough will improve accessibility and safe movement for those who may not own a car or want to use their car and promote sustainable transport.</p> <p>The removal of subways and underpasses as part of improved pedestrian links will reduce crime levels and the fear of crime. Improved public transport links will improve mobility for the young and the elderly who may be more reliant on public transport owing to a lower incidence of car ownership.</p> <p>The employment opportunities associated with retail, office</p>	
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				<p>and commercial development will provide jobs and training for young people in the Central Area and further afield. Concentration of retailing and other uses within a small area reduces the need to travel long distances for convenience and comparison retail shopping.</p> <p>Bringing retail units back into use and promoting the town centre as a leisure and tourism destination in the daytime and evening will reduce crime and the fear of crime.</p>	
<b>Disability</b>	Yes	No	No	<p>Policy will improve accessibility and legibility for all to development and promote inclusive urban design options in relation to the layout and function of development and the public realm. The need of all user groups will be taken into account in the design and implementation stages of development proposals and schemes.</p> <p>New attractive and high quality public realm and public spaces which are accessible to all will increase the enjoyment of the Central Area for users.</p> <p>Adhering to the results of Flood Risk Assessment will ensure that development will either be not at risk of flooding, or where the risk exists, there will be suitable mitigation measures in place to ensure that vulnerable groups will not be put at risk.</p> <p>Improvements to existing pedestrian links and creating links will improve the mobility of disabled people around the Southend Central Area.</p> <p>New, modern, well designed transport facilities and infrastructure will be developed to improve usability for the disabled.</p>	<p>It is considered that the SCAAP will have a positive impact on the quality of life of people with physical disability or with visual impairment. The SCAAP aims to deliver a series of improvements to the public realm that make provision for safe pedestrian movement and crossings including provision for people with restricted mobility, and improvements to public transport that will improve the overall accessibility to various services and facilities. This infrastructure will have an overall positive impact on people with physical disability or visual impairment.</p>

				<p>Concentration of retailing and other uses within a small area reduces the need to travel further than should be necessary for convenience.</p> <p>New housing should be built to the standards set out in the Development Management Document which takes into account the new building standards and access for all.</p>	
<b>Gender Reassignment</b>	Yes	No	No	<p>Reduce the opportunity for sexual harassment and attacks on females by increasing natural surveillance of the street and public spaces.</p> <p>New attractive and high quality public realm and public spaces which are accessible to all will increase the enjoyment of the Central Area for users. This will increase the health and wellbeing of those who have undergone gender reassignment. Well-designed spaces with natural surveillance in the day and evening will reduce the fear of crime.</p> <p>The SCAAP should benefit all groups by contributing to the delivery of strategic and local infrastructure and helping to achieve more sustainable development.</p>	None at this time as no specific impacts have been identified in relation to those people who have undergone gender reassignment.
<b>Marriage and Civil Partnerships</b>	No	No	Yes	<p>Housing policy aims to benefit all in society regardless of marital status by provide high quality affordable homes located in desirable environments.</p> <p>The SCAAP should benefit all groups by contributing to the delivery of strategic and local infrastructure and helping to achieve more sustainable development.</p>	None at this time as no specific impacts have been identified in relation to those people who are married or those in a civil partnership.
<b>Maternity/Pregnancy</b>	Yes	No	No	<p>Policy will improve accessibility to development and promote inclusive design in relation to the layout and function of development and the public realm.</p>	None at this time as no specific impacts have been identified in relation to women who are pregnant or on maternity/paternity leave.



				The SCAAP should benefit all groups by contributing to the delivery of strategic and local infrastructure and helping to achieve more sustainable development.	
<b>Race</b>	Yes	No	No	<p>Reduce the opportunity for racial harassment and violence by increasing natural surveillance of the street and public spaces.</p> <p>SCAAP policies will promote the creation of high quality public spaces and community facilities, which will have a positive impact for community interaction and cohesion.</p> <p>The improvement of public transport including, walking and cycling facilities in particular, may be of benefit to ethnic groups who may in some cases and circumstances rely on public transport more than some other members of the population, owing to a lower incidence of car ownership.</p> <p>The policies which promote employment land (retail, commercial office etc.) in the SCAAP will increase opportunities and access to jobs and training for all through job creation. Development of the leisure and tourism industries will also assist with this.</p> <p>The SCAAP, along with other Local Plan policy documents, will help improve access to affordable housing within the Southend Central Area. This could be through the provision of specific forms of housing.</p>	The overall impact of the SCAAP will help to deliver the much needed regeneration, retail and employment opportunities along with improved infrastructure. It is considered that the SCAAP will have no adverse impacts on race. Indeed by providing opportunities for new community facilities the SCAAP will contribute to promoting good race relations and assist with eliminating unlawful discrimination.
<b>Religion and Belief</b>	No	No	Yes	Policy will promote the creation of quality public spaces, which will have a positive impact for community interaction and cohesion. New community and leisure facilities may also have a positive effect as well as new educational facilities.	None at this time as no specific impacts have been identified that would affect people differently as a result of their religion.
<b>Gender</b>	Yes	No	No	Reduce the opportunity for sexual harassment and attacks on females by increasing natural surveillance of the street and public spaces.	None at this time as no specific impacts have been identified that would affect people differently as a result of their gender.

				<p>Well-designed spaces with natural surveillance in the day and evening will reduce the fear of crime for users both male and female.</p> <p>Removal of subways and improvements to lighting as part of improved pedestrian links will reduce crime levels and the fear of crime.</p> <p>There are more employment and training opportunities for females in areas with high accessibility to public transport.</p>	
<b>Sexual Orientation</b>	Yes	No	No	<p>Reduce the opportunity for sexual harassment and attacks on females by increasing natural surveillance of the street and public spaces.</p> <p>New attractive and high quality public realm and public spaces which are accessible to all will increase the enjoyment of the Central Area for all users. This will increase the health and wellbeing. Well-designed spaces with natural surveillance in the day and evening will reduce the fear of crime for all regardless of their sexual orientation.</p> <p>The removal of subways as part of improved pedestrian links will reduce crime levels and the fear of crime.</p>	None at this time as no specific impacts have been identified that would affect people differently as a result of their sexual orientation.
<b>Carers</b>	Yes	No	No	<p>The policies will provide carers with the opportunity to access well-designed, high quality housing and public realm which is well planned and well lit at night, well designed pedestrian routes for access to new areas of retail and leisure which should enhance their experience of the central area.</p> <p>New infrastructure, such as health centres, should also assist carers with their duties and allow those in their care to live more independent lives with added mobility around the central area.</p>	None at this time as there are no specific impacts have been identified that would affect carers.
<b>Socio-economic</b>				The policies also seeks to provide land for employment,	None at this time as no specific

			<p>education, healthcare, community facilities, public space, transport, leisure and recreation, among other things, which is intended to have a positive effect on people living in the SCAAP area as well as those in the wider Borough.</p> <p>A major element of the SCAAP is to make land available for new mixed use sustainable development in the town centre, central seafront and gateway neighbourhoods. The delivery of new residential development will help to meet housing targets in the Southend Core Strategy and assist with Government objectives to increase the supply of housing.</p> <p>The SCAAP also aims to improve public safety through promoting good urban design and active frontages to create a vibrant public realm which benefits from natural surveillance. Public realm improvements will also ease pedestrian movement around the central area and seafront.</p> <p>The employment opportunities associated with retail, office and commercial development will provide jobs and training for people in the Central Area and further afield. Concentration of retailing and other uses within a small area reduces the need to travel long distances for convenience and comparison retail shopping.</p> <p>SCAAP policies will promote the creation of high quality public spaces and community facilities, which will have a positive impact for community interaction and cohesion.</p> <p>The SCAAP, along with other Local Plan policy documents, will help improve access to affordable housing within the Southend Central Area. This could be through the provision of specific forms of housing.</p>	<p>impacts have been identified that would affect an individual's socio-economic status in relation to others, based on income, education and occupation as well as other socio-economic matters.</p>
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Descriptions of the protected characteristics are available in the guidance or from: [EHRC - protected characteristics](#)

## 10. Community Impact

Is there equality between those who will and will not benefit from this policy/practice/function? Generally, yes.

The purpose of the SCAAP is to facilitate the regeneration and development of the Southend Central Area which includes the town centre, central seafront area and gateway neighbourhoods of Sutton and Victoria in a sustainable manner ensuring that community impacts are taken into account.

The Council recognises that there is a need to ensure that the methods used to consult and engage people in the preparation of the SCAAP are open accessible to all members of the community. To help address this issue the Council has a Statement of Community Involvement (SCI) which sets out the principles of how it will consult and the importance of reducing barriers to consultation. This may be supplemented by a communications or consultation strategy, where necessary. The Council recognises that there is a need to ensure that access and translation need to be considered as well as the broad appeal of consultation and make it attractive to a diverse range of people and groups. At Issues and Options and Preferred Approach stages the Council used a variety of means to publicise and consult on the SCAAP. It also has a key list of stakeholder which will include groups that would be positioned under the umbrella term of 'protected characteristics'.

Consultation has been carried out on the SCAAP in accordance with statutory regulations and the Statement of Community Involvement. Details of which are contained within the Consultation Statement submitted with the SCAAP. The SCAAP has been prepared in consultation with a wide range of community organisation (Appendix A) as part of the stakeholder list. Consultation and engagement opportunities took place throughout the process, including workshops with businesses and the local community, and representations made were taken into account in the iterative stages of plan preparation.

Will the policy/practice/function bring groups/communities into increased contact with each other? Yes, it will allow the interaction of groups and individuals through the provision of a new improved public realm, in the tourism and leisure facilities, new educational establishments, community facilities, and new housing schemes that will include private sector as well as affordable housing.

## 11. Equality Analysis Action Plan

No specific impacts have been identified in the key areas above that require more detailed analysis or mitigating actions at this time, and no negative effects have been identified on the protected group as a result of the preparation and adoption of the SCAAP. Moreover, the implementation of the SCAAP and delivery of much needed regeneration and infrastructure provision will have generally positive impacts on the protected groups, but the application of the policies in the SCAAP will be monitored as part of the Council's Annual Monitoring Report (AMR).

Planned action	Objective	Who	When	How will this be monitored (e.g. via team/service plan)
<ul style="list-style-type: none"> <li>Promoting economic growth and local employment opportunities creating a prosperous economy and improving employment opportunities for benefit of Borough</li> </ul>	<ul style="list-style-type: none"> <li>To reduce unemployment and increase the range of opportunities for rewarding local employment</li> <li>To build a robust and diverse economy and sustain economic development</li> </ul>	Planning Policy Team	2016-2021	Annual Monitoring Report (AMR), Service Plan, Office for National Statistics release, planning policy review, updated evidence base (economic development needs assessment)
<ul style="list-style-type: none"> <li>Extending the range and quality of facilities and services creating a vibrant, thriving town centre</li> </ul>	<ul style="list-style-type: none"> <li>To improve the health of the whole population and reduce health inequalities</li> <li>To improve the education and skills of the population</li> <li>To strengthen community identity, social responsibility and engagement</li> <li>To improve access for all to essential local services and facilities</li> </ul>	Planning Policy Team	2016-2021	Service Plan, AMR, planning policy review, updated evidence base (retail study)
<ul style="list-style-type: none"> <li>Improving the range and quality of the shopping opportunity providing for the needs of all residents and</li> </ul>	<ul style="list-style-type: none"> <li>To build a robust and diverse economy and sustain economic development</li> <li>To raise the profile of the Borough as</li> </ul>	Planning Policy Team	2016-2021	Service Plan, AMR, planning policy review, updated evidence base (retail study)

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creating a more competitive town centre	<p>an attractive location for a range of businesses</p> <ul style="list-style-type: none"> <li>▪ To improve the social and environmental performance of businesses and local economy</li> <li>▪ To reduce unemployment and increase the range of opportunities for rewarding local employment</li> </ul>			
<ul style="list-style-type: none"> <li>▪ Increasing the residential capacity within the town centre whilst accommodating future growth of town centre functions ensuring housing meets local needs including ensuring a supply of affordable housing</li> </ul>	<ul style="list-style-type: none"> <li>▪ To strengthen community identity, social responsibility and engagement</li> <li>▪ To provide everybody with the opportunity to live in a decent home</li> <li>▪ To reduce crime and antisocial activity</li> <li>▪ To improve the health of the whole population and reduce health inequalities</li> </ul>	Planning Policy Team	2016-2021	Service Plan, AMR, planning policy review, updated evidence base (Strategic Housing Market Assessment, Strategic Housing Land Availability Assessment, 5 year supply)
<ul style="list-style-type: none"> <li>▪ Protection and enhancement of the historic environment – protecting the character and distinctiveness of the town centre</li> </ul>	<ul style="list-style-type: none"> <li>▪ To maintain and enhance the quality of landscapes and townscapes</li> <li>▪ To conserve and enhance valued historic environments</li> </ul>	Planning Policy Team	2016-2021	Service Plan, AMR, planning policy review, updated evidence base (conservation area appraisals, review of local list and national listed buildings)
<ul style="list-style-type: none"> <li>▪ Promoting high quality design and townscape improvements enhancing the quality and character of the town centre</li> </ul>	<ul style="list-style-type: none"> <li>▪ To reduce crime and antisocial activity</li> <li>▪ To maintain and enhance the quality of landscapes and townscapes</li> </ul>	Planning Policy Team	2016-2021	Service Plan, AMR, planning policy review, updated evidence base (conservation area appraisals, review of local list and national listed buildings)
<ul style="list-style-type: none"> <li>▪ Promoting sustainable</li> </ul>	<ul style="list-style-type: none"> <li>▪ To improve access for all to essential</li> </ul>	Planning	2016-2021	Service Plan, AMR, planning

<p>development and minimising impacts on the environment ensuring Southend Central Area is an attractive place to live, work, visit and invest</p>	<p>local services and facilities</p> <ul style="list-style-type: none"> <li>▪ To reduce crime and antisocial activity</li> <li>▪ To maintain and enhance biodiversity including habitats, flora and fauna</li> <li>▪ To maintain and enhance the quality of landscapes and townscapes</li> <li>▪ To reduce the effect of traffic and travel on the environment To improve air quality and reduce noise and light pollution</li> <li>▪ To maintain and enhance biodiversity including habitats, flora and fauna</li> <li>▪ To maintain and enhance the quality of landscapes and townscapes</li> <li>▪ To conserve and enhance valued historic environments</li> <li>▪ To reduce contributions to climate change (and make appropriate adaptations)</li> <li>▪ To improve the quality of the water environment, retain good drinking water quality in the long term and reduce flood risk</li> <li>▪ To increase the efficient use of natural resources and make the most appropriate use of land</li> <li>▪ To reduce the environmental impact of waste in accordance with the</li> </ul>	<p>Policy Team</p>		<p>policy review, updated evidence base (conservation area appraisals, review of local list and national listed buildings, Southend State of Nature Report, Southend Economic Development Needs Assessment, Retail Study, other Council Strategies and plans related to crime and environmental monitoring e.g. noise and water quality)</p>
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	<p>waste hierarchy</p> <ul style="list-style-type: none"> <li>▪ To build a robust and diverse economy and sustain economic development</li> <li>▪ To enable the Borough and all areas to achieve their economic potential</li> <li>▪ To increase the amount of both indigenous and inward investment</li> <li>▪ To reduce unnecessary movement of labour and goods in support of improved economic performance</li> <li>▪ To raise the profile of the Borough as an attractive location for a range of businesses</li> </ul>			
<ul style="list-style-type: none"> <li>▪ Improving transport and accessibility and encouraging use of more sustainable means of transport – ensuring use of public transport is promoted and provision is accessible, safe and reliable</li> </ul>	<ul style="list-style-type: none"> <li>▪ To improve access for all to essential local services and facilities</li> <li>▪ To reduce the effect of traffic and travel on the environment</li> <li>▪ To improve air quality and reduce noise and light pollution</li> <li>▪ To reduce contributions to climate change (and make appropriate adaptations)</li> <li>▪ To reduce unnecessary movement of labour and goods in support of improved economic performance</li> </ul>	Planning Policy Team	2016-2021	Service Plan, AMR, planning policy review, updated evidence base (Car Parking Study, Local Transport Plan and associated documents, air quality monitoring by Council, Gross Value Added of Southend per head of population)

The SCAAP will include a number of key policy targets to monitor throughout the plan period. The SCAAP will also be subject to examination in public by an independent planning inspector to test the ‘soundness’ of the plan to ensure that it is delivering sustainable development. It will only be found sound if it delivers social, economic and environmental benefits to the local and wider population, is in



the public interest, and has taken into account the representations made by the local community, businesses and interest groups as well as being informed by a robust evidence base.

## 12. Conclusion

This EIA has undertaken a proportionate, timely approach which has iteratively taken into account quality assessment of the policy making process and has ensured that “due regard” has been had to the policy making. A systematic approach will ensure that there is no discrimination in the policy making. The planning department will ensure that continual monitoring and iterative policy checking continues to ensure policy responds to the spirit of EIA requirements.

Signed:

A handwritten signature in black ink, appearing to be 'John H.', written over a light grey rectangular background.



# Southend-on-Sea Borough Council

Report of the Corporate Director for Place  
To  
Cabinet

On  
20<sup>th</sup> September 2016

Report prepared by: Elizabeth Georgeou, Regulatory  
Services Manager

Agenda  
Item No.  
9

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## Declaration of Air Quality Management Area Cabinet – Executive Councillor: Councillor Flewitt

“A Part 1 Public Agenda item.”

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### 1. Purpose of Report

To advise Cabinet that the air quality at the junction of the A127, Hobblythick Lane, and Rochford Road has reported exceedences of the national air quality objectives for nitrogen dioxide. Following a detailed assessment for air quality at this junction the Council has a statutory duty to declare an Air Quality Management Area (AQMA) across the area detailed in Appendix 1 through an Air Quality Management Order.

### 2. Recommendation

It is recommended that the statutory duty of the Local Authority under the Environment Act 1995 be discharged through:

- 2.1 Noting the outcome of the 2016 Local Air Quality Management Detailed Assessment for Southend on Sea Borough Council for nitrogen dioxide exceedences at the junction of A127, Rochford Road and Hobblythick Lane.
- 2.2 Declaring the proposed Air Quality Management Area boundaries for nitrogen dioxide, detailed within Appendix 1 and the Air Quality Management Area Order 1 (Appendix 2), and approving the making of an Order under the provisions of Section 83(1) of the Environment Act 1995 formally designating the area as an Air Quality Management Area.
- 2.3 Undertaking a consultation for the development of an Air Quality Action Plan in line with Department for Environment, Food and Rural Affairs guidance and implementation of an Action Plan.
- 2.4 Developing an Air Quality Strategy for the Borough.

## **2.5 To note the funding position for this project as set out in Section 6.2.**

### **3. Background**

- 3.1 Under the provisions of the Environment Act 1995, all Local Authorities are required to undertake a review and assessment of air quality within their area. The National Air Quality Strategy details the Government's proposals for tackling air quality on a national basis. The Air Quality Regulations 2000, as amended, lay down air quality objectives, including a timetable for achieving these, for 7 key pollutants. (benzene, 1,3 butadiene, carbon monoxide, lead, nitrogen dioxide, particulate matter and sulphur dioxide).
- 3.2 Local authorities report annually to the Department of Environment, Food and Rural Affairs (DEFRA) on air quality. Where the air quality objectives are exceeded the local authority is required to undertake a Detailed Assessment to provide an accurate assessment of the likelihood of the air quality objective being exceeded at locations with relevant exposure. The air quality objective for nitrogen dioxide is an Annual mean of  $40\mu\text{g}\text{m}^{-3}$ .
- 3.3 The Council assesses air quality through 24 diffusion tubes which measure nitrogen dioxide and they are located across the borough in accordance with DEFRA Technical Guidance (Appendix 3). The Council reports annually on these results to DEFRA and also takes into account major planning proposals, permitted processes, traffic levels, biomass, and background data from the DEFRA automatic monitoring station at Chalkwell.
- 3.4 Nitrogen Dioxide has a known harmful effect on human health and the environment. Road Transport is the major source of air pollution in the UK. Up to 23,500 people die prematurely in the UK each year as a result of exposure to Nitrogen Dioxide and particulate matter, the effect on mortality is equivalent to 29,000 deaths in the UK annually. Generally for those that are young and in a good state of health, moderate air pollution levels are unlikely to have any serious short term effects. However, elevated levels and / or long term exposure to air pollution can lead to more serious symptoms and conditions affecting human health. This mainly affects the respiratory system, but in the longer term can also lead to more serious conditions such as heart disease and cancer. The most at risk from the impact of air pollutions include the elderly, young people and those with heart and respiratory diseases such as asthma and bronchitis.
- 3.5 There are currently no Air Quality Management Areas in Southend, but Southend is part of the DEFRA Southend Urban Plan which is an agglomeration of Southend, Castle Point and Rochford. DEFRA have modelled nitrogen dioxide along the main roads into Southend and their modelling assessment indicates that the annual limit value for  $\text{NO}_2$  was exceeded in 2013 but the objective for air quality is likely to be achieved before 2020 through the implementation of measures introduced by both Rochford and Southend Councils. Monitoring by Southend, as reported to DEFRA, has indicated no previous exceedences of the air quality objectives for nitrogen dioxide at residential receptors.

- 3.6 There are over 700 AQMA's declared in the UK, 90% of the AQMA's are related to traffic emissions. In the Essex area there are AQMA's in Rochford (including Rayleigh Town Centre), Chelmsford, Epping Forest, Thurrock, Brentwood, and Uttlesford.
- 3.7 The main source of air pollution in the Borough is road traffic emissions from major roads, notably the A13, A127 and A1159. Other pollution sources including commercial, industrial and domestic sources also make a contribution to background pollutant concentrations.
- 3.8 Neighbouring Rochford District Council is home to London Southend Airport which is close to the administrative boundary with Southend. The Council monitors air quality at roadside locations nearby. Airside operations are expected to only make an imperceptible contribution to background pollutant concentrations.

**Current Position:**

- 3.9 The 2015 Local Authority Quality Management (LAQM) Updating and Screening Assessment for Southend on Sea Borough Council highlighted the need for a Detailed Assessment to be undertaken for nitrogen dioxide at the junction with the A127, Hopleythick Lane and Rochford Road (The Bell Junction) because the levels found had exceeded the air quality objective with an annual mean concentration for nitrogen dioxide at the Bell junction of  $48\mu\text{g}\text{m}^3$  in 2014.
- 3.10 The Technical Guidance requires that where annual monitoring and local intelligence shows persistent exceedences the local authority is encouraged to consider moving immediately to declaring and establishing an AQMA and the development of an action plan to include measures to improve air quality.
- 3.11 The Guidance also suggests that local authorities who have had few air quality problems can consider obtaining further information / data. As Southend had not previously reported air quality problems and to better inform the Detailed Assessment, the Council installed a temporary monitoring station for 6 months at The Bell junction.
- 3.12 The outcome of the modelling was compared against the AQ objectives and the 2016 LAQM Detailed Assessment for Southend on Sea Borough Council identified exceedences of the National Air Quality Objectives for nitrogen dioxide at this junction and reported that an Air Quality Management Area be declared.
- 3.13 The assessment identified the extent of the area which falls within the  $40\mu\text{g}\text{m}^{-3}$  boundary of the proposed AQMA and also recommended further monitoring at the roundabout where the A127 meets the A1159 (Cuckoo Corner) to determine if there is relevant exposure to exceedences of the air quality objectives in this area. A monitoring tube was installed there in June 2016 and an extension to the Detailed Assessment includes this area within the proposed AQMA. It is recommended that the boundary of the AQMA be extended to  $36\mu\text{g}\text{m}^{-3}$  and where a property is partially within that boundary it is included within the AQMA.

3.14 The findings of the Detailed Assessment must be reported to DEFRA and the Council is now under a statutory duty to declare an Air Quality Management Area. The guidance does not give a definite timeframe for the declaration of the AQMA but indicates that the local authority should not delay this. Once the AQMA has been declared the Council will need to undertake a consultation to develop the air quality action plan to try to improve the air quality in the area. The air quality action plan must be submitted to DEFRA for approval.

#### **4. Other Options**

There is no option the Local Authority must declare an AQMA where there are exceedences of the air quality objectives.

#### **5. Reasons for Recommendation**

The Council has a statutory duty to review air quality in the Borough and assess whether standards and objectives are being achieved. Where exceedences of the air quality objectives are not met the Council must declare an AQMA and develop an air quality action plan to improve air quality.

#### **6. Corporate Implications**

##### **6.1 Contribution to Council's Vision & Corporate Priorities**

The declaration of the AQMA and the development of subsequent action plans to improve air quality will have a positive impact on vulnerable groups. There is no known negative impact to the priorities of the Council.

##### **6.2 Financial Implications**

The LAQM Policy Guidance (PG16) encourages the employment of an individual with AQ responsibility to ensure that Public Health / Transportation / Planning are fully briefed on what is being done and what is needed. Introducing a specialist AQ post to Southend would enable support and co-ordination across all corporate services, will enable all statutory responsibilities to be complied with, and will enable an assessment of any future requirements to be made. Currently no such expertise or resource exists within the Council.

The required role has been assessed at Level 9: £35,093 to £41,441 (with on costs £46,887 to £55,723). The overall cost of this post for the three year period from April 2017 would be £155k at mid-point. The funding required will be considered as part of the forthcoming budget process.

From now until 31 March 2017 there will still be the need for some external advice and support which would cost approximately £20k.

There is also a cost associated with the additional monitoring required around the AQMA and the consultation exercise which is estimated at £5,000.

The funding for the above £25k can be met from the Council's contingency.

There are also likely cost implications to the Council to implement actions to improve air quality. The cost of which cannot be determined until the air quality action plan has been developed. Once resource costs of the action plan have been identified then the funding of these will need to be considered as part of the Annual budget process.

### **6.3 Legal Implications**

The Council has a statutory duty to review air quality in the Borough and to take action in line with DEFRA Technical Guidance (TG) 16. Where the detailed assessment identifies that the air quality objectives have been exceeded the local authority must declare an Air Quality Management Area and develop an action plan to improve air quality.

### **6.4 People Implications**

None

### **6.5 Property Implications**

None

### **6.6 Consultation**

It is recommended in LAQM Policy Guidance (PG16) that the Council develops a Steering Group which includes all the main parties involved in developing an Action Plan and that all departments work together to identify suitable measures to address air quality. This includes measures in relation to local transport, highways, land use and planning, environmental health and public health.

We are writing to all residents and businesses who will be within the AQMA advising them that the Council is declaring an AQMA and providing them with an FAQ information sheet. They will also be directed to the Council's web pages on air quality to provide further information.

The formulation of action plans defines the main actions the Council and key partners can take in pursuit of the air quality objectives.

LAQM Technical Guidance (TG16) April 2016 also indicates that the Authority consult with interested local organisations e.g. residents and businesses affected by the measures proposed. The Authority must also consult with statutory consultees, DEFRA, Environment Agency and the relevant strategic body.

The development of the Action Plan must be completed within 12 months after designation, during which time there will be a need to continue with further monitoring of air quality within the AQMA.

### **6.7 Equalities and Diversity Implications**

None. The action plan to improve air quality will have a positive impact on all groups.

## 6.8 Risk Assessment

At high concentrations NO<sub>2</sub> is an irritant that can cause inflammation of the airways and the delay in the declaration and implementation of an air quality action plan is a risk to the health of residents within the AQMA.

Failure to meet the air quality objectives is a breach of European Union law and fines may be imposed by the EU on the UK. DEFRA have issued a policy statement with respect to Part 2 of the Localism Act 2011 indicating that if fines were to be imposed by the EU and the local authority was responsible for permitting the breach, the authority could be required to pay the full cost.

## 6.9 Value for Money

The Council co-ordinates with Essex to purchase air quality tubes and their subsequent analysis. There is also co-ordination for the development of annual assessments and the hosting of the Essex Air Quality website.

## 6.10 Community Safety Implications

None

## 6.11 Environmental Impact

Air pollutants from transport include nitrogen oxides, carbon monoxide, hydrocarbons and particulates, all of which have a damaging impact on the health of fauna and flora.

## 7. Background Papers

2015 Update and Screening Report for Southend on Sea Borough Council  
DEFRA Policy Guidance (PG) 16 April 2016  
DEFRA Technical Guidance (TG) 16 April 2016  
2016 LAQM Detailed Assessment for Southend on Sea Borough Council

## 8. Appendices

**Appendix 1** Air Quality Management Area

**Appendix 2** Order and Schedules

**Appendix 3** Map of Tube Locations across the Borough





Nitrogen Dioxide Concentrations (ugm/3)



**NO2 Grid Contour**  
— 36 ugm/3

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**SOUTHEND ON SEA BOROUGH COUNCIL****ENVIRONMENT ACT 1995, SECTION 83**

## Air Quality Management Area Order

Southend on Sea Borough Council ("The Council"), in exercise of the powers conferred upon it by Section 83(1) of the Environment Act 1995, hereby makes the following Order.

1. This Order may be referred to as the Air Quality Management Area (Southend on Sea Borough Council) (No.1) Order 2016 and shall come into effect on
2. The effect of the Order is to designate an Air Quality Management Area ("the AQMA"), the area shown outlined on the plan in Schedule 1 which includes the properties listed in Schedule 2 attached.
3. Where the AQMA includes any part of a property, it shall be taken to include the whole of that property (buildings and associated open space) within the same curtilage.
4. This Area is designated in relation to a likely breach of the Nitrogen Dioxide (NO<sub>2</sub>) annual mean Objective as specified in the Air Quality Regulations 2000.
5. This Order shall remain in force until it is varied or revoked by a subsequent Order.

The Common Seal of SOUTHEND ON  
SEA BOROUGH COUNCIL was hereto affixed

DATED: \_\_\_\_\_

SIGNED: \_\_\_\_\_

Attesting Officer

*Address for all communications:*  
Southend on Sea Borough Council, Civic Centre  
Victoria Avenue, Southend, Essex SS2 6ER

Notes: A copy of this Order and associated plan have been deposited and may be seen, free of charge, at the above address during normal working hours and on the Council's website [www.southend.gov.uk/airquality](http://www.southend.gov.uk/airquality). Enquiries should be directed to Customer Services at the above address, via telephone on 01702 215000 or else via the website.

# Air Quality Management Area (Southend on Sea Borough Council) (No.1) Order 2016

## Schedule 2

8 Bell Walk Southend-On-Sea Essex SS2 6GU  
7 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
12 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
6 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
11 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
10 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
3 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
9 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
Domino's 4 - 5 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
Earls Hall Pharmacy 8 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
Premier Computers 1 - 2 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
4A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
5A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
10A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
1A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
6A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
9A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
Flat At 12 Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
2A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
3A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
8A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
7A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
11A Earls Hall Parade Southend-On-Sea Essex SS2 6NW  
143A Hobleythick Lane Westcliff-On-Sea Essex SS0 ORL  
Kalsons Chemist 138 Hobleythick Lane Westcliff-On-Sea Essex SS0 ORJ  
The Barber's Shop 143 - 145 Hobleythick Lane Westcliff-On-Sea Essex  
136 Hobleythick Lane Westcliff-On-Sea Essex SS0 ORJ  
138A Hobleythick Lane Westcliff-On-Sea Essex SS0 ORJ  
141 Hobleythick Lane Westcliff-On-Sea Essex SS0 ORL  
147 Hobleythick Lane Westcliff-On-Sea Essex SS0 ORL  
137 Hobleythick Lane Westcliff-On-Sea Essex SS0 ORL  
139 Hobleythick Lane Westcliff-On-Sea Essex SS0 ORL  
21 Larke Rise Southend-On-Sea Essex SS2 6GQ  
St John Fisher Hall St John Fisher Catholic Church 2 Manners Way Southend-On-Sea Essex SS2 6PT  
St John Fisher Catholic Church 2 Manners Way Southend-On-Sea Essex SS2 6PT  
Presbytery St John Fisher Catholic Church 2 Manners Way Southend-On-Sea Essex SS2 6PT  
5 Manners Way Southend-On-Sea Essex SS2 6QR  
8 Manners Way Southend-On-Sea Essex SS2 6PT  
7 Manners Way Southend-On-Sea Essex SS2 6QR  
10 Manners Way Southend-On-Sea Essex SS2 6PT  
12 Manners Way Southend-On-Sea Essex SS2 6PT  
85 Prince Avenue Southend-On-Sea Essex SS2 6RL  
94 Prince Avenue Southend-On-Sea Essex SS2 6NN  
87 - 89 Prince Avenue Southend-On-Sea Essex SS2 6RL  
95 Prince Avenue Southend-On-Sea Essex SS2 6RL

83 Prince Avenue Southend-On-Sea Essex SS2 6RL  
Prince Avenue Surgery 3 Prince Avenue Southend-On-Sea Essex SS2 6RL  
Carphone Warehouse 53 Prince Avenue Southend-On-Sea Essex SS2 6RL  
Bell Laundrette 79 Prince Avenue Southend-On-Sea Essex SS2 6RL  
Cycles UK 77 Prince Avenue Southend-On-Sea Essex SS2 6RN  
Hyundai 59 Prince Avenue Southend-On-Sea Essex SS2 6RL  
Topps Tiles 53 Prince Avenue Southend-On-Sea Essex SS2 6RL  
Lantern House 126 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
Shell Bell Service Station 96 - 118 Prince Avenue Southend-On-Sea Essex SS2 6RL  
The Bell Hotel Prince Avenue Southend-On-Sea Essex SS2 6RL  
Kegra Mobile Cafe 91 - 93 Prince Avenue Southend-On-Sea Essex SS2 6RL  
EJ Alloys 130 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
Cue Doctor 120 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
DG Supplyline Ltd 122 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
DG Servicing 140 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
134 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
91 - 93 Prince Avenue Southend-On-Sea Essex SS2 6RL  
42 Prince Avenue Southend-On-Sea Essex SS2 6NN  
46 Prince Avenue Southend-On-Sea Essex SS2 6NN  
48 Prince Avenue Southend-On-Sea Essex SS2 6NN  
92A Prince Avenue Southend-On-Sea Essex SS2 6NN  
92C Prince Avenue Southend-On-Sea Essex SS2 6NN  
33 Prince Avenue Southend-On-Sea Essex SS2 6RL  
37 Prince Avenue Southend-On-Sea Essex SS2 6RL  
41 Prince Avenue Southend-On-Sea Essex SS2 6RL  
43 Prince Avenue Southend-On-Sea Essex SS2 6RL  
47 Prince Avenue Southend-On-Sea Essex SS2 6RL  
142 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
144 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
150 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
152 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
156 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
50 Prince Avenue Southend-On-Sea Essex SS2 6NN  
58 Prince Avenue Southend-On-Sea Essex SS2 6NN  
60 Prince Avenue Southend-On-Sea Essex SS2 6NN  
64 Prince Avenue Southend-On-Sea Essex SS2 6NN  
68 Prince Avenue Southend-On-Sea Essex SS2 6NN  
70 Prince Avenue Southend-On-Sea Essex SS2 6NN  
74 Prince Avenue Southend-On-Sea Essex SS2 6NN  
78 Prince Avenue Southend-On-Sea Essex SS2 6NN  
82 Prince Avenue Southend-On-Sea Essex SS2 6NN  
84 Prince Avenue Southend-On-Sea Essex SS2 6NN  
88 Prince Avenue Southend-On-Sea Essex SS2 6NN  
49 Prince Avenue Southend-On-Sea Essex SS2 6RL  
1 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
11 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
13 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
15 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN

17 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
19 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
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5 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
7 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
8 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
30 Prince Avenue Southend-On-Sea Essex SS2 6NN  
36 Prince Avenue Southend-On-Sea Essex SS2 6NN  
38 Prince Avenue Southend-On-Sea Essex SS2 6NN  
1 Prince Avenue Southend-On-Sea Essex SS2 6RL  
19 Prince Avenue Southend-On-Sea Essex SS2 6RL  
21 Prince Avenue Southend-On-Sea Essex SS2 6RL  
25 Prince Avenue Southend-On-Sea Essex SS2 6RL  
5 Prince Avenue Southend-On-Sea Essex SS2 6RL  
9 Prince Avenue Southend-On-Sea Essex SS2 6RL  
128 Prince Avenue Westcliff-On-Sea Essex SS0 ONW  
80 Prince Avenue Southend-On-Sea Essex SS2 6NN  
86 Prince Avenue Southend-On-Sea Essex SS2 6NN  
90 Prince Avenue Southend-On-Sea Essex SS2 6NN  
31 Prince Avenue Southend-On-Sea Essex SS2 6RL  
35 Prince Avenue Southend-On-Sea Essex SS2 6RL  
39 Prince Avenue Southend-On-Sea Essex SS2 6RL  
45 Prince Avenue Southend-On-Sea Essex SS2 6RL  
52 Prince Avenue Southend-On-Sea Essex SS2 6NN  
56 Prince Avenue Southend-On-Sea Essex SS2 6NN  
62 Prince Avenue Southend-On-Sea Essex SS2 6NN  
66 Prince Avenue Southend-On-Sea Essex SS2 6NN  
72 Prince Avenue Southend-On-Sea Essex SS2 6NN  
76 Prince Avenue Southend-On-Sea Essex SS2 6NN  
146 Prince Avenue Westcliff-On-Sea Essex SS0 ONW  
148 Prince Avenue Westcliff-On-Sea Essex SS0 ONW  
154 Prince Avenue Westcliff-On-Sea Essex SS0 ONW  
44 Prince Avenue Southend-On-Sea Essex SS2 6NN  
83A Prince Avenue Southend-On-Sea Essex SS2 6RL  
92B Prince Avenue Southend-On-Sea Essex SS2 6NN  
95A Prince Avenue Southend-On-Sea Essex SS2 6RL  
32 Prince Avenue Southend-On-Sea Essex SS2 6NN  
34 Prince Avenue Southend-On-Sea Essex SS2 6NN  
40 Prince Avenue Southend-On-Sea Essex SS2 6NN  
11 Prince Avenue Southend-On-Sea Essex SS2 6RL  
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23 Prince Avenue Southend-On-Sea Essex SS2 6RL  
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10 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
12 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
14 Princes Court Prince Avenue Southend-On-Sea Essex SS2 6RN  
Flat Above 93 Prince Avenue Southend-On-Sea Essex SS2 6RL  
Flat Above 85 Prince Avenue Southend-On-Sea Essex SS2 6RL  
Managers Accommodation The Bell Hotel Prince Avenue Southend-On-Sea Essex SS2 6RL  
136 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
138 Prince Avenue Westcliff-On-Sea Essex SS0 0NW  
Flat Above 94 Prince Avenue Southend-On-Sea Essex SS2 6NN  
Flat 1 122 Prince Avenue Westcliff-On-Sea Essex SS0 0EW  
87A Prince Avenue Southend-On-Sea Essex SS2 6RL  
89A Prince Avenue Southend-On-Sea Essex SS2 6AL  
51 Prince Avenue Southend-On-Sea Essex SS2 6RL  
236 Priory Crescent Southend-On-Sea Essex SS2 6PR  
240 Priory Crescent Southend-On-Sea Essex SS2 6PR  
244 Priory Crescent Southend-On-Sea Essex SS2 6PR  
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266 Priory Crescent Southend-On-Sea Essex SS2 6PR  
264 Priory Crescent Southend-On-Sea Essex SS2 6PR  
Little Legs 6A Rochford Road Southend-On-Sea Essex SS2 6SP  
Kebab 2000 2 Rochford Road Southend-On-Sea Essex SS2 6SP  
New Indya 4 Rochford Road Southend-On-Sea Essex SS2 6SP  
8A Rochford Road Southend-On-Sea Essex SS2 6SP  
2A Rochford Road Southend-On-Sea Essex SS2 6SP  
Flat Above 4 Rochford Road Southend-On-Sea Essex SS2 6SP  
8B Rochford Road Southend-On-Sea Essex SS2 6SP  
458 Victoria Avenue Southend-On-Sea Essex SS2 6ND  
460 Victoria Avenue Southend-On-Sea Essex SS2 6ND

503 Victoria Avenue Southend-On-Sea Essex SS2 6NL  
507 Victoria Avenue Southend-On-Sea Essex SS2 6NL  
454 Victoria Avenue Southend-On-Sea Essex SS2 6ND  
456 Victoria Avenue Southend-On-Sea Essex SS2 6ND  
452 Victoria Avenue Southend-On-Sea Essex SS2 6ND  
505 Victoria Avenue Southend-On-Sea Essex SS2 6NL



# Nitrogen Dioxide Concentrations (ugm/3)



**NO2 Grid Contour**  
— 36 ugm/3

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# APPENDIX 3



Data Layers

Ordnance Survey	Black & White	Aerial 2014	Aerial 2010	Aerial 2007	Aerial 2003	Aerial 2000	Aerial 1995
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# Southend-on-Sea Borough Council

Agenda  
Item No.

10

Report of Corporate Director for Place

To  
Cabinet

On 20 September 2016

Report prepared by  
Chris Livemore, Sustainability Officer and Jeremy Martin,  
Energy and Sustainability Manager

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## Low Carbon Energy and Sustainability Strategy 2015-2020 Annual Update Report

Place Scrutiny Committee

Executive Councillor: Councillor Holland

*A Part 1 Public Agenda Item*

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### 1. Purpose of Report

- 1.1. To consider Southend on Sea Borough Council's ('the Council') Low Carbon Energy and Sustainability Strategy 2015-2020, Annual Update Report

### 2. Recommendations

- 2.1 **That Members agree the Low Carbon Energy and Sustainability updated Annual Report.**

### 3. Background

- 3.1 In 2015 the Council adopted its Low Carbon Energy and Sustainability Strategy (LCESS), which looked to identify opportunities available to the Council through the growth in the low carbon and environmental sector and the risks from issues including rising energy prices and impacts of climate change on the Borough.

- 3.2 The LCESS covers a five year period and has the overriding objective of establishing Southend on Sea as a 'Low Carbon City' by 2020. The aims and objectives of the second strategy are more ambitious than its predecessor and has identified six key focus areas to target delivery upon:

Focus Area One: Reducing our Carbon Emissions;

Focus Area Two: Policy and Regulation;

Focus Area Three: Delivering a Local Low Carbon Economy;

Focus Area Four: Supporting Low Carbon Communities;

Focus Area Five: Encouraging Sustainable Transport and Travel; And

Focus Area Six: Adapting to Climate Change and Enhancing Biodiversity

- 3.3 As part of the LCESS, the Council joined the Climate Local initiative.
- 3.4 Within LCESS, the Council committed to publishing an Annual Update Report, **(Appendix 1)**, which highlights achievements to date.
- 3.5 LCESS is supported through an Action Plan in the form of Forward Actions outlined in this report.
- 3.6 LCESS was drafted to enable the Council to target European Union funding, particularly Horizon 2020 - a programme with an available budget of €79 billion to spend between now and 2020.

#### **4. Major Achievements**

- 4.1. Over the 18 months of the strategy, the Council has progressed with projects totalling £19m which are progressing through complete delivery. On completion, these projects will have saved approximately 3,500 tonnes CO2 each year and provided revenue and savings to the Council of £700k each year net of the costs of delivery, funding and repayment of capital. These projects include LED street lighting, pilot projects at Southend Adult Community College, and Temple Sutton Primary School, over 1.3MW solar on various sites and energy efficiency projects on Council properties.
- 4.2. In most cases, delivery of projects has involved creating processes to facilitate faster and easier delivery of future projects
- 4.3. Southend Energy was launched in May 2015 and has delivered average savings of £276 per household per year taking up the offer, with over 5,000 customers signing up by the start of September 2016, 6.8% of the households in Southend. Overall, more than £1.36m savings have been achieved. In addition, the environmental policy within the Southend Energy tariff has saved a further 1,200 tonnes CO2.
- 4.4. Working with the Environment Agency (EA) and many departments and external agencies, the Council ran a successful project Targeted Adaptation Support for Councils (TASC) to identify in more detail the likely impacts of climate change locally. This information is now being used to inform projects such as the Queensway Development, Health, Planning, Coastal Defence, Smart Cities and the Future Actions for LCESS.
- 4.5. Working through EA, Green Investment Bank and other agencies, has established the Council as a leader in development of finance packages, climate change and energy development. These lessons are being disseminated through invitations to speak at conferences and through working with other Councils on funding bids.
- 4.6. The Council has won funding from the EU to develop Sustainable Drainage (SuDs) projects to grow internal capability and to find ways to address flooding issues with the Borough. The Council is working closely with Anglian Water on increasing the use of SuDs, on innovative sensing and measurement approaches and ways to better predict and manage excess water and flooding issues.

- 4.7. The energy and sustainability team has been funded through a £200k loan from reserves and has already generated in excess of £200k in direct revenue and recharges within projects delivered although much of the revenue is spread over a 5-year period.

## **5. Future Actions**

- 5.1. A number of projects, funding bids and energy projects are proposed to move the strategy towards delivery. The energy market is subject to enormous change and innovation and many projects are becoming possible due to this change. At the same time, incentive and funding regimes are constantly changing as the market and government policy develops. These projects include solar PV and energy efficiency opportunities and will contribute both carbon reduction and savings for the Council net of funding and implementation cost.
- 5.2. Recent published reports from the Infrastructure Commission, Carbon Trust, and the recent Climate Change Risk Assessment 2017 have reinforced the lessons for local policy learned from the TASC project and the implications locally are being developed to inform the shape of Future Actions and how they will affect Council functions.
- 5.3. To start to address local resilience issues and to exploit the developing energy storage market, the Council is bidding on funding opportunities and is investigating opportunities to provide a solar PV and battery package for residents. When appropriate and possible, the solutions developed will be linked into the developing grid rebalancing market allowing peaks and troughs within the energy market to be used to provide savings and revenue both for residents and the Council.
- 5.4. The Council intends to develop innovative solutions to energy and water management including investigating innovative opportunities to generate energy from tidal and wave power on the Pier, potential innovative wind energy solutions that are quieter and less visually intrusive than traditional turbines and further ways to better manage surface water and drainage where interfacing systems are managed by multiple agencies including the Council. New innovations will be monitored and exploited when they provide more appropriate solutions than traditional methods. The Council will continue to work with Anglian Water on the current flood risks and to extend initiatives to deal with the expected increased flooding risk from climate change.
- 5.5. The Council will continue to work to find ways to reduce energy costs for residents both by expanding Southend Energy and other mechanisms to encourage supplier switching but also by working through the redesigned Energy Company Obligation (ECO) scheme to target hard to heat homes and those who are struggling to afford their energy costs.
- 5.6. The Council will continue to manage LCESS through a self-funding team balancing wider strategic understanding of climate change and development of adaptation measures with projects that yield tangible benefits to the Council net of costs of implementation and funding.

## **6. Reasons for Recommendations**

- 6.1 The recommendations have been made in order to continue to position the Council as a leading local authority in the East of England with regards to the UK's transition to the low carbon economy. Successful implementation of the new strategy will provide the Council with the opportunity to establish new funding streams (including EU funding bids), efficiency savings and revenue sources (including money generated through renewable energy installations).
- 6.2 The Strategy continues to provide an excellent opportunity for the Council to make a real contribution in delivering local objectives around health and well-being, housing, fuel poverty, air quality, transport, education, economic development and community cohesion.

## **7. Corporate Implications**

### **7.1 Contribution to Council's Vision & Corporate Priorities**

The measures listed above are in line with the Corporate Priorities and also the Council's vision in terms of becoming an exemplar authority by developing best practice **and** supporting the Council's vision to become a city and support the regeneration of the Borough.

### **7.2. Financial Implications**

- 7.2.1 A major objective of the Low Carbon and Sustainability Strategy 2015-2020 is to help the Council safeguard against rising energy costs, make efficiency savings and explore new revenue generation models at a time of public sector cutbacks.
- 7.2.2. The new Strategy identifies low carbon and renewable projects that will mostly be 'cash positive' from year one across both the Council's property estate and from utilising assets throughout the Borough. The Council will need to consider funding the projects through private sector suppliers and Energy Service Companies (ESCos), the Public Works Loan Board, existing capital works programmes, a variety of European schemes (Horizon 2020, Life, JESSICA, ELENA), the Euro Energy Efficiency Fund and the Green Investment Bank.
- 7.2.3. Currently no council funding is contained within the existing capital or revenue budgets for any new identified projects in this strategy beyond those approved in the current year's budget. If any projects require Council funding they will need to be considered as individual projects as part of the annual budget process in determining annual Revenue and Capital budgets.
- 7.2.4. Projects are expected to deliver a financial return of between 1-3% each year net of all costs.
- 7.2.5. The current team is mainly funded from earmarked reserves of £200k over 2 years with the current approved funding expiring on 31 March 2017. More than £200k has already been generated in direct revenue and salary charges to projects and continued funding of the team (£100k pa) needs to be considered as part of the forthcoming budget process with the intention of the whole LCESS being cash neutral in terms of delivery resources. Applications are



being made to UK and EU funding sources to fund the team without the self-funding mechanism and the team will seek additional revenue generation using its established position with other Local Authorities.

### **7.3. Legal Implications**

7.3.1 The Low Carbon Energy and Sustainability Strategy 2015-2020 complies with the Climate Change Act 2008 and Carbon Reduction Commitment Energy Efficiency Scheme and supports the UK's targets for energy generated from renewable sources.

### **7.4 People Implications**

7.4.1. The Council will need to maintain the team to deliver the pipeline of low carbon and renewable projects, but this will depend on the scale of the projects that the Council undertake. Most of the costs of staffing projects can be paid from the cost savings and money generated from energy efficiency and renewable generation returns combined with direct revenue generation but there will be some pre-project work and costs which will be more difficult to fund in this way for which budget funding for work such as feasibility will be sought.

7.4.2. The team managing and developing LCESS are on short term contracts which expire on 31 March 2017. To achieve continuity, extensions to these contracts will be required.

### **7.5 Property Implications**

7.5.1. The Council will need to continue working in a coordinated fashion to ensure that priority projects are delivered.

### **7.6 Consultation**

7.6.1. The Council has established a close working relationship with Imperial College one of the leading academic institutions in the energy market and has sought input from a number of organisations based within the low carbon and environmental sector – including Sustainability East, the Energy Saving Trust Environment Agency, BEIS - they have also consulted with local groups (e.g. Trustlinks) and with relevant departments from within the Council. The individual sections within the strategy and the annual report have been drafted by the responsible teams and managers, so already reflects existing Council activities.

### **7.7 Equalities and Diversity Implications**

7.7.1. The Council will need to be aware that the groups in the community most vulnerable to increased energy costs will be those in disadvantaged areas.

### **7.8 Risk Assessment**

7.8.1 There is a risk that if the Council fails to develop the Low Carbon Energy and Sustainability Strategy 2015-2020 that it will not adequately safeguard itself against the risks associated with future fuel security, rising energy costs and climate change.

- 7.8.2 There is a risk that the Council could miss out on future funding and grant bids by not developing the new Strategy.
- 7.8.3 There is a risk that by failing to act and promote the low-carbon economy sufficiently to local residents and businesses that there will be negative health implications and increased risk from not adapting to embedded climate change. e.g. increased number of properties falling within fuel poverty, winter deaths and negative economic implications.

## **7.9. Value for Money**

- 7.9.1 The vast majority of work proposed in the new Strategy already falls within existing work streams at the Council and although additional staff time will be required to embed low-carbon and sustainable considerations into the Council's decision-making processes the benefits will far outweigh the costs.
- 7.9.2 The team working on energy projects and LCESS is largely self-funding through direct revenue and projects whilst the projects completed and underway will deliver approximately £700k each year revenue and savings net of implementation and funding costs.
- 7.9.3 The Low Carbon Energy and Sustainability Strategy 2015-2020 has been drafted to help the Council deliver local objectives around health, congestion, education, housing, air quality, economic development and social inclusion.

## **7.10 Community Safety Implications**

- 7.10.1 Adopting a proactive approach on the transition to a low-carbon economy will help to address poverty and health inequalities.

## **7.11 Environmental Impact**

- 7.11.1 Development of the Low Carbon Energy and Sustainability Strategy 2015-2020 will help the Council improve its overall environmental performance and result in a reduction in carbon emissions and the promotion of the sustainable use of resources throughout the Southend on Sea Borough.

## **8 Background Papers**

Low Carbon Energy and Sustainability Strategy 2015-2020 <Z:\Home\Sibel Yuzudik\LCESS Final Version.pdf>

Smart Power: A National Infrastructure Commission Report 2016  
[https://www.gov.uk/government/uploads/system/uploads/attachment\\_data/file/505218/IC\\_Energy\\_Report\\_web.pdf](https://www.gov.uk/government/uploads/system/uploads/attachment_data/file/505218/IC_Energy_Report_web.pdf)

Carbon Trust Report: Can storage help reduce the cost of a future UK electricity system? <https://www.carbontrust.com/media/672486/energy-storage-report.pdf>

Climate Change Risk Assessment 2017: Projections of future flood risk in the UK <https://www.theccc.org.uk/wp-content/uploads/2015/10/CCRA-Future-Flooding-Main-Report-Final-06Oct2015.pdf.pdf>

## **9 Appendices**

**Appendix 1 - Low Carbon Energy and Sustainability Strategy 2015-2020  
Draft Annual Report**

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SOUTHEND-ON-SEA  
BOROUGH COUNCIL

**LOW CARBON ENERGY  
& SUSTAINABILITY  
ANNUAL REPORT**

**2016**

**DRAFT**

# CONTENTS

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This document includes:

- the Council's key achievements over the past year and
- 2015/2020 corporate vision to become a 'Low Carbon, Smart City by 2020

Councillors and senior managers monitor progress against the targets set out in the Council's Low Carbon Energy & Sustainability Strategy 2015-2020 on a regular basis.

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# WELCOME

This is Southend-on-Sea Borough Council's first annual Sustainability Report that sets out to provide an update on the progress that we are making in our bid to become a 'Low Carbon, Smart City' by 2020.

Many councils across the UK are taking positive steps to address challenges around energy generation, carbon reduction and managing the impacts of a changing climate - we wanted to be amongst the first that committed to publishing an annual Sustainability Report that detailed our progress and actively encourage other councils to do the same.

In 2015 we launched our second Low Carbon Energy & Sustainability Strategy (LCESS), which has the vision of transforming Southend-on-Sea into a 'Low Carbon, Smart City' by 2020 and place Southend-on-Sea at the forefront of both the low carbon and smart city agendas.

LCESS set a series of very ambitious targets, which will result in a more sustainable, resilient and vibrant local economy and society that will benefit all of our residents, local communities and businesses, even in the short-term.

This Sustainability Report provides an overview of the work we have completed since the launch of LCESS

Being a catalyst for change and making a positive impact throughout Southend-on-Sea are both at the heart of our ambitions, as is effective long-term planning for future energy security and a changing climate.

Getting our strategy on energy and climate change right at the start is an integral part of ensuring business continuity, safeguarding people and places, protecting the natural environment and contributing to a more resilient local economy.

A lot of the actions are low, or no-cost, but where costs are more significant we look to identify both direct and indirect financial benefits in the form of cost savings, income generation or avoided future costs.

Such actions can also play an instrumental role in helping to deliver wider objectives, such as promoting local economic growth, supporting health and well-being initiatives and protecting vulnerable communities from issues such as fuel poverty.

Since the launch of LCESS we have helped save local residents in excess of £1.3 million on their energy bills through Southend Energy; delivered the largest solar PV project in the Borough and have been recognised as the 'greenest' urban area in the UK in two national reports.

We want to continue making progress in this area and hope you will join us on our journey to become a Low Carbon, Smart City.

# FOCUS AREAS

## FOCUS AREA ONE

Reducing our Carbon Emissions

## FOCUS AREA TWO

Policy & Regulation

## FOCUS AREA THREE

Delivering a Local Low Carbon Economy

## FOCUS AREA FOUR

Supporting Low Carbon Communities

## FOCUS AREA FIVE

Encouraging Sustainable Transport & Travel

## FOCUS AREA SIX

Climate Change Adaptation

# ACHIEVEMENTS

Generated enough  
SOLAR energy  
to power over  
**1,000**  
households

Part of a **£1m**  
sustainable transport  
project with  
Innovate UK

Delivering a  
**£13.5m**

LED Street Light  
Replacement  
Programme

Recognised as the  
'GREENEST' city  
in the UK

Success in **2**  
EU funding bids  
TRACE & SPONGE

Completed the  
award winning  
IDEAS IN MOTION  
campaign

Enabled

**£1.36m**

of energy savings  
for our residents

Winner of the  
Environment  
Agency's  
national 'TASC'  
competition

**1st** local  
authority in Essex  
to sign up to  
Climate Local

Secured savings of  
**£22m**  
on our new waste  
contract

Completed a **£1.29m**  
energy efficiency programme at  
Temple Sutton Primary School



# 1. REDUCING OUR CARBON EMISSIONS

Our Vision is to lead by example by taking positive action to reduce carbon emissions and improve the energy efficiency performance and sustainability across our property estate.

We will identify ways to reduce our energy bills, generate new revenue streams, ensure buildings are energy efficient and increase the uptake of renewable and low carbon technologies in our buildings and infrastructure.

We aim to improve our overall environmental performance and become a more sustainable organisation in the process.

## OUR ENERGY HIGHLIGHTS

Over the course of the last 8 years we have managed to reduce our consumption of electricity by 12% and gas by 25% throughout our own property estate, we are looking to continue this trend and here are some of our highlights:

We were delighted to deliver the Borough's **first carbon positive project** at the Pier Lift Tower. The installation of a solar PV system has meant that the building now generates more energy than it uses.

We have been working closely with Southend Adult Community College to deliver a comprehensive energy efficiency project to help the college reduce their energy spend. The programme resulted in CO<sub>2</sub> savings of 137 tonnes and included the installation of solar PV, a biomass boiler, new heating controls, insulation and LED lighting.

Since 2012 we have supported WWF's **Earth Hour**, an annual event that asks participants to go without light for an hour and demonstrate a commitment towards delivering a more sustainable future for our residents.

Over the course of the last 12 months we have undertaken a series of feasibility studies to identify the potential of installing solar PV throughout the Borough - the work has resulted in an agreement to include the roll out of a solar PV installation programme in our Capital Programme so that we can benefit further from sustainable energy in the future.

In terms of solar PV projects already underway, we have already installed (or are planning to install) solar panels on Temple Sutton Primary School; Edwards Hall Primary School; Heycroft Primary School; Southend Adult Community College; the Pier Lift Tower and the Beecroft Art Gallery.

In total this will produce over 454KWp of solar energy per annum - equivalent of enough energy to power over 1,000 households.

### OUR 2015/2016 CRC SUBMISSION:

Each year the Environment Agency require us to submit an annual report of our carbon emissions under a government scheme called the Carbon Reduction Commitment. The scheme covers emissions from our street lights and qualifying buildings including the Civic Centre.

This year our emissions totalled 8,465 tonnes of carbon, which was a 1.7% reduction from our 2014/2015 submission. With our ongoing energy efficiency and LED street light replacement programmes we fully expect to report further reductions next year.



## ENERGY SAVINGS IN SCHOOLS

In 2015, we established an energy efficiency and cost saving programme that would help prioritise energy projects for schools throughout the Borough.

The aim of our programme is to help schools identify and then deliver energy projects in their buildings and importantly promote the benefits of energy efficiency and sustainability to pupils in the process.

We want to help schools in the Borough to secure savings on their energy bills through energy efficiency projects and even generate new revenue streams through the installation of renewable technologies.

Essentially the programme also looks at how the learning and educational environment of a school can also be improved for the benefit of pupils and teachers. For example, implementing new LED lights can enhance the

lighting in a classroom, whilst better heating management systems can ensure that the temperature of the rooms are set to optimise the working environment.

To date we have delivered energy efficiency projects at Temple Sutton Primary School; Edwards Hall Primary School; and Heycroft Primary School.

We'd also like to thank Ashden and 10:10 - two incredible charities who have supported us to engage with schools in Southend-on-Sea through Ashden's LESS CO2 Programme and 10:10's inspirational campaign Solar Schools.

You can follow both charities on social media for some great energy related tips

@LessCO2 @1010

## TEMPLE SUTTON PRIMARY SCHOOL

Temple Sutton Primary School was built in the 1940s and as with many buildings that are over 70 years old suffered from poor energy performance. For example, the school had many of its original crital windows that were poorly fitting, very draughty and impacted upon the learning environment for the school's pupils.

In 2015 we approved a £1.29 million energy efficiency programme for the school that would deliver £2.84 million gross savings across the 25 year lifetime of the project

(before costs) and result in an annual saving of 285 tonnes of CO<sub>2</sub>. In addition, it has proved an excellent opportunity to engage pupils on the subject of sustainable energy.

Work included the installation of the largest solar PV project in the Borough, together with a biomass boiler and ground source heat pump, whilst energy efficiency measures included new LED lighting, enhanced heating controls and essentially replacing all of the school's crital windows with new double glazing.



258 tonnes of  
CO<sub>2</sub> saved!

# 2. POLICY AND REGULATION

Our Vision is to apply the local planning framework positively to ensure maximum benefit for our residents, communities and businesses.

We will ensure all new development in the Borough is designed and built to be energy, climate and resource efficient (including water) and that existing development is adapted, where feasible, to enhance its long-term sustainability.

We will seek to increase the amount of energy derived from renewable technologies across the Borough and work to identify areas of opportunity for renewable, decentralised and low carbon projects.

## PLANNING FOR THE FUTURE

Ensuring that places, spaces and buildings are designed in ways that support low-carbon lifestyles and help shape a sustainable future for Southend-on-Sea, as well as promoting sustainable and active transport options are all key elements of our planning system.

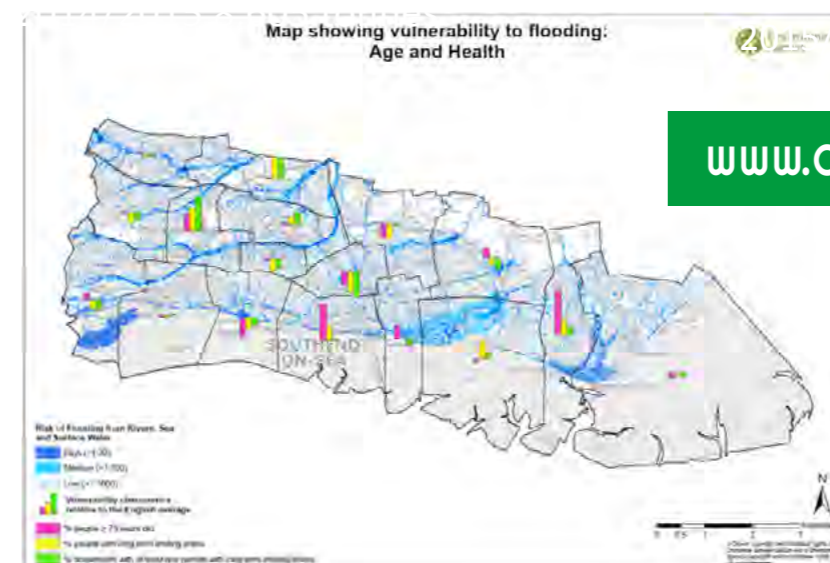
We wanted to demonstrate our commitment to taking positive action on addressing the future impacts of climate change throughout the Borough. In January 2015, we became the first local authority in Essex to sign up to the Local Government Association’s ‘Climate Local’ initiative.

Climate Local is the successor to the Nottingham Declaration and aims to drive, inspire and support local authority action on climate change adaptation and resilience measures. Signing Climate Local ensures a commitment from our senior management to report annually on our progress in this area.

In early 2016 we worked with the Environment Agency to make use of the Joseph Rowntree Foundation’s Climate Just data to develop a series of Southend-on-Sea specific climate vulnerability maps.

The maps demonstrated that the impacts of climate change, combined with the social vulnerability associated with it, were very real and present threats that are likely to worsen in the future without proactive action today.

We are working with a cross sector of departments to further enhance our understanding of areas that are most vulnerable to a changing climate and how these can be embedded in the new Local Action Plan.



[www.climatejust.org.uk](http://www.climatejust.org.uk)

Exploring the opportunity to test new 'bladeless' wind turbines.

Our Capital Programme will progress solar PV projects on third party buildings throughout the Borough, including Southend Hospital, Southend Tennis and Leisure Centre and the Cliffs Pavilion

Work is underway to determine viable tidal energy projects off the coast of Southend

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# RENEWABLE ENERGY OPPORTUNITIES

After undertaking a report in 2013 to identify potential areas in the Borough that would be suitable for renewable energy projects, we have continued to explore how we can ensure that our residents, communities and businesses can benefit from cleaner and more sustainable sources of energy.

In 2015, we were successful in securing a grant from the Department of Energy and Climate Change's Heat Network Delivery Unit. The funding was used to commission a study that would help us to identify areas of high heat usage and determine the opportunities to run a decentralised energy network in Southend-on-Sea.

The project found three suitable sites where there was potential to operate a district heating network - Victoria Avenue and the Town Centre, which could link with the proposed Queensway Development and the Southend Airport Business Park.

Such a decentralised energy network could provide district heating, cooling and low carbon generated electricity to residents and communities throughout the Borough and we will continue to work on how this opportunity can be realised.

As part of our strategy to become a 'Low Carbon, Smart City', we are also looking to determine the most appropriate sites in Southend-on-Sea that could benefit from both solar and wind generation.

## EMBEDDING CLIMATE CHANGE INTO POLICY

The low carbon agenda is already embedded within our local planning framework. The Southend Core Strategy (2007) requires development proposals to include appropriate measures in design, layout, operation and materials to achieve a reduction in the use of resources and requires at least 10% of the energy needs of new development to come from on-site renewable options.

Furthermore the Development Management Document (2015) requires new development and alterations to existing development to be energy and resource efficient. This includes: water efficiency; urban greening measures and the promotion of biodiversity, whilst prioritising the use of sustainably sourced material.

Our emerging Southend Central Area Action Plan provides a planning framework for the town centre and central seafront area and contains proposals for defined policy areas that include supporting energy efficiency in development proposals and the opportunity for a decentralised energy supply.

# 3 DELIVERING A LOCAL LOW CARBON ECONOMY

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Our Vision is to facilitate the growth of a local low carbon economy in Southend-on-Sea, which will see benefits around the creation and retention of jobs, support local businesses become more energy efficient and aide local regeneration.

We want to ensure that organisations operating within the Borough can benefit from reducing their energy consumption and carbon emissions in terms of associated cost savings.

Our aim is to see these savings being invested back into the local economy and promote the associated benefits in health and well-being and financial strength.

## CREATING SUSTAINABLE BUSINESSES

A key element of our Low Carbon Energy & Sustainability Strategy 2015-2020 was to support businesses become more sustainable and reduce their carbon emissions.

The main means of achieving this was through a European Regional Development Fund (ERDF) funded programme called the Thames Gateway South Essex Low Carbon Business Programme.

The programme focused upon provided local businesses with the support, assistance and funding that they required to help them deliver low carbon projects that would significantly reduce their carbon emissions.

We are now working on a follow up ERDF bid with other local authorities in the East of England that will allow us to build upon the projects key successes:

## KEY ACHIEVEMENTS

£2.6 million of annual cost savings delivered by SMEs through the Programme

£4.0 million of private sector investment leveraged by the Grants Programme

£6.3 million of total low carbon investment in the East of England

150 tonnes of waste diverted from landfills sites

A total of £2.3 million awarded to 560 SMEs

4,900 tonnes of CO<sub>2</sub> emissions saved during the Programme

# CREATING A LOW CARBON, SMART CITY

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One of the overriding objectives of our Low Carbon Energy & Sustainability Strategy is to establish Southend-on-Sea as a 'Low Carbon, Smart City' and determine how we can best ensure that our residents, local communities and businesses can all benefit from a low-carbon lifestyle.

A major focus of our work is to ensure that we consider low-carbon lifestyle choices and sustainability at the heart of our decision-making process and better understand both the risks and opportunities that a changing climate presents for the whole of Southend-on-Sea. By doing so we will be in the best position possible to develop a low carbon economy.

Not only will we need to ensure leadership and encourage innovation in sustainable finance, but we will have to develop our planning system to enable the development of the low carbon economy and actively encourage more energy efficient buildings and transport systems, whilst taking steps to increase the percentage of energy produced by low-carbon means.

Our initial focus has been to establish a team that brings together members of staff from the Energy, Transport, ICT, Health, Planning and Policy teams to work together in order to

identify how we can best integrate the smart city agenda into existing projects and future developments, such as A Better Queensway and the Southend Airport Business Park, whilst identify new funding opportunities to deliver innovative and exciting projects.

For example, through 'A Better Queensway' we want to establish an exemplar project in terms of energy efficiency in new homes, identify innovative means to generate energy and enhance transport in the area. This will help us to harness expertise and build capacity of a low-carbon and more sustainable lifestyle for residents and businesses who will call this development home in the future.

The project will also seek to encourage walking, cycling and the use of public transport, whilst enhancing green space through the use of green roofs, walls and water features. This will help us to better understand and develop best practice in tackling climate change, which we can share to enhance other developments in the Borough.



## EXPLORING THE EUROPEAN CONTEXT

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We are continuing to review the implications of the 2016 referendum, but currently it continues to provide excellent funding opportunities for us to deliver projects that can help us create a more sustainable and vibrant future for all those who live and work in Southend-on-Sea.

Available funding programmes include Horizon 2020, the European Regional Development Fund, Interreg and the LIFE programme. We will ensure that we identify appropriate calls and determine how they can help us best achieve the aims and objectives of our Low Carbon Energy & Sustainability Strategy.

To further showcase our commitment on this important agenda we are in the process of making an application to join the Covenant of Mayors, which is a European-wide movement that commits members to increase energy efficiency and use of renewable energy sources in their territories.

We are a Member of the **EU Smart Cities European Innovation Partnership** and a chair of the '**Small Giants**' EU cluster - a network of very proactive small to medium sized cities that are exploring how they can work together to maximise the benefits that the Smart City agenda can provide cities and towns.

The cluster will also encourage towns and cities to tackle and overcome common challenges, which include providing a secure future energy source for residents and communities, whilst ensuring that urban areas are resilient to the future impacts of climate change.

# 4. SUPPORTING LOW CARBON COMMUNITIES

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Our Vision is to place communities at the heart of Southend-on-Sea's transition to a low carbon and sustainable future.

We will help to facilitate the growth of community led action to help meet local needs, address fuel poverty and develop local energy projects.

We envisage that this will provide our residents, communities and businesses with the opportunity to use more sustainable forms of energy and save money on their fuel bills.



## SOUTHEND-ON-SEA - THE UK'S 'GREENEST' CITY

**We were delighted to have Southend-on-Sea recognised as one of the 'greenest' locations in the UK, in two national reports.**

Southend-on-Sea was recognised as the 'greenest' location in the **2016 UK Vitality Index** - an annual assessment on the health of every large town and city outside of London, an improvement on the second place the town secured in 2015.

The Index identifies those towns and cities that have healthy and expansive economies;

are best placed to support growth; and provide opportunities for businesses to expand.

The top ranking was secured due to an improvement in the proportion of household waste recycled and a reduction in CO<sub>2</sub> emissions.

In addition, Southend-on-Sea was ranked joint first in the **Cities Outlook 2016** in terms of urban environment with the lowest CO<sub>2</sub> emissions per capita. Cities Outlook provide the economic index of the 63 largest cities and towns in the UK.



# CUTTING ENERGY BILLS FOR LOCAL RESIDENTS



In May 2015, we launched Southend Energy - an award winning partnership between Southend-on-Sea Borough Council and OVO Energy (an independent energy supplier) - that provides the Borough with its own energy company for the first time in over 60 years.

Southend Energy is an energy switching initiative that enables residents and businesses within the Borough to get a fairer deal on their energy bills by offering them the chance to switch to a Southend-on-Sea specific energy tariff.

The initiative has so far seen over 4,600 Southend residents take advantage of a reduction in their energy bills and through Southend Energy's green sourcing policy we have saved in excess of 1,200 tonnes of CO<sub>2</sub> in the process!

Further more, we have an agreement in place with OVO to access £250,000 worth of ECO funding to help deliver energy efficiency improvements to households in the Borough that are in fuel poverty by March 2017.

## Overview

**£1.36m**

savings

**6.8%**

market share

**£277**

saved on average

**5025**  
customers



# 5. ENCOURAGING SUSTAINABLE TRANSPORT & TRAVEL

Our Vision is to reduce the carbon emissions, congestion and air pollution that are associated with transport throughout the Southend-on-Sea Borough.

We want to re-think the way we travel. Facilitating the use of electric vehicles is part of the solution, as is developing an integrated public transport system that makes it an easy choice to use the train, bus, walk or cycle for all the members of our community.



## KEEPING THE LIGHTS ON

Our pioneering £13.5 million LED Street Light Replacement Programme has set out to provide Southend-on-Sea residents and drivers with brighter, cleaner light that substantially reduces the Borough's energy consumption and CO<sub>2</sub> emissions.

We became the first Local Authority in England to seal a pioneering financial arrangement with the Green Investment Bank, to accelerate our project to replace all of the Borough's 14,000 street lights with energy - and essentially cost and CO<sub>2</sub> - saving LED street lamps.

The project will cost a total of £13.5 million to deliver and is split between an £8.2 million arrangement with the Green Investment Bank and a £5.1 million central Government grant from the Department for Transport.

The project has also been extended to include all illuminated street furniture, such as lit signs, and replacing or refurbishing ageing street lighting columns.

A new central management system will also allow the Council to remotely manage its lanterns across the Borough.

Within the LED street light project, all of the street furniture is being reviewed in line with new regulations. Bollards and signs are generally being upgraded using reflective surfaces so that they are visible when lit by vehicle headlights but are not lit generally lowering energy use and reducing light pollution.

Some signs will be removed as part of the decluttering initiatives and any sign that is still to be lit is being converted to LED. The review of signs and bollards includes a specific safety assessment.

To date 8,073 columns and luminaires have been changed and the overall project is in line to be completed by August 2017 and generate energy savings of 57%.



## AWARD WINNING IDEAS IN MOTION

Developed with funding from the Department for Transport's Local Sustainable Transport Fund, Ideas in Motion is a campaign that encourages people to think differently and make more active and sustainable travel choices.

It does this by demonstrating the benefits, such as spending less money, getting fit and saving time, and by providing information and support to help them make small changes to their travel habits.

Ideas in Motion's successful approach has been recognised at a number of national award schemes, including shortlistings for the MJ Behaviour Change Award 2014 and the LGC Community Involvement Award 2016.

Ideas in Motion is achieving its goals, with fewer people travelling short distances by car. When the campaign launched, travelling by car was the predominant mode of transport for

journeys under five miles in Southend; but by the end of 2015 more people travelling five miles or less did so by bike (33%) than by car (31%).

The latest independent campaign evaluation data shows that:

30% of people are using the car less, and 19% plan to drive less in the future

46% of people are walking more, and 33% plan to walk more in the future

40% of people are cycling more, and 16% plan to cycle more in the future

The campaign is helping reduce congestion and increase economic growth, while also having a positive impact on public health and wellbeing in the borough.

It is used as an umbrella under which a number of wider Council initiatives, including sustainability, health, education and business projects, now sit.

## 089 MORE FUNDING SUCCESS

Since launching our Low Carbon Energy & Sustainability Strategy we have been looking at how we can build on the success of previous funding bids, including the £4.8 million we obtained through Central Government's Local Sustainable Transport Fund in 2011 and initiatives such as Cycle Southend, E-Bikes and Evalu8.

In March 2016, we were delighted to be part of a consortium that secured close to £1 million from Innovate UK for a project titled '[Utilising Emobility Hubs to Enhance the End to End Journey](#)'.

It is a two year project that will involve the installation of electric vehicle charging posts, an electric car club, bike and e-bikes for hire, a website for bookings and an integrated smart card system.

We were also successful in a Horizon 2020 bid called '[TRACE](#)', which aims to build on the Ideas and Motion campaign by helping to promote walking and cycling throughout the Borough by determining the best use of technology and ICT tracking tools.

# 6. CLIMATE CHANGE ADAPTATION

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Our Vision is to enhance both the natural and historic environment of Southend-on-Sea and ensure that we adopt a proactive and positive response to pressing environmental issues by prioritising local actions, especially around climate change adaptation and resilience measures.

We will play our part in tackling the global issue of climate change and promote a more sustainable future for our residents, communities and businesses in the process.



## A POSITIVE APPROACH TO CLIMATE CHANGE

Southend-on-Sea became the first local authority in Essex to become a signatory to the Local Government Association's Climate Local initiative.

Extreme weather events, such as flooding and heatwaves are occurring with increased regularity due to our changing climate. Climate scientists recognise that today's extreme weather events are happening five times more frequently than before the Industrial Revolution.

The increased scale and number of these weather events represents both risks and opportunities to people, services and communities across the Borough. We need to ensure that we increase our resilience to extreme weather and acknowledge the integral role that we have in responding to the challenge of climate change and build resilience of our services and the wellbeing of local communities.

In June 2015 we were selected by the Environment Agency as the local authority to work in partnership with their Climate Ready team to support action on adapting to climate change across many areas of our operations and services.

The project, entitled Targeted Adaptation Support for Councils (TASC) aimed to help facilitate action on climate resilience by using Climate Ready's tools and methods to support progress at a local level in Southend-on-Sea.

The project was completed in March 2016 and has helped us to enhance our understanding in how we can respond to the challenge of climate change and extreme weather across all departments, services and beyond.

# ENHANCING BIODIVERSITY & GREEN SPACES

Boosting the number of 'green' spaces in Southend-on-Sea provides an excellent opportunity to address the future impacts of climate change and build a more resilient Borough for our residents, communities and businesses.

We have started work with the Essex Wildlife Trust (supported by volunteers) to produce a **State of Nature Report** that will determine the current land use throughout the Borough - for example, how much is grassland or woodland.

This data will be compared to maps created in the 1980s and 1990s to determine how much the Borough's land use has altered over time. We aim to gather information on key species and identify the overall health of our habitats through the Report.

The work will help us to determine the state of nature in the Borough so that informed decisions can be made in the future with regards to management and development and highlight the co-benefits of green space, including enhancing air quality, support for biodiversity and improving health and well-being of residents.

We also launched the initiative **Make Southend Sparkle**, which aims to inspire positive action in the community to help improve the local area. For example, taking part in a beach clean or volunteering some time to help support a local project. It is about providing opportunities for local residents to make a real difference in the town. Tips, advice and inspiration can be found on [www.facebook.com/makesouthendsparkle](http://www.facebook.com/makesouthendsparkle).

## FIVE GREEN FLAGS

In July 2015, five of Southend-on-Sea's parks won prestigious 'Green Flags' - recognition that they are ranked amongst the country's best parks and green spaces.

## CLEAN BEACHES

Our seven miles of seafront are considered as some of the cleanest stretches of seaside in the UK and have been awarded three Blue Flags, seven Quality Coast Awards and Seven 'Keep Britain Tidy Seaside Awards'.

## KEEPING SOUTHEND TIDY

Southend-on-Sea won the Love Essex anti-littering campaign 'Diamond Jubilee Partnership Award' at the 2015 Keep Britain Tidy Awards.

# REDUCING LOCALISED FLOODING

Southend-on-Sea has experienced multiple events of flooding in recent years, in these instances intense rainfall coinciding with high tides resulted in flooding from surface water, sewer and fluvial sources - resulting in damage to residential properties and business premises.

To address this issue we have undertaken a series of initiatives that aim to reduce the risk and impact of localised flooding and help make the Borough's environment cleaner and healthier in the process.

We are working to install a series of Sustainable Drainage Systems ('SuDS') throughout Southend-on-Sea, which will act as drainage solutions that are designed to manage and use the rainwater close to where it falls on the surface and incorporating vegetation.

SuDS techniques can provide resilience to

the predicted increase in frequency and magnitude of rainfall events by providing areas for water storage and increasing the amount of water intercepted by vegetation. Our aim is to ensure that SuDS are deployed in all future developments in Southend-on-Sea, which will help us adapt to climate change and enhance local biodiversity.

We were also able to secure funding from Defra's Repair and Renew grant scheme, which provided allowances of up to £5,000 per property impacted by flooding between 1 April 2013 and 31 March 2014, as well as properties located in known flooding hotspots to install free flood-mitigation measures.

The engineering firm AECOM were appointed to oversee the delivery of the scheme, which included installing flood-mitigation measures on over 100 properties, including flood-defender doors, anti-flood airbricks, flood defender barriers and sumps and pumps.

# WASTE & RECYCLING

We have been able to offer an enhanced and extended waste and recycling service to our residents via a new waste contract that will be delivered by Veolia.

Not only will we save £22 million over the course of the 15 and-a-half-year deal but will benefit from a dedicated beach cleaning machine, weekly recycling and waste collection services, as well as winter gritting.

It was a key consideration to deliver an efficient waste and recycling collection that minimises the amount of waste going to landfill because this has the added benefit of reducing the fuel used to collect and transport the waste, as well as reducing the volume of greenhouse gases generated by waste in landfills.



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# Southend-on-Sea Borough Council

Agenda  
Item No.

11

## Report of Corporate Director for Place

to  
Cabinet

on

20<sup>th</sup> September 2016

Report prepared by:  
Dean Hermitage – Group Manager  
for Planning & Building Control

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### Joint Development Brief for Land at Fossetts Way Place Scrutiny Committee Executive Councillor: Cllr Flewitt

#### A Part 1 Public Agenda Item

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#### 1. Purpose of Report

- 1.1 To seek Members agreement for the Development Brief for the land at Fossetts Way and its adoption as Corporate Policy.

#### 2. Recommendations

- 2.1 That Members note the content of this report and agree that the following recommendation be presented:
- I. **To agree that the development brief is in accordance with the corporate vision and objectives of Southend Borough Council and that the brief be adopted as corporate policy.**
  - II. **To delegate to the Corporate Director for Place in connection with the Portfolio Holder for Housing, Planning and Public Protection to make any minor amendments to the wording of the document as required.**

#### 3. Background

- 3.1 The NHS acquired 5.8ha (14.8acres) of vacant land at Fossetts Way in 2001 with the intention of developing it as a Diagnostic Treatment Centre. Planning permission was granted for the centre in 2005, but it was never taken forward.
- 3.2 In the Autumn Statement 2014, the Government announced an increased ambition for public sector land and committed to releasing enough land for up to 150,000 homes between 2015 and 2020. The site in question is unused public sector land. The Government has encouraged NHS Trusts to dispose of such land, however it seems progress in doing so has been slow, as Trusts have been reluctant to relinquish their land holdings. The Infrastructure Act 2015 now allows for the HCA to assemble unused public land for disposal on behalf of the Government. It is the Trust's desire to oversee the disposal of the land

and therefore retain receipts within the Trust and therefore Southend. The Trust faces a challenging timetable by which to do this.

- 3.3 At present the land is identified as 'safeguarded land' to meet the long terms development needs of the borough. In order to achieve best value for the land, the Trust is seeking an indication from the Council as to what type and scale of development may be acceptable.

#### 4. Preferred Option

- 4.1 Whilst the preferred approach would be to deal with this site through the local plan process (see paragraph 4.4) for the reasons set out in this report, officers recommend the adoption of a joint development brief in order to guide the future development of the site. It is proposed that a brief be adopted as 'corporate policy'. A brief would enable the Council to set out the parameters for the future development of the site and the document would serve as a material consideration when future planning applications for the site are considered.

- 4.2 The content of the brief has been developed jointly by the Trust and Council officers. Key points are summarised as follows:

- The brief will not be a statutory document and does not form part of the Southend Development Plan. It should be read in conjunction with the adopted Development Plan.
- The site, along with neighbouring land to the east and west (Fossetts Farm), was removed from the Green Belt in March 1999 and designated as 'safeguarded land', largely for future employment needs.
- The site remains capable of development and that a high quality residential-led mixed use scheme, which could also include a range of small scale supporting community uses is appropriate.
- All development must meet Southend's Development Plan policies, should seek to address the need for family housing in the borough and provide open space.
- The scale should generally be 2 - 3 storey.
- The development proposals should provide parking in line with the latest applicable standards.
- 30% affordable housing and CIL contributions will be required.
- A high quality design approach will be expected, which should be highly sustainable including minimising the use of natural resources, the integration of renewable energy, sustainable transport measures and ecology.

- 4.3 The draft development brief is attached as **Appendix 1**.



## **Other Options**

### *Local Plan Review*

- 4.4 The National Planning Policy Framework (NPPF) advises that planning permission for the permanent development of 'safeguarded land' should only be granted following a Local Plan review which proposes the development. The local plan-making process would allow for development to be planned at the strategic level to ensure that there is a balance of land uses, which provide for the needs of the local and wider community now and in the future in the most sustainable locations. The Local Plan review process is likely to take approximately 24 – 36 months, which would not assist the Trust in being able to dispose of the site prior to it being disposed centrally (as discussed in 3.2 above). Given the likely timetable for disposal the preparation of a brief has been necessary in the circumstances.

### *Do Nothing*

- 4.5 The land would either be sold as employment land by the Trust and a future buyer would then likely seek a change of use to achieve greater values; or the land would be disposed of centrally at a later date. The Council may not have the opportunity to guide the future development of the site without a brief.

## **5. Reasons for Recommendation**

- 5.1 To enable a set of development parameters and the vision for the future of development of the site to be conveyed, in the interests of bringing the site forward as a development that will meet local needs, provide a high quality development, and to be developed in a manner that takes account of other development proposals and strategic planning issues in the Borough.

## **6. Corporate Implications**

### **6.1 Contribution to Council's Vision & Corporate Priorities**

The proposed policy will support the following Corporate Priorities for 2016/17:-

Prosperous: Maximise opportunities to enable the planning and development of quality, affordable housing.

Healthy: Work with the public and private rented sectors to provide good quality housing.

### **6.2 Financial Implications**

Any financial contributions received through S106 agreements and CIL will be spent on enabling the Council to deliver affordable housing and infrastructure within the Borough.

At this stage these sums have not yet been determined but will be identified as the Development proposals progress.

### 6.3 Legal Implications

In terms of planning legislation, a Corporate Policy will be given less weight than an adopted planning policy. However, a Corporate Policy will constitute a material consideration in the determination of any planning applications; and until such time that the Local Plan review is concluded.

### 6.4 People Implications

None.

### 6.5 Property Implications

None.

### 6.6 Consultation

No public consultation required as the document is not proposed to be adopted as a formal planning document. Consultation will take place at planning application stage in accordance with the Southend on Sea Statement of Community Involvement.

### 6.7 Equalities and Diversity Implications

The proposed Corporate Policy seeks to ensure an equitable approach to housing and infrastructure delivery.

### 6.8 Risk Assessment

There is a risk that the site could be developed in isolation, whereas as part of the Local Plan review option it could be considered alongside neighbouring sites. However, this point is raised within the brief.

### 6.9 Value for Money

The proposed Corporate Policy is an important means of ensuring value for money for the wider community from development.

### 6.10 Community Safety Implications

None.

### 6.11 Environmental Impact

Consideration as to Environmental impacts and Habitats' screening is required at planning application stage.

## 7. Background Papers/Reference Documents

None

## 8. Appendices

### Appendix 1 Draft Joint Development Brief for Land at Fossetts Way.

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# INFORMAL DEVELOPMENT BRIEF LAND AT FOSSETTS WAY



Prepared by:  
Southend NHS Trust and Southend on Sea Borough Council  
July 2016                  Final Draft



## Contents

- 1 Introduction
- 2 Site Description and Context
- 3 Site Constraints and Design Parameters
- 4 Planning Policy Context
- 5 Development Guidelines
- 6 Summary

## Appendices

- 1 Planning Permission 04/00550/FUL
- 2 Prittlewell Camp Scheduling Notice
- 3 Royal Commission for Historical Monuments England assessment of Prittlewell Camp
- 4 Examples of Exemplar Housing Developments

	<b>Document Control</b>		
1	AP Initial Draft	29/04/2016	
2a	Dean Hermitage Comments	20/05/2016	With track changes
2b	Matthew Thomas and AG comments	31/05/2016	With track changes and additions
3	AP Revised Draft	07/06/2016	Incorporating all track changes and additions plus further additions
4	Site Boundary Plan Amended	20/06/2016	
4a	SBC Comments added	28/06/2016	
5	Comments on S106 added	04/07/2016	
6	Minor typos corrected	08/07/2016	
Final	V6 Approved by DH final questions resolved; appendices added	26/07/2016	

# 1.0 Introduction

This development brief sets out informal planning guidance for the redevelopment of approximately 5.8ha (14.8acres) of vacant land at Fossetts Way in Southend on Sea. This site, referred to hereafter as the subject site, is within the ownership of **Southend University Hospital NHS Trust** and was allocated as ‘Safeguarded Land’ for employment purposes within the second alteration to the Southend Local Plan. It is now recognised that the site has long term development potential to provide a high quality, sustainable mixed-use residentially led scheme to provide new housing and supporting uses.



## Status of this document

This brief provides a framework for guiding and managing any planning application submission brought forward on the site. It is not a statutory document and does not form part of the Southend Development Plan. It should be read in conjunction with the Southend-on-Sea Core Strategy (2007). Although it will not be formally adopted by the Council as a Supplementary Planning Document, it has been the subject of public consultation and has been adopted by the Council as corporate policy and will be considered as a material consideration in the determination of any planning application submitted for the development of the site.

## 2.0 Site Description and Context

The site extends to approximately **5.8 ha** and is located within the wider Fossett's Farm site, which totals to over 30 ha. The site is irregular in shape and comprises vacant 'safeguarded' land, formerly in agricultural use. It lies on the northern edge of both the Southend urban and administrative area.

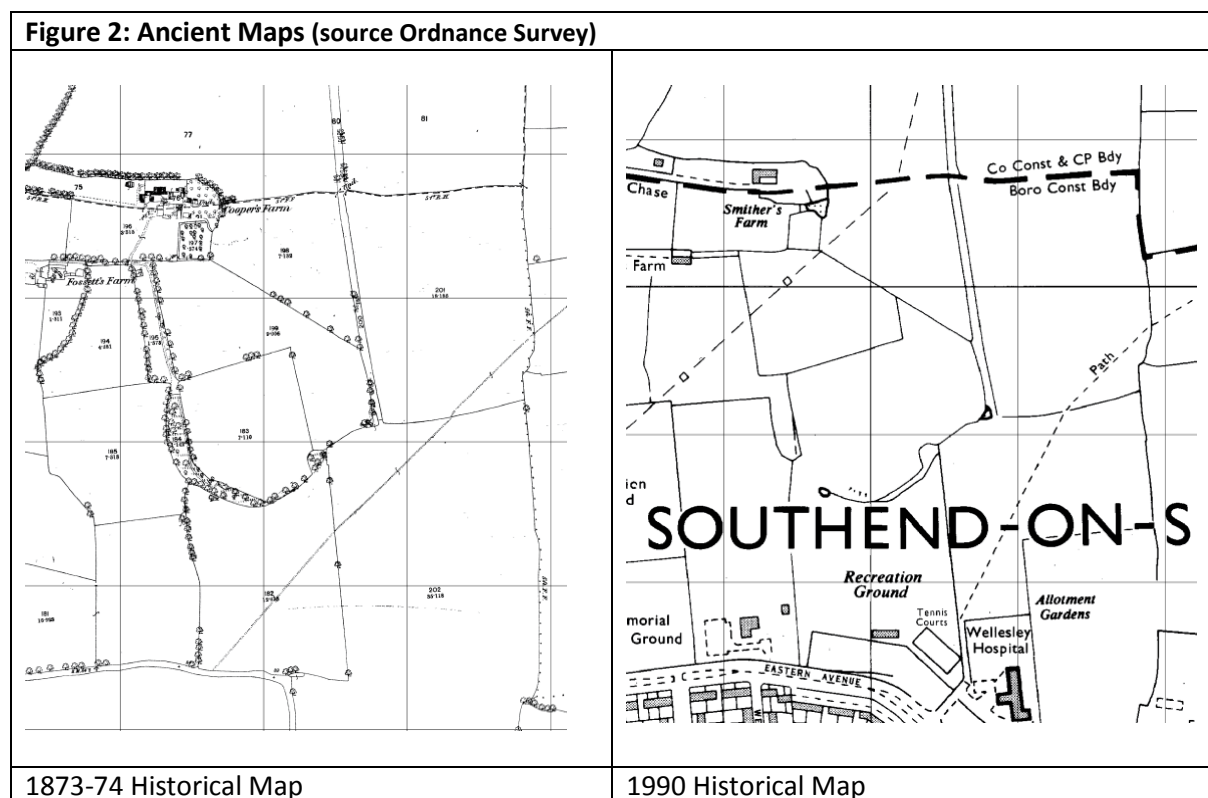
The immediate site boundaries are defined to the north by the Fossetts Way Link Road where there is an access already constructed into the site. A green lane, running adjacent to Fossetts Way, forms the eastern boundary. To the west the site adjoins further disused arable fields. To the south, the site is bordered by a large circular enclosure known as Prittlewell Camp, a Scheduled Monument (SM).

The wider Fossett's Farm site was removed from the green belt and safeguarded for development in the Southend-on-Sea Borough adopted local plan second alteration of March 1999. Fossetts Way was constructed as a link road to serve the new development area.

To the south of the SM, lies an area of out of centre retailing comprising a Waitrose superstore with petrol filling station, a large B&Q retail warehouse and a small freestanding terrace of retail warehouses (Majestic Wine, Maplin and Vacant) known as Fossetts Park. The Spire Wellesley hospital lies to the south of the retail terrace, overlooking the junction of Fossetts Way and Eastern Avenue.

### Previous Use

The historic use of the site is for agricultural purposes. There is no evidence of any other use since ancient times.



### Planning History

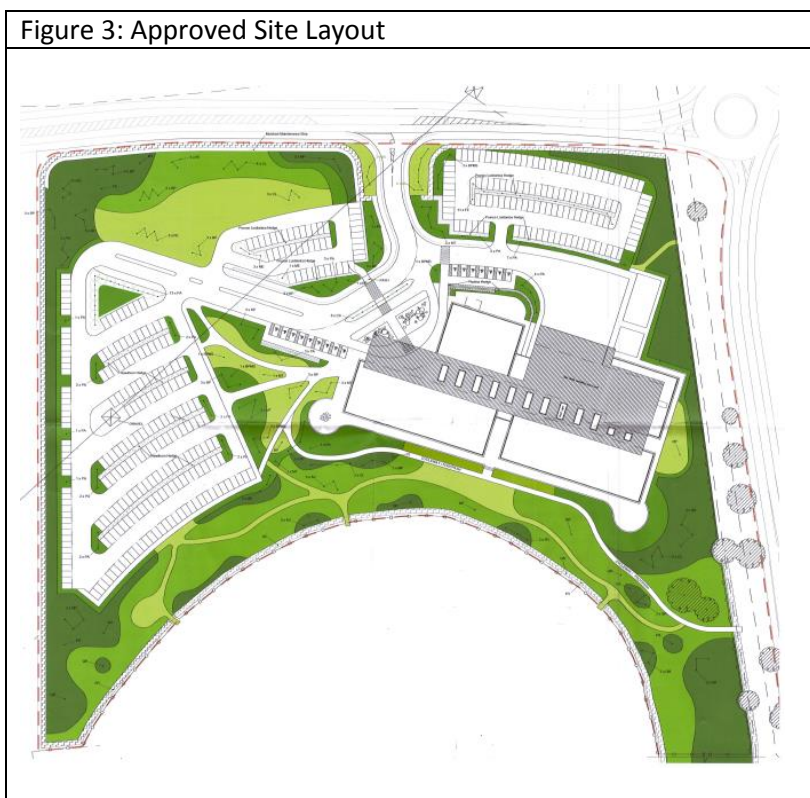
Fossetts Farm Link Road was constructed under planning permission SOS/03/00884/FUL as a planning obligation attached to the development of a B&Q Warehouse to the south-east of the subject site, which itself was approved in 2004 subject to a S106 Planning Obligation.



Under the terms of the S106, the subject site falls within the 'Restricted Area' where the procuring, application or development for certain specified land uses, including housing, is restricted until such time as the council serve on the developer a 'Restricted Area Release Certificate'. No such certificate has yet been served but it is intended that one will be produced in conjunction with this brief.

On 29 November 2005 full planning permission (reference 04/00550/FUL) was granted for a "diagnostic and treatment centre" comprising four 2 storey blocks, totalling 11,800 sq m (126,968 sq ft) gross floorspace, arranged around a central atrium. The buildings had a mix of flat and curved roofs with the external materials comprising a mix of rendered walls and timber cladding (red cedar and Siberian larch) with a zinc covering on the curved roofs. The maximum height of the buildings was c.30m (100 ft). The approved Site layout is shown in **Figure 3** below and a copy of the decision notice is included in **Appendix One**. This consent was never implemented and has subsequently lapsed.

Figure 3: Approved Site Layout



### Nearby Development

There have been several iterations of a scheme for the development of a new football stadium on land adjacent to the west of the application site. Initial proposals were granted on appeal (06/01300/FUL) in 2007 for a 22,000 seater football stadium and associated facilities. This permission has since lapsed however Southend Borough Council made a resolution to approve a revised scheme (11/00583/EXTM) in April 2013 for 22,000 seater stadium together with 114 bedroom hotel, 67 apartments, 16,400 sq m retail floorspace and 279 sq m restaurant supported by 2,074 space car park. More recently two scoping opinions have been issued. At the end of 2015 (15/01603/RSO) in respect of a new scheme for a 21,000 seater stadium and associated training facilities, 74 apartments, 32,000 sq m retail and restaurant floorspace, a 12 screen cinema and parking for up to 1590 cars. During May 2016 (16/00654/RSO) in respect of a 21,000 seater stadium and associated training facilities, 2 soccer domes, 131 bed hotel, 257 apartments, 25,200 sq m retail and 4,153 sq m restaurant floorspace, a 12 screen cinema and parking for up to 2330 cars.

Planning permission (11/00224/TIME) was granted by Rochford District Council for three outside training pitches, an all-weather floodlit training pitch and surface car parking on land to the north of Fossetts Way. This permission has been implemented although it has not been completed.

Two retail warehouses have subsequently been constructed to the east of Fossetts way (04/01785/FUL) and a Waitrose supermarket to the west.

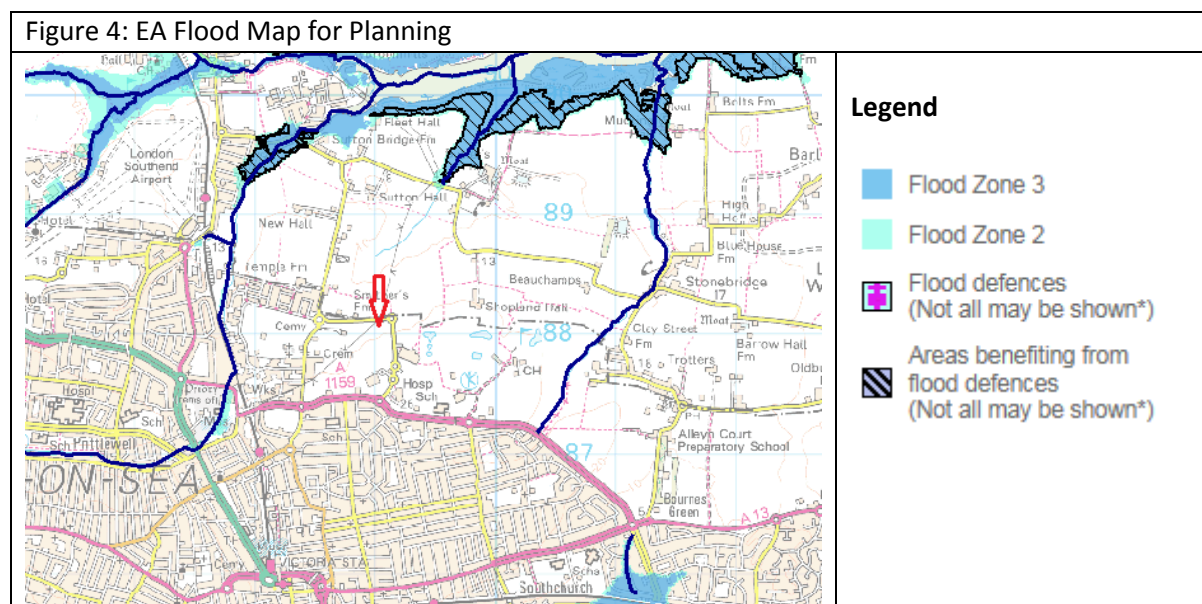
### **Accessibility**

Fossetts Way is served by a single bus route providing approximately half hourly services in each direction. The nearest stops are close to the Sutton Road junction to the west and outside the B&Q car park to the east. There is an off road cycleway along the full length of Fossetts Way, comprising a shared foot and cycle path on both sides of the road.

### 3.0 Site Constraints and Design Parameters

- **Flood Risk**

The Environment Agency’s ‘Flood Map for Planning’ shows the site lying within Flood Zone 1 which is defined as having a ‘low probability’ of flooding defined as less than 1 in 1,000 annual probability of river or sea flooding. A Flood Risk assessment addressing sustainable drainage will be required as the site is in excess of 1ha.



- **Ecology**

There are no formal Nature Conservation designations relating to the site. A reptile survey was undertaken in September/October 2004 which found that there was a small population of common lizards located in the adjacent hedgerows and the rank grassland fringing these. A new ecological survey will be required as part of new development proposals.

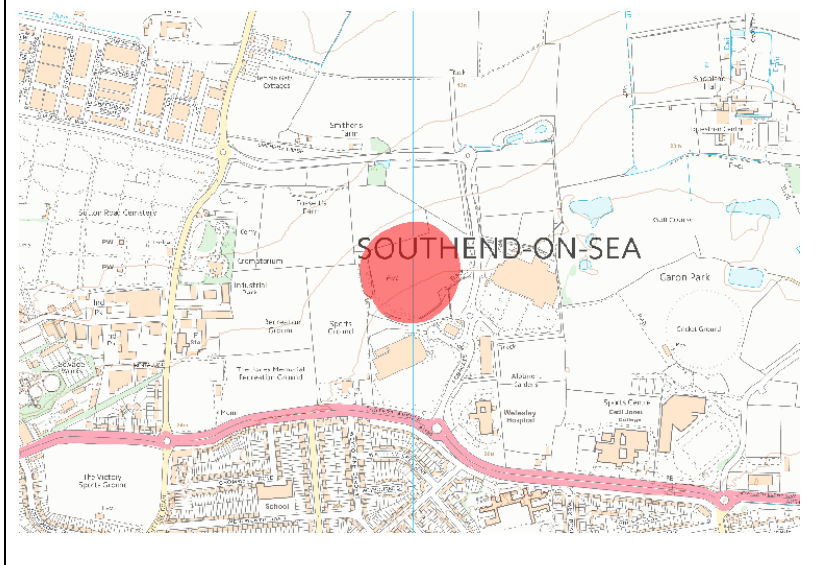
- **Heritage**

There are no listed buildings on site or in the vicinity and the site does not fall within a conservation area. There is a Scheduled Monument immediately abutting the southern boundary of the site known as “**Prittlewell Camp**” described as “a slight univallate hillfort” (list entry number 1017515). A copy of the list entry is attached at **Appendix Two**.

The extent of the Scheduling is shown in **Figure 5** below. It includes the earthworks of a circular enclosure and a mound on its south-eastern bank.

The hillfort dates from the later Bronze Age or Early Iron Age and is nearly circular in plan, measuring approximately 250m in diameter. The south western third of the perimeter is defined by an earthen bank and external ditch which survive within a wooded belt. The bank averages 3.5m in width and 0.9m high. The ditch is less clearly visible, having been partly used as a corporation dump in the 1920s, although some sections remain exposed and measure up to 4m in width and 1.4m in depth.

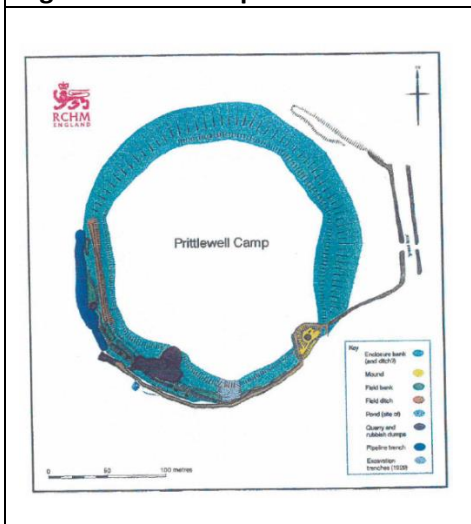
**Figure 5: Extent of Scheduling**



The northern and eastern sections of the ramparts have been reduced by ploughing, although undulations marking the line of the defences were noted in the early part of this century and the line of the bank has been recorded from the air as a cropmark (a variation in crop growth caused by buried features). Observation of a pipeline trench to the Barling Outfall Works in 1929 revealed that the external ditch may not have continued around this side of the hillfort, perhaps as the approach from this side was already restricted by marshy land.

The site has been the subject of several archaeological investigations including by Essex County Council in advance of cutting the Rochford to Southend pipeline in 1997. The former Royal Commission for Historical Monuments England undertook a survey of the site in 1999. **Figure 6** comprises their interpretation of the site based on this survey. The survey document is attached at **Appendix Three**.

**Figure 6: AM Interpretation**

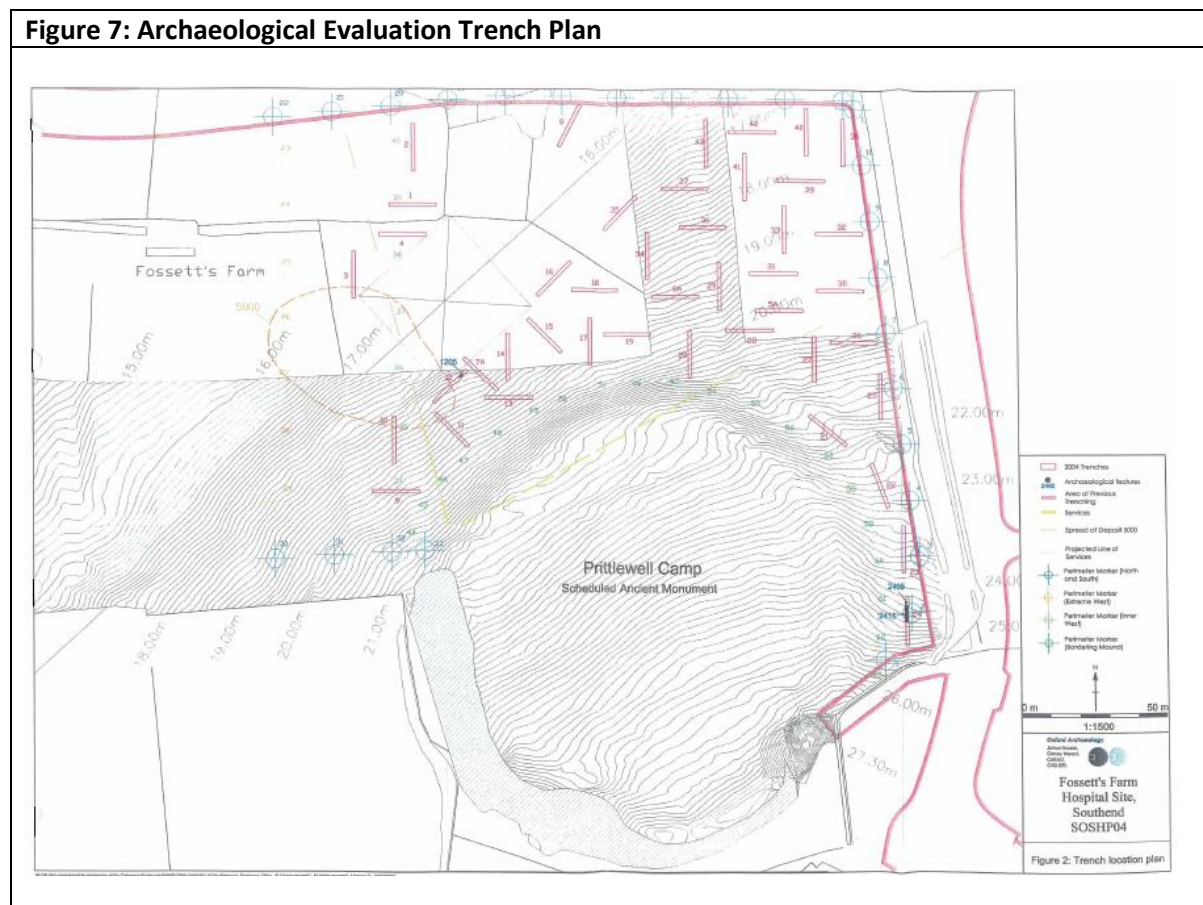


The planning application for a diagnostic and treatment centre in 2003 (see above) was accompanied by a Desk Based Archaeological Assessment and Archaeological Evaluation Report based on 43 trenches (each measuring 25m length by 2m width) across the subject site. The location of the trenches is shown in **Figure 7** below.

The Evaluation found significant archaeological deposits were sparse with only two trenches (numbers 12 and 24) revealing any significant finds. Trench 12 produced Post-Medieval deposits whilst Trench 24 produced early deposits including some worked flints, flint flakes, bone and pottery fragments and a possible hollow way which may have led to the entrance to the camp.

Evidence of a large municipal dump was also found on the western part of the site. The report concluded that the low density of remains may indicate that any early settlement or activity was confined to the hilltop whilst later Medieval and Post-Medieval activity has consisted solely of agricultural land use.

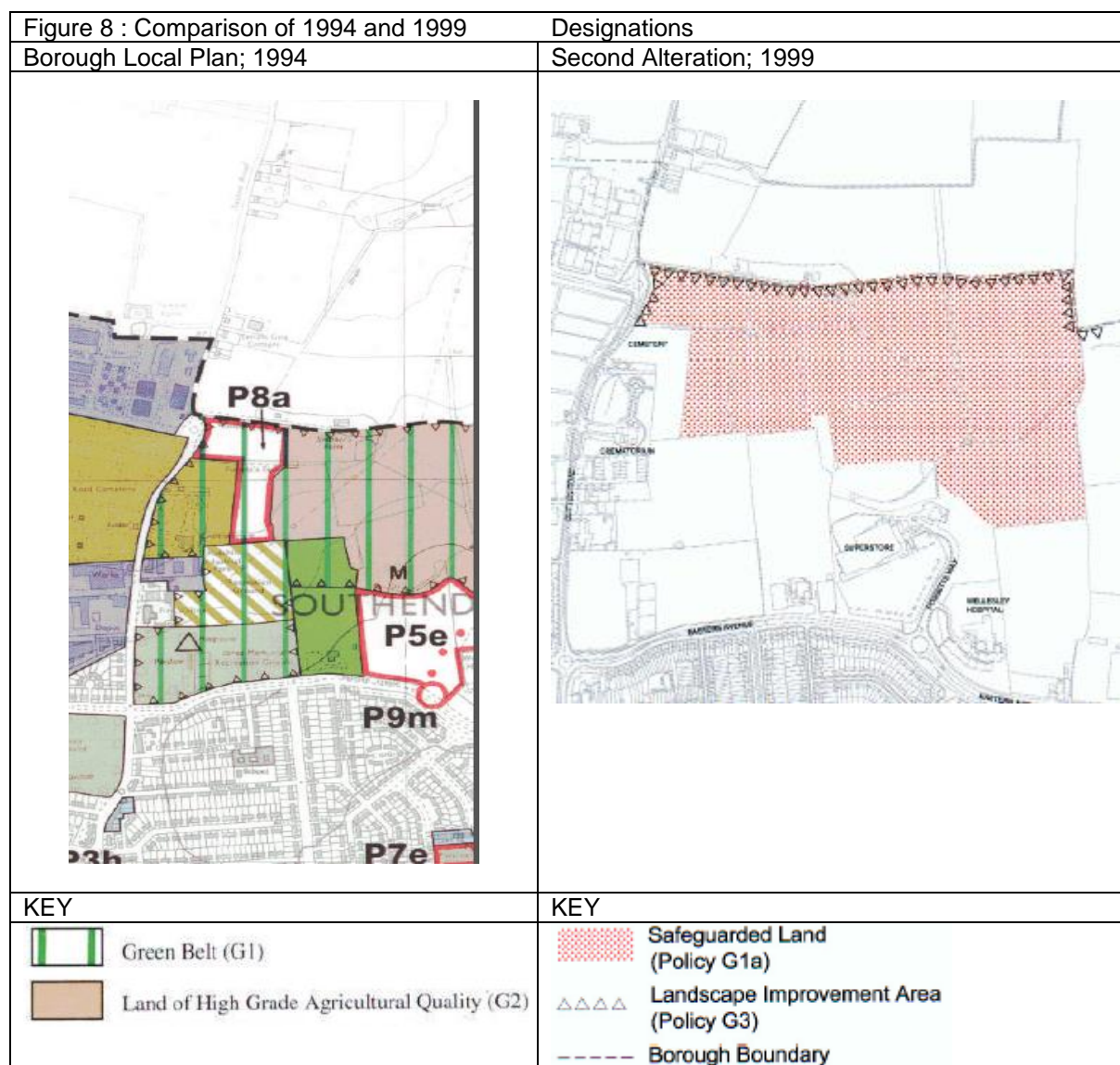
**Figure 7: Archaeological Evaluation Trench Plan**



## 4.0 Planning Policy Context

**The Southend-on-Sea Borough Local Plan** – adopted in 1994 – provided the statutory framework and forms part of the development plan for Southend. It shows the site as falling within the green belt and as land of high agricultural quality. Following a successful legal challenge to this designation by Swan Hill Developments, the Council was required to re-examine the designation of this site.

**The Second Alteration Plan** was adopted in March 1999 and designates land at Fossetts Farm as ‘safeguarded land’, subject to new policy G1a. The land was removed from the green belt but was not intended to be developed until beyond 2001, after the end of the plan period, to meet long term development requirements examples of which at that time were considered to include employment, a football stadium, or crematorium /cemetery extension. The Planning Inspector appointed to oversee the Second Alteration recommended that the site could be suitable for a number of possible future uses but that no development should be permitted that would prejudice or limit options for comprehensive redevelopment.



The new policy introduced in the 2<sup>nd</sup> Alteration applying to the safeguarded land reads as follows:

**POLICY G1a - SAFEGUARDED LAND**

*On the Safeguarded Land defined on the Proposals Map, no development, including change of use, will be permitted unless it is necessary in order to support an existing use of the land; in any event no development will be permitted which would prejudice or limit options for the possible comprehensive development of the land to meet any future development requirements.*

The Foreword makes clear that the Second Alteration will guide development relating to land at Fossetts Farm up to 2001. This policy, however, remains in place pending the Local Plan review.

The **Core Strategy** was adopted in December 2007. The Core Strategy Key Diagram, which illustrates the Key Growth and Regeneration Areas, shows the area of Fossetts Farm as an Industrial/Employment Area.

Policy KP1 sets out the spatial strategy and designates it as a Priority Urban Area defined on the Key Diagram and also expressly supports the principle of the relocation of Southend United Football Club stadium to the Fossetts Farm area.

The **Development Management Development Plan Document** was adopted in July 2015 and is accompanied by the Policies Map. An extract from this map is shown at **Figure 9** below. It does not show any designations affecting the site albeit the broad location of Prittlewell Camp is shown and the site remains outside the green belt. Appendix 9 identifies those saved policies which are replaced by the DMDPD and these do not include policy G1a.

**Figure 9: Extract from Polices Map Adopted Version – June 2015**



## Commentary

The subject site, along with neighbouring land to the east and west, was removed from the green belt in March 1999 and designated as 'safeguarded land'. At that time it was intended that the site would be developed after 2001, which was the end of the plan period, to meet long term comprehensive development requirements.

The character of the area has changed significantly since that time with the construction of Fossetts Way, which effectively separated the subject site from the open countryside. Fossetts Way also opened up the area for development with the construction of Waitrose, B&Q and the Fossetts Park retail scheme. Provision was also made for the development of the safeguarded land with access points being constructed as part of the road development to the subject site and the land to the east.

Planning permission has been granted on the subject site for a health related use, although this has now lapsed. Similarly planning permission has also been granted for a football stadium and related facilities on land to the west of the subject site although this too has lapsed. There remains an implemented permission on land to the north for training facilities for the football club.

New national planning policy was published in 2012, the National Planning Policy Framework. It advises that local plans should make clear that the safeguarded land is not allocated for development at the present time and that planning permission for the permanent development of safeguarded land should only be granted following a Local Plan review which proposes the development. The 2<sup>nd</sup> Alteration complied with this approach, both pushing back development until after 2001 and safeguarding the land to meet long term development requirements. The flexibility inherent in this approach is recognised and reinforced by the planning permissions which have subsequently been granted.

The construction of Fossetts Way opens up the potential to development the land as separate parcels but still forming part of a comprehensive approach to the whole area. This approach is a continuation of the approach to the retail area to the south of the site and allows a managed approach to the development of a large area of land in different ownerships.

The management of development proposals will need to co-ordinate the design approach to the development of the area and the next section identifies those considerations which are relevant to the subject site. This consideration can also be applied to the land to the east, currently in the ownership of Lansbury Holdings, to allow for comprehensive development and a second phase rather than 2 separate developments which may not acknowledge their close proximity and relationship – this should be considered through the respective design and access statements or more comprehensive masterplanning of the two sites.



## 5.0 Development Guidelines

The subject site has been removed from the green belt and safeguarded for development post 2001, a criterion which has now been met. The acceptability of the site for development has previously been recognised and a highway access point constructed. There are constraints in terms of its relationship with the adjacent Scheduled Monument but these were delineated in the earlier permission. At the current time it is accepted that the site remains capable of and is suitable for development, and that a suitable form of development is a sustainable, high quality residentially led mixed use scheme, which could also include a range of small scale supporting community uses.

### Mix of Uses

#### (i) Residential

The housing policies seek to ensure that new housing reflects the needs and demands of Southend's existing and future residents and improves the quality and mix of new market and affordable housing in the Borough. The SHMA identified a shortage of family accommodation in Southend despite an acute demand for this type of dwelling. To address this shortfall and meet demand, residential development proposals are expected to incorporate suitable family accommodation including affordable family homes. This site is considered well situated to help meet this requirement in accordance with the preferred mix set out in the Development Management DPD as shown below:

Housing Type	Dwelling Mix			
	1-bed	2-bed	3-bed	4-bed
Market Units	9%	22%	49%	20%
Affordable Units	16%	43%	37%	4%

The Affordable Housing target is 30% of units to be delivered in accordance with a registered provider. The indicative tenure mix is 60:40 between social and/ or affordable rented accommodation and intermediate housing although this will need to be adjusted to take into account the emerging requirements for starter homes.

The size of each dwelling should comply with the "Technical housing standards – nationally described space standard" published by the Department for Communities and Local Government.

The internal environment of all new dwellings must be high quality and flexible to meet the changing needs of residents. To achieve this all new dwellings should meet Building Control requirements and at least 10% should be either wheelchair user dwellings, or easily adaptable for residents who are wheelchair users.

The development should be safe and secure and take account of crime prevention and community safety considerations as required by Building Regulations Part Q.

#### (ii) Local Service Uses

Small scale local community services can be provided to meet the needs of local residents. These services could include a local convenience store and/or local shops providing related services (such as hairdressing, off-licence, take away etc.) or community facilities such as a doctor's surgery or child care facilities.

#### (iii) Open Space

A landscaped buffer to Scheduled Monument of the same scale as provided in the previous planning permission will be required. This area should be landscaped in accordance with any approved plans

and then made over to SoSBC with a commuted payment (through a S106 obligation) for 10- year maintenance provision.

#### **(iv) Play Area**

A small play area for younger children should be included within the buffer area to the SM or other open space within the development. This needs to be conveniently placed with good natural surveillance.

#### **Scale of Development**

The scale should generally be 2-3 storey although there may be scope for some slightly larger scale, say 3- 4 storeys, to western side of site if the proposed new stadium is approved. Development along this edge of the site needs to be designed so that it does not appear out of place if stadium is not built and so the development context on this boundary is dependent on the form and scale of the development coming forward on the adjacent land.

#### **Sense of Place**

The site is presently isolated from other residential areas so there is an opportunity for a new character to be created (a new community) – integrating the development into the landscape and setting of the SM and creating sustainable, low carbon, high quality homes should be key drivers. This development should create a best practice or exemplar example for any future urban extensions which may be considered in Southend or the surrounding area. Urban greening needs to be considered as part of the design process for any development (such as green roofs). Development should not be car dominated in its approach to the design, although recognising the need to park and store motor vehicles even if they are not used regularly.

A number of best practice examples are cited at end of this document as a guide for developers.

#### **Sustainability**

Development proposals on the site should be low carbon and seek to make a reduction in the use of resources, including the use of renewable and recycled resources, and demonstrate how they will maximise the use of renewable and recycled energy, and minimise consumption of water and other resources. This applies during both construction and the subsequent operation of the development. Opportunities for urban greening techniques should be explored at the design stage to assist with creating a low carbon development and to help integrate the development into the surrounding countryside.

Thermal insulation should seek to exceed building regulation requirements and, wherever feasible, and to meet the requirements of Policy KP2, at least 10% of the projected energy needs of the new development must come from on-site renewable options (and/or decentralised renewable or low carbon energy sources). The development will also provide for the collection of re-usable and recyclable waste.

Surface water drainage should adopt SuDS principles and should be an integral part of the landscaping scheme.

#### **Landscaping**

The new housing should be set within a natural and informal landscape context to reflect the edge of the urban area setting. The buffer to the SM should be landscaped to enhance its setting (i.e. as an extension of its character with natural planting to provide low impact delineation of the SM). A clear view corridor from the SM to the open countryside to the north should be maintained through the

site which could be along an access road or linear public space. The exact location of this should be agreed with the local planning authority at an early stage in the design process. The houses surrounding the SM should face onto the open space rather than turn their back to ensure good natural surveillance and enhance the setting of the monument. The buffer should include pedestrian and cycle links and the playspace.

### **Ecology**

Notwithstanding previous surveys, a new survey of the whole site is required due to the passage of time. The ecological analysis should identify any necessary mitigation measures and also inform the landscape character of the development, especially the planting scheme.

### **Heritage**

The proximity of the Scheduled Monument and related archaeology across the site will be the key considerations. The built development will need to leave a landscaped buffer of comparable scale to that approved in the 2005 planning permission (reference 04/00550/FUL). The need for additional archaeological evaluation will be determined once the layout of new development is known.

### **Sustainable Transport**

Due to the size and location of the site there is a good opportunity to influence the approach to sustainable travel in the local area, with the location of the site being suitable to encourage future residents and employees to walk, cycle or use public transport.

The site is located in close proximity to public transport facilities with bus stops and main food shopping facilities located within a short walk of the site. The site should be permeable for pedestrians and cyclists with good connections to all sides in line with Building for Life criteria.

To promote sustainable travel the scheme would prioritise quality direct routes and spaces for pedestrians and cyclists over private cars. Measures would include the provision of high quality pedestrian and cycle routes and facilities, including cycle parking, and the production of a Travel Plan covering all land uses within the site. Electric vehicle charging points, parking spaces for electric vehicles should also be included.

### **Parking and Access**

The development proposals should provide parking in line with the latest applicable standards (currently set out in the Development Management DPD). The site will provide a balanced level of parking and allow for on-street parking within new estate roads, whilst not providing excessive parking which could encourage the use of the private car.

The site will also provide sufficient cycle parking in line with the latest applicable standards for all land uses. These provisions would encourage the opportunity to travel by bike, which will be supported by an improved public realm and connecting links. Options needs to be considered for integrate the development into existing bus routes as part of the design of the layout.

For convenience, the current car and cycle Parking Standards are:

Dwelling Size	Car Parking	Cycle Parking
	Minimum Standards	Minimum Standards
1 Bedroom Dwelling	1 space per dwelling	1 secure covered space per dwelling.
2+ Bedroom Dwelling	2 spaces per dwelling	None if garage or secure area is provided within curtilage of dwelling

Retirement developments (e.g. warden assisted independent living)	1 space per dwelling	1 space per 8 units (for visitors)
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## Infrastructure Costs

### (i) Planning Obligations

The Council adopted an SPD on Planning Obligations in July 2015 to explain how the Planning Obligation regime will operate following the introduction of CIL. The Council will normally require a planning obligation where it is felt that a proposed development of whatever nature or scale, will in itself give rise to unacceptable pressure on public infrastructure or where the development is of such a nature or scale that it is considered that it should contribute to the supply of affordable housing in the Borough. Obligations will not be sought in respect of infrastructure projects to be funded through CIL as set out in the Council's 'Regulation 123 Infrastructure List'.

### (ii) Community Infrastructure Levy

The CIL Charging Levy came into effect on the 27 July 2015. The site falls within Zone 1 where the CIL rates are as shown in the table below:

Development Type	CIL Rate per sq m
Residential (Classes C3 and C4)	£20
Extra Care Retirement Housing	£20
Supermarkets, superstores and retail warehousing (net retailing space over 280 sq m)	£70
Development by a predominantly publicly funded or 'not for profit' organisation <sup>4</sup> (see below for definition) including medical and health services, social care, education, emergency services, waste facilities, community facilities, sport and leisure facilities only	£0
All other uses not cited above	£10

There are exemptions available for minor development, residential annexes or extensions, development by registered charities, affordable housing and self-build housing. Any claim for relief/exemption from paying CIL must be submitted and processed before commencement of the development.

## 6.0 Summary

This document sets out planning guidance for the redevelopment of vacant former agricultural land fronting Fossetts Way. The overall objective for the site is to provide a sustainable high quality exemplar residential led mixed development, potentially with complementary retail and service uses. The residential units should predominantly comprise family housing reflecting the housing needs of the Borough and should be set within a landscape context which preserves the setting of the Prittlewell Camp Scheduled Monument and a view corridor from there to the open land to the north. The development should also consider its relationship with other potential neighbouring and similar development sites as part of more comprehensive development. Indeed much of the guidance contained within this document will also be relevant to these areas when they come forward for development. It is expected that this site will be the first phase and set the benchmark for the quality and approach to new residential led development in this area.

In addition to a high quality design approach the site will be expected to be highly sustainable including minimising the use of natural resources, integration of renewable energy, sustainable transport measures and ecology. The potential for a heat network should also be explored as part of wider development of the area.

There is the potential for other complementary uses to be provided on the site. The level of these should be that required to support the amount of residential development proposed. The exact number and mix of uses and facilities, which will be considered on their own merits during the application process, and should complement the remainder of the development and not be in conflict with the provisions of the development plan.

**Appendix 1**

**Planning Permission 04/00550/FUL**

## **Appendix 2**

### **Prittlewell Camp Scheduling Notice**

# **A slight univallate hillfort known as Prittlewell Camp, 500m east of Sutton Road crematorium**

## **List Entry Summary**

This monument is scheduled under the Ancient Monuments and Archaeological Areas Act 1979 as amended as it appears to the Secretary of State to be of national importance. This entry is a copy, the original is held by the Department for Culture, Media and Sport.

Name: A slight univallate hillfort known as Prittlewell Camp, 500m east of Sutton Road crematorium

List entry Number: 1017515

## **Location**

The monument may lie within the boundary of more than one authority.

County:

District: Southend-on-Sea

District Type: Unitary Authority

Parish:

National Park: Not applicable to this List entry.

Grade: Not applicable to this List entry.

Date first scheduled: 27-Feb-1961

Date of most recent amendment: 08-Dec-1997

## **Legacy System Information**

The contents of this record have been generated from a legacy data system.

Legacy System: RSM

UID: 29408

## **Asset Groupings**

This list entry does not comprise part of an Asset Grouping. Asset Groupings are not part of the official record but are added later for information.



# List entry Description

## Summary of Monument

Legacy Record - This information may be included in the List Entry Details.

## Reasons for Designation

Slight univallate hillforts are defined as enclosures of various shapes, generally between 1ha and 10ha in size, situated on or close to hilltops and defined by a single line of earthworks, the scale of which is relatively small. They date to between the Late Bronze Age and Early Iron Age (eighth - fifth centuries BC), the majority being used for 150 to 200 years prior to their abandonment or reconstruction. Slight univallate hillforts have generally been interpreted as stock enclosures, redistribution centres, places of refuge and permanent settlements. The earthworks generally include a rampart, narrow level berm, external ditch and counterscarp bank, while access to the interior is usually provided by two entrances comprising either simple gaps in the earthwork or an inturned rampart. Postholes revealed by excavation indicate the occasional presence of portal gateways while more elaborate features like overlapping ramparts and outworks are limited to only a few examples. Internal features included timber or stone round houses; large storage pits and hearths; scattered postholes, stakeholes and gullies; and square or rectangular buildings supported by four to six posts, often represented by postholes, and interpreted as raised granaries. Slight univallate hillforts are rare with around 150 examples recorded nationally. Although on a national scale the number is low, in Devon they comprise one of the major classes of hillfort. In other areas where the distribution is relatively dense, for example, Wessex, Sussex, the Cotswolds and the Chilterns, hillforts belonging to a number of different classes occur within the same region. Examples are also recorded in eastern England, the Welsh Marches, central and southern England. In view of the rarity of slight univallate hillforts and their importance in understanding the transition between Bronze Age and Iron Age communities, all examples which survive comparatively well and have potential for the recovery of further archaeological remains are believed to be of national importance.

Despite having been reduced by ploughing and obscured by dumping, the slight univallate hillfort known as Prittlewell Camp remains substantially intact and will retain significant archaeological information. The circuit of defences is clearly defined by earthworks to the south and west and evidence exists for the buried remains of the remaining part of the circuit. Buried features related to the period of occupation will survive beneath the ploughsoil of the interior and these, together with the earlier fills of the surrounding ditch, will contain evidence for the date of the hillfort's construction and for the duration and character of its use. Environmental evidence reflecting the appearance of the landscape in which the monument was set and the economy of its inhabitants may also survive in these buried deposits and on the old land surface sealed beneath the bank.

The hillfort's location on a low-lying plateau rather than a summit or ridge is somewhat unusual, although far from unique within the low-lying topography of the region. Comparison between these sites and, more specifically, with other forms of contemporary habitation between the Roach and the Thames, will provide valuable information concerning the hillfort's position in the settlement pattern and social structure of the period.

Although the interpretation of the 'Look-out' mound as the base of a medieval post mill has not been proven, evidence from the 1929 excavation does support this conclusion. Such mounds were designed to raise the windmill and to stabilise a vertical post (or tree) which

allowed the superstructure to be turned to face the wind. Post mills are known to have existed from the 12th century onwards and although no medieval examples of the timber superstructure survive today, their appearance is recorded in contemporary illustrations. Examples of mounds which retain organic remains or form components of other sites are considered worthy of protection. The mound at Prittlewell, located on the line of the earlier defences, provides an interesting insight into the subsequent use of the hillfort and the medieval economy of the surrounding area.

## History

Legacy Record - This information may be included in the List Entry Details.

## Details

The monument includes a slight univallate hillfort of the later Bronze Age or Early Iron Age which is located on the northern outskirts of Southend-on-Sea, some 500m east of the Sutton Road crematorium.

The monument occupies the northern edge of a broad terrace which is not particularly elevated and yet commands extensive views over the valley of the River Roach to the north, east and west. The monument has been recognised as a prehistoric enclosure since at least 1893, when pottery from the 'oppidum' (defended settlement) at Prittlewell was exhibited at a meeting of the Essex Field Club.

The hillfort is nearly circular in plan, measuring approximately 250m in diameter. The south western third of the perimeter is defined by an earthen bank and external ditch which survive within a wooded belt. The bank averages 3.5m in width and 0.9m high. The ditch is less clearly visible, having been partly used as a corporation dump in the 1920s, although some sections remain exposed and measure up to 4m in width and 1.4m in depth.

The northern and eastern sections of the ramparts have been reduced by ploughing, although undulations marking the line of the defences were noted in the early part of this century and the line of the bank has been recorded from the air as a cropmark (a variation in crop growth caused by buried features). Observation of a pipeline trench to the Barling Outfall Works in 1929 revealed that the external ditch may not have continued around this side of the hillfort, perhaps as the approach from this side was already restricted by marshy land.

A trial trench, cut through the southern ramparts and across the southern edge of the interior in 1929, provided evidence for the composition of the bank and the original profile of the ditch.

The excavators also examined a pronounced mound (known locally as 'The Look-out') situated on the south eastern part of the perimeter. This mound, which measures some 20m in diameter and 1.5m high, was found to be completely artificial and to contain quantities of tile and medieval pottery spanning the period from the 13th to the 15th century. A depression in the centre of the level summit was found to have resulted from a previous, unrecorded excavation. The excavators were unable to account for the origin of the mound, although the evidence which they recorded is now thought to indicate the base of a medieval post mill, sited on the highest point on the ramparts in order to take advantage of the prevailing wind.

All fences and fence posts are excluded from the scheduling although the ground beneath is included.

MAP EXTRACT The site of the monument is shown on the attached map extract.

## Selected Sources

### Books and journals

Mephram, W A, 'Trans Southend-on-Sea & District Antiq & Hist Soc' in Prittlewell Camp: Report of Excavations 1929, (1930), 29-48

Mephram, W A, 'Trans Southend-on-Sea & District Antiq & Hist Soc' in Prittlewell Camp: Report of Excavations 1929, (1930), 29-48

Wymer, J J, Brown, N R, 'East Anglian Archaeology' in Settlement and Economy in South East Essex 1500BC - AD1500, (1995), 157

Wymer, J J, Brown, N R, 'East Anglian Archaeology' in Settlement and Economy in South East Essex 1500BC - AD1500, (1995), 157

### Other

Oblique monochrome (copy in SMR), RAF, 58/192/P1/5041, (1949)

RCHME, Inventory of Historic Monuments in Essex, (1923)

Recent discovery of AP evidence, Gould, S (ECC Archaeology), Cropmark evidence at Prittlewell Camp, (1997)

National Grid Reference: TQ 88991 87828

## Map



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The above map is for quick reference purposes only and may not be to scale. For a copy of the full scale map, please see the attached PDF - [1017515 .pdf](#)

The PDF will be generated from our live systems and may take a few minutes to download depending on how busy our servers are. We apologise for this delay.

This copy shows the entry on 26-Jul-2016 at 10:12:31.

## **Appendix 3**

### **Royal Commission for Historical Monuments England Assessment of Prittlewell Camp**



**PRITTLEWELL CAMP  
PRITTLEWELL  
ESSEX**

Earthwork Survey

NMR INDEX No: TQ 88 NE 18  
NGR: TQ 8899 8783

Report by: A. Cooper & P. Pattison  
Survey by: M. Brown, D. Garrow, A. Oswald & P. Pattison  
Drawings by: A. Cooper

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Tel: 01793 414700 Fax: 01793 414707 e-mail: info@rdms.gov.uk Internet: <http://www.rchme.gov.uk>



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## 1. INTRODUCTION

Prittlewell Camp lies on the northern fringe of Southend-on-Sea, at TQ 8899 8783, approximately 1 mile to the north-east of Prittlewell church (Fig 1). It is a Scheduled Ancient Monument (SAM no Essex 117) which includes the earthworks of a circular enclosure and a mound on its south-eastern bank. A 'green-lane' runs close to its eastern side. The enclosure sits just off the summit of a broad ridge, on a gently rising brick-earth covered gravel terrace. The site commands extensive views over the Roach Valley to the north.



Figure 1  
Location map

An archaeological survey of the site was carried out by the Royal Commission on the Historical Monuments of England (RCHME) in August 1998, at the request of Essex County Council Planning Department and Southend Borough Council, with the aims of improving on the current definition and understanding of the monument and of providing a basis from which to devise a plan for its long term management.

Today, the site is seriously neglected and it has been significantly modified by post-medieval activity. Municipal rubbish dumping occurred here in the 1920's and it now lies immediately adjacent to a large supermarket on the fringe of the built up area of Southend. Dense young tree growth and scrub cover the southern and western arcs of the enclosure where its earthworks are best preserved: here also are multiple pathways and several small rubbish pits. The remainder of the enclosure lies within an arable field where the perimeter bank has been reduced considerably by intensive ploughing. The significance of the earthworks within the arable field has just been reaffirmed and until recently, only the southern and eastern earthworks were scheduled. The complete circuit of the earthworks and the interior of the enclosure are now scheduled.

## 2. PREVIOUS INVESTIGATIONS

The site is known variously as Prittlewell Camp, Fossetts Camp and Grove Field Camp and its archaeology has been investigated on a number of occasions from the late 19th century onwards. However, this research has brought little real insight to its origin and purpose, both prehistoric and medieval origins have been suggested for the enclosure. Similarly, a medieval date was put forward for the large mound on its south-eastern bank on the basis of the 13th-century pottery found within. Previous considerations include a 'look-out mound' for the enclosure, a windmill mound, and even a Bronze Age barrow! The origin of a 'green lane' which runs close to the enclosure is also uncertain.

The substantial scale of the earthworks at Prittlewell ensured the recognition of their archaeological significance from the mid 19th century onwards. The earliest account describes:

*'the remains of an ancient earthwork or intrenchment, situate upon a hill, having a gentle ascent from all approaches. It is of an oval shape, having two sides and a vallium wall defined. It embraces about eight acres, and can be traced throughout in several fields, but the greater portion is in Grove's field. At the south east corner on the Temple Farm, is a circular mound, covered with brush and timber, which has been lowered, but still rises about ten feet around the surrounding land. This was probably the keep or stronghold.'*

(Benton 1873, 498).

Shortly afterwards, an excavation of 'the oppidum of the mound of Prittlewell' was mentioned at a meeting of the Essex Field Club in 1893 but the nature and location of this work were not recorded (Mephram 1930, 32).

A later account presents the site as one of several elliptical camps, enclosures and dykes in Essex and a more detailed description, including comment on the form and condition of the earthworks, with a plan and section drawings, was made in 1923 (VCH 1903, 275; RCHM 1923, 114; Fig 2).

In 1929, W A Mephram made a more concerted attempt to understand the nature of the earthworks. He began by cutting a series of trenches through the southern bank of the enclosure and at intervals in a line from there to the mound (Fig 3). These revealed the dimensions and form of the bank at this point, where it was some 5m across, standing to a height of 1.5m, and had a steep inner scarp and a more gently sloping outer scarp. The original strata within the confines of the enclosure bank appeared to be undisturbed, prompting the suggestion that 'the earth forming the ramparts had not been taken from within the enclosure, it had evidently been moved from the exterior fosse' (Mephram 1930, 35). The lack of finds in these trenches meant that no secure date could be assigned to the enclosure.

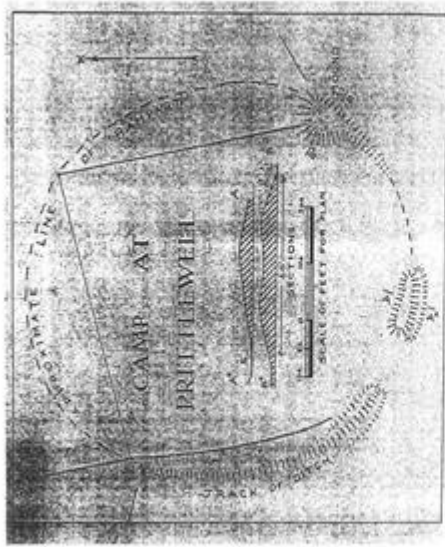


Figure 2  
1923 survey plan  
of Prittlewell Camp  
(RCHME 1923,  
114)

Mepham dug a further eight trenches and two 2.5m deep holes through the mound on the south-eastern bank of the enclosure (Fig 4). Recent disturbance had formed a crater in the centre, but finds from the mound material included 13th-century pottery, worked tufa, oyster shells, tiles, animal bones, iron nails, carved stone and the spring from a Roman padlock.

Mepham also surveyed and sketched the area which allowed him to interpret some of the earthworks, suggesting the position of a possible entrance to the enclosure in its western bank, and also mentioning the disturbance to the southern bank which he explained as the product of gravel extraction and rubbish dumping. Finally, he examined the sections of pipeline trenches that were cut through the western bank of the enclosure in the same year but found these uninformative (Fig 3).

Despite the substantial nature of his investigations, finds were few and consequently, Mepham's conclusions were hesitant. Although he favoured a pre-historic origin for the construction of the enclosure, he admitted that this could not be proven but suggested that 'its strategic value is obvious' and 'it would provide a refuge for people and cattle in times of danger but would not necessarily be a garrisoned fortress' (Mepham 1930, 45). He seemed

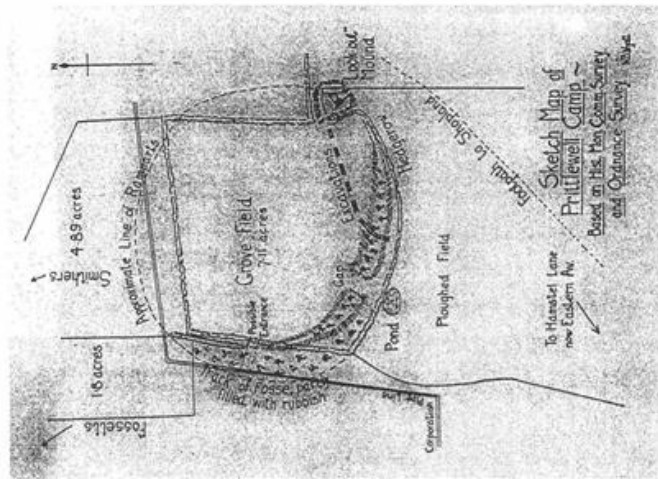


Figure 3  
Sketch map of  
Prittlewell Camp  
showing the 1929  
excavation  
trenches (Mepham  
1930, 31)

more certain that his findings provided secure dating evidence for the mound but this disregards the fact that the mixed, dispersed nature of the finds within the mound might suggest that their inclusion was incidental and that it was constructed at a later date.

The most recent archaeological investigation in the survey area was an evaluation carried out by Essex County Council in advance of the cutting of the Rochford to Southeast pipeline in 1997. A trench to the north-west of Prittlewell Camp produced the only finds, which were very similar in nature to those found in the 1929 excavation of the mound (Tripp 1998).





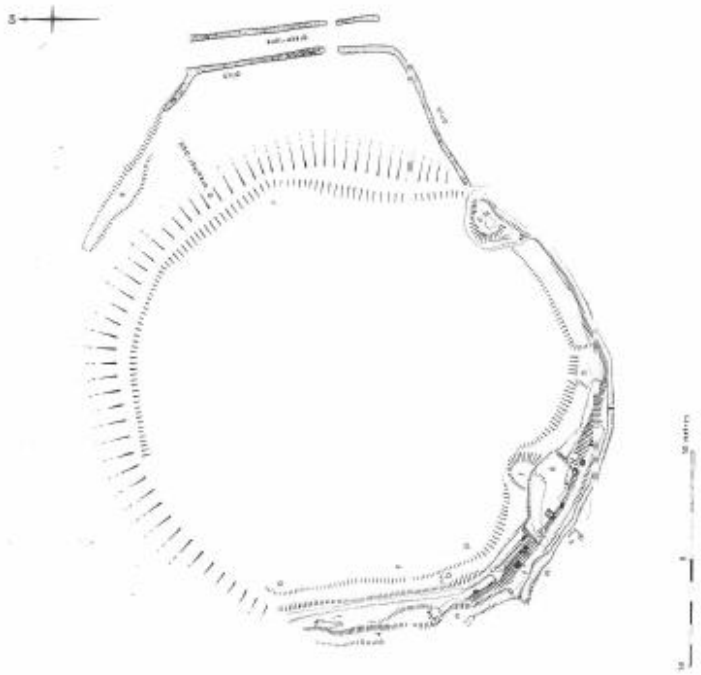


Figure 6 RCHME survey plan of Prittlewell Camp (surveyed at 1:1000 scale)

### 3. DESCRIPTION AND INTERPRETATION

For letters which appear in bold in the text, see Fig 5.  
A summary interpretation is provided by Fig 6.

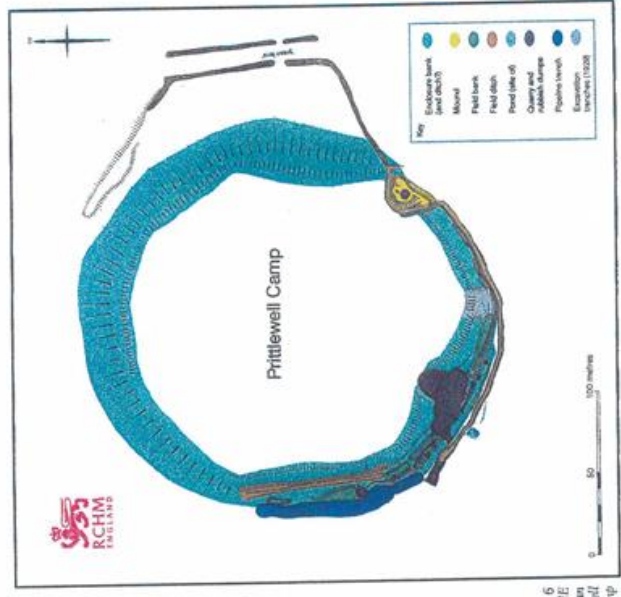


Figure 6 RCHME Interpretation plan of Prittlewell Camp

#### THE ENCLOSURE

This comprises a broad bank defining a near circular area of approximately 5.4 hectares (13.3 acres). Early accounts (see Benton 1873; RCHME 1923; Meopham *ibid.*) refer to an outer ditch or 'fosse', but the only remaining length of this lies in a seriously disturbed area on the

south western side of the enclosure and its antiquity cannot be assumed on the basis of earthwork evidence alone.

#### The northern and eastern arcs

This part of the enclosure bank lies within an arable field and it has been heavily spread by ploughing to as wide as 35.0m; nevertheless the external face still stands to a height of 1.0m and the feature is clearly visible. In profile, it has a long outer slope and a shorter, steeper inner slope, agreeing with the form shown for the southern arc in the RCHM survey (1923, 114) and in section drawings from the 1929 excavations (Mepham 1930, 34). There is no evidence here of an external ditch and any such feature is unlikely to have survived several centuries of ploughing in the field.

#### The southern and western arcs

The edges of this section of the enclosure bank, although forming a more prominent earthwork, are harder to distinguish because of their situation in thick woodland and undergrowth but also because of damage caused by later activities, particularly Corporation rubbish dumping in the early 20th century: there are various rubbish-filled hollows, mounds and grooves along the enclosure bank. A wide linear feature, *a*, running along the western edge, represents the line of the 1929 corporation pipeline that Mepham described and investigated. This trench has left a pronounced scar, which could be concealing the line of an original external ditch to the enclosure. Further evidence of the pipeline can be found to the north-east, in the arable field, in the form of a breather pipe.

The inner scarp of the enclosure bank is slight, it runs within the line of the modern field boundary and has been significantly reduced by ploughing within the field and stands to a maximum height of 0.2m to 0.4m. A slight scarp, *b*, running across the bank on its western side, is the point where Mepham postulated a possible entrance to the enclosure. There is no evidence to support his interpretation: both inner and outer scarps of the enclosure bank are continuous at this point, with no hint of a change in the earthwork that might indicate a blocked or infilled entrance.

From a casual glance, a larger break in the enclosure bank at *c* and *d*, might seem a more plausible candidate for an entrance. However, it was correctly explained by Mepham as the result of gravel extraction. The smaller hollow, *e*, is now in the ploughed field; it has deflected the inner face of the enclosure bank to the interior. Outside it, the larger disturbance, *d*, has cut away most of the enclosure bank but it is still possible to trace the line of the outer scarp of the bank running continuously across it. Along the northern side of the hollow is a faint linear depression, perhaps a track leading away from *c*.

The outer scarp of the enclosure bank has been severely damaged in several places. The western side at *e* appears to have been flattened out but further south, on each side of the hollow *d*, there are better-preserved stretches, *f*. These are both some 35.0m long and stand to a height of 1.7m, with a sharp and steep profile. It is here that there are traces of an outer ditch, but the degree of disturbance in this area must cast doubt upon its origin. To the south-east, the outer scarp is adopted by a later field boundary and can be traced up to the mound at *g*. This will have obliterated any trace of an earlier enclosure ditch. The mound sits squarely over the enclosure bank, utilising the outer scarp but obscuring the inner scarp with a pronounced inward bulge.

The large breach, *h*, in the centre of the southern arc, is a trench from Mepham's excavations in 1929.

#### The interior

There are no internal features visible on the surface and superficially, there was an absence of finds in the ploughsoil. However, the ploughed area would benefit from thorough fieldwalking to recover any small finds which might assist in dating.

#### THE MOUND

A large mound, *g*, is situated astride the enclosure bank on the south-eastern side. The prominence of the mound over the enclosure and the way a field boundary ditch loops around it, suggests that the mound is secondary to the enclosure. However, the archaeological relationship between the two has yet to be clearly defined. The mound utilises the steep outer scarp of the enclosure on the south-east whereas on the north-west its plan is rounded and there is a more gentle slope which extends beyond the line of the inner face of the enclosure bank. The resulting shape of the mound is nearly oval, some 22.0m by 18.0m, standing to a maximum height of 1.8m above the current ground surface. This contrasts with the height of 3.0m recorded in the late 19th century which, according to Benton (*ibid.*), had already been lowered. However, this might be explained by the known practice of measuring the slope, not the elevation.

A small crater in the top of mound could be the area of disturbance mentioned by Mepham but probably also partly the product of his own extensive excavations in 1929.

The mixed material produced from his excavations included 13th-century pottery; this suggests that it was built some time during or after the 13th century but doesn't allow for a more precise date to be assigned. The mound first appears in a documentary source on a map of 1796 (ERO: D/DCw P13). Although it is not possible to connect this mound directly to the features in the evaluation trenches that were dug to the north-west of the survey area

(Tripp 1998), the occurrence of a wealth of 13th century finds in both (cooking pots in particular), implies that the area of the enclosure was a focus of activity in the later medieval period.

A concrete and brick base on top of the mound, together with chunks of similar debris on the flanks, may represent the position of a military structure of Second World War date which appears on aerial photographs from 1943. This cannot be directly associated with the heavy anti-aircraft battery that lay to the north-east at TQ 899 889. It is unlikely that it marks the position of a pill box because of the density of tree cover above the mound and the insubstantial nature of the remains, although there are several pill boxes in the surrounding area. If it did fulfill any military function, it seems most likely to have held a light anti-aircraft gun or a Blacker Bombard (Spigot Mortar) (NMR APs: HLA/655/6005-6).

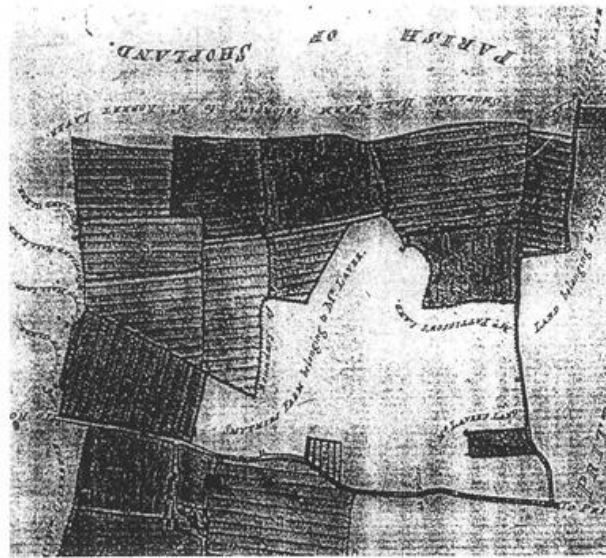


Figure 7  
Extract of the  
1796 Estate Map  
of Rochford Hall  
Estate (reproduced  
courtesy of the  
Essex Record  
Office: D/DCw  
P13)

#### Prittlewell Camp in the Later Landscape

Surviving maps, from the mid 18th century onwards, indicate that the enclosure remained a notable feature long after its original functions had ceased. Part of its bank was utilised by field boundaries in the post-medieval landscape, which explains the better survival of the southern and western arcs (ERO D/DMA P4; D/DCw P13; D/CT/276/1B). These maps indicate that the basic structure of enclosed fields around Prittlewell Camp remained relatively unchanged from the mid 18th century onwards.

A short distance to the east of the enclosure is a 'green lane'. This runs from north to south and is bounded by shallow, steep sided ditches. It is recorded as a short and narrow strip of land on the same maps and it seems likely to be preserving part of a former trackway, perhaps linking vanished medieval fields or settlements. It is described on the Tithe apportionment of 1841 as 'waste' and seems primarily to have provided access between fields, to the north,

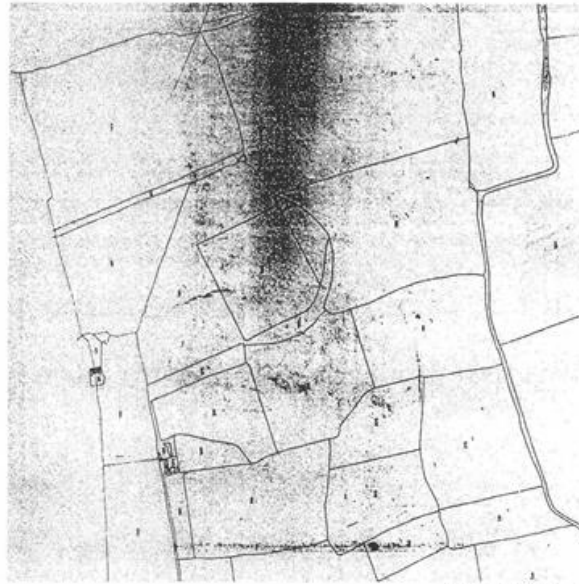


Figure 8  
Extract of the  
1841 Tithe Map  
for Prittlewell  
(reproduced  
courtesy of the  
Essex Record  
Office:  
D/CT/276/1B)

it crossed the parish boundary between Prittlewell and Sutton but continued only for the length of a single field; the southern end was similarly abrupt against a small pond which was still there in 1880; north east of the enclosure there was a similar, but broader strip of land which ran parallel to the green-lane (ERO: D/DCw P13; D/CT276/1B; Ordnance Survey 1st Edn 6-inch Sheet Essex 78).

Several of these field boundaries have left their mark on the enclosure: there is a slight bank, *j*, running along the top of the enclosure bank near its outer edge. Another lies outside the enclosure to the north-east, where there is a broad but shallow bank, *k*, in the ploughsoil; it is depicted as a functioning boundary on a map of 1796, and on others thereafter (Fig 7). A ditch, *m*, skirting the southern edge of the enclosure bank and crossing it to the west of mound *g* appears on the same map (ERO: D/DCw P13).

A later ditch and bank, *n*, form the current field boundary and have done so since 1841 when the southern and western arcs of the enclosure were enveloped by a narrow crescent of woodland, as today. A number of pollarded oak trees of some age still exist within this crescent, marking the productive role of this woodland within the otherwise arable later medieval landscape. At this time the interior of the enclosure contained a rectangular field which, although now removed, has left its mark by biting into the enclosure bank to leave quite sharp changes of alignment at *p*, *q* and *r* and at the north-eastern tip of mound *g* (ERO: D/CT276/1B; Fig 8).

Against the south-western edge of the enclosure, a shallow depression, *s*, is a former pond (Ordnance Survey 1st Edn 6-inch Sheet Essex 78).

#### 4. CONCLUDING REMARKS

The principal outcome of this survey has been to clarify the nature, extent and condition of the earthworks at Prittlewell Camp. It has clearly defined the more recent damage, enabling existing interpretations to be explored and a more informed understanding of the site to be gained.

However, on present evidence there can be no certain interpretation of the function and date of the enclosure. Taking form alone, it is likely to be of the later prehistoric period: its location over the brow of the hill, rather than a more strategic defensive position on the top of the hill, and its roughly circular form with a single bank, suggest that it may be best understood as one of a range of prehistoric enclosures that were built across the region from the late Bronze Age onwards. These include sites such as the later Bronze Age settlements at Mucking, Springfield Lyons and Loffs Farm (Brown 1988; 1996) as well as an array of sub-circular enclosed sites that are thought to be of early Iron Age origin and appear to have been used in very different ways.

In its scale, the enclosure bank was probably once an impressive sight, but the gentle incline of its outer slope and the lack of a substantial encircling ditch, might be seen to imply that the purpose of its construction was not purely defensive. This argument has been put forward for some hillfort sites in Wessex (e.g. Danebury, Hambledon) at which the defensive purpose of the enclosure ramparts is thought to have been secondary to the social and symbolic importance of their construction and maintenance and the activities that took place within their bounds (Hill 1995). These ideas have been generated from the detailed examination of material and earthwork evidence from within and around the sites concerned, and more fieldwork will be necessary before they can be considered as pertinent to the interpretation of contemporary enclosures in Essex.

The absence of evidence for an entrance is also intriguing and may simply be due to the poor preservation of the site as a whole. On the other hand, it could be, as elsewhere in Britain, that the entrance faced to the south-east (Hill 1996) and so may be obscured by the mound.

The lack of well-stratified archaeological evidence from the enclosing bank or the interior is unusual and might imply that if it was ever occupied, this habitation was ephemeral rather than long term. A similar lack of evidence for permanent or intensive occupation has been observed at other early Iron Age enclosures in the region. For example, the majority of the material evidence that was found at Asheldham Camp (Bedwin 1991), was of middle Iron Age date although the construction of its enclosure bank was dated to the early Iron Age. This implies that here, the enclosure was not extensively occupied until several centuries after its initial construction. Recent evaluation of the earthworks and interior of Shoobury Camp have produced similar evidence. It is interesting in this respect that the majority of

evidence for early Iron Age occupation and productive activity in the region has actually derived from open or simple palisaded settlements, such as those at Asheldham, North Shoebury, Maldon, Great Wakering and Fox Hall (near Prittlewell Camp) (Brown 1996, Sealey 1996), several of which lie close to roughly contemporary enclosure sites. This and the co-ordination of people and activities within this landscape, deserve further investigation.

The absence of dating evidence also makes it difficult to understand the local and regional context of the evidence at Prittlewell, but this matter does need consideration. Several early Iron Age enclosures have been excavated and surveyed in Essex, but none are well understood. They have been described as 'a rather disparate group of earthworks' for which the *single unifying factor seems to be their siting in positions of strategic importance relating to rivers, estuaries and the coastline* (Bedwin 1991, 26). In addition it can be observed that several such sites are roughly oval or sub-rectangular in plan; have simply constructed, unvallate enclosure banks; have produced no certain dating evidence; have single gap or indeterminate entrances; and show evidence of earlier prehistoric cultivation or activity in the area prior to its enclosure (Morris and Buckley 1978). Morris and Buckley (*ibid*) have also noted that these sites tend to be situated along navigable rivers rather than being constructed at regular intervals in a 'territorial' manner, as has been suggested for enclosed sites elsewhere in Britain (cf. Cunliffe 1984).

Several of the later prehistoric enclosures in the region were re-used during the Saxon and later medieval periods; the mound at Prittlewell Camp might be evidence of this practice. At Maldon and Asheldham the re-use involved the recutting of the enclosure bank and ditch (Bedwin 1990, 1991); the enclosed site at Ambresbury was apparently re-occupied in the Saxon period, and several of the later prehistoric enclosures listed by Morris and Buckley (1978) have mounds adjacent to or overlying their earthworks (e.g. Asheldham and Danbury) although the purpose of these needs investigation. The presence of 13th-century material from two locations at Prittlewell is tantalising in this respect; but not so convincing as to give credence to suggestions that the enclosure was a ringwork castle. The most likely explanation for the mound is that it was a windmill of medieval date (13th-century or later): its location on the highest point of the enclosure bank in an exposed position, favours this interpretation.

It is known from documentary sources that post-mills became widely used in Essex during the 12th and 13th centuries AD (Farris 1981), but only recently has archaeological evidence been found to support this. A combination of field-walking and trial trenching undertaken as part of the Essex 'Cropmark Enclosures Project' over the last four years has revealed the sites of 12th- or 13th-century windmills at Great and Little Bentley. The excavator of another early post-mill at Borham airfield described 'a crude base construction comprising a central large pit (5m diameter and 1 m depth), surrounded by a ring ditch and possible bank or mound

(18m total diameter)' (Bennett and Gilman 1996). The form and dimensions of this are remarkably similar to those of the mound at Prittlewell although here, the central pit is likely to be the product of more recent disturbance rather than representing the position of the removed base of the post-mill. Whilst this by no means provides conclusive evidence of the purpose of the Prittlewell mound, it certainly makes this interpretation a strong possibility.

The green lane is a rare survival in Southend of a once common landscape feature. Another example 800m to the west at Temple Farm was destroyed during the construction of an industrial estate in the 1980's. Whilst the lane cannot be directly linked to the use of the enclosure or mound with any certainty, the survival of this ensemble of ancient landscape features in close proximity to the edge of a dense urban area is remarkable. Their long term preservation and management should be considered together.



## 5. SURVEY AND RESEARCH METHODS

The topographical survey at Prittlewell Camp was carried out by Morig Brown, Duncan Garrow, Alastair Oswald and Paul Pattison of the RCHME. A divorced survey method was employed by establishing control stations using a Wild TC1610 electronic theodolite with integral EDM. The data was captured on a Wild GRM 10 Rec Module and plotted digitally on a Hewlett Packard Designjet 750C Plus plotter. These stations were used as a framework from which to record the archaeological features with tapes using normal graphical methods, at a scale of 1:1000.

This report was researched and written by Anwen Cooper and Paul Pattison. The illustrations were prepared by Anwen Cooper using AutoCAD and CorelDraw, and the report was assembled by Morig Brown using Corel Ventura software.

The site archive has been deposited in The National Monuments Record Centre, Great Western Village, Kemble Drive, Swindon SN2 2GZ (NMR Number TQ 88 NE 18; HOB Uid: 418915). Further copies may also be obtained from this address.

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- 540/1699/206-7 (12/08/1955)



*The National Monuments Record contains  
all the information in this report – and more:  
original photographs, plans old and new,  
the results of all RCHME field surveys, indexes of  
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## **Appendix 4**

### **Examples of Exemplar Housing Developments**

Horstead Park, Kent



Green Lanes, Cambridge



New Hall, Harlow



New Hall, Harlow



**Southend-on-Sea Borough Council**

**Report of Corporate Director  
for Place  
to**

**Cabinet**

**On  
20 September 2016**

**Agenda  
Item No.**

**12**

Report prepared by: Paul Jenkinson, Parks Technical Officer

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**Carriage and Wagon Shed Options – Shoebury Garrison**

**Place Scrutiny Committee  
Executive Councillor: Councillor Ann Holland  
Part 1 (Public Agenda Item)**

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**1. Purpose of Report**

- 1.1. To make Members aware of the options for the future management and operation of the Carriage and Wagon Shed Shoebury Garrison.
- 1.2. To request that Members agree to lease the building to a third party organisation.

**2. Recommendations**

- 2.2 **That the Carriage and Wagon Shed is leased to the new Shoebury Coastal Community Interest Company as detailed in section 4.**
- 2.3 **Delegate authority to the Corporate Director of Place to advertise the opportunity to develop and manage the building for the benefit of residents and visitors to the borough if the Shoebury Coastal Community Interest Company is unable to take on the lease of the building by 30<sup>th</sup> September 2017.**
- 2.4 **Delegate authority to the Corporate Director of Place to negotiate and agree terms of any lease associated with the Carriage and Wagon Shed in consultation with the Council's Asset Management and Legal Teams.**
- 2.5 **Support the delivery of any fit-out of the Carriage and Wagon Shed as set out in the Coastal Communities Funding application.**
- 2.6 **Note the financial position as set out in section 7.2.**

### **3. Background**

- 3.1.** As a result of the development of the former Shoebury Garrison site by Avant Homes, (formerly Country and Metropolitan Homes), the developer took on an obligation under the S106 agreement to refurbish one of the listed structures, the Carriage and Wagon Shed, for use as a visitor centre and community facility and to transfer the freehold to the Council on completion of the works (subject to a pre-emption for £1 in favour of the developer in the event of sale within 21 years). The original S106 agreement required the developer to provide a fully fitted out building. However, due to economic pressures, Development Control Committee agreed to vary the S106 in 2015 at the developer's request. The variation limited the required works to the external shell of the building and the basic internal core and removed the previously agreed commuted sum to assist with the running and maintenance costs of the building. The building is due to transfer to the council in September 2016.
- 3.2.** Cabinet received a report in September 2011, and resolved to work with a community group or charity to manage the Gunners Park Heritage Centre once completed.
- 3.3.** Following the Cabinet resolution, a group was formed consisting of The Southend Educational Trust, Essex Wildlife Trust, The Shoebury Society and The Shoebury Archive. The aim of the group was to take on the management of the Carriage and Wagon Shed as a heritage centre, once the developer completed the refurbishment of the building. The group constituted itself as the Shoebury Heritage Centre Board.
- 3.4.** To provide financial stability for the Shoebury Heritage Centre Board, The Southend Educational Trust agreed to be the accountable body for the group.
- 3.5.** However, due to delays in the delivery of the building by the developer and changes within the Southend Educational Trust, in 2015 the Trust had to step down as the accountable body. Later that year, the Shoebury Heritage Centre Board was dis-banded.
- 3.6.** In July 2015, the Government asked for expressions of interest in setting up Coastal Community Teams. Local groups, businesses, and residents expressed an interest in setting up a Coastal Community Team for the Shoeburyness area. In October 2015 the Shoeburyness Coastal Community Team was set up.
- 3.7.** As a result of the creation of the Shoeburyness Coastal Community Team, a bid for Coastal Revival Funding was possible, allowing for work to be undertaken on option appraisals for the building and detailed internal layout to be undertaken. See Appendix 1 and 2.
- 3.8.** In May 2016, the Shoeburyness Coastal Community Team voted to set up a Community Interest Company with the intent of taking on a lease for the Carriage and Wagon Shed and using it for the benefit of the community and local area.

- 3.9.** The company is currently being set up with its aims outlined in the following activities: Provide benefits to communities; individuals; visitors; schools and young people in and around the Shoeburyness area of Southend-on-Sea (the Shoeburyness and West Shoebury Wards as defined within the Government Boundary Commission for England (May 2015)) With a particular focus on coastal projects.

#### **4. Proposal**

- 4.1 To lease the Carriage and Wagon Shed on a full repairing and insuring lease, to a third party organisation to manage and operate as a community focused facility.
- 4.2 To work with the Shoebury Coastal Community Interest Company as the preferred organisation to lease and operate the Carriage and Wagon Shed.
- 4.3 The Shoebury Coastal Community Interest Company must be in a position to lease the building by 30<sup>th</sup> September 2017 with a suitable and sustainable business plan in place.
- 4.4 The lease for the building to be negotiated with the Shoebury Coastal Community Interest Company, based on a peppercorn rent for years 1 to 10 with a review in year 10 and an option to phase in a commercial rent from year 11 onwards.
- 4.5 The lease on the building should be negotiated with a duration of up to 26 years but recognising the Council's obligation to comply with s.123 Local Government Act 1972 for any lease over 7 years.
- 4.6 If the Shoebury Coastal Community Interest Company is not in a position to take on the lease for the building by 30<sup>th</sup> September 2017, the opportunity will be advertised openly for a charity or community interest company to submit proposals to lease and operate the building.
- 4.7 The responsibility for selecting an alternative organisation to take on a lease and manage the Carriage and Wagon Shed is proposed to be delegated to the Corporate Director for Place.
- 4.8 Prior to leasing the building to a group or organisation, the Council will deliver the fit-out of the building, providing the Coastal Community Funding bid for £700,000, submitted in 2016 is successful.
- 4.9 In the event that the Coastal Community Funding bid is unsuccessful, the Council will work with the leaseholder of the building to secure funding and deliver the internal fit-out.

#### **5 Other Options**

- 5.1 Option 1 - Do nothing and leave the building to decline. The Carriage and Wagon Shed is a listed building and the Council will retain a responsibility to maintain the frame and structure in the event that the building is not used and allowed to decline.

- 5.1.1 This option is not recommended due to the listed status of the building and the interest already displayed by local groups in leasing and managing the building.
- 5.2 Option 2 – Advertise the opportunity for a charity or community interest company to lease and manage the building and not ring-fence the opportunity to the Shoebury Coastal Community Interest Company.
  - 5.2.1 This option is not recommended as it is recognised that the Shoebury Coastal Community Interest Company members have already been involved with the building previously and that this group has been set up with the interests of Shoebury as a fundamental aim.
- 5.3 Option 3 – Not lease the Carriage and Wagon Shed to a third party organisation and instead manage the facility in-house.
  - 5.3.1 This option is not recommended due to the additional staff and resources that would be required by the Council to successfully run and manage the facility.
- 5.4 Option 4 – Advertise the opportunity to lease and manage the Carriage and Wagon Shed as a commercial opportunity on a commercial rent.
  - 5.4.1 This option has not been recommended as a not for profit Shoebury Coastal Community Interest Company should be in the best place to deliver on the wants and needs of the local community.
  - 5.4.2 The preferred option allows a commercial rent to be considered and introduced from year 11 of the lease.
- 5.5 Option 5 – Sell the Carriage and Wagon Shed and ring fence the capital raised for investment in the other council owned historic structures in Gunners Park and the Garrison.
  - 5.5.1 This option has not been recommended as the S106 agreement listed the Carriage and Wagon Shed as a building to be transferred to the Council and to be used as a community facility. If the building is sold future use of the building as a community facility could not be guaranteed although the Council would have to agree to amend the S106 so retains some control.
  - 5.5.2 This option is also not practical at this time due to a covenant within the transfer requiring the Council to offer the building back to the developer for the sum of £1 if we wish to sell the building within 21 years of the transfer date. As a last resort, the building could be offered back.

## **6. Reasons for Recommendations**

- 6.1. The Shoeburyness Coastal Community Team and the new Shoebury Coastal Community Interest Company have been set up for the benefit of the Shoeburyness area.
- 6.2. The Shoeburyness Coastal Community Team and the new Shoebury Coastal Community Interest Company is made up of groups and individuals with

interest in the Shoeburyness area and includes individuals and organisations previously involved with the Shoebury Heritage Centre Board. Details of the aims of the Shoeburyness Coastal Community Team and its members can be found in the Coastal Community Team economic plan (Appendix 3).

- 6.3. Leasing the building to a community interest company whose aims are to benefit the Shoeburyness area should ensure that the Carriage and Wagon Shed is managed for the benefit of the local area.
- 6.4. Leasing the building on an initial peppercorn rent and for duration of up to 26 years will enable the community interest company to develop the offer of the building and apply for external funding.
- 6.5. Leasing the building to a community interest company will help safeguard the Carriage and Wagon Shed for the future and minimise pressures on Council resources.
- 6.6. The option to advertise the building to an alternative organisation, if the Shoebury Coastal Community Interest Company is unable to take on the lease, will provide the best opportunity to retain the Carriage and Wagon Shed as a benefit to the town and preserve its heritage and community value.

## **7. Corporate Implications**

### **7.1 Contribution to Council's Vision & Corporate Priorities**

- 7.1.1 Healthier Southend: Providing well-managed parks facilities within the town helps to encourage participation in healthy outdoor activity.
- 7.1.2 Safe Southend: Strong communities help reduce crime.
- 7.1.3 Clean Southend: Well-maintained facilities and community activities give a better image of the town.
- 7.1.4 Prosperous Southend: An attractive townscape can assist in encouraging investment within the town.
- 7.1.5 Excellent: A well-maintained townscape enables the Council and community groups to enter regional and national awards.
- 7.1.6 Minimising our environmental impact: The fit-out of the building will take into account climate change and the local environment.

### **7.2 Financial Implications**

- 7.2.1 No revenue budget or funding is currently allocated to this project. It is recommended that the building maintenance budget be increased by £5,000 to cover alarm-monitoring costs and general maintenance. It is also recommended that £500 is added to the budget to cover utility costs. This additional funding is required for 2017/18 budget period only and can be funded from the Council's contingency. However, if the building is not leased during 2017, the funding will need to continue in successive years until an operator is

secured. The funding of these further costs will need approval at the appropriate time.

7.2.2 Staff time for working on the project will need to be allocated. This will be found within existing staff teams.

7.2.3 Costs associated with the drafting and agreeing the lease for the building will need to be met from existing internal resources.

7.2.4 The capital cost and funding of the internal fit out of the building will be externally funded and there is no Council capital contribution required.

### 7.3 Legal Implications

7.3.1 The Council will be best placed to secure the community objectives and mitigate its exposure to liabilities through the leasing of the building to a third party organisation as proposed. All legal implications will be carefully managed through the leasing process and the relevant teams will be engaged as required to support this.

### 7.4 People Implications

7.4.1 No people implications have been identified at this time.

### 7.5 Property Implications

7.5.1 Management of the Carriage and Wagon Shed until leased to a third party organisation.

7.5.2 Leasing of the Carriage and Wagon Shed to a third party organisation.

7.5.3 Fit-out of the inside of the building subject to funding.

7.5.4 Generally otherwise as set out in the report.

### 7.6 Consultation

7.6.1 Consultation has been undertaken as part of the planning process.

7.6.2 Additional consultation will be undertaken on aspects of the project managed by the council when requirement is identified.

### 7.7 Equalities and Diversity Implications

7.7.1 No Equality Analysis has been undertaken. It is not envisaged that an analysis will be required. However, consultation with the Senior Advisor for Equalities and Inclusion will be undertaken.



## 7.8 Risk Assessment

<b>Risk</b>	<b>Mitigation</b>
No third party organisation able to lease the building	Work with group already interested in the building. Reserve the opportunity to sell the building.
Funding bid for internal fit-out unsuccessful.	Look for alternative funding opportunities and submit bids.
Shoebury Coastal Community Interest Company not able to develop a viable option to manage and lease the building.	Operation and management of the building would be advertised openly for an alternative community of commercial operator.
Objections during the planning application process.	Work with the local community via the Shoeburyness Coastal Community Team during this phase.

## 7.9 Value for Money

7.9.1 All works will be procured in line with the Council policies and procedures.

## 7.10 Community Safety Implications

7.10.1 None identified.

## 7.11 Environmental Impact

7.11.1 The operation of the Carriage and Wagon Shed will look to minimise the impact on the local environment.

## 8 Background Papers

- Options Appraisal for Carriage and Wagon Shed
- Designs for Internal Layout
- Shoeburyness Coastal Community Team Economic Plan

## 9 Appendices

Appendix 1 – Location Map of Carriage & Wagon Shed

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Saint Andrew South Shoebury...

Baby Sensory Shoeburyness

Local School of Motoring

Gunners Park

Southend Foreshore

The Shorehouse

ATM (Shorehouse Harvester)

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## SOUTHEND-ON-SEA BOROUGH COUNCIL

### Meeting of Southend-on-Sea Local Development Framework Working Party

**Date:** Tuesday, 6th September, 2016  
**Place:** Committee Room 6 - Civic Suite

# 13

**Present:** Councillor M Flewitt (Chair)  
Councillors M Assenheim, T Callaghan, J Garston, C Walker,  
N Folkard and D Norman MBE

**In Attendance:** Mr D Hermitage, Mr R Harris, Ms A Greenwood, Mr M Thomas and  
Mr M Sheppard  
Also in attendance: Mr S Bishop, DWG Consultants

**Start/End Time:** 6.30 - 8.00 pm

#### **1 Apologies for Absence**

Apologies for absence were received from Councillor Borton (no substitute).

#### **2 Declarations of Interest**

The following Members declared interests as indicated:-

(a) Councillor J Garston – Item 5 (SCAAP) – non-pecuniary interest – lives in the vicinity of the Clifftown Policy Area referred to in the SCAAP;

(b) Councillor Walker – Item 5 (SCAAP) – non-pecuniary interest – founding member of the Horse Owners and Riders SE Essex (HORSE) group who were consultees;

(c) Councillor Flewitt – Item 5 (SCAAP) – non-pecuniary interest – a number of the consultee groups listed in the SCAAP document are known to him;

#### **3 Minutes of the meeting held on 15th September 2015**

Resolved:

That the minutes of the meeting held on 15<sup>th</sup> September 2015 be confirmed as a correct record.

#### **4 Proposed New Local Listings**

The Working Party considered a report of the Corporate Director for Place which presented for consideration new designation requests for local listing which were received for the following buildings:

1. The Co-op building, Sutton Road, Southend
2. Westcliff Police Station, West Road
3. Former Lloyds Bank, London Road, Westcliff
4. Civic Centre Fountain

The Working Party had a general discussion and sought clarification on the criteria used for local listing and requested a list of all local listed buildings/structures, etc. Officers advised that the details are on the Council's website and referred to the criteria for local listing set out in the report.

The Working Party made reference to the Britannia Public House and officers advised that this was a locally listed building. Members suggested that the Shoebury Police Station should be considered for inclusion as a locally listed building.

Resolved:

That the Cabinet be recommended to designate the following buildings as Locally Listed Buildings/Structure:

1. The Co-op building, Sutton Road, Southend
2. Westcliff Police Station, West Road
3. Former Lloyds Bank, London Road, Westcliff
4. Civic Centre Fountain

## **5 Southend Central Area Action Plan - Proposed Submission Document**

The Working Party considered a report of the Corporate Director for Place which presented the proposed submission version of the Southend Central Area Action Plan (SCAAP) which was scheduled for statutory consultation for a minimum of 6 weeks. The views of the Working Party were sought on the SCAAP, prior to its consideration by Cabinet at its meeting on 20<sup>th</sup> September 2016.

Members also received two PowerPoint presentations covering:

- An overview of the SCAAP;
- An overview of the SCAAP Parking Study results;

The Working Party asked a number of questions which were responded to by the officers and Mr S. Bishop from Steer Davies Gleave (SDG) who carried out the SCAAP parking study.

Members discussed the SCAAP in detail and made a number of comments which mainly related to the general provision of parking in the borough which was a significant issue/concern for residents, visitors, businesses, etc. Officers acknowledged the Members concerns regarding parking in general but highlighted that the purpose of the SCAAP was to deliver regeneration and growth in the Southend Central Area, of which one element is to take a strategic network approach to the provision of public parking within the SCAAP area to ensure the vitality and viability of the town centre and central seafront area.

Resolved:

That the Cabinet be recommended to approve the Proposed Submission version of the SCAAP and associated documents to be published for public consultation under Regulation 19 of the Town and Country Planning (Local Planning) (England) Regulations 2012.

# Southend-on-Sea Borough Council

Agenda  
Item No.

14

Report of Corporate Director for Place  
to  
**Traffic and Parking Working Party and  
Cabinet Committee**  
on  
**19<sup>th</sup> September 2016**

Report prepared by: Zulfiqar Ali, Group Manager, Highways  
and Traffic Group

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## Members Requests List

Portfolio Holder – Councillor Tony Cox  
*A Part 1 Public Agenda Item*

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### 1. Purpose of Report

- 1.1 For the Traffic and Parking Working Party & Cabinet Committee to receive, note and consider new “Member’s Requests” and Officers’ recommendations as detailed in Appendix 1 of this report.

### 2. Recommendation

- 2.1 That the Traffic and Parking and the Cabinet Committee consider the views of the Working Party and Officer recommendations on each of the proposals as detailed in **Appendix 1** to this report, and agree:
- a) **To proceed with Officers’ recommendations; or,**
  - b) **To proceed with Officers’ recommendations; or,**
  - c) **To take no further action.**
  - d) **That all agreed actions will be added to the existing work programme unless members have indicated higher priority.**
  - e) **The use of the attached” Members Request” form for all future requests.**

### 3. Background

- 3.1 The cabinet Committee at its meeting in January 2016 agreed policy criterion for traffic and parking investigations. All request of this nature from Members are considered against the new policy requirements. As we are now working to these policy requirements, Officers have agreed a standard Proforma that all Members will be asked to complete and return for all future Members requests a copy is attached as appendix 2. A copy of this will be made available on line for Members use.
- 3.2 Officers receive and add all such requests to the “Members list” and report these back to the Traffic & Parking Working Party and the Cabinet Committee. Any recommendations agreed will then become part of the work programme. Officers’ initial recommendations are based on limited findings of the investigation and/or the outcome of surveys/consultations where possible. If the

Working Party/Cabinet Committee agree for items to be further investigated, updates will be presented to future Traffic and Parking Working Party & Cabinet Committee meetings for consideration and decision, as and when they become available.

- 3.3 The Committee is aware of the increasing workload resulting from “Members Requests”. This is a small team with limited financial and staffing resources to address all requests which require extensive investigations in most cases. As such there is a need to prioritise these on the basis of impact on safety, accessibility and traffic flows and programmed against the limited budget and staffing available to undertake necessary investigations to deliver these in the most efficient way.
- 3.4 It needs to be noted that once a formal conclusion has been reached on the individual items, to the agreement of the Traffic and Parking Working Group & the Cabinet Committee, these will be removed from the list and where appropriate, added to the work programme. In such cases, the Working Party and the Cabinet Committee is asked to agree future prioritisation of each of the items on the basis of impact on safety and accessibility.
- 3.5 Officers will update Members of the progress of their individual requests and will inform them of the findings, investigations, the recommendations and reasons thereof, as well as the decisions made by this Committee.

#### **4. Reasons for Recommendations**

- 4.1 To provide a rationalised and consistent management and decision-making process for all formal requests for highways and traffic management improvements by Ward Councillors via the Traffic and Parking Working Party & Cabinet Committee.

#### **6. Corporate Implications**

- 6.1 Contribution to Council’s Vision & Corporate Priorities

The Members Requests List is a mechanism for Ward Councillors to request issues within their wards which they believe may be a safety hazard and improving traffic flow contributes to a Safe and Prosperous Southend.

#### **6.2 Financial Implications**

Requests which are recommended for any action will be funded via existing budgetary resources. However, the resources are limited and the Working Party and the Cabinet Committee has an ongoing agreed priority programme based on its earlier decisions. Unless the Committee agrees to allocate a priority for the new requests, these will be added to the bottom of the list and undertaken subject to availability of financial and staffing resources.



### **6.3 Legal Implications**

Where requests involve any requirement for a Traffic Regulation Order, the relevant statutory procedures will be followed including the requirement for formal consultation with affected frontagers' and advertisement in the local press.

### **6.4 People Implications**

There are limitations in staff time and an increase in Members' requests can place additional strain on limited resources which may lead to delays in investigations and reporting back to the Working Party and the Cabinet Sub Committee.

### **6.5 Property Implications**

None

### **6.6 Consultation**

Formal and informal consultation will be carried out, as required, and directed by this Committee. In addition all ward councillors are to be informed of the consultation process prior to its commencement.

## **7. Background Papers**

None

## **8 Appendices**

**Appendix 1** - List of Members requests

**Appendix 2** – Proposed Proforma for Members Requests.

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**MEMBERS REQUESTS LIST FOR HIGHWAYS, TRAFFIC MANAGEMENT AND PARKING SCHEMES**

**Note: Cabinet Committee in January 2016 agreed the following criterion for dealing with requests of waiting restrictions:-**

**Waiting Restrictions**

These will only be considered if one of the following criteria is met;

- 1) Where a road safety problem has been identified by collision studies (3 Personal injury accidents in 3 years) and it is clear that an actual reduction in collisions may follow the introduction of such an Order.
- 2) Where evidence of the obstruction of the highway or visibility at junctions occurs on a frequent and severe basis, causing particular difficulties for emergency service vehicles and/or public transport.
- 3) Where commerce and industry are seriously affected by presence of parked vehicles.
- 745 4) Where the installation of TROs is essential to provide maximum benefit from capital investment.
- 5) On strategic routes and major distributors appropriate waiting and loading restrictions can be used to ensure that adequate road space is available for moving traffic waiting restrictions will not be provided for individual private accesses in isolation.
- 6) Cost of schemes and likely savings through accident reduction need to be part of priority consideration.

Reference Number	Date 1 <sup>st</sup> Reported (Month/Year)	Ward Member	Subject of Request	Update
14/15	March 14	Cllr Assenheim	Widen pedestrian refuge, Ness Road	<p>Widening the refuge on the northern side would involve significant alterations to existing kerblines in order to maintain existing carriageway width. It is estimated that this will cost approximately £45,000. Costs would be significant as area would have to be excavated and formed into carriageway standard surface, requiring suitable drains and relocation of the existing utility equipment. There is no accident history at this location.</p> <p>Concerns have been raised that buses over run the kerb however no issues identified on several site visits. In terms of the cost and benefit, the level of investment requirement does not support the principle of value for money.</p> <p><b>Recommend no further action and remove from list.</b></p>
15/01	March 15	Cllrs Ayling and VanLooy	Amend priority North, South and Central Avenues	Investigation on-going. Outcome to be reported in due course.
15/07	June 15	Cllr Salter	Consider pedestrian crossing Elmsleigh Drive near Rayleigh Drive.	<p>To be investigated when resources allow during financial year 2016/17.</p> <p><b>Member concern at suggested location due to loss of parking, Ward Members to identify alternative location.</b></p>

Reference Number	Date 1 <sup>st</sup> Reported (Month/Year)	Ward Member	Subject of Request	Update
15/08	July15	Cllr Holland	Consider hardening of verge, eastern end of Riviera Drive	<p>Majority of properties have no off street parking and frontages not adequate to allow for off street parking to be provided. Members will be considering a report requesting a proposal for permit parking controls be advertised in this area which will reduce parking pressure and it may be advisable to defer this request pending the outcome.</p> <p><b>Defer request</b></p>
15/18 747	August 15	Cllr Jarvis	<p><b>Saxon Gardens, Delaware Crescent, Blyth Avenue and Bunters Avenue.</b> Residents are parking on green areas and have requested that this is formalised by additional parking being created. There are also areas of waiting restrictions which require investigation for removal or reduction.</p>	<p>When resources allow, a review will be undertaken to determine any locations where waiting restrictions can be amended to provide additional parking. There is a recommendation elsewhere on the committee's agenda in this regard. Report will be submitted to this committee detailing results.</p> <p><b>Recommend this request investigated during 2016/17, as part of the wider review of the historic waiting restrictions.</b></p> <p>The three streets are subject to parking pressure however property frontages are of adequate size to facilitate off street parking and residents should pursue the option to provide this. Where the properties are flats, discussion should be held with South Essex Homes as to potential remedies.</p> <p><b>Recommend no further action as off street parking areas can be provided and remove from list.</b></p>

Reference Number	Date 1 <sup>st</sup> Reported (Month/Year)	Ward Member	Subject of Request	Update
15/19	August 15	Cllr Jarvis	<b>One Way system in Saxon Gardens.</b> This is associated with the above request and has been motivated by the success of the Delaware Crescent scheme.	One-way traffic flow may be beneficial; geometry of road is likely to prevent any potential speed increase which can be a general feature of one-way traffic flows.  <b>Recommend ward Members undertake survey of residents to assess preferred direction of flow, Committee is requested to authorise the advertisement of resulting proposals.</b>
16/01  748	Feb 16	Cllr Buckley	Rochford Road Service Road. Propose restrictions to deter parking.	Parking is believed to be related to the airport and preventing parking in an isolated area will merely displace these vehicles.  <b>Recommend no further action at this time and suggest ward Councillors consider whether an area wide parking scheme would be appropriate in accordance with the policy requirements.</b>

Reference Number	Date 1 <sup>st</sup> Reported (Month/Year)	Ward Member	Subject of Request	Update
16/02	Feb 16	Cllr Buckley	Feeches Road junction with Rochford Road. Extend existing junction protection.	Junction currently protected with 15 metres of restrictions which is in excess of Highway Code guidance  <b>Recommend no further action.</b>
16/03	April 16	Cllr Walker	Implement limited waiting restrictions – shops between school and Jones Corner	Area currently has no restrictions resulting in long term parking.  <b>Recommend limited waiting restrictions proposed to encourage parking turnover for local shop customers.</b>
16/04	May 16	Cllr Courtney	Harden verges Silversea Drive	Very narrow verges along length of street are generally subject to parking due to the width of the road being unable to accommodate two-side parking without encroachment onto these areas.  <b>Recommend ward Members undertake consultation with residents in accordance with verge hardening policy.</b>  <b>In the event that at least 40% of residents respond and that of these responses, 70% are supportive of the proposal, recommend to progress the request with the design incorporating “soft areas” at positions along the street for natural drainage including the consideration of tree planting.</b>

Reference Number	Date 1 <sup>st</sup> Reported (Month/Year)	Ward Member	Subject of Request	Update
16/05	May 16	Cllr Assenheim	Extend restrictions Frobisher Way towards ASDA exit end.	<p>Does not meet criteria. Parking has been displaced following the introductions of restrictions further along the street. While the street is a bus route, the displaced parking has not encouraged any complaints from the bus operators.</p> <p><b>Recommend no further action.</b></p>
1/06 750	May 16	Cllr D Garston	Southchurch Boulevard. Propose restrictions by Centenary Place	<p>Does not meet criteria. The request is to provide restrictions adjacent to the vehicular access of a new development. At the planning permission stage, developments are assessed for both impact on the highway networks and access/exit arrangements. Concerns over visibility were not raised by colleagues considering the application. There have been no change in circumstances since then.</p> <p><b>Recommend no further action</b></p>
16/07	May 16	Cllr Hadley	Campfield Road. Propose waiting restrictions on bend by Cumberland Packaging	<p>Does not meet criteria. Vehicles waiting on the highway to access the factory, the driver is with the vehicle at all times therefore waiting restrictions would not prevent this practice.</p> <p><b>Recommend no further action.</b></p>



Reference Number	Date 1 <sup>st</sup> Reported (Month/Year)	Ward Member	Subject of Request	Update
16/08	August 16	Cllr Kenyon	Thorpe Hall Close – propose restrictions to protect driveway.	Does not meet criteria. The powers delegated to the traffic authority in relation to Traffic Regulation Orders do not include proposing waiting restrictions to protect a private access.  <b>Recommend no further action.</b>
16/09  751	June 16	Chalkwell Councillors	Lansdowne Avenue. Propose one-way traffic flows southbound.	No accidents recorded and as a residential street, traffic flow is not considered a high priority however, the road is heavily parked resulting in few passing places being available for vehicles to give way to oncoming traffic resulting in anti-social behaviour. Amending the traffic flow would resolve this issue while maintaining the much needed on street parking provision. Members have consulted residents who are supportive of the suggestion.  36 residents support the suggestion to amend the traffic flow to one-way southbound 10 residents prefer northbound 2 residents not in favour of either option  <b>Recommend advertise proposals to amend traffic flow.</b>



753				<p>available for moving traffic Waiting restrictions will not be provided for individual private accesses in isolation. Not met</p> <p>6) Cost of schemes and likely savings through accident reduction need to be part of priority consideration. Not met</p> <p>Members are reminded that residents were recently consulted as to permit parking controls to manage parking but the level of responses was not adequate to progress the suggestions (considered June 2016).</p> <p>Whilst the policy requirements for waiting restrictions are unmet, the primary purpose still is to manage parking. However any restrictions will lead to displacement in the neighbouring roads.</p> <p>Members could consider either of the following options:-</p> <ul style="list-style-type: none"> <li>a) To agree no action is needed.</li> <li>b) To consider representations from ward councillors in this regard and agree proposals</li> <li>c) Make an exception to reconsider permit parking option already discussed at June 2016 meeting, if ward councillors wish to undertake further consultation for a Residents Parking Scheme</li> </ul> <p>Members are asked to note the outcome of the residents parking consultation in this area which was reported to the June committee. The policy requirements were unmet mainly due to lack of response from a large proportion of residents with drives. The Members may</p>
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				wish to consider this in their discussions and decision  Members' views are sought.
Reference Number	Date 1 <sup>st</sup> Reported (Month/Year)	Ward Member	Subject of Request	Update
16/11  754	August 2016	Cllr Woodley	Colbert Avenue. Propose waiting restrictions on west of church.	Does not meet criteria. No accidents recorded at or near to the location. It is considered that parking at the location is probably acting as a speed reduction feature. However concerns have been expressed by ward Councillors, the church and residents to deal with parking at the bend which is perceived to be dangerous. If agreed to consult, it is proposed ward councillors consult local residents for a consensus approach as previous efforts in this regard have been inconclusive.  Member's Views are sought

# Southend-on-Sea Borough Council

Report of Corporate of Place  
To  
**Traffic & Parking Working Party & Cabinet  
Committee**  
On  
**19<sup>th</sup> September 2016**

Report prepared by:  
Cheryl Hindle-Terry - Team Leader, Parking, Traffic  
Management and Road Safety Team

Agenda  
Item No.

**15**

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**Petition Requesting Permit Parking Controls  
Southend East**  
**Executive Councillor: Councillor Tony Cox**  
***A Part 1 Public Agenda Item***

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## **1. Purpose of Report**

- 1.1 To advise Members of a petition signed by 320 residents of the roads north of Southend East Railway Station requesting parking controls to deter all day parking by commuters.

## **2. Recommendation**

**That the Traffic & Parking Working Party and Cabinet Committee:**

- a) **Note the petition and thank the residents for taking the time to compile the petition; and agree to;**
- b) **Advertise a proposal to introduce a Permit Parking Area in the streets detailed in Appendix 1.**
- d) **Further agree that in the event that no objections are received to the proposal, the Traffic Regulation Order be confirmed. Any objections will of course be referred to this Committee for consideration.**

## **3. Background**

- 3.1 The streets north of Southend East Railway Station feature a mixed design of properties. Several streets feature houses with adequate frontage to accommodate vehicles on the property whereas other streets front directly onto the street resulting in residents relying solely on parking provision on street.
- 3.2 Southend East Railway Station attracts large numbers of commuters, the level of parking by non-residents is increasing resulting in residents being prevented from parking near to their homes. A resident has organised a petition requesting parking controls be introduced to deter the all-day parking.

- 3.4 The petition has attracted a large response with 57% of residents signing the petition and the majority are supportive of parking controls.
- 3.3 A Permit Parking Area operational for part of the day will ensure parking availability for the residents.

#### **4. Other Options**

- 4.1 Take no further action. The Council is required to consider petitions related to parking controls and success from other permit parking style controls demonstrates that we can improve the parking situation for residents by introducing controls.

#### **5. Reasons for Recommendations**

- 5.1 To manage parking increase parking provision.

#### **6. Corporate Implications**

##### *6.1 Contribution to Council's Vision & Corporate Priorities.*

- 6.1.1 The road is not a major route and generally subject to local and residential traffic only. The proposals will likely reduce vehicle speeds as the road will be visually narrowed contributing to a safe Southend.

##### *6.2 Financial Implications*

- 6.2.1 Any costs are met through existing budgets.

##### *6.3 Legal Implications*

- 6.3.1 The statutory consultation will be undertaken prior to any further action.

##### *6.4 People Implications*

- 6.4.1 None.

##### *6.5 Property Implications*

- 6.5.1 None.

##### *6.6 Consultation*

- 6.6.1 Traffic Regulation Orders are subject to statutory consultation requirements.

##### *6.7 Equalities and Diversity Implications*

- 6.7.1 Waiting restrictions are amended to manage parking, reduce accidents and/or improve traffic flows. The objectives of managing parking and improving safety

takes account of all users of the public highway including those with disabilities and childcare responsibilities.

6.8 *Risk Assessment*

6.8.1 None.

6.9 *Value for Money*

6.9.1 N/A.

6.10 *Community Safety Implications*

6.10.1 None. However, the removal of the existing waiting restrictions are proposed to increase parking which in turn, reduces the likelihood of neighbourhood disputes.

6.11 *Environmental Impact*

6.11.1 None.

**7. Background Papers**

7.1 None.

**8. Appendices**

Appendix 1 Plan of area.

Appendix 2 Breakdown of results by street.

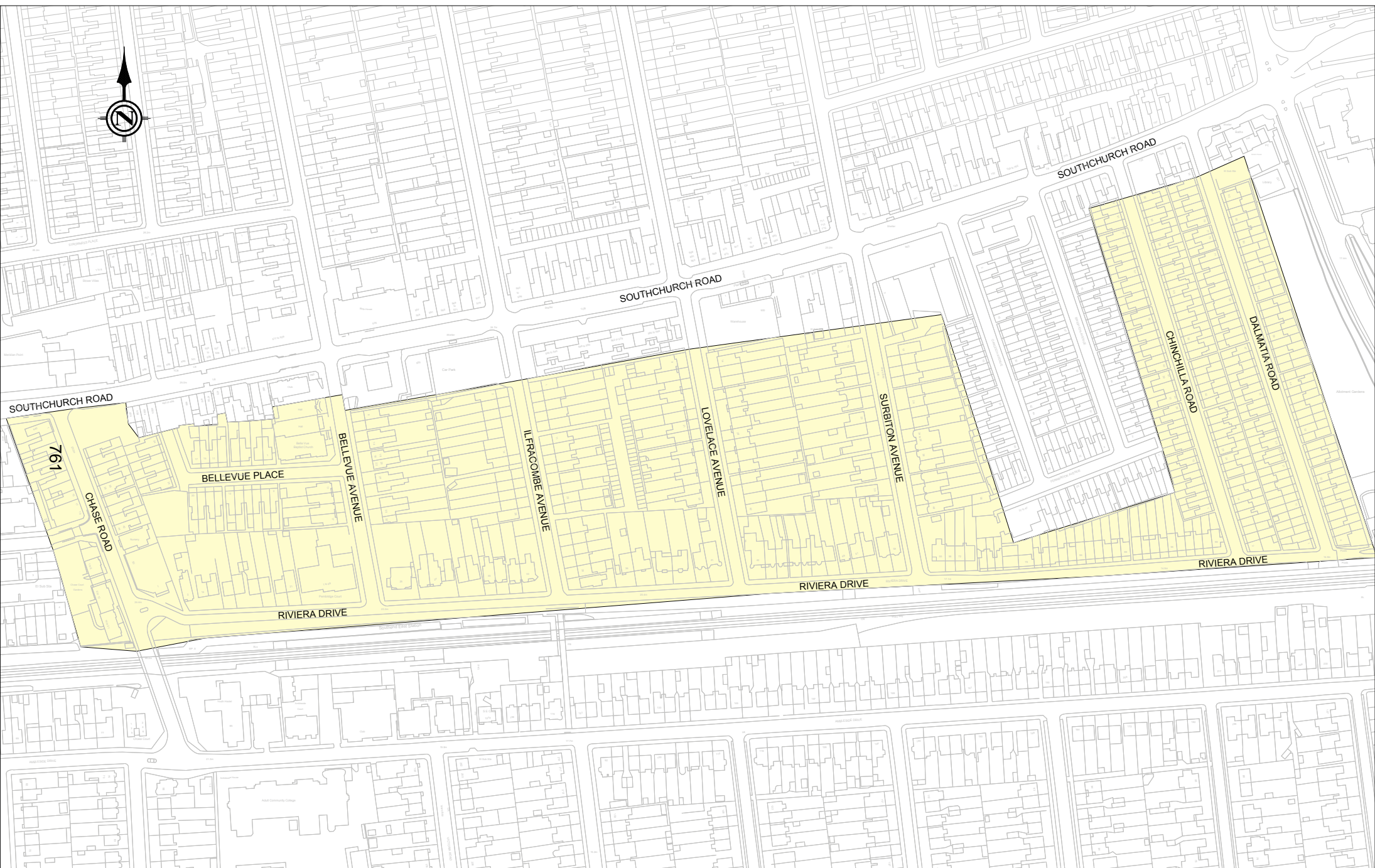
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Southend East – Appendix 1

Road	No. Properties	No. Responses	% Return	% Supportive
Belle Vue Place/Avenue	69	36	52	99
Ilfracombe Avenue	41	25	60	99
Lovelace Avenue	49	26	53	99
Surbiton Avenue	80*	30	37	94
Chinchilla Road	89	73	82	100
Dalmatia Road	96	68	71	99
Riviera Drive	125	62	49	100
Total	549	320	57%	99 %

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# SOUTHEND EAST AREA

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# Southend-on-Sea Borough Council

Report of Corporate of Place  
To  
**Traffic & Parking Working Party & Cabinet  
Committee**  
On  
**19<sup>th</sup> September 2016**

Report prepared by:  
Cheryl Hindle-Terry - Team Leader, Parking, Traffic  
Management and Road Safety Team

Agenda  
Item No.

**16**

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**Petition Requesting Amendment to Existing Parking Controls  
Shaftesbury Avenue  
Executive Councillor: Councillor Tony Cox  
*A Part 1 Public Agenda Item***

---

**1. Purpose of Report**

- 1.1 To advise Members of a petition signed by 28 residents of Shaftesbury Avenue requesting amendments to the existing parking restrictions.

**2. Recommendation**

**That the Traffic & Parking Working Party and Cabinet Committee:**

- a) **Note the petition and thank the residents for taking the time to compile the petition; and agree to;**
- b) **Agree to advertise the proposal to remove the existing alternate month parking restriction which will increase parking availability;**
- d) **Further agree that in the event that no objections are received to the proposal, the Traffic Regulation Order be confirmed. Any objections will of course be referred to this Committee for consideration.**

**3. Background**

- 3.2 The section of Shaftesbury Avenue between Liftstan Way and Warwick Road is subject to a parking restriction which prohibits parking on alternate sides of the road depending on the calendar month.
- 3.2 The road is fairly narrow with widths varying between 7.0 metres and 7.3 metres however unrestricted roads in the area are of a similar width and no issues have been identified with access for larger vehicles.
- 3.3 The area is subject to high parking demands and the removal of the restrictions will increase parking availability.

#### **4. Other Options**

- 4.1 Take no further action. The Council is required to consider petitions related to parking controls and where additional on street parking can be created; it is prudent to advertise the proposals and assess any feedback.

#### **5. Reasons for Recommendations**

- 5.1 To increase parking provision.

#### **6. Corporate Implications**

##### *6.1 Contribution to Council's Vision & Corporate Priorities.*

- 6.1.1 The road is not a major route and generally subject to local and residential traffic only. The proposals will likely reduce vehicle speeds as the road will be visually narrowed contributing to a safe Southend.

##### *6.2 Financial Implications*

- 6.2.1 Any costs are met through existing budgets.

##### *6.3 Legal Implications*

- 6.3.1 The statutory consultation will be undertaken prior to any further action.

##### *6.4 People Implications*

- 6.4.1 None.

##### *6.5 Property Implications*

- 6.5.1 None.

##### *6.6 Consultation*

- 6.6.1 Traffic Regulation Orders are subject to statutory consultation requirements.

##### *6.7 Equalities and Diversity Implications*

- 6.7.1 Waiting restrictions are amended to manage parking, reduce accidents and/or improve traffic flows. The objectives of managing parking and improving safety takes account of all users of the public highway including those with disabilities and childcare responsibilities.

##### *6.8 Risk Assessment*

- 6.8.1 None.

##### *6.9 Value for Money*

6.9.1 N/A.

6.10 *Community Safety Implications*

6.10.1 None. However, the removal of the existing waiting restrictions are proposed to increase parking which in turn reduces the likelihood of neighbourhood disputes.

6.11 *Environmental Impact*

6.11.1 None.

**7. Background Papers**

7.1 None.

**8. Appendices**

There are no appendices.

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# Southend-on-Sea Borough Council

Agenda  
Item No.

17

Report of Corporate Director for Place

to

**Place Scrutiny**

on

**10<sup>th</sup> October 2016**

Report prepared by: Scott Dolling, Head of Service -  
Economy, Regeneration & Tourism

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## Skills Development

**Place Scrutiny Committee**

**Executive Councillor: Councillor Ann Holland**

**Part 1 (Public Agenda Item)**

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### **1. Purpose of Report**

- 1.1 To set out the current skills priorities and action plan within the Council's Economic Development Team and ensure Member's views and aspirations are captured within its programme.
- 1.2 That Member's note that this is being considered as a Pre-Cabinet Scrutiny item prior to formal consideration by Cabinet.

### **2. Recommendations**

- 2.1 That Member's note existing Skills Programme and Action Plan.
- 2.2 Views are invited on the report to help inform a new Southend Skills Strategy.

### **3. Background**

- 3.1 Southend's economy consists of a broad range of sectors with around 6,500 businesses with a working age population of 110,000 people. Most firms are small or micro within the government definition of SME (small to medium sized enterprises).
- 3.2 Educationally, Southend boasts some enviable academic results through its high performing schools. The growing Further and Higher Education Campus has also contributing towards improved trends of level 4 and 5 qualifications seen for Southend in recent years.
- 3.3 Despite these educational improvements, there has been significant commentary during consultation exercises with the business community that local employment opportunities are being lost due to lack of appropriate skills. This restricts our business community's development, the ability to attract new

businesses to the town and equally affects the opportunity for local people to find employment within the Borough.

- 3.4** The skills gap experienced in different ways by different sectors. Employers in engineering and medical technology sectors struggle to fill high skilled posts while across the board, employers report shortages of more common attributes such as customer service.
- 3.5** Some research has been delivered by the Essex Employment and Skills Board this year which provides an evidence base for key skills needs which can be used to assist our planning. It identifies that there are vacancies in a broad range of sectors which requires further focused local interpretation.
- 3.6** We recently appointed a Skills Officer to focus attention on delivering strategic objectives with partners and a much needed Action Plan. The role will provide a focus on meeting current and future skills needs as articulated by the business community.
- 3.7** The post is based within the Economic Development Team with its close connection to the business community, yet is also aligned with the Education Team within Southend Borough Council's People Department to ensure a seamless Council approach.
- 3.8** We are now seeking to ensure that the Skills Strategy and Action Plan will capture Members' aspirations as well as feedback from other stakeholders – business community and education providers.
- 3.9** Targeting young people in new and innovative ways to address issues of career choices and opportunities must be developed. The language and approach must be fit for purpose and relevant to the user. Initiatives such as the development of a game to inspire career options is one such example being considered. Other examples recently include a Southend school which considered the techniques in building an aircraft to excite young people in engineering.
- 3.10** Engineering is one such key area where recruitment locally has been challenging for employers. The development of a Studio School (a form of technical college linked to private enterprise) is an ambition for Southend and is included in aspirations for the town.
- 3.11** The CONNECT Project is led by London Southend Airport and partnered by a number of aviation and public sector partners including the Council and is exploring trade, tourism and education links between Southend and Carlisle utilising the recently announced route between these airports. Skills is recognised as a key element of the two economies and with synergies between them the opportunity for a cross region skills presence is being explored.
- 3.12** Essex is part of the fifth wave of Area Based Reviews, which are being carried out nationally and considering the shape of all post-16 education and training across the area and how well this fits with local economic and educational needs. In line with the Area Review Guidance (revised March 2016) the review will focus on colleges. This will commence in November and the Council will

have a role in supporting the review activity. The outcomes of which will be vital to the Action Plan going forward in identifying gaps and duplication in provision.

**3.13** Southend and Thurrock Councils have already been successful in securing funding from the Careers and Enterprise Company to develop an Enterprise Advisor Network across the Boroughs. Enterprise Advisors (EAs) are volunteers drawn from businesses who work directly with the school's leadership team to develop effective employer engagement plans.

**3.14** Career Ready is another initiative we are linking with that will help support the provision of practical and work based advice.

### **60 Minute Mentor**

**3.15** The 60 Minute Mentor Programme is a significant initiative that has proved successful in addressing some of the perceptions around business interaction. It is proposed that it will feature in an expanded role as part of the Action Plan.

**3.16** The Council championed the 60 Minute Mentor Project which places key speakers from the business community in front of young learners 14-19 for an hour. The 60 minutes gives an insight into the world of work and the opportunities for employment and the associated skills needed for individuals to be successful. The process benefits both learners and the businesses who, as a result, understand more about each other's needs.

**3.17** The benefits of 60 Minute Mentor have been demonstrated externally and it has been included in a recent funding application which we are expecting to be confirmed in the next few weeks. This should support the Programme's growth across South Essex.

**3.18** A targeted variation of the 60 Minute Mentor to assist parents and future parents of vulnerable and deprived groups is being developed to assist the Better Start Programme.

**3.19** The 60 Minute Mentor Programme and other initiatives will also include elements of entrepreneurialism to capture and support the business start-up culture which is very strong in Southend and South Essex. Equipping individuals with knowledge of how to avoid some of the common failures yet encourage the spirit of enterprise should help secure better starts for new business initiatives.

### **Virtual Skills Academy**

**3.20** Southend-on-Sea has some significant regeneration projects currently in development, including Better Queensway and the Airport Business Park. Using these projects as a stimulus, a project to deliver a Virtual Skills Academy is now in place.

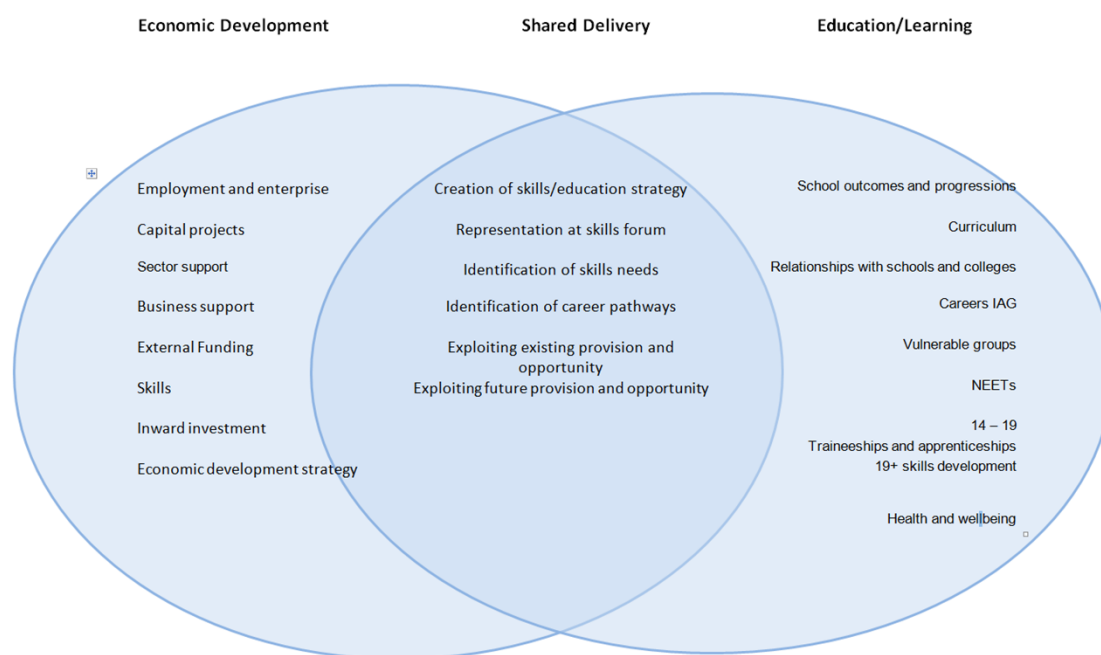
**3.21** The Virtual Skills Academy will seek to address the future needs of developers and employers by linking with Education Providers and the Council. The key regeneration projects for the Borough have indicative employment numbers surrounding needs for construction, project management etc, which can help

determine pipeline of skills and training needed. An outline plan with milestones for this Academy is appended (see Appendix 1).

**3.22** The Academy is not a new building, but a virtual concept. It will facilitate an active working relationship between the education, public and private sectors and help develop a pipeline of supply of suitably skilled people for future skills needs.

**Broader Education Links**

**3.23** During the development of our outline Forward Programme, there has been some analysis of the joint and discrete roles within the Education and Skills Agenda by the Council’s People and Place Departments. The analysis is the result of positive and proactive interdepartmental working and are summarised in the diagram below:



**3.24** A plethora of external forums and groups exist that relate to both Economic Development and Educational aspects of skills. The Place and People Departments align to participate as appropriate to focus on the overriding benefit for Southend.

**3.25** Education, Economic Development and the external Further Education providers regularly network to ensure that opportunities are maximised for our communities. This work is leading to the development of a formal Skills Strategy. In particular, we have developed excellent relationships with Southend Adult and Community College and South Essex College.

**3.26** The focus of the joint activity is on the following areas:

- Developing the Virtual Skills Academy
- Identification of career pathways
- Careers and Enterprise Initiatives/Connexions
- Targeted intervention and support for vulnerable groups
- Exploiting existing/future provision and opportunity
- Work with Education Providers to ensure students are work ready
- Engage Member's in actively promoting skills friendly policy

**3.27** Following a recent successful bid by Essex University on behalf of partner organisations in Essex, including Southend, finance will become available to support targeted groups of students to aspire to, attain and stay in programmes of Higher Education. This initiative compliments our prioritisation of the skills gap for disadvantaged groups in Southend, and we will be working closely with the University and schools to make the most of this opportunity

**3.28** Members' views are sought during this Pre-Cabinet Scrutiny period to provide specific outcomes on what they would like to see tackled in the Skills Strategy and Action Plan.

#### **4. Other Options**

4.1 This broad direction has been developed following feedback from partners and is the result of considerable consultation with education partners and the business community.

#### **5. Reasons for Recommendations**

5.1 Tackling skills gaps identified can assist our young residents into employment and enterprise in the Borough supporting overall prosperity.

#### **6. Corporate Implications**

6.1 Contribution to Council's Vision & Corporate Priorities

6.1.1 Ensuring that our local workforce is skilled for appropriate work in the local community assists all of our objectives to lead towards a Safe, Clean, Healthy Prosperous and Excellent Southend.

6.2 Financial Implications

6.2.1 The activity is funded through existing budgets and external partnerships and funding.

### 6.3 Legal Implications

6.3.1 There are no Legal implications.

### 6.4 People Implications

6.4.1 There are no People implications.

### 6.5 Property Implications

6.5.1 There are no Property implications.

### 6.6 Consultation

6.6.1 This Pre-Scrutiny process is designed to engage Member's prior to developing a full Skills Action Plan and Strategy

### 6.7 Equalities and Diversity Implications

6.7.1 There are no Equality and Diversity implications.

### 6.8 Risk Assessment

6.8.1 There are no Risk assessment implications.

### 6.9 Value for Money

6.9.1 There are no Value for Money implications.

### 6.10 Community Safety Implications

6.10.1 There are no Community Safety implications

### 6.11 Environmental Impact

6.11.1 There are no Environmental impact implications

## 7. **Background Papers**

7.1

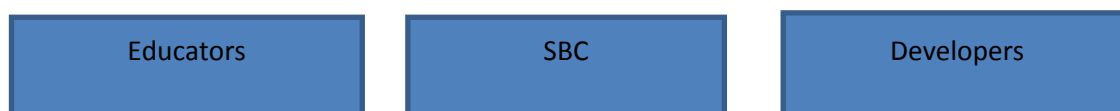
## 8. **Appendices**

8.1 Appendix 1 – Outline Plan for Academy

## Towards a Virtual Skills Academy for Southend

### What?

- This paper proposes the formulation of a “Virtual Skills Academy”. This is a tripartite agreement between local partners to **ensure that a sufficient local workforce is available to meet the future workforce requirements of upcoming regeneration projects.**
- There are three stakeholders groups critical to the project, with which we are consulting:



### Consultation Status:

Body	State
South Essex College	Providing feedback on initial paper
Southend Adult Community College	Providing feedback on initial paper
SEEVIC	Not yet consulted
Procat	Not yet consulted
Anglia Ruskin University	Not yet consulted
University of Essex	Not yet consulted
Henry Boot plc	Not yet consulted

### Why?

At present Southend Borough Council is implementing and planning a number of significant regeneration projects that will revitalise and improve the built environment across the borough, to be implemented over the next 20 years.

- These include:
  - Southend Airport Business Park (6500 Permanent, FT posts)
  - The Better Queensway Project (2500 Temporary Construction posts)
- SBC has prioritised the development of ‘skills projects’ and improving the opportunities on offer to Southend residents – seeking to close the skills gaps. Specific resources have been dedicated in this area.
- SBC has a responsibility to ensure that local residents have access to the opportunities that stem from the regeneration projects taking place in the borough.

### Roles of the Key Partners

- In order to close the potential skills gaps, all of the partners involved need to be willing to share a greater level of information.
- We propose that the initial roles and responsibilities detailed within the VSA agreement include, but are not limited to:

Partner	Role	Input/Responsibility
- Southend	Lead Partner for the	- To work to identify and define the

Borough Council	Better Queensway and Airport Business Park Projects	<p>employment opportunities that will be generated through development of projects and communicate these through the Virtual Skills Academy.</p> <ul style="list-style-type: none"> <li>- To utilise procurement policies and practises, wherever possible, to ensure that benefits for local residents are maximised through contracting &amp; purchasing (to support local upskilling)</li> </ul>
<ul style="list-style-type: none"> <li>- South Essex College</li> <li>- Southend Adult Community College</li> <li>- SEEVIC</li> <li>- Procat</li> <li>- Anglia Ruskin University</li> <li>- University of Essex</li> </ul>	Local Skills Delivery Body	<ul style="list-style-type: none"> <li>- To provide education and training provision, wherever practicable, to meet the employment needs of the Virtual Skills Academy.</li> <li>- To facilitate employment links between suppliers, contractors and the students graduating through locally provided courses.</li> <li>- To increase student awareness of local projects and the associated employment opportunities, via co-ordinated partnership work with other stakeholders.</li> </ul>
<ul style="list-style-type: none"> <li>- Development Partners (tbc)</li> <li>- Henry Boot Plc</li> </ul>	Developer/Contractor/Delivery Partner	<ul style="list-style-type: none"> <li>- To inform and advise the VSA on the forecasted employment opportunities/skills requirements involved in the delivery of the project (For example Carpenters, Plumbers, Electricians, Site Officers, Energy Consultants, Heating and Cooling System specialists)</li> <li>- To work with local partners and the VSA to ensure employment opportunities are advertised locally and made accessible to local people, wherever practicable.</li> <li>- Wherever practicable, to facilitate the progress of Southend residents into jobs within the focus industries, for example through the use of practice interviews, and liaison with the partner education/training bodies.</li> </ul>

#### When, How, Who?

Item	When	How
1	August 2016	Queensway Project Board
2	August 2016	Drafted "Towards a Virtual Skills Academy" paper



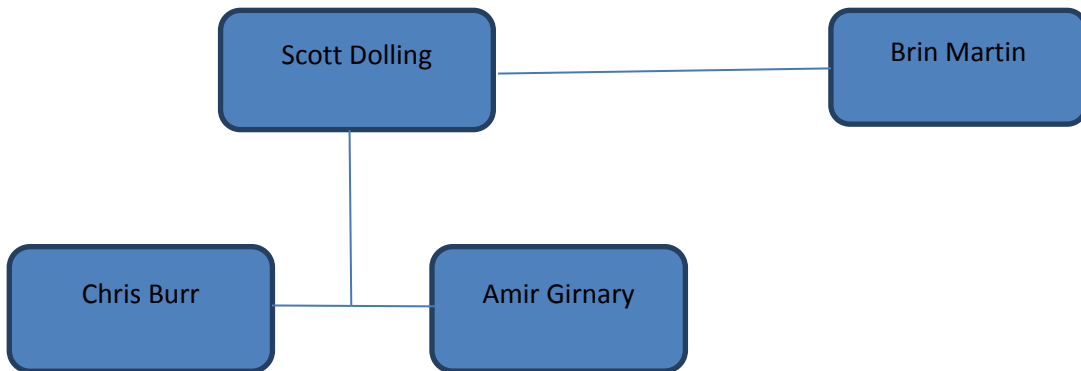
3	September 2016	Initial light touch consultation
4	September 2016	DMT Meeting
5	October 2016	CMT Meeting
6	October, November and December 2016	Detailed consultation: (a) workshops and (b) Interviews
7	January 2017	Draft Agreement
8	February 2017	Statement of Intent produced
9	March 2017	Document signed by all parties
10	April- June 2017	Marketing/Communications of the initiative, to the wider community.

The Virtual Skills Academy is one of a range of new pilot initiatives being developed by the Economic Development Team, in partnership with the Learning Team, to facilitate a skilled local workforce capable of meeting the needs of local businesses.

Once the Virtual Skills Academy is in place, we plan to explore the potential for similar initiatives targeting other sectors and organisations.

**Who**

Project Team



END

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Place Scrutiny Committee  
In-depth Scrutiny Project 2015/16

## 20 mph speed restrictions in residential streets



Final Report & Recommendations



## 20 mph Speed Restrictions in Residential Streets

### 1. Background

1.1 At its meeting on 13<sup>th</sup> July 2015, the Place Scrutiny Committee agreed that its in-depth study for the year should be “20mph in residential streets” (Minute 125 refers). The project plan was approved by the Place Scrutiny Committee at its meeting on 12th October 2015 (Minute 312 refers). The study was undertaken on behalf of the Scrutiny Committee by the project team whose membership comprised:

Councillors Habermel (Chairman), Assenheim, Callaghan, Cox, Evans, Kenyon, Mulroney and Ware-Lane.

1.2 The project team considered a variety of research documentation, legislation and national guidance. It also heard from a variety of witnesses through presentations and written submissions. These included the Council’s transport policy officers, representatives from the Safer Essex Roads Partnership and the Council’s Public Health Team. It also had regard to Council policies and the evidence from the existing 20mph zones and limits in the Borough.

1.3 The project team was supported in its investigations by the following officers:

Cheryl Hindle-Terry, Adrian Watling and Lewis Pearmain (Technical Support) and Tim Row (Project Co-ordinator).

### 2. Framework for the Study

2.1 It was agreed that the framework for the study should be:

- (i) To investigate and consider the feasibility of introducing 20mph speed restrictions in “residential streets”;
- (ii) To investigate the potential benefits of 20 mph speed restrictions on road safety in terms of reducing vehicle speeds, casualty numbers and injury severity and the implications;
- (iii) To investigate whether reductions in traffic speeds and improvements in road safety are likely to be achieved without the need for physical calming measures and regulated by signage and road markings (20 speed limits) or whether such calming features are necessary to reduce traffic speeds to 20 mph (20 mph zones);
- (iv) To consider the potential consequences of any displacement of traffic as a result of introducing lower speed limits and encourage the appropriate and efficient use of the road network; and
- (v) To consider and compare the potential costs and/or savings of implementing 20 mph restrictions including environmental impacts.

### 3. Definition of “Residential Streets”

3.1 For the purposes of this scrutiny study, the term “residential street” was defined as a road fully or predominantly made up of residential dwellings excluding distributor and local distributor routes, unless there was a clear and valid reason for their inclusion. This definition is consistent with the Department for Transport’s (DfT) guidance.

### 4. Context and Drivers

4.1 The key drivers for the study to be undertaken included road safety, the potential to safely increase traffic levels, the environmental benefits in terms of air and noise pollution levels, the development of safe shared spaces and the perception of safety by the public/residents.

4.2 It is evident that the demand for the default speed limit to be reduced from 30 mph to 20 mph has increased in recent years. This has essentially been in response to national and local campaigns by local residents, pressure groups, etc. As a result, a number of local authorities have now implemented or are investigating the implementation of 20 mph schemes.

4.3 ‘Brake’ is a road safety charity that works with communities and organisations across the UK to stop the tragedy of road deaths and injuries, making streets and communities safer for everyone. It also supports people bereaved and seriously injured on roads. One of their campaigns is GO20, which seeks safe walking and cycling for all, through slower speeds in communities and changing the default urban speed limit to 20 mph.

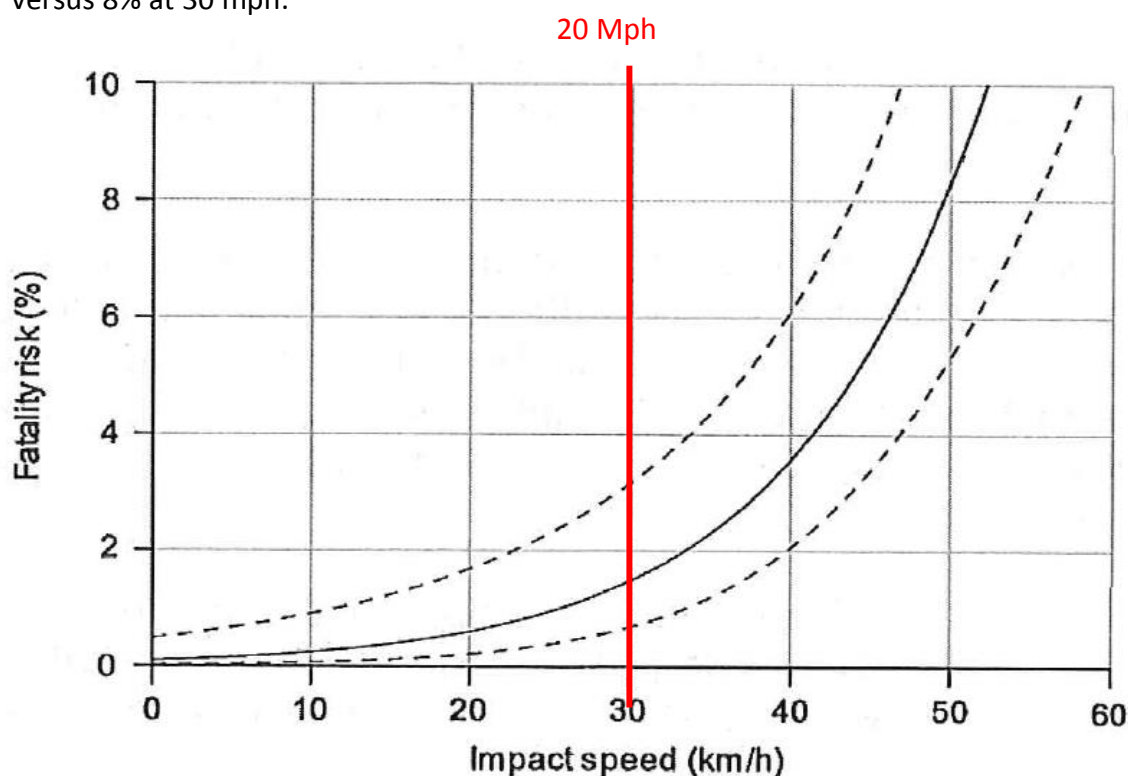
4.4 ‘20s Plenty for Us’ is a “not for profit” organisation which campaigns for the default speed limit on residential and urban roads to be reduced to 20 mph. It has been a driver for many communities to seek a reduction of the default to 20 mph.

4.5 Traffic travelling at speeds less than 20mph gives the driver a shorter stopping distance and gives more room and time to react. This is illustrated in the diagram below, which has been taken from the official driving theory test.



4.6 These braking and overall stopping distances greatly increase depending on the weather/road conditions. In wet conditions, the braking and overall stopping distance will be at least doubled. In icy conditions, these can increase to up to ten times.

4.7 The project team heard that the risk of death, or serious injury, when an adult pedestrian is hit by a motor vehicle follows a curve. Speed significantly increases the chance of being injured in a collision. Evidence from the Royal Society for the Prevention of Accidents stated that research had shown that the risk of death for pedestrians struck by cars increases at higher impacts speeds, although the exact risk levels varied between the studies. One of the first studies of pedestrian injury and car impact speed<sup>1</sup> found that at 20mph there was a 2.5% chance of being fatally injured, compared to a 20% chance at 30mph, although this study is now regarded as having overestimated the risks. A recent review identified the studies which had produced the most reliable modern estimates<sup>2</sup>. The results from one of these studies is presented in the table below, which shows a fatality risk of 1.5% at 20 mph versus 8% at 30 mph.



The solid line is the most likely estimate and the dotted lines show the 95% confidence limits

## 5. Local Transport Policy

5.1 The Project Team noted that the Council has a variety of policies and plans that support the principles of 20mph schemes. Policies CP3 and CP4 of the Council's Core Strategy 2001-2021 are particularly relevant to this study. These are set out below:

<sup>1</sup> Ashton S J and Mackay G M **Some characteristics of the population who suffer trauma as pedestrians when hit by cars and some resulting implications** 4th IRCABI International Conference, Gothenborg. 1979

<sup>2</sup> Erik Rosén, Helena Stigson, Ulrich Sander, **Literature review of pedestrian fatality risk as a function of car impact speed**, Accident Analysis and Prevention Volume 43, 2011

Policy CP3: Transport and Accessibility - safeguarding and enhancing the environment of ‘Environmental Rooms’, as defined in the Southend Local Transport Plan

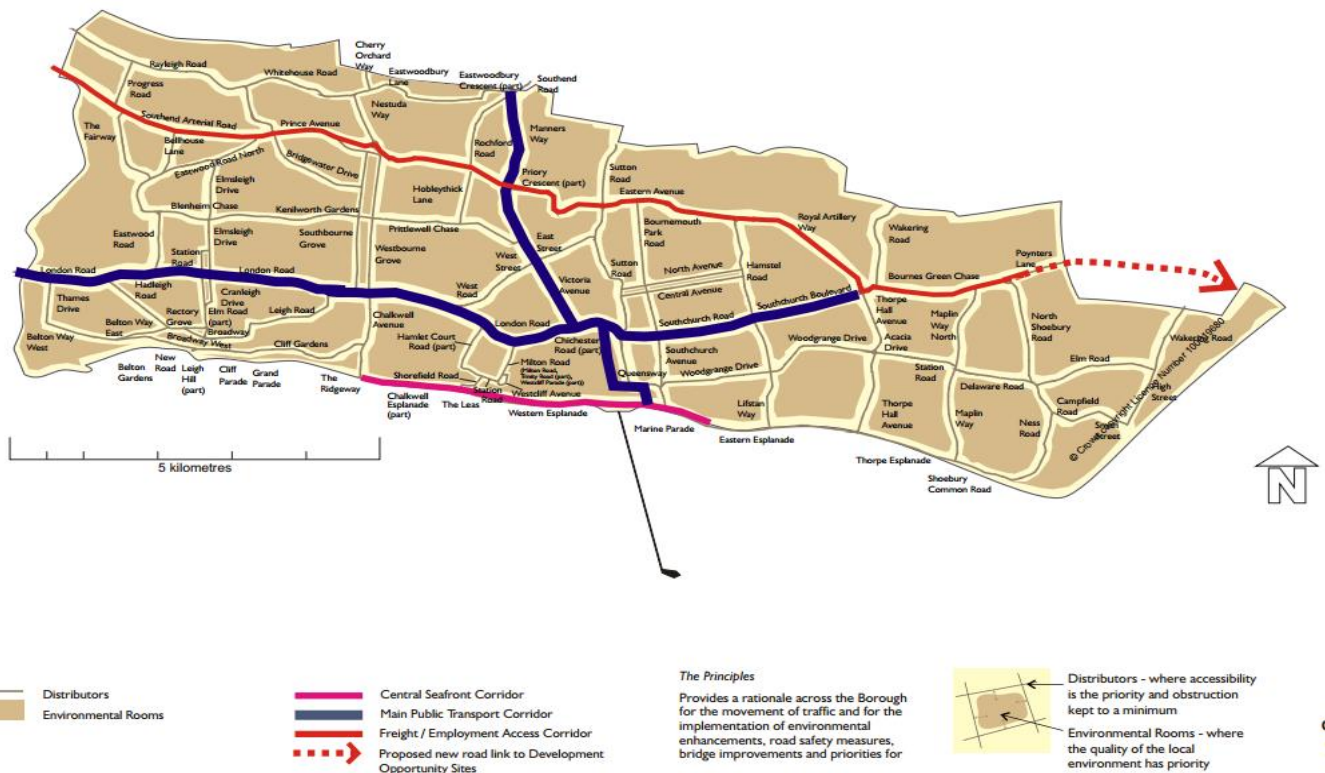
Policy CP4: The Environment and Urban Renaissance - creating safe, permeable and accessible development and spaces that encourage walking and cycling within ‘Environmental Rooms’.

5.2 The Local Transport Plan (LTP) is a vital tool in ensuring that the transport policies are part of a holistic approach to deliver the objectives of a “Safe, Clean, Healthy, and Prosperous Southend”.

5.3 The Local Transport Plan 2 established the principle, which provided a rationale across the Borough, for the movement of traffic and the implementation of environmental enhancements, road safety measures, bridge improvements and priorities for Distributors, where accessibility and the quality of the environment is the priority and obstruction is kept to a minimum.

5.6 A plan illustrating the concept and layout of the environmental rooms and distributor routes for the Borough is shown below.

Environmental Rooms and Distributors



5.7 This concept had been further developed in the Local Transport Plan 3, which aims to encourage healthier lifestyles by providing environments that promote good physical and mental health (e.g. through promotion of active modes of travel, improvement of local air quality and tranquillity levels). It set out an undertaking that the Council



would consider the introduction of residential 20 mph limits in the Borough after their potential impact has been assessed; not just on road safety but also wider impacts such as congestion, wider economic impacts, and CO<sub>2</sub> emissions. It identified the potential of 20mph speed limits in residential streets giving modal priority to pedestrians, bicycles, buses and cars. This is illustrated in the relevant excerpt from the Local Transport Plan 3 below.

Route Hierarchy Category	Mode Priority	Special Characteristics (see Footway Hierarchy below)	General Characteristics	Traffic Flows (Annual Average Daily Traffic Flows)
Strategic Primary A127 and A1159	HGV Bus *Car/Bicycle /Walking	High Quality Streetscape Cycle Route Pedestrian Focus	Top Priority traffic route	> 20,000
Primary Distributor including A13	Bus HGV *Car/Bicycle /Walking	High Quality Streetscape Cycle Route Pedestrian Focus	Second Priority traffic routes	15,000 to 19,999
Secondary Distributor	Bus LGV *Car/Bicycle /Walking	High Quality Streetscape Cycle Route Pedestrian Focus	Link the top and 2 <sup>nd</sup> priority routes to local areas of the town. All 30 mph	10,000 to 14,999
Local Distributors	Bus *Car/Bicycle /Walking LGV	High Quality Streetscape Cycle Route Pedestrian Focus	Local traffic – 30 mph	5,000 to 10,000
Residential Roads	Walk Bicycle Bus Car	High Quality Streetscape Cycle Route Pedestrian Focus	Local traffic – 20mph?	< 5,000

5.8 The LTP3 also seeks to contribute to an improvement of road safety for users of all modes of transport through measures such as:

- Traffic management e.g. 20mph zones, traffic calming and signing;
- Accident investigation including accident databases and road safety audits;
- Engineering schemes and enforcement;
- Education, training and publicity;
- Safe paths for walking and cycling.

5.9 The design of traffic calming needs to be carefully considered to avoid negative effects on the effective operation of public transport, e.g. road humps may adversely affect operation of low floor buses. The LTP also3 indicates that a more radical approach to street design with people oriented understanding of public space, known as “shared space” or “Home Zones” should be given serious consideration where appropriate. Such design of streets and other public spaces allows tackling not only safety but also congestion, economic vitality and community severance.

5.10 The following policies within the Local Transport Plan support the principles of establishing 20 mph speed restrictions in residential streets in the Borough:

Policy 15: Support safety partnerships and promote safer communities includes: *'The consideration of residential areas within the Borough to have 20 mph limits'*

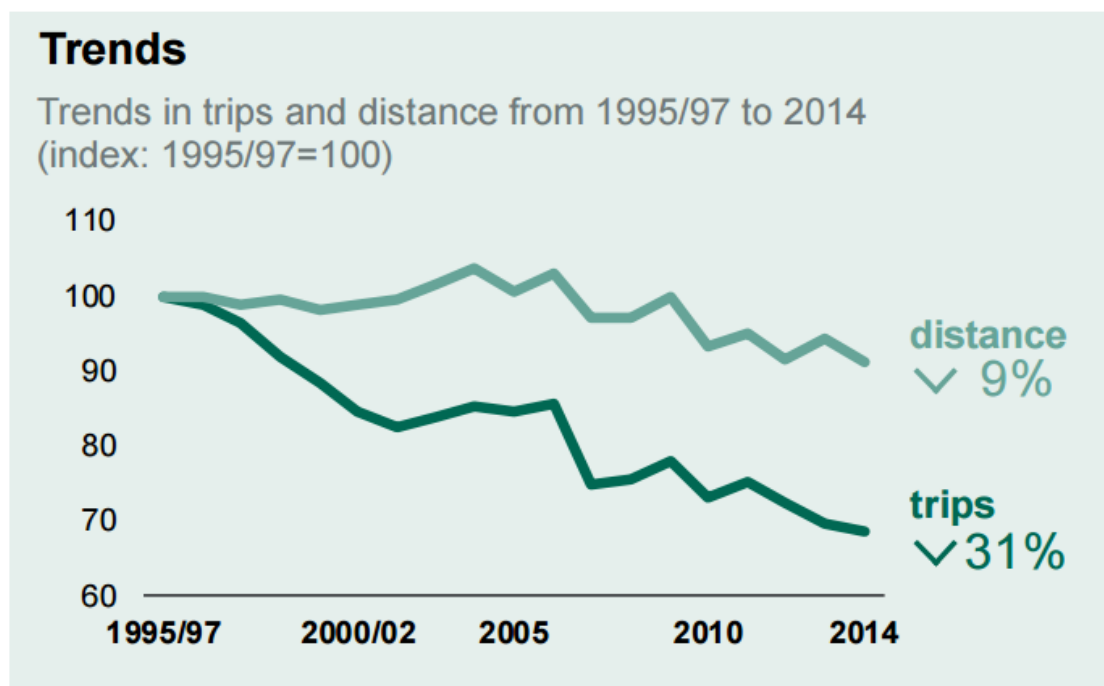
Policy 21: Tackle health inequalities by increasing the number of adults and children who walk and cycle for work, education and leisure

Policy 22: Ensure all public transport is fully accessible by 2017 includes: *'Seek to ensure pavements and pedestrianised areas are maintained to a good standard to aid ease of mobility for vulnerable road users'*.

5.11 Nationally, the annual British Social Attitudes Survey (BSA) continues to report that public opinion is pro-20mph. For example, research in 2010 showed that 71% of British people support 20mph. This was 72% when the BSA reported last on the issue in 2012.

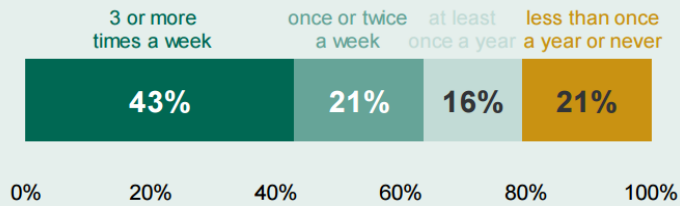
5.12 The National Travel Survey (NTS) is a household survey designed to monitor long-term trends in personal travel and to inform the development of policy including a DfT business plan indicator. It is the primary source of data on personal travel patterns by residents of England within Great Britain.

5.13 The survey collects information on how, why, when and where people travel as well as factors affecting travel (e.g. car availability and driving licence holding). The 2014 National Travel Survey (NTS) is the latest in a series of household surveys designed to provide a consistent source of data on personal travel behaviour across England. It shows, in general terms, that walking trends are declining.

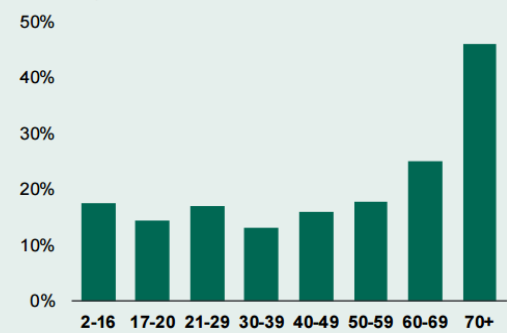


## Frequency of walking

64% of people aged 2+ walk for 20 minutes at least once a week.



Proportion who walk for 20 minutes less than once a year or never, 2014



## 6. Public Health

6.1 Policies in the Local Transport Plan have clear linkages with public health, aside from the obvious target of reducing and preventing accidents. Although the numbers of those killed or seriously injured on roads is a nationally recognised target, these numbers are relatively small on a local scale so can be skewed heavily by a single incident. Trend data can be used to identify outliers and give a clearer picture of the burden of injuries.

6.2 There are a wide range of associated public health benefits that relate to transport and street environments and the slowing of vehicle speeds. These can support the public health outcomes identified in the tables below:

High level outcomes	Wider determinants	Health improvements	Healthcare improvements
<ul style="list-style-type: none"> <li>• Healthy life expectancy</li> <li>• Health inequalities</li> </ul>	<ul style="list-style-type: none"> <li>• Children in poverty</li> <li>• Pupil absence</li> <li>• 16–18 year old NEET</li> <li>• Employment for people with a LTC</li> <li>• Sickness absence rate</li> <li>• Killed and seriously injured on the road</li> <li>• Violent crime</li> <li>• Population affected by noise</li> <li>• Use of green space for exercise</li> <li>• Social connectedness</li> <li>• Older people's perception of safety</li> </ul>	<ul style="list-style-type: none"> <li>• Low birth rate</li> <li>• Breastfeeding</li> <li>• Early childhood development</li> <li>• Childhood obesity</li> <li>• Wellbeing of looked after children</li> <li>• Diet</li> <li>• Adult obesity</li> <li>• Physical inactivity</li> <li>• Diabetes</li> <li>• Self-reported wellbeing</li> <li>• Falls and fall injuries in the over-65s</li> </ul>	<ul style="list-style-type: none"> <li>• Preventable deaths</li> <li>• Premature deaths from cardiovascular disease</li> <li>• Premature deaths from all cancers</li> <li>• Early death from respiratory disease</li> <li>• Suicide</li> <li>• Quality of life for older people</li> <li>• Hip fractures in the over-65s</li> <li>• Dementia</li> </ul>
<b>Health protection</b> <ul style="list-style-type: none"> <li>• Air pollution</li> <li>• Sustainable development plans for public sector organisations</li> </ul>			

Overarching indicators:

Outcome 1: Increased healthy life expectancy

Outcome 2: Reduced differences in life expectancy and healthy life expectancy between communities

- Killed and seriously injured casualties on England's roads
- The rate of complaints about noise
- Percentage of physically active and inactive adults
- Hospital admissions caused by unintentional and deliberate injuries in under 18s
- Fraction of mortality attributable to particulate air pollution
- Mortality rate from causes considered preventable

- Health related quality of life for older people
- Hip fractures in people aged 65 and over

6.3 Guidance issued by National Institute for Health and Core Excellence (NICE) also supports the introduction of a 20mph speed limit. This is reflected in the following points:

- ensuring that people can easily access local services on foot or bicycle
- ensuring that new developments prioritise physically active lives, including walking and cycling
- prioritising pedestrians and cyclists by restricting motor vehicle access, reallocating road space, traffic calming
- safe and attractive walking and cycling networks accessing public facilities, workplaces, shops, social destinations, public open spaces
- new sites are laid out to encourage walking and cycling

6.4 The Project Team heard that being more active is about lifestyle change. It is not solely about joining a gym, it can include active travel, such as walking and or cycling instead of using the car. Creating safer, more attractive walking and cycling routes through a reduction of the speed limit to 20mph could significantly increase numbers of walkers and cyclists and contribute to a shift to active travel. It can also create safer access to parks and public spaces creating home zones and play streets. Cyclists feel safer in quieter, residential areas than on distributor roads. This was evident when we compare the plans illustrating the environmental rooms with the Cycle Southend travel maps. By reducing speed limits in these roads, numbers of people choosing to walk or cycle may increase. Such initiatives can support the ambitions of the Southend-on-Sea Physical Activity Strategy (2016-2021). This is reflected in the British Heart Foundation’s position statement which states:

*“Areas with slower vehicle speeds are associated with increased opportunities for walking and cycling. Taking into account the wide health benefits of physical activity, including protection against the risk factors of cardiovascular disease, the National Heart Forum supports a reduction in the default speed limit for built up areas to 20 mph.”*

6.5 Walking and cycling not only makes a very positive contribution to improving health and increasing physical activity levels, it can also contribute positively to improving accessibility and tackling congestion, and reducing carbon emissions and improving the local environment. Engines of vehicles travelling at lower speeds and at a steady pace essentially use less fuel and therefore generate less pollution from particulates. These environmental benefits can also have a positive impact on respiratory problems and other associated health issues.

6.6 Older People feel safer in environments where the speed of traffic is low. They are therefore more likely to use streets where vehicle speeds are low to visit neighbours creating a form of exercise and increasing their social contact. It is recognised that there is an increased risk of injury when older people or the more-frail fall, however, the other public health benefits outweigh this risk.

## 7 20 mph Limits and 20 mph Zones

- 7.1 20 mph zones and limits are now relatively wide-spread, with more than 2,000 schemes in operation in England, the majority of which are 20 mph zones<sup>3</sup>.
- 7.2 The setting of local speed limits is the responsibility of local Highway Authorities, taking account of national guidance and legislation. In its circular 01/2013, the Department for Transport (DfT) published a draft revised circular entitled “Setting Local Speed Limits”. It included new advice and guidance to local Highway Authorities on the implementation of 20mph schemes.
- 7.3 The guidance recommends that whilst the standard speed limit in urban areas is 30 mph, which represents a balance between mobility and safety factors, for residential streets and other town and city streets with high pedestrian and cyclist movement, local traffic authorities should consider the use of 20 mph schemes. However, where they do so, general compliance needs to be achievable without an excessive reliance on enforcement.
- 7.4 A summary of the Government’s guidance on the implementation of 20mph schemes is set out below:-
- Successful 20mph limits and zones are those that are generally self-enforcing.
  - Self-enforcement can be achieved either, by the existing road conditions or using measures such as signing or traffic calming to attain mean speeds compliant with the speed limit.
  - To achieve compliance there should be **no** expectation on the police providing additional enforcement unless explicitly agreed.
  - The full range of options should be considered before introducing 20mph schemes.
  - Zones should not include roads where motor vehicle movement is the primary function.
  - While the Government has reduced the traffic calming requirements in zones they must be self-enforcing and include at least one physical traffic calming feature such as a road hump or build out.
  - 20mph limits are generally only recommended where existing mean speeds are already below 24mph.
- 7.5 There are two different methods of implementing 20mph speed restrictions; 20mph limits, which rely solely on signing, and 20mph zones which require traffic calming to reduce speeds. Highway Authorities also have the powers to introduce 20mph speed limits that apply only at certain times of day. These variable limits may be particularly relevant where a school is located on a major through road that is not suitable for a full-time 20 mph zone or limit.
- 7.6 A local traffic authority may introduce a 20 mph speed limit or 20mph zone without obtaining consent from the Secretary of State. A consultation process must be

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<sup>3</sup> Setting Local Speed Limits – DfT circular 01/2013

followed before implementing a new speed limit or zone as an essential part of the implementation process and needs to include local residents, local government, the police and emergency services and any other relevant local groups.

- 7.7 It is for local authorities to determine whether a speed limit is appropriate to the area but they must have regard to national guidance. National guidance on all local speed limits, including national guidance on 20mph limits and zones, is set out in the DfT Circular 01/2013 'Setting local speed limits'

## **8. 20mph Speed Limits**

- 8.1 20mph speed limits cover areas where the speed limit has been reduced to 20mph in a similar way to other local speed limits including 30mph or 40mph. 20mph speed limits do not require physical traffic calming measures, such as road humps or speed cushions, but may do so. 20 mph speed limit areas are signed on entry on both sides of the road, with corresponding 30mph signs on exit, and include further 20mph speed limit repeater signs or markings within the area. According to government guidelines, 20 mph limits can be introduced where there are significant numbers of vulnerable road users.
- 8.2 Research shows that 20mph speed limits are most appropriate for roads where average speeds are already low. This is consistent with the guidance from the DfT which suggests that average speeds should already be below 24mph. The layout and use of the road must also give the clear impression that a 20mph speed or below is appropriate. This is generally the case for the majority of local access residential roads, particularly where the roads are narrow thorough engineering or where on-street parking is available. The early evidence suggests that 20 mph limits without any traffic calming reduce mean speeds by about 1 mph on average.
- 8.3 There is generally less resident opposition to the use of 20 speed limits in comparison to 20 mph zones as no other physical features are required to reduce the speed of traffic. Achieving compliance with 20mph limits however, can be a challenge. Nevertheless, 20mph limits (as opposed to 20mph zones) are now the more common approach, due to their lower implementation cost and because they do not require physical traffic calming measures which can be controversial.

## **9. 20 mph Zones**

- 9.1 20mph zones must use physical or physiological traffic calming measures throughout the area to enhance conformity and encourage compliance with the speed limit. Zones can include a range of traffic calming measures such as road humps, raised tables, speed cushions, traffic chicanes, road narrowing, coloured surfacing and other physical or visual measures to emphasise the nature of the road. Signage is required on both sides of the road at the entry and exit points of the zone, which do not need to be illuminated. Repeater signs within the zone are not required. Research shows that 20 mph zones are on the whole more effective in reducing vehicle speed (typically 9mph reduction) and casualty numbers. This is largely due to the inclusion of the physical traffic calming measures.

9.2 Some calming measures can be unpopular due to the inconvenience and discomfort caused to road users, including the emergency services and public service vehicles. It can also lead to road users accelerating between the calming features. The introduction of physical traffic calming measures also leads to a significant increase in costs, in terms of installation and on-going maintenance. This ultimately results in a smaller number of areas being treated from any available budget. The *Institute of Advanced Motorists* has confirmed that there is significant engineering investment in bringing in zones while speed limits were cheaper.

## 10. Variable 20mph Limits

10.1 The Council, as a local traffic authority, may also introduce 20 mph speed limits that apply only at certain times of day. To indicate these limits, variable message signs can be used. These variable limits may be particularly appropriate in areas where a school is located on a road that is not suitable for a full-time 20 mph zone or limit, such as a major through road. To help reduce costs and sign clutter, the Department has confirmed it will consider authorising the placing of a single variable message sign on the approaching traffic lane (rather than signs on both sides of the road) on a case by case basis.

10.2 The Secretary of State has provided a special authorisation for every English traffic authority to place an advisory part-time 20mph limit sign, with flashing school warning lights. This can be a more cost-effective solution, where appropriate, and reduces the requirement for signing.

10.3 If signage is only active at certain times, it is more likely that drivers will actually see it and take note. One of the issues identified in general safety areas is local drivers not “seeing” signs due to familiarity.

## 11. Enforcement

11.1 Essex Police, in common with other police forces, are supportive of 20mph limits or zones where appropriate. They point out however, that it is important, in line with DfT guidance, and ACPO policy, that these limits ‘stand on their own 2 feet’, essentially that they are self-enforcing, through site conditions such as signing or traffic calming leading to a mean traffic speed which is compliant. Police intervention should be minimal if any scheme is to be a success, the reality is that if enforcement is required, then the scheme is essentially a failure, as if it is to achieve its aims and be truly safe, it must work 24 hours per day, with or without police presence. There should be no expectation on the police to provide additional enforcement beyond routine activity. It must therefore be appreciated that in reality, that the police will not be in a position to enforce 20 limits or zones.

11.2 According to revised ACPO guidelines (October 2013), enforcement will be considered in all clearly posted limits, given other priorities, and this will be by:

1. Targeted enforcement where there is deliberate offending/disregarding and the limits are clear;

2. Where limits are not clear (they don't feel like/look like the limit or are on inappropriate roads), they will not be routinely enforced but may be targeted where there is intelligence of obvious deliberate disregard
- 11.3 Since November 2013, the National Driver Offender Retraining Scheme (NDORS) have introduced a speed awareness course specifically tailored to speeding offences in 20mph limits where, at the discretion of the police, offenders who are either "mistaken or simply unaware of the limit" would benefit from education. Speeding in a properly marked/engineered limit or zone may not be suitable for a course.

## **12. ACPO Guidelines for Speed Limit Enforcement**

- 12.1 ACPO guidelines have been formulated taking into account, amongst other things, the need for targeting in order to maximise the potential of scarce police resources and to make a substantial contribution to the multi-agency road death and injury reduction effort.
- 12.2 It goes on to say that "driving at any speed over the limit is an offence and the police are not restricted and may prosecute. In deciding on enforcement means and deployment, one of the factors will be how unclear or confusing the relevant signage is or how a site simply does not feel like a road of that speed. Although this is not an excuse and any driver may be prosecuted, it will be considered when deciding on the prosecution and amount of police enforcement. The enforcement of speed limits is generally related and proportionate to the risks to all road users using that road and availability of resources but not restricted in any way."
- 12.3 "Where police officers consider that an offence has been committed, there will be no restriction on proceedings, however, they should consider whether it is appropriate to take enforcement action in that case, taking into account such facts as the level of signing and engineering to support the limit and whether it was clear to the motorist that there was a limit and what that limit was."
- 12.4 "The guidance to police officers is that, when they feel enforcement is necessary, it is anticipated that, other than in most exceptional circumstances (arrest), the issue of fixed penalty notices or summonses are likely to be the minimum appropriate enforcement action (with certain offenders offered a course of education at the lower and less harmful speeds."
- 12.5 "The guidance is that enforcement by prosecution should not be considered when it is lower than the speeds reached in accordance APCO guidelines. These guidelines do not replace police officer discretion."

## **13. Local Context - Existing Schemes**

- 13.1 There are currently thirteen 20 mph zones (comprising areas and/or individual roads) and five 20 mph limit areas in the Borough. A list detailing the 20mph zones and 20mph speed limit areas is attached at **Appendix 1**. Plans illustrating the 20mph



schemes within the Borough are attached at **Appendix 2**. Of the five speed limit areas, one is enforced by average speed cameras; one is a short service road for the Southend Victoria Station; one is a short length of road in the town centre; and two are small residential areas. These have been introduced on an ad-hoc basis in response to public demand and road safety.

- 13.3 The speed data for these areas supports the principle that compliance with the 20mph speed restriction requires a physical or psychological traffic calming feature or that they have been implemented in areas where the speed of traffic is already below 24mph. This is particularly evident some of the roads in the areas around West Leigh School (20mph speed limit) and Darlington School (20mph zone) where the average speed of traffic in West Leigh Avenue and Pavilion Drive is 17mph and 20mph respectively. Equally average vehicle speeds in Cromwell Road and Tunbridge Road are 17mph and 23 mph respectively. Most of these roads are quite narrow and experience high levels of on-street parking on both sides.
- 13.4 Conversely, although the speed of traffic in Caulfield Road (which is part of the Shoebury High School 20mph Zone) and Boston Road (which is in the St Mary's School 20mph zone) is 23mph, 73% and 77.7% of vehicles exceeded the speed limit in those roads respectively, despite the introduction of physical traffic calming measures.
- 13.5 The average speed of traffic in Tunbridge Road, which is within a 20mph speed limit area, is also 23 mph, but again 70% of vehicle exceeded the speed limit. This is probably due to the nature of the road.
- 13.6 The most effective 20 mph speed limit area in Southend is the Marine Parade "City Beach shared space" area. The average speed of traffic is 20 mph, with only 0.5% of vehicles exceeding the speed limit. This area is enforced by average speed cameras. This scheme was funded externally by KeyMed through its road safety initiative fund. Although highly successful, the use of average speed enforcement cameras is an extremely expensive option to ensure compliance.

#### **14. Safer Essex Roads Partnership (SERP)**

- 14.1 The Safer Essex Roads Partnership (SERP) has brought together the three local authority areas of Essex County Council, Southend-on-Sea Borough Council and Thurrock Council to provide a road safety service across 'Greater Essex'. The other SERP partners are Essex Police, Essex Fire and Rescue Service, Highways England, The Essex and Herts Air Ambulance Trust, The East of England NHS Trust and The Safer Roads Foundation.
- 14.2 The partnership's purpose is to reduce death and serious injury on Essex roads to zero. It is recognised that this is an extremely ambitious vision and one cannot be tackled alone: each road user plays a part. The partnership promotes road safety and casualty reduction through a number of activities, interventions, programmes and products which involve a combination of education, engagement, engineering

and enforcement. The Partnership's Joint Road Safety Delivery Plan (JRSDP) details the activities each partner is to deliver with partnership support and funding.

- 14.3 The Project Team noted the work currently being undertaken by the Partnership to identify wards in Essex where the highest number of collisions are generated by residents. From the information provided, it showed that whilst not all the collisions happen in the ward in which the residents lived, approximately 78% of the collisions did happen in Southend. The Partnership would therefore be targeting the home environment of people causing collisions. Vulnerable road users continue to provide a challenge and it would be essential to target these if future targets are to be achieved. Investment in education of vulnerable road users will never be wasted.
- 14.4 SERP supports the idea of blanket 20mph speed restrictions and would help wherever possible. However, it is recognised that 20mph restrictions are hard to justify on casualty grounds, as the costs involved in making them enforceable and look 'nice' are invariably much higher than the first year rate of return or even life time savings. If speeds are genuinely lowered (or kept low) then more people are more than likely to choose to walk and cycle, making the environment a nicer place in which to live.
- 14.5 It is extremely difficult/expensive to retro fit zones and much consultation and engagement would be necessary. Camera enforcement is a possibility and SERP would be happy to take on the enforcement should this be the route followed. 20mph zones and limits can be mixed to reduce costs but community buy in and a good-looking result are essential for success.
- 14.6 SERP therefore feels it might therefore be more appropriate to lobby central government to introduce a default speed limit of 20mph where there are street lights, unless signs show otherwise. This supports the SERP's target of "Vision Zero" and the aims of the SERP to reduce casualties to zero.

## **15. Royal Society for the Prevention of Accidents' (RoSPA) Policy Position on 20mph Speed Limits**

- 15.1 RoSPA state that 20mph zones are very effective at preventing injuries and would like to see their wider use in residential areas. 20mph zones significantly decrease the risk of being injured in a collision and their greater use, especially in residential areas, would help to reduce the number of traffic injuries in the UK.
- 15.2 RoSPA supports and encourages the wider use of 20mph limits. They believe 20mph limits are most appropriate for roads where average speeds are already low, below 24mph, and the layout and use of the road also gives the clear impression that a 20mph speed or below is the most appropriate. Although a high proportion of urban roads are suitable for 20mph limits, RoSPA does not believe that 20mph speed limits are suitable for every road in a local authority area. They should be targeted at roads that are primarily residential in nature and on own or city streets where pedestrian and cyclist movements are high (or potentially high), such as around schools, shops, markets, playgrounds and other areas. Roads which are not suitable

for 20mph limits are major through routes. This is entirely consistent with the guidance by the DfT in its circular “Setting Local Speed Limits”.

- 15.2 They believe Local Authorities are responsible for determining where 20mph zones and limits should be introduced but should take advantage of opportunities to introduce them where they are needed.
- 15.3 Consultation and engagement with local communities and other stakeholders is vitally important, to ensure that safer roads are prioritised where needed and that local communities have input into the schemes development.

## **16. The AA**

- 16.1 The AA supports the setting up of 20mph speed limits where residents along those roads want them. They state that “a headlong rush towards blanket 20mph zones in many UK towns and cities needs to slow down and take into account the views of residents.”
- 16.2 “Neighbourhoods face differing challenges from traffic: some may need to slow down their own residents and reduce the risk of accidents; others have a 'rat-running' problem that a 20mph speed limit on its own won't address.”
- 16.3 “The case for lowering speed outside vulnerable locations, such as schools and hospitals, is generally accepted. However, sweeping 20mph restrictions that slow down commuters, business deliveries and services, and the pace of a town or city in general are not.”
- 15.4 An AA-Populus survey of 24,351 AA members undertaken between 21<sup>st</sup> February and 3<sup>rd</sup> March 2014 showed that, if a 20mph speed limit is set along a road, support for speed camera enforcement is evenly split between those who agree (41%) and those who disagree (38%). Targeted speed camera enforcement, when and where a specific problem emerges, receives much greater support (61%).
- 16.5 There is a lot of fear among drivers that, with 20mph being a relatively unfamiliar speed, widespread speed camera use will make them look more at their speedometers than at what is happening on populated streets in front of them.
- 1.6 Use of speed-indicating signs may help to educate and familiarise drivers with the lower speeds, while proven urban road engineering features may also influence behaviour - while deterring rat-running.

## **17. Costs**

- 17.1 The costs for implementing a 20mph scheme differ drastically depending on the size, scale, method of enforcement, compliance and environmental measures.
- 17.2 For example, in Bristol, a pilot 20mph programme has already been extended city-wide in a £2.3m sign-only scheme.

- 17.3 In Portsmouth, where signage was utilised alone, the total scheme cost £0.57 million for a population of 200,000, whilst in Haringey where traffic calming measures were proposed, the estimated cost was £10 million for a population of 225,000.
- 17.4 Notably, the London Borough of Islington also utilised a signage only scheme at a cost of £1.6 million for a population of 200,000: the increased cost in comparison to Portsmouth was attributed to the lighting for the signs (Haringey Local Authority, 2011).
- 17.5 In Brighton, approximately £1.5 million was set aside for the whole 20mph project to be rolled out over four years. The actual budget spend for 2012/13 and 2013/14 was £326,134.68 and £333,245.36 respectively.

## **18. Conclusions**

- 18.1 Many local authorities have implemented blanket 20mph schemes for their areas the outcomes of which have added to the evidence base. However, do they achieve their purpose and what can we actually conclude from the evidence?
- 18.2 Research into the impacts of 20mph speed limits and zones has been undertaken by Steer Davies Gleave for London Borough of Merton on behalf of the London Environment Directors' Network (LEDNet). The purpose of the study was to conduct desktop research, in order to examine the available evidence and inform future 20mph policy in London. Whilst the study focused on London, by bringing together the available evidence it can help other authorities around the UK in their decision making regarding 20mph speed limits. The conclusion of the report dated November 2014 found that reducing vehicle speeds can result in fewer and less severe collisions, particularly for vulnerable road users.
- 18.3 It also found that whilst 20mph zones appear to have been reasonably successful at reducing speeds by using physical traffic calming measures, limited resources and relaxed regulations mean that signed-only 20mph limits are now preferred. These tend to achieve smaller decreases in vehicle speeds and therefore smaller improvements in road safety. The challenge is therefore to identify imaginative and effective ways to achieve larger reductions in speeds in signed-only 20mph limits, so that road safety benefits are maximised. Undoubtedly, enforcement is required to ensure compliance, but this is only part of the solution. The key to achieving sustained and meaningful speed reductions is to change drivers' attitudes to urban driving speeds. This suggests that education and other supporting measures to change driver culture need to be an integral part of all 20mph schemes.
- 18.4 The risk of being killed or seriously injured if hit by a car travelling at 20 mph rather than 30 mph reduces significantly. What is less evident is whether the introduction of a Boroughwide 20mph speed limit on residential roads would actually achieve this aim and whether vehicles would actually comply with the new limit.

- 18.5 Statistics collected by Islington Council do suggest traffic has slowed, but only marginally. Before 20mph limits were introduced, 85% of the traffic on Islington's main roads was travelling at an average of 28mph. After the limits were introduced, this average decreased by just 1mph to 27mph. However, before and after surveys covered less than a year all told. Results from Bristol and Brighton's pilots of 20mph limits tell a similar story, with daytime speeds in Bristol dropping by around 1mph to an average of 23mph. In Brighton, the Council saw a 1mph decrease a year after 20mph speed limits were introduced in 2013, although the average speed of traffic on central roads was already 20mph.
- 18.6 The data from the current 20mph schemes in the Borough also show a mixed picture, except where the average vehicle speeds were already in the low 20mph or where there is vigorous enforcement through average speed cameras. Data from the DfT shows that the average speed on locally managed 'A roads' in Southend has been between approximately 18-19 mph for the last 7-8 years. However, these roads would not be included in a blanket 20 mph scheme and serious accidents do still occur.
- 18.7 When it comes to speeding behaviour, many people do not necessarily feel and do the same things. Data also shows that the majority of the traffic exceeds the speed limit in some of the roads within the existing 20mph zones where traffic calming features have been introduced. This is essentially a result from vehicles increasing speed between calming features. Such driver behaviour can in turn lead to more noise for local residents in the street, increased fuel consumption and detrimental effects to the environment. Moreover, other vulnerable road users become more at risk due to the perceived safety of using a road with a 20mph speed limit.
- 18.8 According to the analysis of Government data by the Institute of Advanced Motorists (IAM), the number of serious accidents on 20mph roads increased by 26% in 2014 and that the number slight accidents on 20mph roads also increased by 17%. The IAM also state that the number of serious casualties in 20mph zones also increased by 29% while slight casualties went up by 19%. This could be due to an increase in the diversity of road users now using roads within 20mph area as they have a perception that the roads are safer to use.
- 18.9 Government guidance (DfT circular 01/2013 – Setting Local Speed Limits) states that “unless a speed limit is set with support from the local community, the police and other local services, with supporting education, and with consideration of whether engineering measures are necessary to reduce speeds; or if it is set unrealistically low for the particular road function and condition, it may be ineffective and drivers may not comply with the speed limit.”
- 18.10 “A comprehensive and early consultation of all those who may be affected by the introduction of a 20 mph scheme is an essential part of the implementation process. This needs to include local residents, all tiers of local government, the police and emergency services, public transport providers and any other relevant local groups (including for example, groups representing pedestrians, cyclists, drivers, or equestrians).”

- 18.11 “It is important to consider the full range of options and their benefits, both road safety and wider community and environmental benefits and costs, before making a decision as to the most appropriate method of introducing a 20 mph scheme to meet the local objectives and the road conditions.”
- 18.12 Different road users perceive risks and appropriate speeds differently, and drivers and riders of motor vehicles often do not have the same perception of the hazards of speed as do people on foot, on bicycles or on horseback.
- 18.13 Also attitudes and actions of road users can be at odds – what people say and what they actually do can differ drastically. There is no evidence of modal shift resulting from the implementation of signed only 20 mph limits, although those that do not cycle and walk think it might make people feel safer, or that modal shift will happen as a result. There is also no evidence that people ARE safer in signed only 20mph limit areas, although there is some evidence that people may FEEL safer.
- 18.14 There is no doubt that a reduction in the speed of traffic from 30mph to 20mph and below in areas where there are high level of accidents would save lives. 20mph zones reduce speeds, directly related to the amount of traffic calming included. Thus providing they are robust they will reduce speeds, which in turn may reduce casualties, where there have been speed related casualties before. The implementation and on-going maintenance, together with public resistance to traffic calming features, significantly increases the cost of any scheme. 20 mph limits on the other hand are cheaper and only reduce speeds by a very small margin, but will not bring speeds to under 20mph, unless the pre-scheme speeds were at or below 20mph or if there is constant rigorous enforcement.
- 18.15 A blanket 20mph speed limit on residential roads will not guarantee that traffic speeds will reduce to the desired levels and should not be seen as a perfect solution to reduce the numbers of those killed or seriously injured. It should also not be seen as a tool to establish a modal shift to active travel. It could also potentially have an adverse effect due to the perception that vehicles in a 20 speed limit are actually travelling at that speed, giving a false sense of safety with an increase in more vulnerable users. Additionally, a substantial level of funding would need to be identified from existing budgets and is unclear where the additional funding will come from, particularly given the levels of savings the Council are having to make year on year.
- 18.16 The Council could consider consulting its residents, Leigh Town Council, the Police and other emergency services, local transport providers etc. on the feasibility of introducing a blanket 20 mph speed restriction in residential streets to help inform the debate. However, as mentioned above no budget provision has been identified for this. Furthermore, should there be general overall support for a blanket 20mph speed limit in residential streets, further additional funding would need to be identified from existing decreasing budgets.

18.17 The Department for Transport has commissioned a three-year, £715,000 study on their effectiveness, which will bring together data from different regions as well as new research carried out on the roads. The results of this study are due in 2017.

## **19. Recommendations**

19.1 That Cabinet be recommended:

- (i) To note the outcome of the study;
- (ii) To wait until the results of the study by the DfT are published before considering undertaking any consultation on the introduction of a Borough wide 20mph speed restriction in all residential streets;
- (iii) To work with SERP and other agencies to reduce death and serious injury on roads in Southend;
- (iv) To consider the introduction and prioritisation of 20mph schemes, including the use of variable speed limits within the Borough where and when necessary, particularly around local schools and other appropriate locations; and
- (v) To write to the Secretary of State for Transport to suggest that they consider the merits of reducing the default urban speed limit in roads with street lighting be reduced from 30mph to 20mph.





## LIST OF EXISTING 20mph SCHEMES IN THE BOROUGH

Location / Area	20mph ZONE	20mph LIMIT
Boston Avenue Area	X	
High Street - Old Leigh	X	
Milton Area	X	
Caulfield Road	X	
Chalkwell Esplanade Area	X	
Cromwell Road	X	
Westborough Area	X	
Greenways	X	
Wentworth Road	X	
Windermere Road	X	
Westcliff High Schools Area	X	
Edwards Hall School Area	X	
Temple Sutton School Area	X	
Marine Parade City Beach Area		X
Westleigh School Area		X
Tunbridge Road Area		X
Victoria Station		X
Chichester Road		X



**INDEX TO PLANS OF EXISTING 20mph SCHEMES IN THE BOROUGH**

- Plan 1** Boston Avenue Area 20mph Zone and Tunbridge Road Area 20mph Speed Limit
- Plan 2** High Street Old Leigh 20mph Zone
- Plan 3** Milton Area 20mph Zone, Victoria Station and Chichester Road 20mph Limits
- Plan 4** Caulfield Road 20mph Zone
- Plan 5** Chalkwell Esplanade Area 20mph Zone
- Plan 6** Cromwell Road, Wentworth Road and Temple Sutton Area 20mph Zones
- Plan 7** Westborough Area 20mph Zone
- Plan 8** Greenways 20mph Zone
- Plan 9** Windermere Road 20mph Zone
- Plan 10** Westcliff High Schools Area 20mph Zone
- Plan 11** Edwards Hall School Area 20mph Zone
- Plan 12** Marine Parade City Beach Area 20mph Limit
- Plan 13** West Leigh School Area 20mph Limit





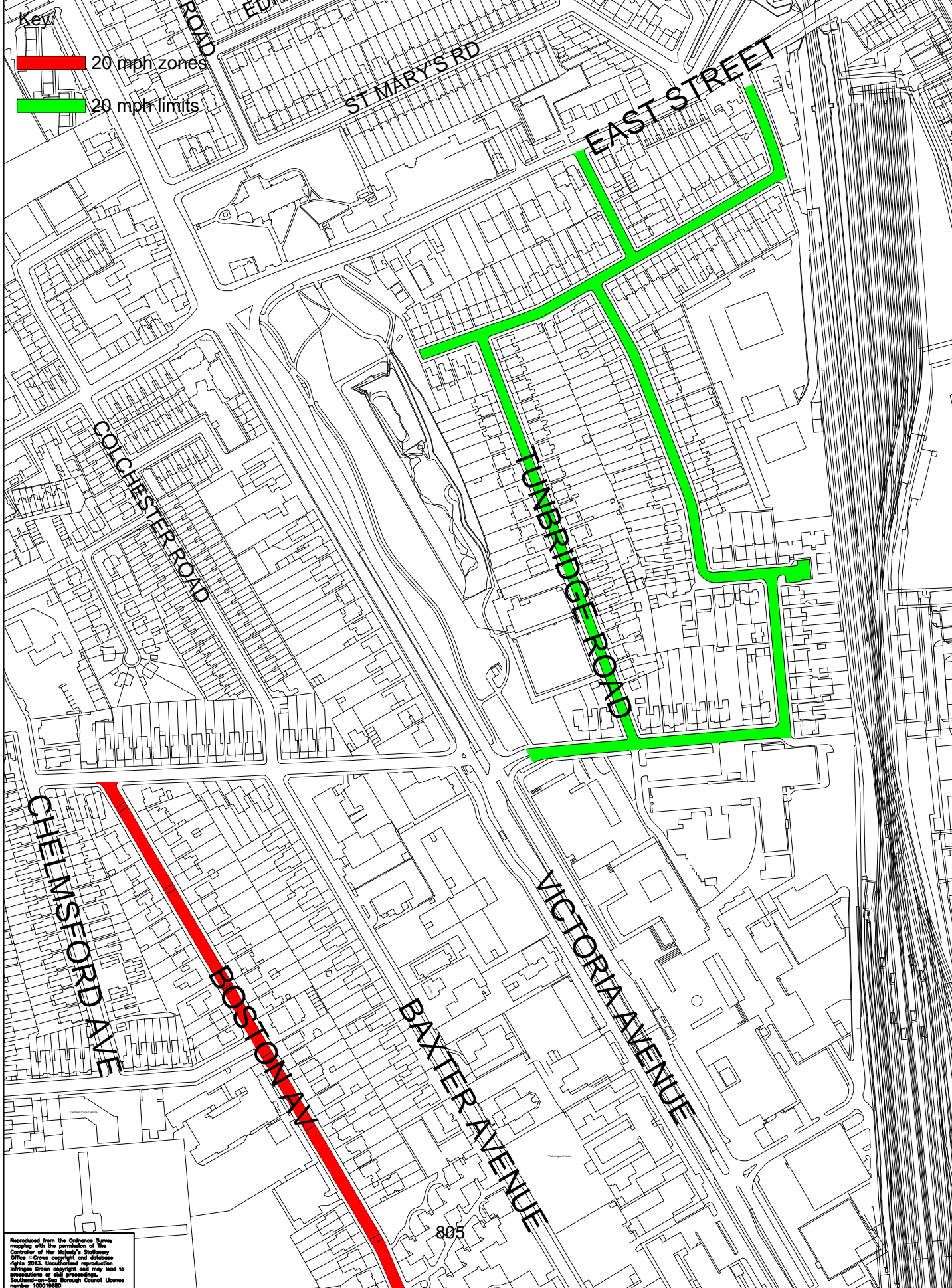
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# Southend on Sea Borough Council

Enterprise, Tourism  
and the Environment  
PO Box 5560, Civic Centre  
Victoria Avenue, Southend on Sea,  
SS2 6ZQ

Project Title			Existing 20 mph zone & limits		Date	15/08/16	
Drawing Title			Various Roads			Quality Project No.	
Designed			Drawn by	Checked by	Approved by	Scale © A4 NTS	
			L.PEARMAIN			Drawing Number	

Drawing Status	Preliminary		Tender		Working		As Constructed
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Project Title Existing 20 mph zone

Date 15/08/16

Drawing Title High Street, Old Leigh

Quality Project No.

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## GLENDALE GARDENS

Key

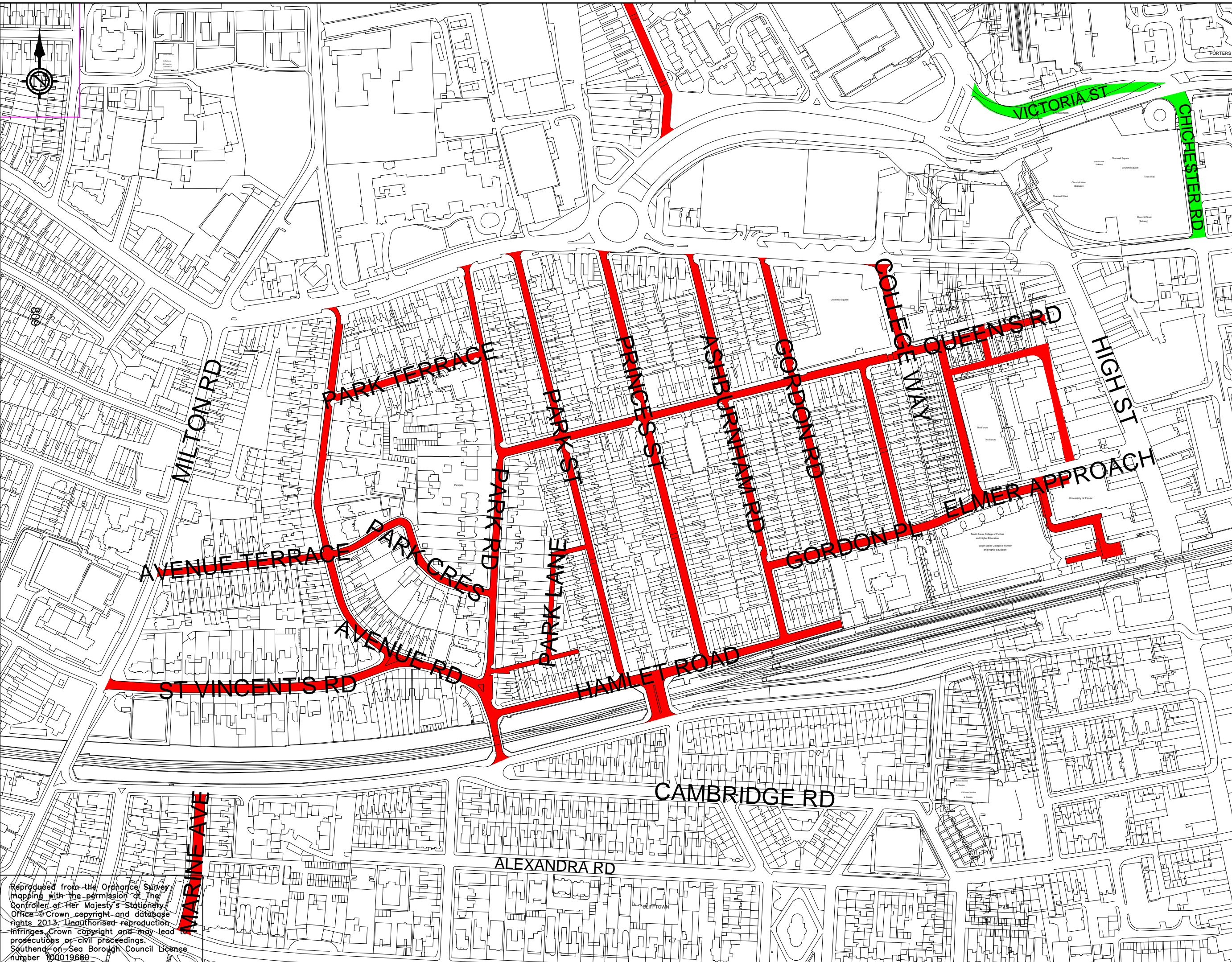
20 mph zones

20 mph limits

BROADWAY WEST

# HIGH STREET OLD LEIGH

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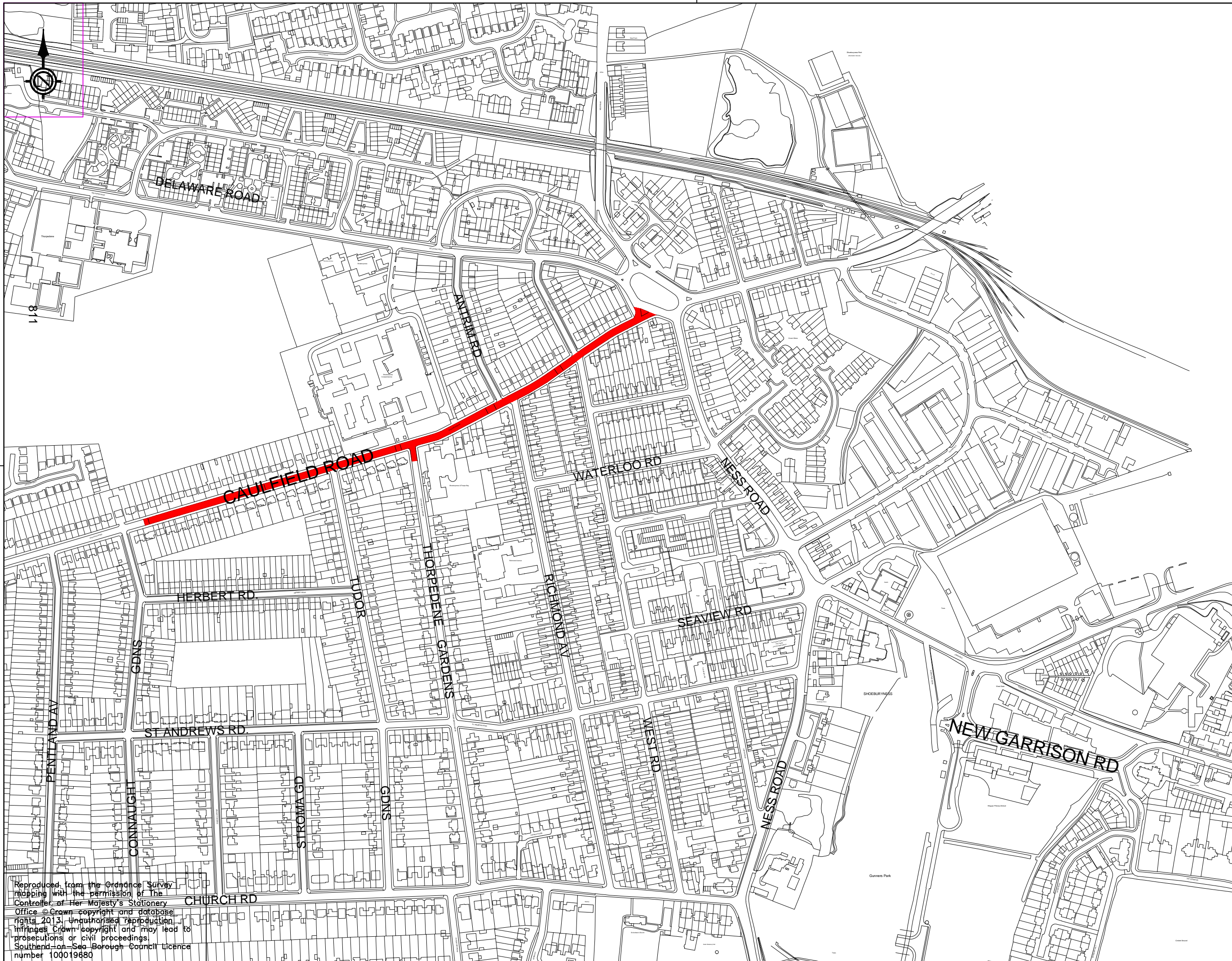


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█ 20 mph limits

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Quality Project No. :	PROJECT_No
Project Title	
Existing 20 mph zones & limit	
Drawing Title	
Various Roads	
Designed by	Scales © A3
L.PEARMAIN	NTS
Drawn Date	15/08/16
Checked Date	/ /
Approved Date	/ /
Issued Date	/ /
Drawing Status	
Preliminary	Working
Tender	As Constructed
Drawing Number	

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█ 20 mph zones  
█ 20 mph limits

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Quality Project No. : PROJECT\_No

Project Title  
 Existing 20 mph zones

Drawing Title  
 Caulfield Rd, Shoeburyness

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Drawn Date 15/08/16	NTS
Checked Date / /	
Approved Date / /	issued Date

Drawing Status	
Preliminary	Working
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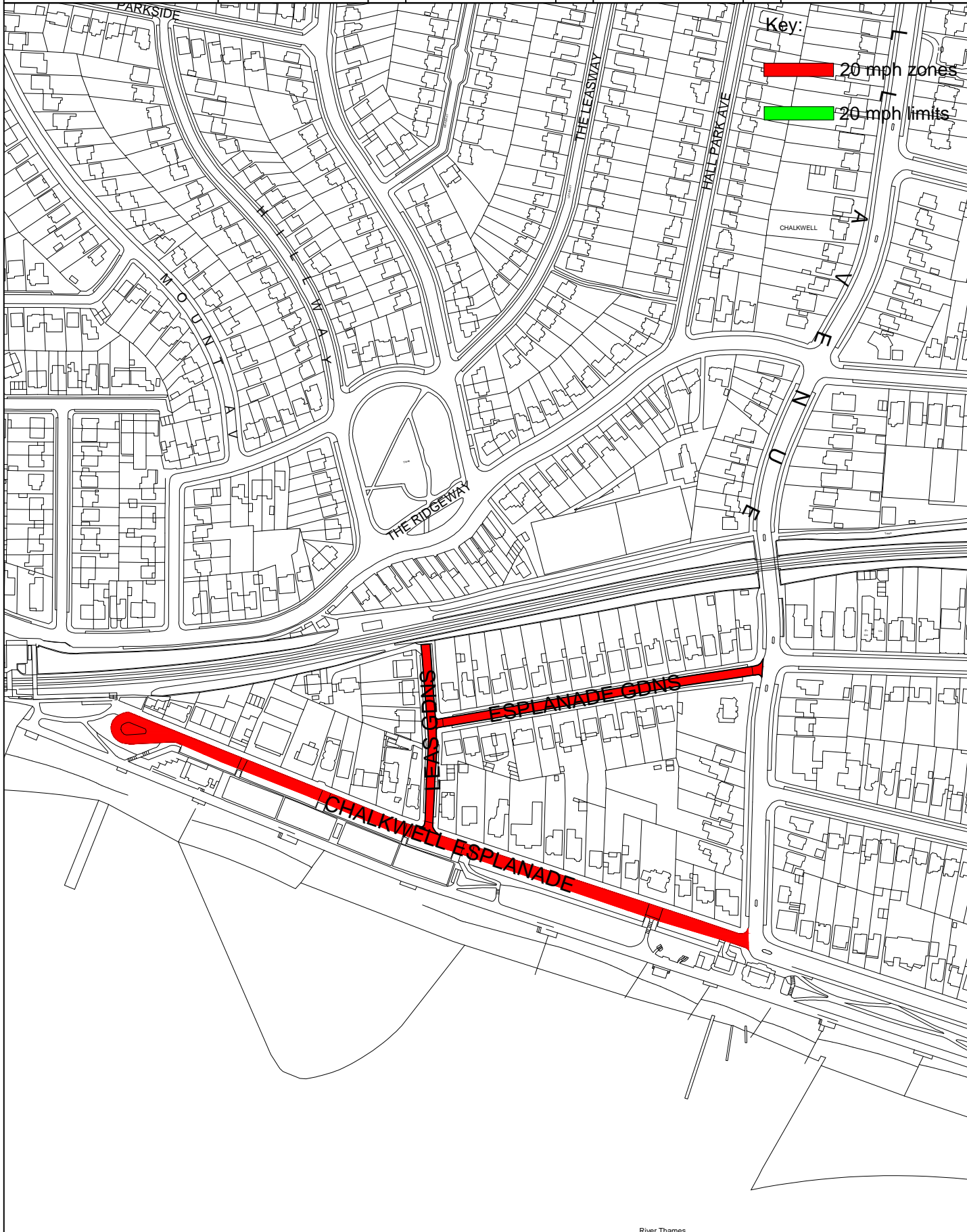
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Project Title		Existing 20 mph zone		Date	22/08/16
Drawing Title		Chalkwell Esplanade Area		Quality Project No.	
Designed		Drawn by	Checked by	Approved by	Drawing Number
		L. PEARMAIN			

Scale @ A4  
NTS

Drawing Status	Preliminary	Tender	Working	As Constructed
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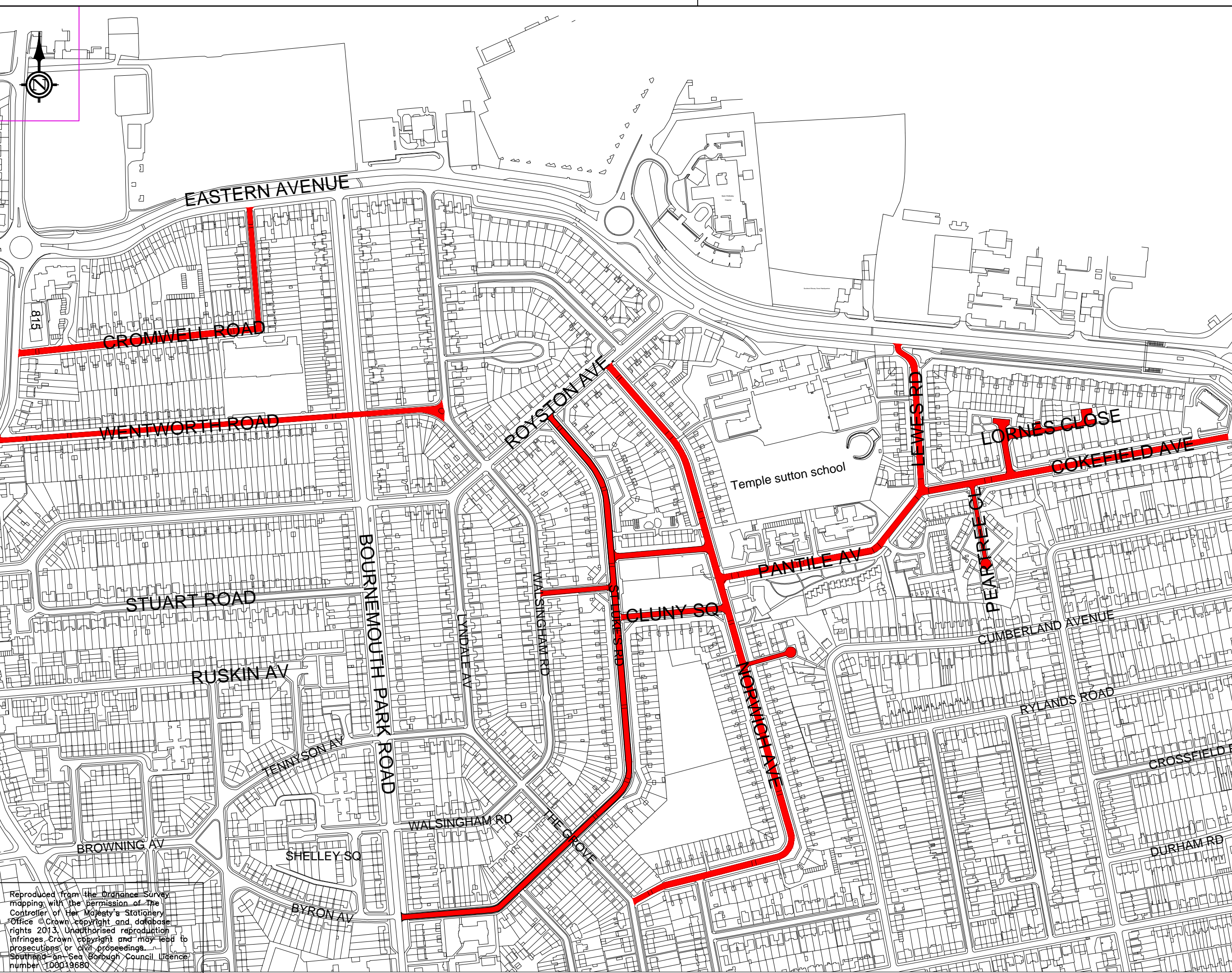




Key:

█ 20 mph zones

█ 20 mph limits



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Quality Project No. : PROJECT\_No

Project Title

Existing 20 mph zones

Drawing Title

Temple Sutton School Area

Designed by L.PEARMAN Scales A3

Drawn Date 23/08/16 NTS

Checked Date

Approved Date Issued Date

Drawing Status

Preliminary Working

Tender As Constructed

Drawing Number

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Key:  
█ 20 mph zones  
█ 20 mph limits

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Quality Project No. : PROJECT\_No

Project Title  
 Existing 20 mph zones

Drawing Title  
 Westborough area

Designed by L. PEARMAN	Scales A3
Drawn Date 22/08/16	NTS
Checked Date / /	
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Preliminary	Working
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Project Title Existing 20 mph zone

Date 15/08/16

Drawing Title Greenways

Quality Project No.

Scale © A4  
NTS

Designed

Drawn by

Checked by

Approved by

Drawing Number

L.PEARMAIN

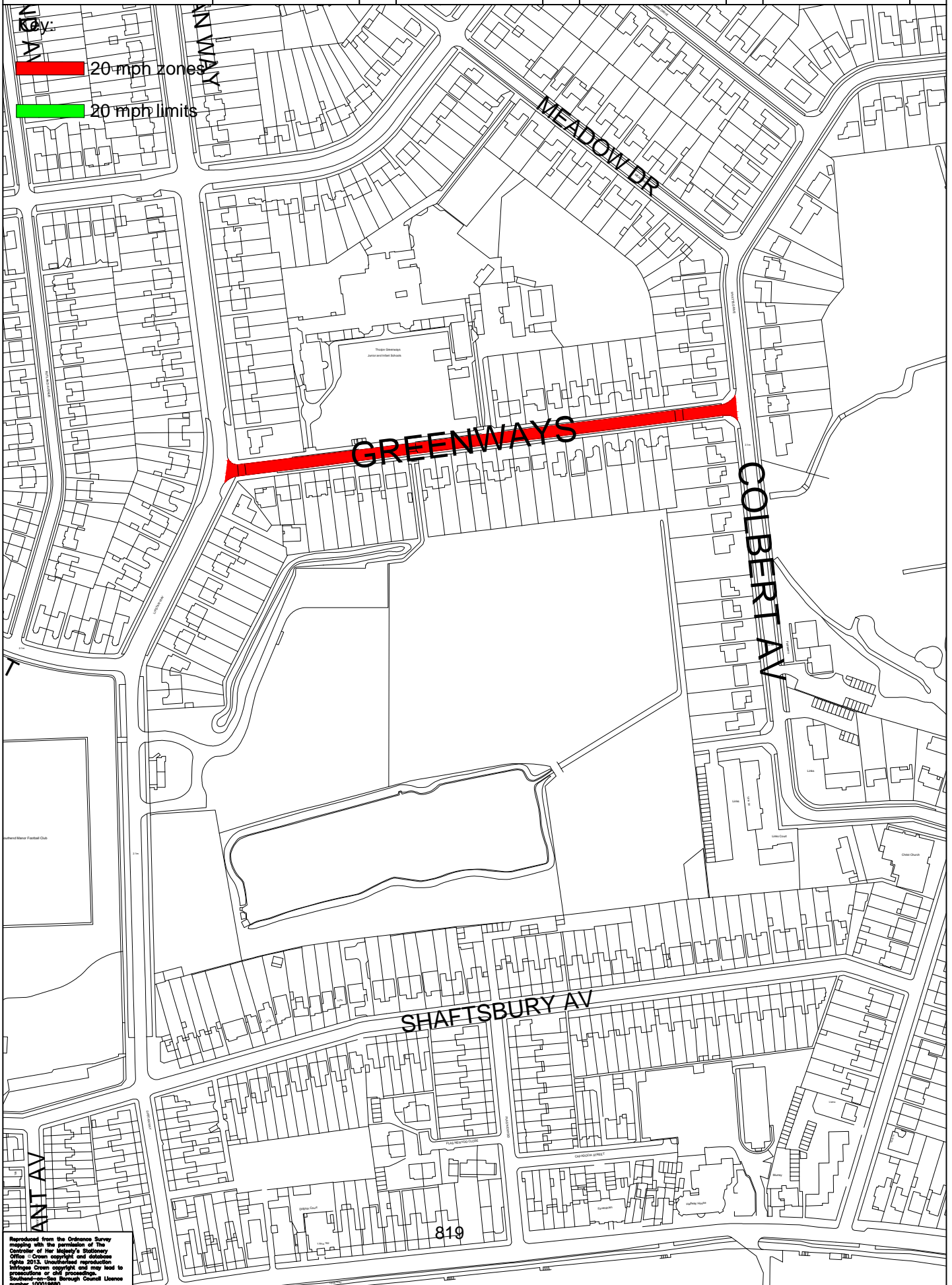
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Tender

Working

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Project Title Existing 20 mph zone

Date 15/08/16

Drawing Title  
Windermere Road

Quality Project No.

Scale © A4  
NTS

Designed

Drawn by

Checked by

Approved by

Drawing Number

L.PEARMAIN

Drawing Status


Preliminary

Tender

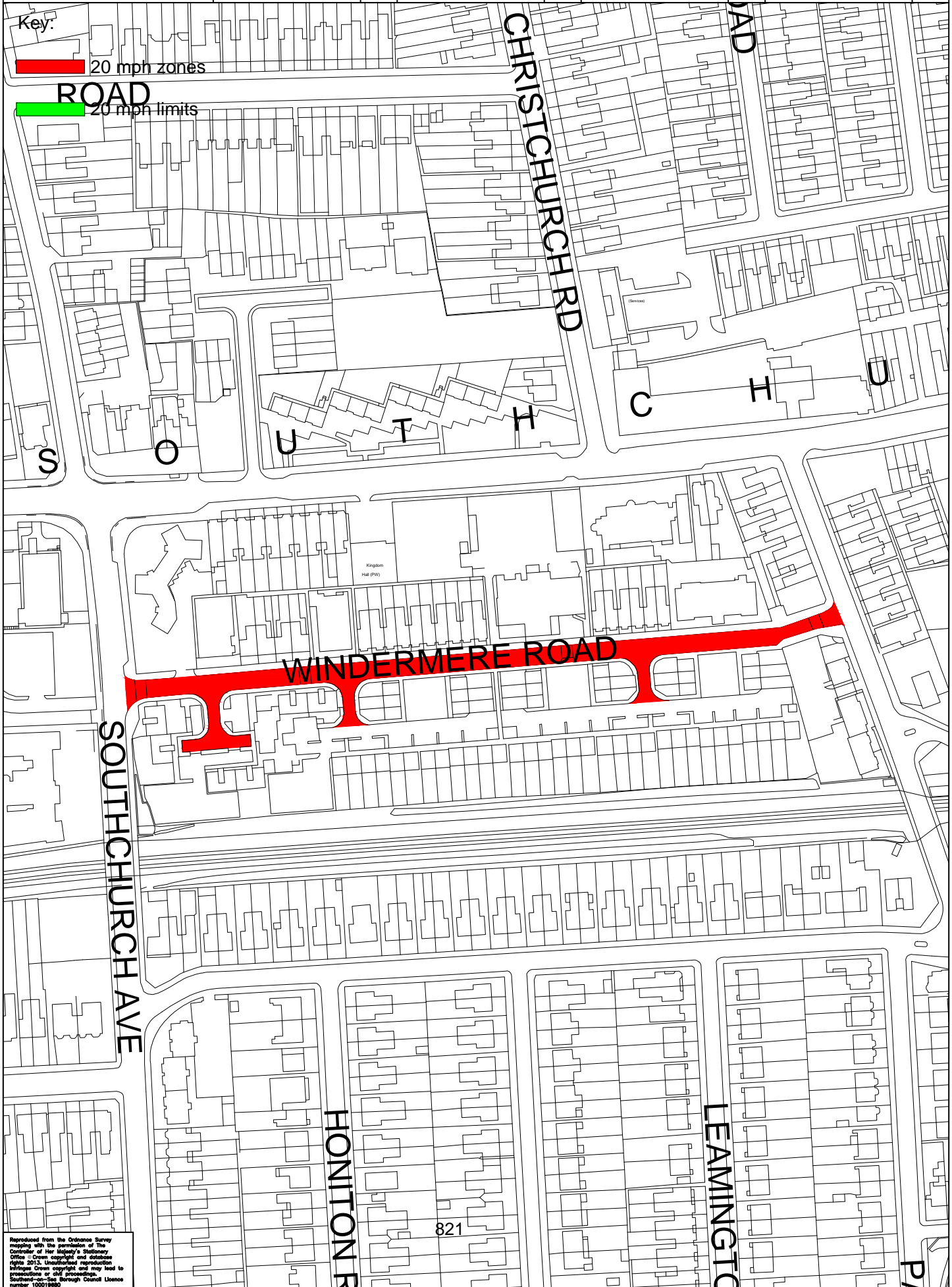
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Key:

 20 mph zones

 20 mph limits



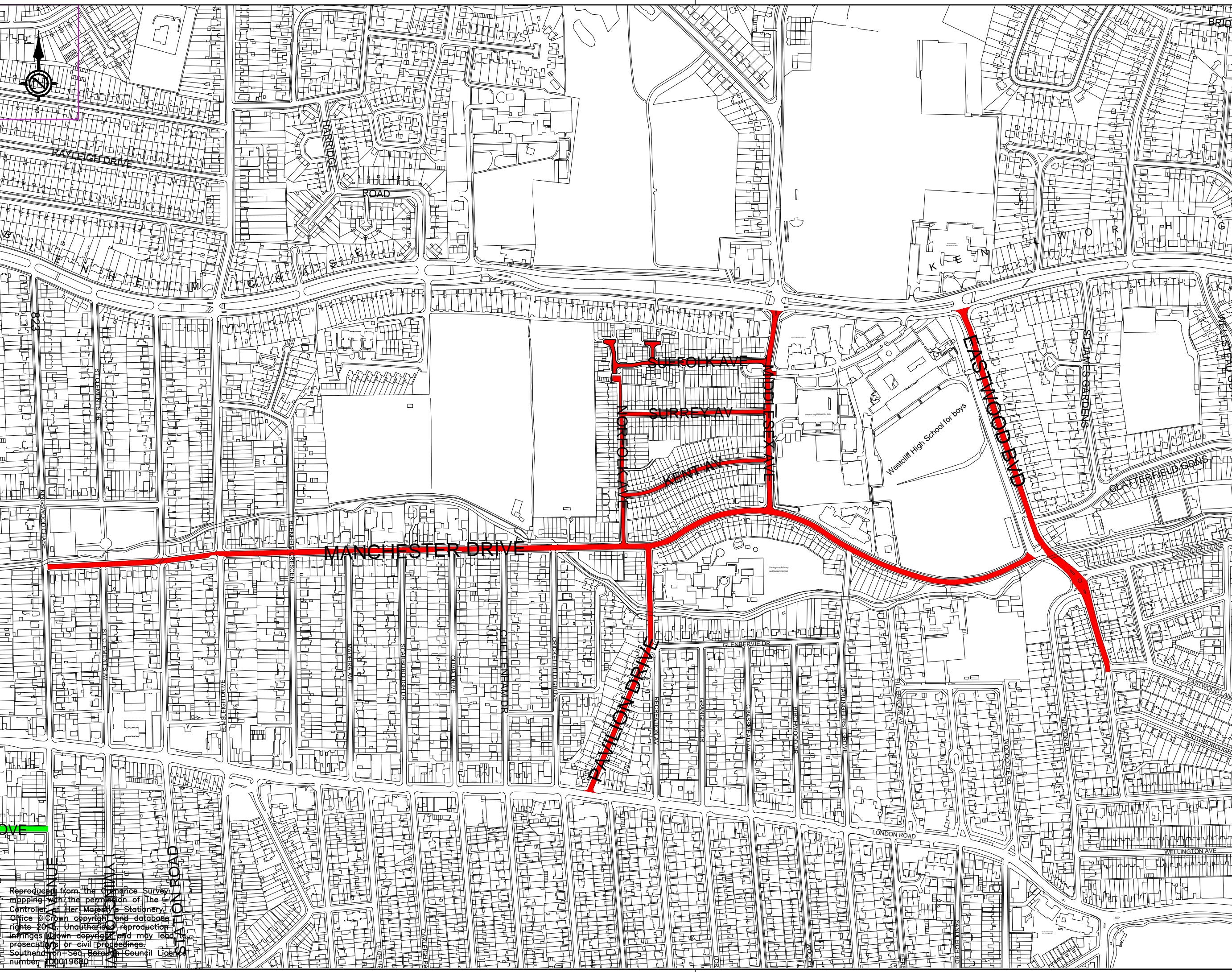
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Key:

- █ 20 mph zones
- █ 20 mph limits

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Quality Project No. :	PROJECT_No
Project Title	Existing 20 mph zones
Drawing Title	Westcliff High for Boys Area
Designed by L.PEARMAIN	Scales @ A3
Drawn Date	NTS
Drawn 22/08/16	
Checked Date	
Approved Date	Issued Date
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Drawing Status	
Preliminary	Working
Tender	As Constructed
Drawing Number	

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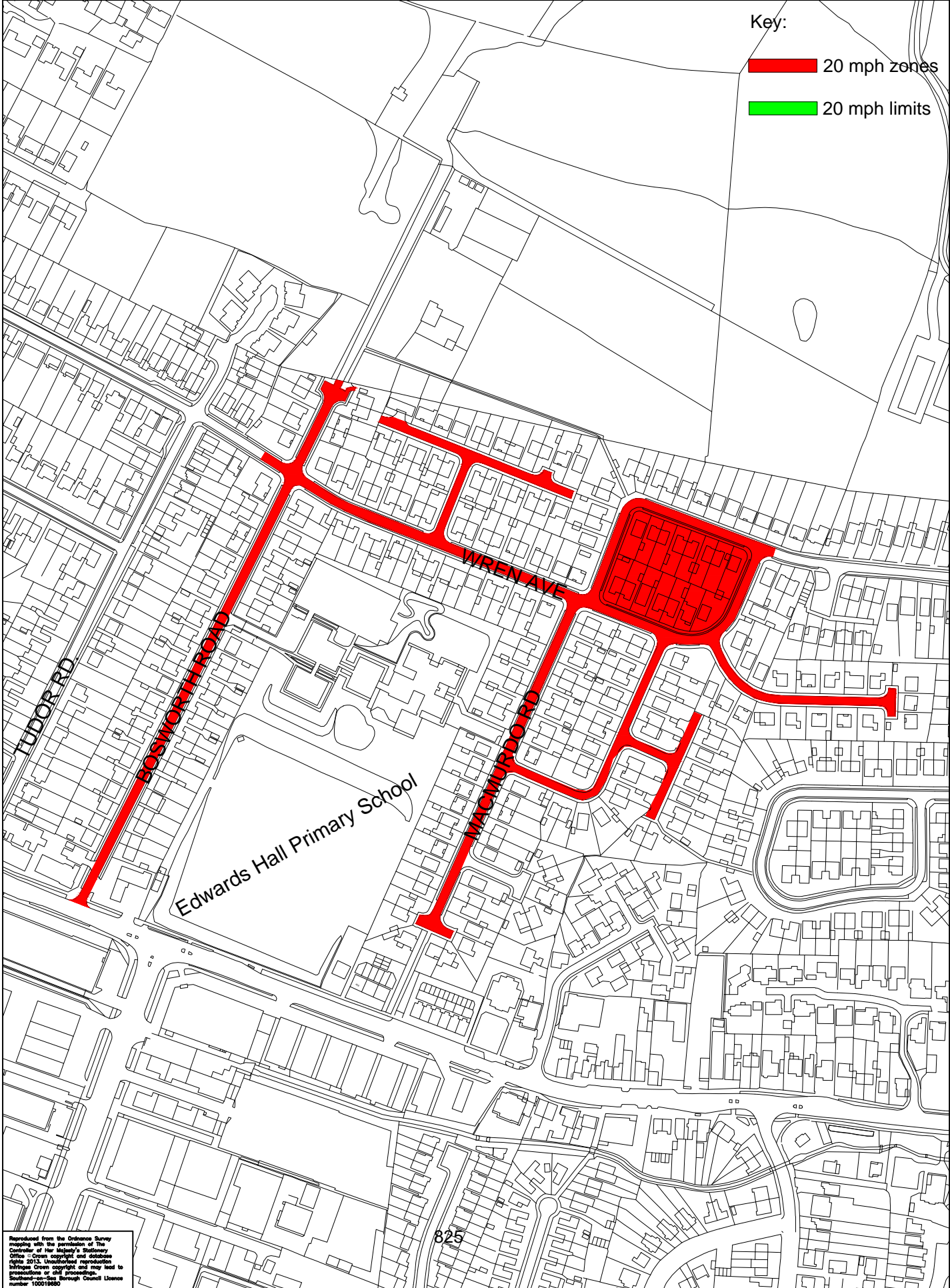
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Project Title <b>Existing 20 mph zone</b>			Date <b>15/08/16</b>
Drawing Title <b>Edwards Hall Primary School</b>			Quality Project No.
Designed	Drawn by <b>L.PEARMAIN</b>	Checked by	Approved by
			Scale $\odot$ A4 <b>NTS</b>
			Drawing Number

Drawing Status	Preliminary	Tender	Working	As Constructed
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Key:  
 20 mph zones  
 20 mph limits



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Project Title Existing 20 mph Limit

Date 22/08/16

Drawing Title Marine Parade

Quality Project No.

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NTS

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L.PEARMAIN

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Drawing Number

Drawing Status

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Tender

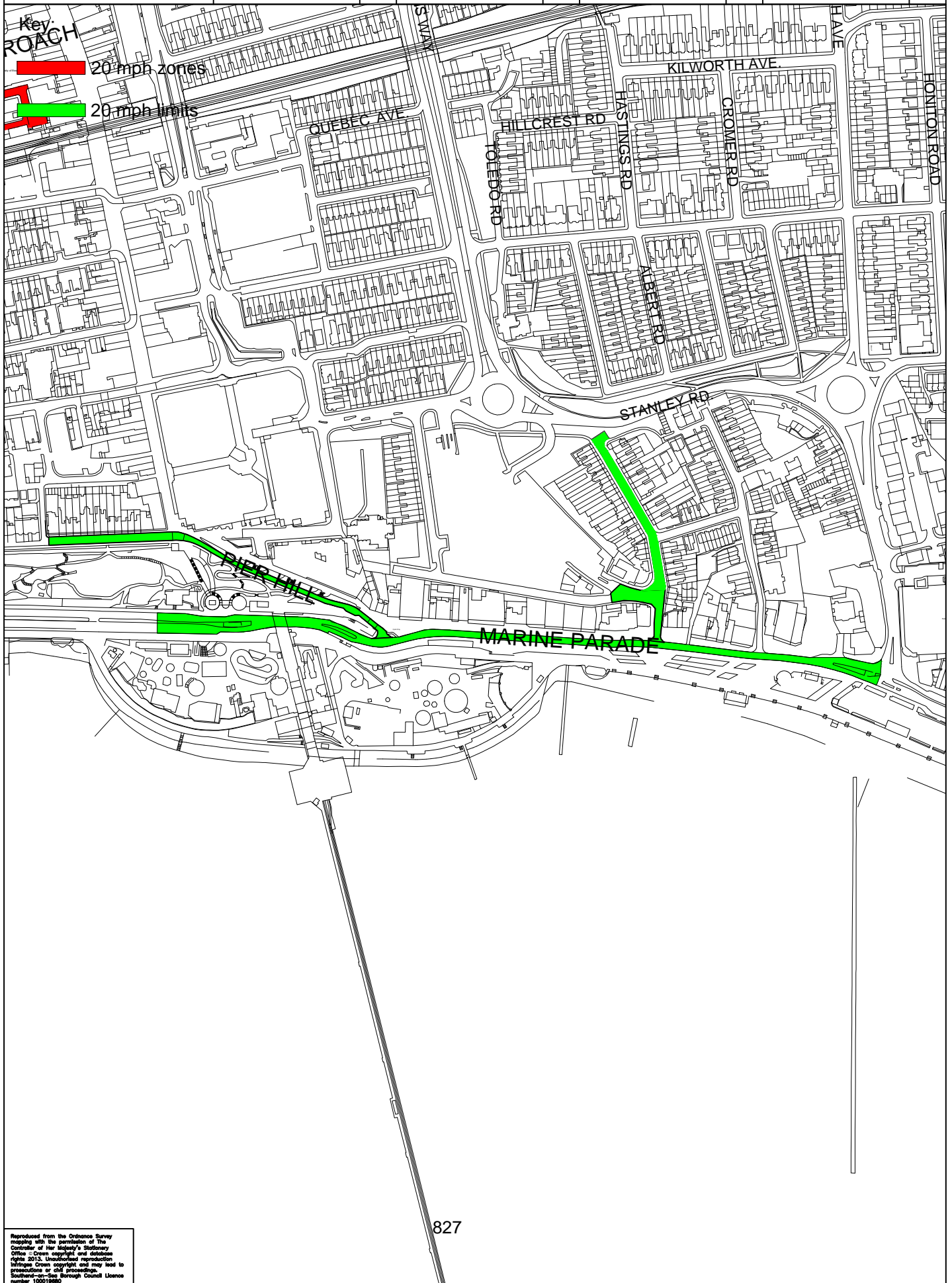
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Key  
ROACH

20 mph zones

20 mph limits



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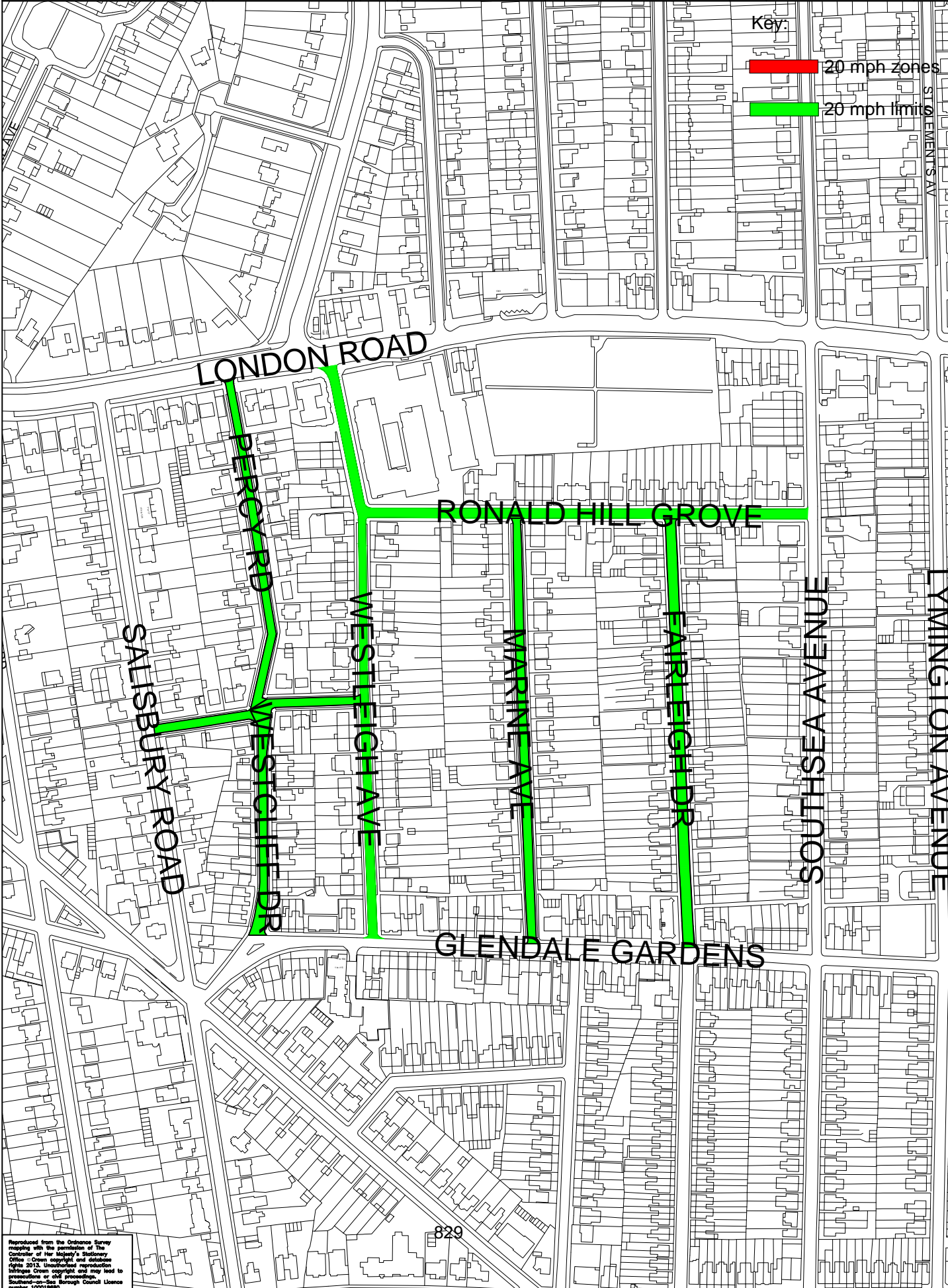
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SS2 6ZQ

Project Title		Existing 20 mph limit		Date	22/08/16
Drawing Title		Westleigh Junior School Area		Quality Project No.	
Designed		Drawn by	Checked by	Approved by	Scale © A4 NTS
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**PLACE/POLICY & RESOURCES SCRUTINY COMMITTEE  
IN-DEPTH STUDY 2016/17**

**TOPIC: 'THE COUNCIL'S COMMUNITY LEADERSHIP ROLE IN PROMOTING SAFER COMMUNITIES'**

19

**FRAMEWORK FOR SCRUTINY / SCOPE OF PROJECT:**

- I. To investigate the possibility of the Council increasing resources for enforcement activity including consideration of the council employing its own PCSOs or financing the provision of additional 'Specials' specific consideration should be given to whether financial support could be offered to such officers and how they would be dedicated to the Borough of Southend-on-Sea;
- II. To consider how such PCSOs or additional 'Specials' could contribute to an improved level of service in connection with the enforcement of public protection, waste, graffiti, street scene etc.
- III. The Committees also agreed that *'officers proceed with background work in advance of the scope of the topic being fully developed'*.

**Method:** Through project team meetings, witness sessions and/or workshops.

**Target date:** April 2017

**MEMBERSHIP:**

Cllrs: Arcscott, D Garston, J Garston, Burzotta, Bright, Robinson (Vice-chair), Gilbert, Ayling (Chair), Assenheim, Callaghan.

**Officer / partner support** – Lysanne Eddy, Tim MacGregor, Ade Butteriss, Kelly Clarke, Rob Walters, Dipti Patel, Simon Ford, Carl Robinson and Tim Row.

**SOURCES OF EVIDENCE**

The evidence base will be:

- (a) Data, profiles, trends and patterns
- (b) Crime performance indicators and information / key issues
- (c) 'Enforcement' performance indicators (ASB/Licensing/Parking/StreetScene)
- (d) Strategic Intelligence Assessment
- (e) Community Safety Plan
- (f) Local community meeting minutes
- (g) Examples from comparable authorities and partners
- (h) Cost profiling
- (i) Clarity of Policing functions and wider 'enforcement'

**POTENTIAL WITNESSES:**

- (a) Chair Southend Community Safety Partnership or nominee
- (b) Southend Police Representative
- (c) PCC or nominee
- (d) Neighbourhood
- (e) Regulatory Services
- (f) Street Scene
- (g) SMAART rep
- (h) Parks warden
- (i) Pier & Foreshore
- (j) BID Rangers
- (k) Voluntary sector (SOS Bus etc)
- (l) Seafront

*Scrutiny process is structured to add value and is supportive of the challenges already set to be delivered, but has limited resources, which need to be focused on providing the front line service and the priority outcomes for the Council.*

**RECOMMENDATIONS:**

To make appropriate recommendations to the Council & partners

This review links to the Council's Corporate Priority 1 – continue to reduce crime, disorder and anti social behaviour.